# The Star Key Site

# Urban Design Report

September 2021 Issue A — Job No 2200827

# ETHOS URBAN



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# 1.0

# INTRODUCTION

# 1.1 Purpose of this Report

The Star is a globally competitive, integrated resort offering a unique Sydney experience The Star forms a crucial piece of the tourism landscape in New South Wales and provides an essential contribution to Sydney's reputation as a global city. However, to remain competitive, The Star must continue to invest in their ongoing transformation.

The significance of the facility to the New South Wales economy as well as its location within the Pyrmont Peninsula at the doorstep of the emerging global waterfront precinct warrants a thorough approach and design. Careful consideration of the current Pyrmont Peninsula Place Strategy (PPPS), released by The Department of Planning, Industry and Environment (DPIE) has been taken to form a response which seeks to align the directions and objectives identified for the Darling Island sub-precinct as part of the broader peninsula vision. Under the strategy The Star has been identified as one of two key sites within the peninsula as catalyst sites and have been subject to a sitespecific planning process culminating in the requirement for the development of a site-specific Master Plan. This will be submitted to DPIE for further consideration as part of a more detailed strategy which will expand on the initial PPPS and provide specific details for development around the Darling Island sub-precinct.

The purpose of this report is to present an urban context analysis that considers the opportunities and constraints for re-development within the PPPS in the context of the existing development on the site. This is the context in which the design outcomes proposed for the site in relation to the strategic re-development project are then to be considered.

Importantly, this Urban Design Report does not undertake the environmental impacts of the proposed design. Please refer to **Section 1.7 - Relationship to Other Reports** for more information.



**01** Night view of The Star from Pyrmont Bay Source: The Star

# Introduction 1.0

# 1.2 Scope of the Key Site Master Plan

To consider the overall opportunity for change, this report will focus on developing a suite of analysis which expands upon the PPPS, with a specific focus on identifying specific approaches to respond to the Darling Island sub-precinct place priorities to inform The Star's Key Site Master Plan. The analysis is undertaken at a high level and utilises the technical consultant team's advice to inform key findings which inform the Master Plan. Greater detail relating to specific technical parameters including traffic, landscape, flooding, acoustics, wind and sustainability may be found in their respective technical reports issued as part of the broader package of works.

### Key Site Master Plan Site Definition:

- In response to the PPPS, a zone of influence was defined to assist with identification of key public benefits that The Star could assist in delivering or provide contributions to. Following discussions with both the DPIE and the City of Sydney (CoS) regarding the zone of influence, the subsequent sitespecific Master Plan will focus upon development on Star Entertainment Group Ltd. (SEGL) held land. This comprises of the main Star site on 20-80 Pyrmont Street (northern site) as well as 37-69 Union Street (southern site). These two sites will be referred to as The Star Key Site.
- In early correspondence with the CoS, The Star were advised that the CoS would be preparing a contributions scheme for these areas, which would necessitate the CoS preparing a design and schedule of works for these lands. The Star's site-specific Master Plan Public Domain works identify potential desired works and activities, as well as programs for the surrounding open spaces.
- The Public Domain component of The Star's site-specific Master Plan Public Domain will also identify potential works to Pirrama Road, Edward Street, Jones Bay Road and Pyrmont Streets, subject to discussions and sign off by Transport for New South Wales (TfNSW).
- The Public Domain works identified in the landscape component of the Key Site Master Plan will also identify potential works up to the building envelope of The Star's property to interface with key points of activation, particularly along Pirrama Road and Union Street.



02 The Star Key Site, zone of influence and Darling Island sub-precinct boundaries

LEGEND

The Star Key Site Darling Island sub-precinct Zone of influence

# 1.3 The Proposal

The Star Key Site Master Plan is proposing to rezone 20-80 Pyrmont Street and 37-69 Union Street, Pyrmont to establish new planning controls to enable redevelopment on the site to accommodate future mixed uses including retail, commercial, hotel and residential. The Star Key Site is outlined in Figure 2.

The rezoning and proposed planning controls have been informed by detailed site planning considerations as well as existing and future local context analysis. The proposed new controls that comprise amendments to the Sydney Local Environmental Plan 2012 (Sydney LEP 2012) and a Design Guide, respond to the objectives for The Star Key Site Master Plan as listed in the PPPS as well as the Strategy's directions, big moves and place priorities.

It should be noted that subsequent development applications will be required in line with the relevant provisions of the Environmental Planning & Assessment Act 1979 to deliver the proposed developments.

The key development outcomes sought to be achieved for The Star Key Site from the proposed Master Plan include:

### Northern Site (20-80 Pyrmont Street)

- A new 27 storey six star hotel (RL110) on Pirrama Road (North Tower) comprising;
  - 6 storey podium that retains the existing ground level setback on the main Star site
  - 21 storey tower with 1.5m street setback from podium and increased minimum of 7m street setback to the north in line with wind advice and view sharing principles
  - Total gross floor area (GFA) of approximately \_ 26,000m<sup>2</sup> (excluding through-site link)
  - New porte-cochere drop off servicing hotel
- Additional built form to Level 5 rooftop of the main Star site comprising;
  - A collection of indoor and outdoor spaces with complementary functions such as indoor/outdoor dining opportunities, recreational spaces, wellness spaces and/or hotel amenities, including an existing hotel pool
  - Total GFA of approximately 3,000m<sup>2</sup> (additional to existing)
- Opening up of Pirrama Road frontage to reveal light rail and to provide improved connectivity to public realm and waterfront including;
- Active uses at street level such as F&B, retail and/or wellness uses
- Total GFA of approximately 200m<sup>2</sup> (additional to existing)

- New through-site link connecting Jones Bay Road and Pirrama Road
- Re-configured and expanded entry to the Lyric Theatre
- Facade upgrades to The Star Grand Hotel and Residences
- Refurbishment of the existing function space associated with the Multi-Use Entertainment Facility (MUEF)

### Southern Site (37-69 Union Street)

- A new 37 storey mixed use building (RL140) on Union Street (South Tower) comprising;
  - 5 storey mixed use podium with a 3m ground level setback along the Pyrmont Bridge Road boundary to increase footpath width, comprising uses such as retail, residential and hotel amenities and/or dedicated hotel levels
  - 32 storey mixed use tower generally setback 5-7m from the podium, comprising uses such as hotel rooms and/or residential apartments
  - Total GFA of approximately 32,000m<sup>2</sup>

### Public Realm

- Upgrades to corner of Union Street and Edward Street
- Upgrades to corner of Union Street and Pyrmont Street
- Improvements to public domain along Edward Street
- Improvements to public domain along Pirrama Road
- Upgrades to Union Street with potential for shared zone, including upgrades to walkway and cycleway

Once new planning controls are adopted, The Star will progress with the detailed design and planning of the future development on the site, including progressing with a design competition and securing development approval for the winning design.

# 1.4 The Vision

The Star's vision is to create a World Class Integrated Resort within the Darling Island sub-precinct, offering the best variety of gaming, restaurants, bars, hospitality, tourism and entertainment experiences. The Star's Key Site Master Plan will provide much needed stimulus for growth in tourism investment and the creation of thousands of jobs, coinciding with the arrival of the new Metro set to connect Pyrmont to the innovation corridor to the west and Sydney CBD to the east. Please refer to Section 5.1 - Precinct Vision for further information on our vision for the sub-precinct.

# 1.5 The Star

The Star is an ASX 100 listed company that owns and operates The Star Sydney, Treasury Brisbane and The Star Gold Coast.

The Star Sydney is Sydney's leading entertainment, dining and tourism destination. More than 11 million people, including locals, domestic visitors and international tourists visit The Star annually, facilitated by a workforce of approximately 4,500 people (pre-COVID). As Sydney's only integrated resort, The Star Sydney focuses on the development of tourism and entertainment products across four key segments - accommodation, food and beverage (F&B), gaming and entertainment.

# 1.6 Structure of this Report

For ease of interpretation, the main body of the Urban Design Report will be organised in the following manner:

### • Darling Island Sub-precinct Site Analysis:

Taking in observations about the Darling Island sub-precinct and identifying a range of key findings about the site, its existing impact on the current context and key findings to set parameters for the Master Plan.

### Public Domain Opportunities and Constraints:

Distillation of the site analysis to inform key public domain opportunities on and around The Star Key Site.

### • Precinct Vision and Urban Design Principles:

A vision for the Darling Island sub-precinct supported by a series of clearly defined public domain, ground plane and built form principles which respond to the opportunities and constraints identified through the sub-precinct site analysis.

### Key Moves:

This will enable the facilitation of a series of key moves to inform the Key Site Master Plan and enable the definition of a public domain strategy.

### Proposed Master Plan:

Overview of the proposed Key Site Master Plan and Public Domain Master Plan.

### **Relationship to Other Reports** 1.7

This report provides a common reference point to inform the site-specific Master Plan response for the project as outlined in a suite of documents including:

- Master Plan Report
- Public Domain Landscape Master Plan Report

The consultant team for this Key Site Master Plan is as follow:

- Ethos Urban Planning and Urban Design
- fjmt studio Architectural Master Plan
- Urbis Public Domain Landscape Master Plan

Supporting reports including disciplines from wind (WindTech), acoustics (Renzo Tonin & Associates), services, environmental sustainability, traffic and transport (WSP) and visual impact assessment (Ethos Urban) to support the proposal.

### **Urban Design Report**

Establishes the existing and future context based on the outcomes outlined in the PPPS and supplemented by analysis framed specifically to consider The Star and its relationship to the Darling Island sub-precinct and adjacent sub-precincts of Pirrama and Pyrmont Village (Harris Street). This analysis was prepared with the purpose of informing the site-specific Master Plan through the identification of appropriate approaches to the public domain, streetscape and appropriate built form that responds to both the project brief and the PPPS.

### Master Plan Report

The Master Plan Report by fimt studio will include an overview of the site and respond to the opportunities and considerations identified in the Urban Design Report. It will outline of the overall vision for the site, a summary of more detailed design development and testing including an assessment of the built form, detailed massing response and architectural articulation.

The report will also include the environmental impact of the proposal in the context of the detailed design outcomes of the built form envelopes outlined in the proposal.

# Report

The Public Domain Landscape Master Plan Report has been prepared by Urbis for the proposed alterations and changes to The Star, with a specific focus around identifying key public benefits for the public domain around The Star Key Site with a view to improving access and movement for pedestrians around the site. The concepts anticipate the potential for Metro at or near the Union Street site, however is not contingent on it. The public domain landscape design incorporates:

- Street streetscape design
- Pyrmont Street and Jones Bay Road streetscape upgrades
- Level 5 rooftop upgrades

### Public Domain Landscape Master Plan

- Union Street, Pirrama Road and Edward
- Pirrama Road crossing and transition to Pyrmont Bay Park and the foreshore

# 1.8 Assumptions

It should be acknowledged this report has been prepared based on the provided information in the PPPS and the technical consultant reports that accompany the document. Assumptions have had to be made in order to make a reasonable assessment of the precinct-wide matters related to:

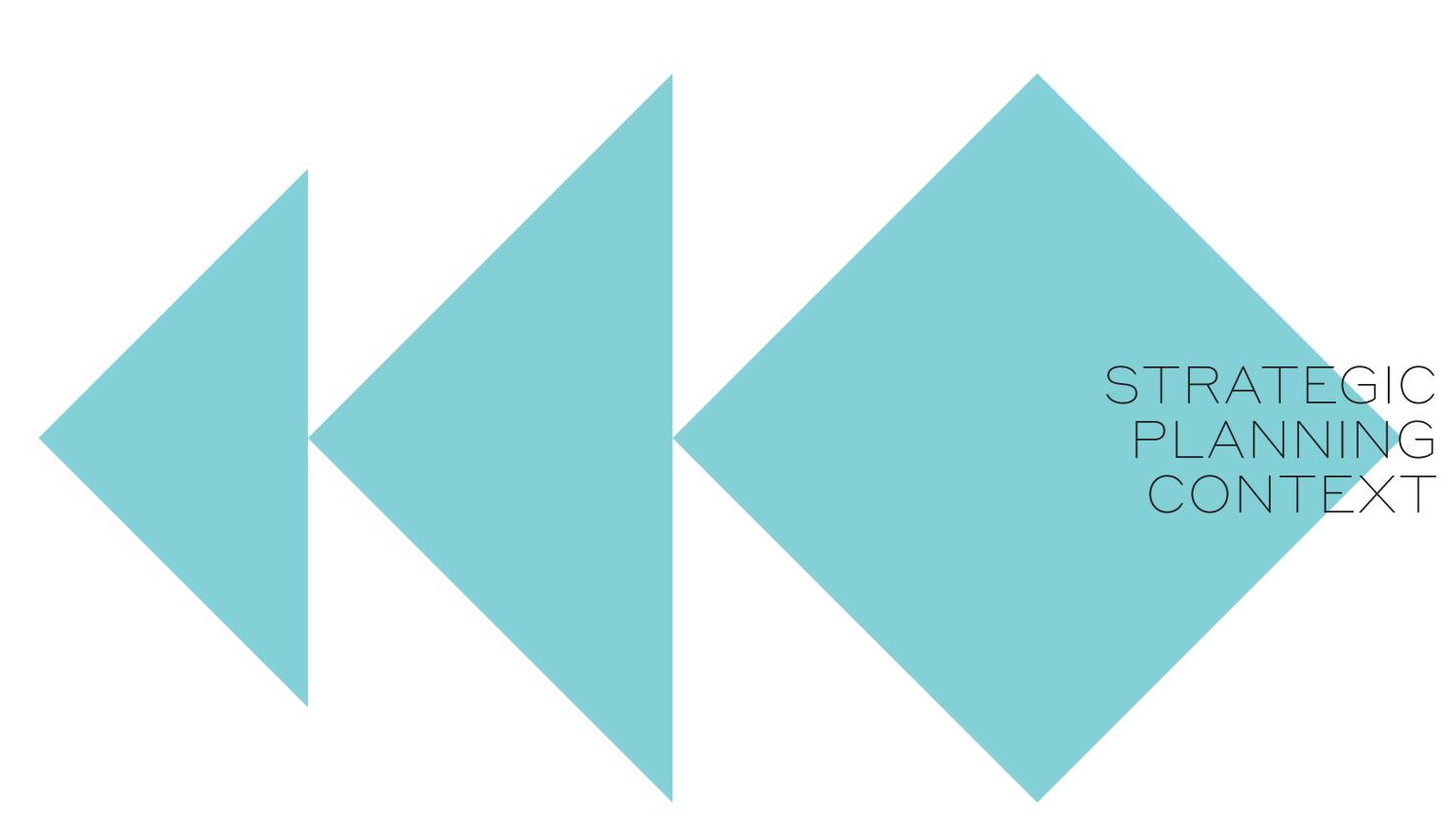
- Likely future urban form and land use particularly regards to taller towers and changes in height and density controls along Union Street, Pyrmont Bridge Road and Edward Street. This includes street wall heights and setback controls with regard to the taller buildings cluster south of Union Street;
- Identification of key streets which will be subject to change, to what extent including changes to road alignments, footpaths, bikes and planting (for integration into Complete Streets Guide).

Other disciplines will also provide a set of assumptions based on the information available to them at the time of preparation of the Key Site Master Plan.

# 1.9 General Requirements

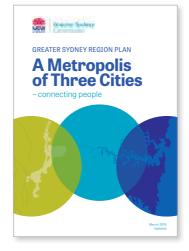
This report has been prepared with reference to the General Requirements for Preparing Key Site Master Plans under the PPPS and the alignment review prepared by the DPIE dated 26 April 2021.

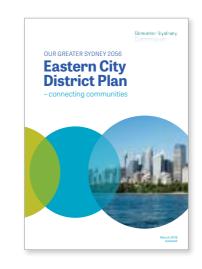
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# 2.0

### Strategic Planning Documents 2.1





# **Eastern City District Plan**

The Eastern City District Plan (District Plan) is a 20 year plan to manage growth in the context of economic, social and environmental matters to achieve the 40 year vision for Greater Sydney. Key planning priorities related to increased housing supply, growing a stronger and more competitive Harbour CBD, delivering integrated land uses and enhancing the tourist and visitor economy as well as reducing carbon emissions are all applicable to The Star site.

### A Metropolis of Three Cities - Greater Sydney Region Plan

The Greater Sydney Region Plan (GSRP) sets a 40 year vision to 2056 and outlines a 20 year plan to manage growth and change for Greater Sydney. It is the overarching strategic planning document for Greater Sydney and informs the preparation of both district and local plans. The site is located within the Eastern Harbour City, which is identified as Sydney's Metropolitan Centre, providing globally competitive financial, professional, health, education and innovation services. One of the GSRP's objectives is for the Harbour CBD to be stronger and more competitive and that includes distinct assets that support the Harbour CBD's global role in entertainment, cultural, tourist and conference facilities as well as providing high amenity and high density residential precincts.



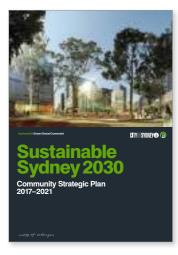
### **Better Placed**

Better Placed is an integrated design policy for the built environment of NSW that has been developed by the Government Architect of NSW (GANSW). GANSW defines a well-designed built environment as being; healthy, responsive, integrated, equitable and resilient. It is guided by a set of objectives which are intended to achieve good design throughout the development process and which have been considered in the development of The Star Master Plan.



### **Greener Places**

Greener Places is a design framework produced by GANSW to guide planning, design and delivery of green infrastructure in urban areas across NSW. The Draft Greener Places Design Guide provides information on how to design, plan and implement green infrastructure in urban areas throughout NSW. It provides advice on open space for recreation, urban tree canopy and bushland and waterways.



### Sustainable Sydney 2030

Sustainable Sydney 2030 is Council's community strategic plan. Its purpose is to identify the community's main priorities and aspirations for the future and to identify how to achieve these goals. The Plan builds on Council's commitment toward a green, global and connected city by 2030. Sustainable development has been a key consideration throughout the development of the Master Plan.



### City Plan 2036

City Plan 2036 is Council's Local Strategic Planning Statement which sets out the 20 year vision for land use planning in the city. It sets out planning priorities and actions needed to achieve Council's vision - "Green, Global and Connected". The LSPS is intended to guide future changes to the planning controls in Council's Local Environmental Plan and development control plans. The LSPS acknowledges Pyrmont's integral location in the Eastern City District's Innovation Corridor. Capitalising on Pyrmont as a location for business and enterprise has been recognised as important qualities to ensuring the Harbour CBD continues to contribute to Sydney's global competitiveness.



### **Future Transport Strategy**

Future Transport 2056 sets the 40-year vision, directions and principles for customer mobility in NSW, guiding transport investment over the longer term. The Strategy aims to achieve greater capacity, improved accessibility to housing, jobs and services and continued innovation in the transport sector.



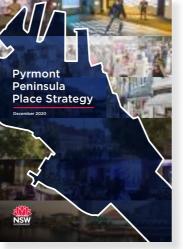
# 2.2 Pyrmont Peninsula Place Strategy

The PPPS provides a 20-year framework that identifies areas that can accommodate growth in Darling Island, Blackwattle Bay, Tumbalong Park and Ultimo sub-precincts, while enabling more growth in the Pyrmont Village and Wentworth Park sub-precincts. The PPPS is implemented in the statutory planning system by a Ministerial Direction that requires all land use and planning proposals to be consistent with the PPPS.

The first phase in implementing the PPPS is the preparation of master plans for each of the seven sub-precincts that make up the Peninsula. As a 'Key Site' located in the Darling Island sub-precinct, The Star has been identified to progress its own Master Plan for its 'Key Site' alongside the broader Precinctwide master planning being undertaken by the DPIE, in consultation with the CoS.

The PPPS identifies 5 big moves underpinned by 10 directions which will steer the development for the entire peninsula to realise its vision as an innovative, creative and cultural precinct that forms the engine room of the Eastern Harbour CBD.

In addition to the directions and big moves, the document has also outlined a range of 'special master plan considerations' and 'public benefit opportunities' specific to The Star Key Site that will also be used as a guide to steer the master plan outcome.







DIRECTION #2

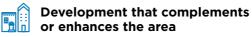
DIRECTION #3

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DIRECTION #6





Centres for residents, workers and visitors



DIRECTION #8

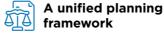
DIRECTION #9

DIRECTION #7





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14

Great homes that can suit the needs of more people

The Star Key Site Master Plan seeks to prioritise the following directions as part of its proposal:

### **DIRECTION #1**

# Jobs and industries of the future

Investment and innovation to boost jobs, creativity, tourism and night-life

Recognising that it is a key part of the tourism and night-life sector in the Pyrmont Peninsula, with its proximity to Central Sydney and neighbouring sub-precincts like Tumbalong Park, The Star is seeking to introduce new tourism uses and boost jobs through the continued growth of the hospitality sector. The delivery of new hotels and associated commercial uses will boost visitation and tourism to the Darling Island sub-precinct and support the patronage of other subprecincts within the area.

This growth will be sought with a view to promote activities under the 24-Hour Economy Strategy in a way that addresses impacts to residential amenity such as noise, traffic and transport.

### DIRECTION #2



# **Development that complements** or enhances the area

New or upgraded buildings fit with the peninsula's evolving character

The Star notes that the character of the Darling Island sub-precinct will continue to evolve over time. The PPPS places an emphasis upon increased employment and dwelling capacity closest to the potential new Metro station and note the need for buildings along Union Street to transition in height to the harbour.

Opportunities will also be sought to 'open up' connections through the main Star site to facilitate improved walking and cycling connections, while preserving harbour views, protected heritage items and green space.

DIRECTION #3

# Centres for residents, workers and visitors

New lively, attractive centres for everyone to enjoy

The Star Key Site is located at the southern edge of the Darling Island sub precinct and its adjacency to the Tumbalong Park and Pyrmont Village sub-precincts mean that uses along Union Street is not only a key connection, but its uses at street level will need to enhance and complement the rich grain of uses already seen at Union Square and Harris Street. In addition, opportunities to successfully facilitate the activation of Pirrama Road and Pyrmont Bay Park will also be a priority as part of this Master Plan.

### **DIRECTION #5**



# A tapestry of greener public spaces and experiences

Better Spaces, streets and parks; a rich canopy of trees and access to the foreshore

The Key Site Master Plan will make an active contribution towards the improvement of the public domain, particularly around improving streets and where possible preserving and enhancing pedestrian amenity to achieve the 25% canopy cover across the precinct.

Approaches to the public domain around The Star will focus around Pirrama Road, Jones Bay Road, Edward and Union Streets and include Pyrmont Street. The desire to integrate the foreshore walk and Pyrmont Bay Park will form the basis of an attractive, safe and connected places - suited to walking and cycling, reinforced where appropriate with active uses such as shops, cafes, outdoor dining, public art, and community spaces.

**DIRECTION #7** 



Safer, greener streets integrating with new public transport

The Star has actively sought to prioritise Union Street and Pirrama Road as key movement interfaces in anticipation of the potential arrival of the Metro station which will see a significance shift in how visitors and residents will access Pyrmont as well as improving access to the foreshore.

Given the site's proximity to one of Pyrmont's most significant pieces of transport infrastructure, the proposal will seek opportunities to provide better access to and connectivity between public transport modes.

The prioritisation of Union Street and Pirrama Road as major active transport and traffic corridors will also focus on upgrades to walking and cycling access around the headland, as well as improve changes between transport modes. The Star is will also continue to work with TfNSW and other government agencies to ensure improvements are consistent with the objectives for the sub-precinct.

**DIRECTION #8** 

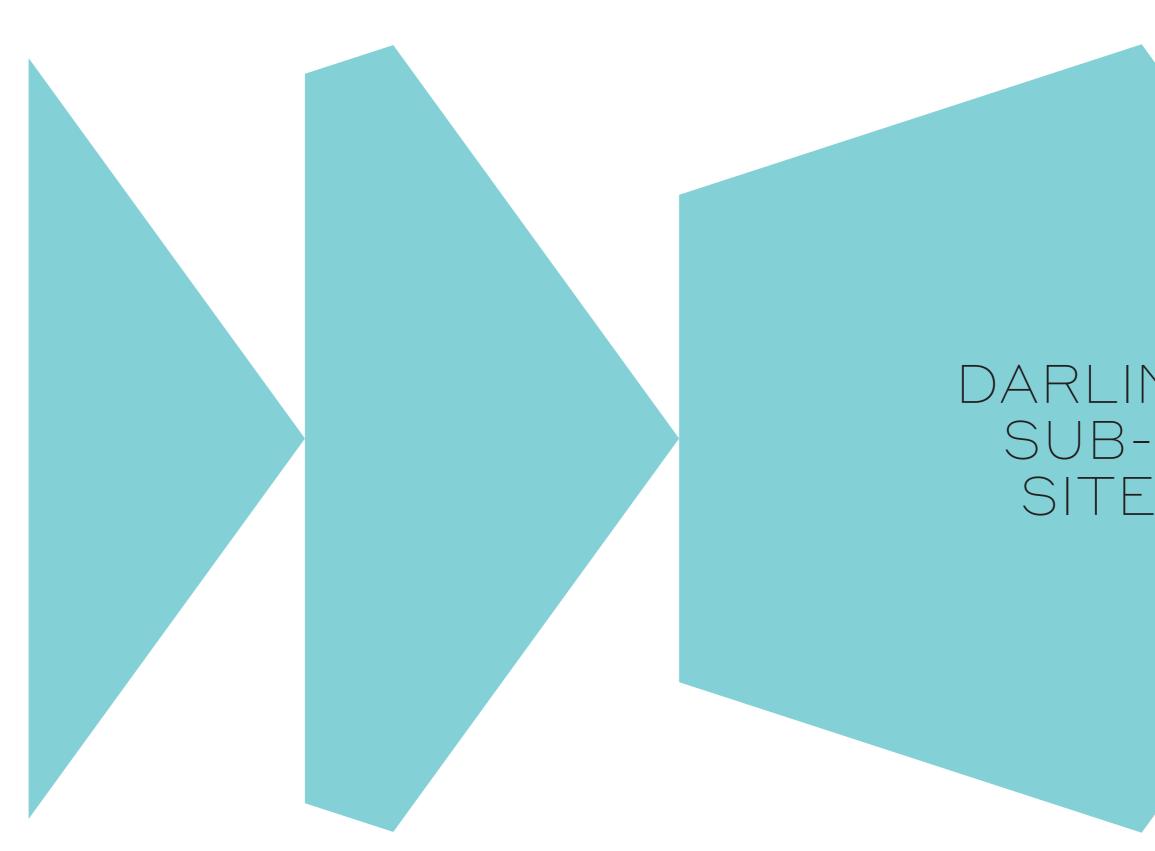


performance precinct.

The Key Site Master Plan will include a Precinct Sustainability Strategy and a Buildings Sustainability and Circular Economy Strategy. These strategies will provide targets and commitments around various initiatives that The Star will seek to undertake.

# An adaptive, sustainable and resilient built environment

The Star recognises that a precinct-wide approach and response to sustainability is key for contributing towards a low carbon, high This page has been intentionally left blank.



# 3.0

# DARLING ISLAND SUB-PRECINCT SITE ANALYSIS

# 3.1 Darling Island Sub-precinct

The Darling Island foreshore has historically formed part of the working harbour and waterfront. This includes The Star's northern site, which was the location of the Pyrmont Power Station until 1989.

As such, much of the sub-precinct is characterised by several low to medium scale large-format industrial buildings up to 6 storeys in height, including the old wharf buildings. These buildings are punctuated by large open spaces connected by the foreshore walk that hugs the edge of the Pyrmont Peninsula. These buildings are now home to a range of uses, including commercial and residential dwellings. The sub-precinct and The Star itself is highly visible from many vantage points around the harbour, including Barangaroo and Balmain.

In contrast, the neighbouring sub-precinct of Pyrmont Village to the southern and western edges of Darling Island is vastly different. Characterised in the PPPS as a fine grain village, the northern edge of the sub-precinct along Pyrmont Bridge Road has a vastly different grain which comprises primarily residential perimeter block strata units that occupy long north-south blocks along Murray Street. Similarly, the western edge formed by Pyrmont Street is characterised by rear entries to buildings which front Harris Street.

Both sub-precinct edges form part of The Star's southern and western boundary conditions. The more significant of the two interfaces is the Pyrmont Bridge Road interface, which will be a key transition to this neighbourhood, despite having being earmarked in the PPPS as a location for taller buildings.

As indicated under the PPPS, the Darling Island sub-precinct is earmarked for additional growth by expanding its role as a major cultural and entertainment destination and facilitating further jobs growth through commercial uses.



03 Sub-precincts as indicated under the Pyrmont Peninsula Place Strategy (PPPS)



	The Star Key Site
5 Z 3	Darling Island sub-precinct
[[]]]	Zone of influence
(])	Darling Island sub-precinct edges and interface with Pyrmont Village sub-precinct
	Pyrmont Village sub-precinct
	Tumbalong Park sub-precinct
	Blackwattle Bay sub-precinct
	Pirrama sub-precinct
	Wentworth Park sub-precinct



**04** A number of newer campus style commercial buildings have been developed along the Darling Island foreshore in the past 10 years along side refurbished large format industrial buildings.



**05** The existing condition along Pyrmont Street adjacent The Star is consistently coarse in grain, forming the back entries to many of the buildings fronting Harris Street.



07 Residential dwellings along the southern side of Pyrmont Bridge Road which form the edge of the Darling Island and Pyrmont Village sub-precincts.



**06** A number of buildings along the foreshore have been adaptively re-used to house commercial and employment uses.



**08** Many of the residential flats to the south of the Darling Island sub-precinct are oriented northsouth and are perimeter block, up to 8 storeys in height.

# 3.2 Darling Island Capacity for Change

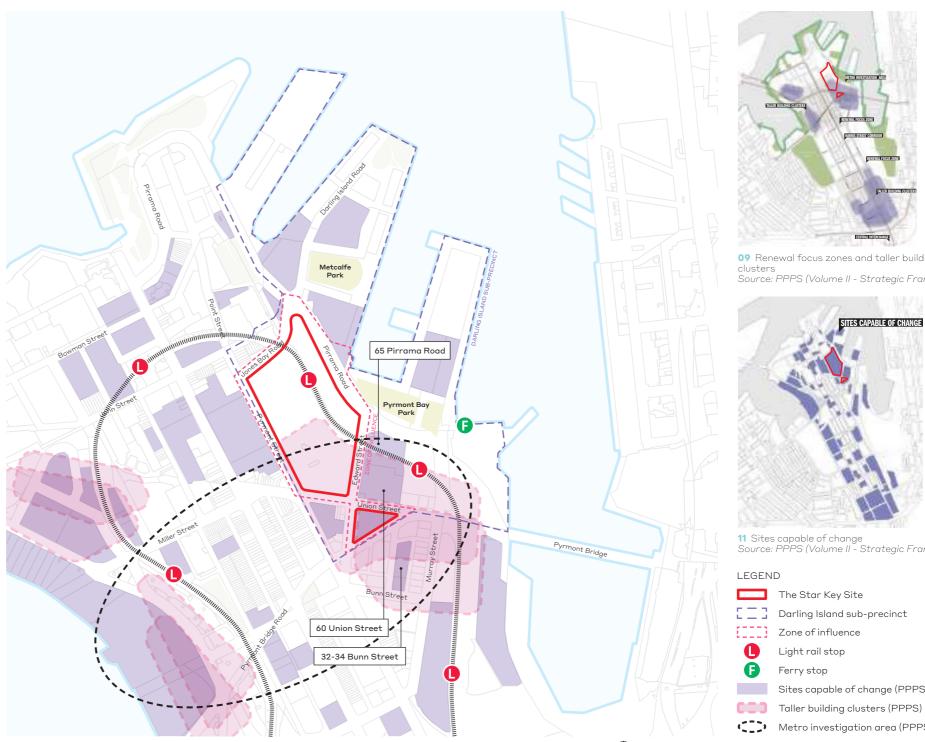
The taller building clusters and Metro investigation area identified under the PPPS will frame the southern edge of the Darling Island sub-precinct as the location of the new Pyrmont centre reflected by its height and density.

More contemporary buildings such as 60 Union Street and 65 Pirrama Road are on large sites capable of increased capacity over time. Most buildings, particularly along the foreshore, are heritage buildings with limited capacity for future expansion.

There is also an existing cluster of strata-titled residential apartments south of the Darling Island sub-precinct boundary that fall under the taller building clusters identified in the PPPS. However, 32-34 Bunn Street is the only building in this residential cluster that has been identified as a site capable of change.

### **Key Findings**

- Future tower development along Union Street will need to consider setbacks above podium level as demonstrated by The Darling development to minimise visual and wind impact from the street of a minimum of 5 metres, consistent with wind advice.
- Taller buildings around Union Street, Pyrmont Bridge Road to Pyrmont Street will need to carefully consider the cumulative impacts of overshadowing to the residential dwellings to the south and the effects of wind.
- The podium height established around Union Street and Pyrmont Bridge Road will also need to be consistent with the bulk and scale of existing development along with Union and Edward Streets.



10 Sites capable of change, taller building clusters and Metro investigation area identified under the PPPS



09 Renewal focus zones and taller building Source: PPPS (Volume II - Strategic Framework)

11 Sites capable of change Source: PPPS (Volume II - Strategic Framework)

Sites capable of change (PPPS) Taller building clusters (PPPS) Metro investigation area (PPPS)

() SCALE 1:7000 @ A3



12 60 Union Street

13 60 Union Street



14 65 Pirrama Road

**15** 65 Pirrama Road



# 3.3 Solar Access to Adjacent Residential Development

The existing residential cluster south of Pyrmont Bridge Road fall under the taller building clusters identified in the PPPS. However, the lots in this area consist of multiple strata-titled residential buildings, including 32-34 Bunn Street, 1-5 Harwood Street and 17-21 Pyrmont Bridge Road.

While only 32-34 Bunn Street is identified as a site capable of change, this residential cluster overlaps with the taller building clusters identified under the PPPS and will be impacted by the South Tower on The Star Key Site.

## **Key Findings**

• In order to understand the impacts that The Star Key Site's South Tower will have on future development in this residential cluster, assumptions around the potential future development envelopes in this cluster will need to be developed and tested.



22

### LEGEND

	The Star Key Site
$\mathbb{C} \boxtimes \mathbb{C}$	Darling Island sub-precinct
[[]]]	Zone of influence
l	Light rail stop
6	Ferry stop
	Sites capable of change (PPPS)
( ) )	Taller building clusters (PPPS)
203	Existing residential cluster impacted by The Star Key Site South Tower



18 32-34 Bunn Street

17 32-34 Bunn Street





20 17-21 Pyrmont Bridge Road



# 3.4 Hydrology + Landform

The Darling Island sub-precinct landform has been modified heavily over its industrialised, European history and was once part of a low-lying area between Darling Island and the Pyrmont Peninsula downstream from the higher ground along Harris Street.

The Darling Island sub-precinct is prone to localised flooding, particularly around the two key low points around Pirrama Road and Pyrmont Street, which were depressions in the landscape of between 2 and 4 metres.

The layering of sandstone, landscape and development characteristic of other parts of the Pyrmont Peninsula identified under the PPPS are not necessarily applicable given the topographic features described as part of the subprecinct.

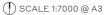
# **Key Findings**

• Additional work will need to be done around Pyrmont Bay Park to ensure that the flood events are mitigated and heavily engineered to minimise flood events, with extensive stormwater works located under Pyrmont Bay Park and Pyrmont Street. This should be considered and integrated as part of public domain improvements, particularly to Pirrama Road.



21 Hydrology and landform of the Darling Island sub-precinct

The	Star	Kev	9





22 Sandstone enscarpment along Pirrama Road towards northern end of the Pyrmont Peninsula 23 Low point along Pirrama Road adjacent to Pyrmont Bay Park







25 Low point along Pyrmont Street

# 3.5 Road Network

### Vehicular Movement

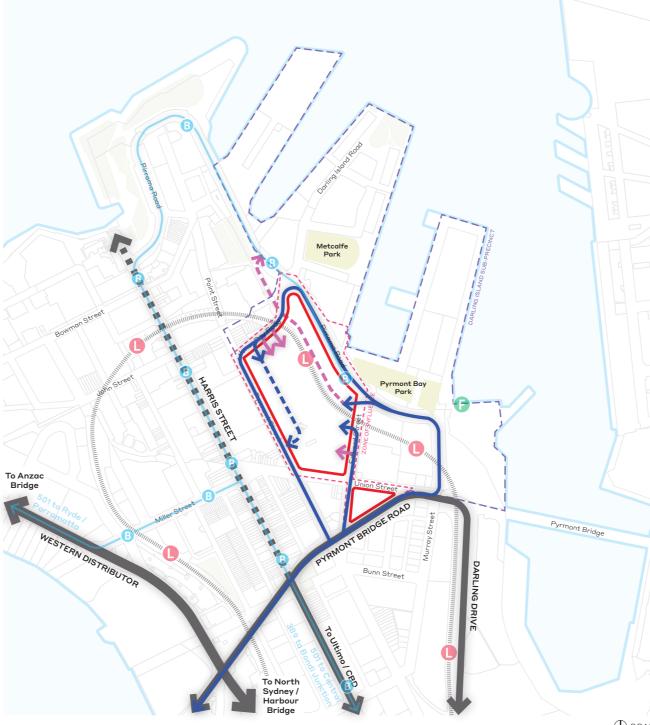
Traffic to the peninsula arrives predominately via the Western Distributor in east and westbound directions. Pyrmont Bridge Road plays a significant role in distributing the traffic across the peninsula, taking traffic from the Western Distributor and funnelling vehicles south down Harris Street or east towards Darling Drive and Harbourside.

Pyrmont Bridge Road is the main vehicular access point for entry into the Darling Island sub-precinct, providing secondary access to Pirrama Road, Edward, Pyrmont, and Union Streets for vehicles arriving at The Star and the foreshore.

The traffic movements generated by The Star comprise a mix of service and private vehicles which use Pirrama Road and Pyrmont Bridge Road as a loop to access the various entry points on Edward Street and Jones Bay Road, including the porte-cochere parallel to Pyrmont Street. An internal service road also runs parallel to Pirrama Road, which services The Star Express minibuses and loading. This internal service road runs northwards under Jones Bay Road and exits to Pirrama Road.

Beyond these peak demands, the Darling Island sub-precinct is subject to more minor, localised traffic movements consistent with the surrounding commercial and residential community. Local traffic accounts for only a small portion of the movement, with many residents, rely upon public and active transport options, underlined by one of the lowest car ownership rates in metropolitan Sydney.

With the potential for Metro, any development on the southern site will require parking and loading access to be located off Edward Street in order to minimise cross-overs with active transport corridors along Union Street and Pyrmont Bridge Road, which will be further discussed in **Section 3.6 - Active Transport.** 



26 Vehicular access and movement to and around The Star



2202118		
The Star Key Site		
Darling Island sub-precinct		
Zone of influence		
Light rail stop		
Ferry stop		
Bus stop		
Arterial / through traffic		
Local serving high street		
Traffic serving The Star		
Access to porte-cochere		
Loading / servicing		
Internal service road		

() SCALE 1:7000 @ A3

### **Key Findings**

- As a 24/7 venue, The Star generates a significant amount of movement, including visitors arriving by taxi and cars, loading and supply vehicles as well as private coaches to service The Star Grand Hotel and Residences.
- Significant traffic challenges occur during peak periods during significant events and weekends, where visitors' peak arrival and departure cause significant congestion around the entire road network.
- Safety measures such as traffic calming on all streets, particularly Union Street and Pirrama Road, will need to be considered as part of a broader public domain upgrade to better accommodate pedestrian safety, particularly with the potential arrival of the Metro to better align with Direction 7 of the PPPS - Making it easier to move around.
- Minimisation of traffic movements to and from The Star will need to consider measures to contain movement within the complex itself and provide additional capacity for taxis and coaches to park on-site to maintain the amenity of the existing street network.
- The existing internal service road could be re-organised to increase its capacity to facilitate additional ride share bus and coach drop off and pick up points.
- While there will not be an increase in overall car parking capacity for The Star, there is the possibility of exploring a multi-utility hub.
- No changes to existing loading arrangements will be required with the exception of any new development to be established on the southern site.
- Changes to the road network will focus on retaining existing vehicle capacity with the understanding that the potential arrival of the Metro and improvements in active transport would likely offset the increase in passengers and people on the street.



27 Pyrmont Bridge Road

28 Porte-cochere exit along Pyrmont Street







30 The Star internal service road exit to Pirrama Road



# 3.6 Active Transport

### Cycling + Walking

The primary cycling route within the Darling Island sub-precinct, as identified by the PPPS, is along Union Street, with an approach from Pyrmont Bridge Road eastbound, connecting to Miller Street westbound towards the Anzac Bridge.

It consists of a grade-separated cycleway connecting cyclists to Harris Street, Darling Drive and the Anzac Bridge.

At peak, the cycle connection along Pyrmont Bridge supports 2,500 movements per day and up to 1,500 activities to the Anzac Bridge, the highest across the peninsula and the busiest outside of Central Sydney.

Other more localised transport movements are located along Darling Drive but support far less movement north-south.

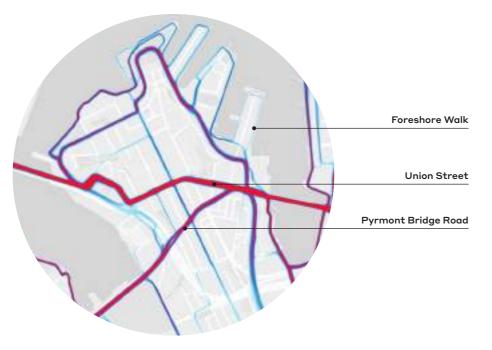
The foreshore walk, currently identified as Big Move 1 under the PPPS, supports recreational cycling, walking, and running opportunities.

### **Key Findings**

- Union Street and Pyrmont Bridge Road are key cycle corridors: Union Street is a critical east-west connection across the peninsula connecting the Inner West to Central Sydney. Local cycle movements generally occur via Pyrmont Bridge Road across to neighbouring suburbs, including Glebe.
- The Foreshore Walk is important for walkers + runners: As indicated in the Strava heat map information, it is clear that Pirrama Road is favoured heavily by walkers and runners.
- Linkages to the water + foreshore walk are limited: The Darling Island sub-precinct only has three linkages to the foreshore through the public domain, two of which are incomplete (Jones Bay Road and Edward Street), which makes for a poor walking experience to the foreshore.



LEGEND		
	The Star Key Site	
C I 3	Darling Island sub-precinct	
[[]]]	Zone of influence	
0	Light rail stop	
G	Ferry stop	
Ū	Taxi rank	
	Street network	
	Harbour foreshore walk	
	Incomplete harbour foreshore walk	
	Harris Street	
	Cycleway / shared pedestrian path	
	Connections to the foreshore	
	Incomplete connections to the foreshore	
	Light rail crossing	
	Narrow footpath along Pyrmont Bridge Road	

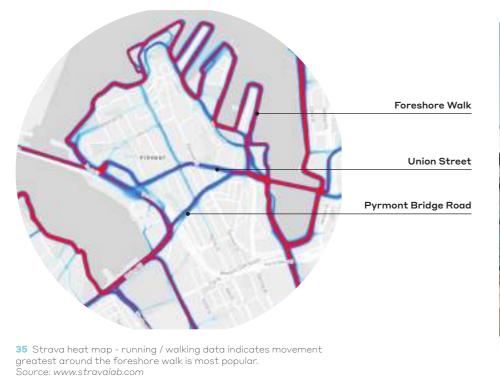




**33** Union Street cycleway

34 Union Street cycleway towards Pyrmont Bridge

**32** Strava heat map - cycling data indicates movement greatest around the foreshore walk is most popular. *Source: www.stravalab.com* 





**36** Harbour foreshore walk

37 Harbour foreshore walk





# 3.7 Public Transport

### Light Rail

The Star light rail stop is part of the T1 line from Dulwich Hill to Central. The stop is one of two in the Darling Island sub-precinct, with the other, Pyrmont Bay light rail stop, located next door.

Both stops are located on grade but sleeved behind commercial frontages making them challenging to find. In the instance of The Star light rail stop, this is exacerbated by limited access points and poor sightlines to the stop from Pirrama Road.

Poor wayfinding to the station is characterised by poorly indicated access points from Pirrama Road and convoluted access points from within The Star via escalators at higher levels.

### Bus + Ferry

The bus is currently served by the 389 line to Bondi Junction via Central Sydney, running a loop around the Pyrmont headland along Harris Street and Pirrama Road, stopping at The Star and the top of Union Square.

Regular ferry services run out of the pier located on the south-eastern edge of the Darling Island sub-precinct with connections to Balmain, Milsons Point, Circular Quay and Barangaroo.

### Metro

The recent announcement of the potential for a Metro station on the corner of Edward and Union Streets will profoundly impact the Darling Island subprecinct, with access to the potential station placing pressure on the existing pedestrian and active transport networks.

# **Key Findings**

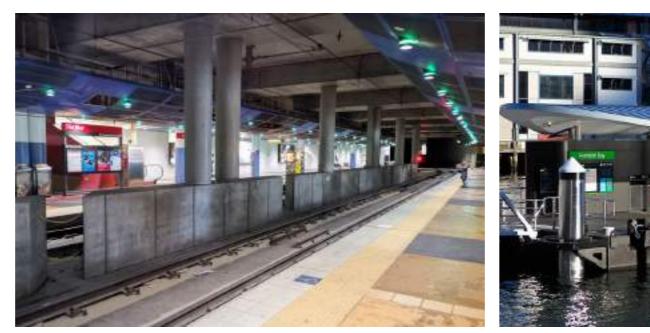
- Buses, ferries, and light rail are in close proximity to one another but are poorly integrated in the Darling Island sub-precinct and demand better, more convenient interchanges, particularly with the potential arrival of the Metro. This is important to ensure that the Master Plan aligns with Big Move 3 of the PPPS - Connect to Metro.
- A unifying, legible and safe public domain for pedestrians from Pirrama Road to the light rail stop should be considered in short to medium term, while opportunities to open Edward Street by removing visual barriers to facilitate improved lines of sight and enhanced access at street level should be further investigated.



38 Public transport infrastructure around the Darling Island sub-precinct

### LEGEND

The Star Key Site Darling Island sub-precinct Zone of influence 1222 Light rail stop O B Ferry stop B Bus stop O Taxi rank 389 bus route



**39** The Star light rail

40 Pyrmont Bay ferry



41 Pyrmont Bay light rail







# 3.7 Public Transport

# Alternative Light Rail Configuration

Edward Street presents an important opportunity to provide a genuine public benefit not just for the Darling Island sub-precinct but for other sub-precincts on the peninsula as well, by having the potential to provide an improved northsouth connection to the foreshore and provide a meaningful contribution towards Big Move 1 of the PPPS - A world-class harbour foreshore walk.

As part of the site analysis process, two scenarios around improving Edward Street were investigated. Both strategies aim to facilitate ease of mode change between public transport options and improve connectivity to the foreshore.

- The two scenarios tested are:
  - Scenario 1 Edward Street at grade crossing (keeping current light rail stop)
  - Scenario 2 Integrated light rail stop on Edward Street



### LEGEND

	The Star Key Site
C I 3	Darling Island sub-precinct
[[]]]	Zone of influence
0	Light rail stop
F	Ferry stop
	Connections to the foreshore
	Incomplete / future connections to the foreshore

### **Key Findings**

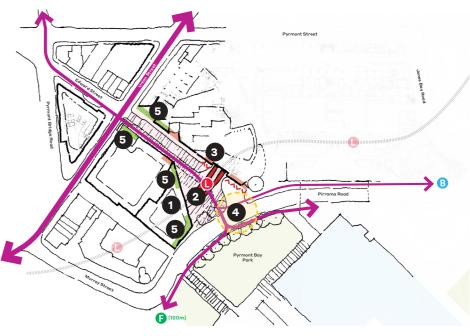
- The location of a new consolidated light rail stop on Edward Street would be unlikely given the challenges indicated under Scenario 2 on the right. Both The Star and Pyrmont Bay light rail stops will likely remain as is with the potential to provide an a grade crossing on Edward Street towards the foreshore. The Master Plan will be developed assuming that Scenario 1 is the preferred scenario, subject to further discussion with TfNSW.
- Notwithstanding the above, consolidating The Star and Pyrmont Bay light rail stops on Edward Street to facilitate a more integrated transport hub will enable a better commuter experience between changes in modes of transport and should remain a long-term aspiration for the Darling Island sub-precinct.

### Scenario 1 - Edward Street at grade crossing (keeping current light rail stop)

- Removal of fencing along Edward Street to enable level crossing with automated barriers.
- Reconfiguration of The Star car park entry to facilitate pedestrian movement.
- Signalling infrastructure upgrades on light rail line to facilitate (subject to further investigations with TfNSW).
- Widening of footpath along Edward Street via the removal of car parking along Edward Street on southern edge.

# Scenario 2 - Integrated light rail stop on Edward Street

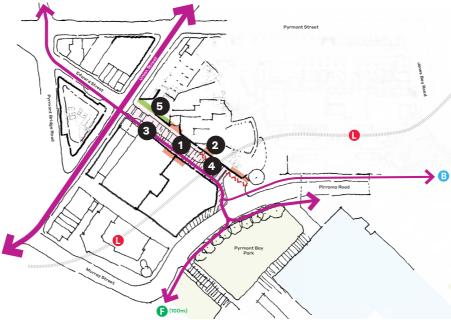
- arrangements mean Lyric Theatre to remain.
- Edward Street on southern edge.
- accommodate additional pedestrians.
- may pose a flooding constraint.
- removal would be unlikely.



- 45 Scenario 2 Integrated light rail stop on Edward Street
- 1 development). 2 barriers. 3
- (4)
- 5 New active frontages.



46 Light rail crossing at the corner of Edward Street and Pirrama Road



- 44 Scenario 1 Edward Street at grade crossing
- 1 Public domain upgrade shared way paving.
- Reconfigure The Star entry to single lane exit. 2
- 3 Extended pedestrian path to eastern edge of Edward Street.
- 4 Remove barriers and upgrade signals.
- 5 New active frontage.



• Assumes the re-development of 63-65 Pirrama Road would enable positioning of a new stop, with extension to public domain.

• Retention of The Star Lyric Theatre and loading areas on long term lease

• Reconfiguration of The Star car park entries to facilitate pedestrian movement.

• Widening of footpath along Edward Street via the removal of car parking along

• Capacity at crossing of Pirrama Road and Edward Street upgraded to

• Raised platform perpendicular to Edward Street (which is an overland flow path)

• Integrated light rail stop reliant upon neighbouring site to enable - the Lyric Theatre on long term ownership (not under the control of The Star) means

Reconfigured site to accommodate light rail stop (future

Location of integrated light rail stop between buildings, removal of

Gradual decommissioning of car park, consolidate exit to single lane.

Reconfigured square / scramble crossing to Pyrmont Bay Park.

# 3.8 Built Form

The Darling Island sub-precinct is defined heavily by the presence of large format warehouse and industrial building typologies of a low-rise character, long-span character which reflected the area's industrial past.

Large warehouse buildings along Pyrmont Street form a continuous street frontage that backs on the high street and stitches the back edge of Darling Island with the Harris Street Village sub-precinct. Combined with the unbroken façade of The Star, the buildings are unrelenting, only softened by significant tree cover to both sides of the street.

Union Street defines the southern edge of the Darling Island sub-precinct. It marks a change in scale from low, large-format industrial buildings to finer residential-scale buildings mixed with warehouse conversions south of Pyrmont Bridge Road.

The built form scale of the street is a mix of low to medium-rise buildings of varying scales and uses. The fall of the road from west to east means the height of the street wall varies from 9 metres on the western side up to 20 metres to the east, with taller buildings such as The Darling Hotel set back 20 metres from the street.

The transition located at the top of Union Street at Union Square, close to Harris Street, marks the beginning of the Pyrmont Village sub-precinct, which features a similar grain of low-rise retail, commercial and mixed-use frontages.

### Heritage

There is little heritage to speak of on The Star site itself, given the almost complete demolition of the building that stood before it. Not withstanding, the historical context of Pyrmont and its role in the harbour is part of a more complex and nuanced story around both First Nations Indigenous and European narratives, which will be examined as part of a Heritage Assessment, as part of any future development application for the site.



LEGEND		
	The Star Key Site	
$\mathbb{C} \boxtimes \mathbb{C}$	Darling Island sub-precinct	
[[]]]	Zone of influence	
	Pyrmont Village sub-precinct	
	Pirrama sub-precinct	
	Tumbalong Park sub-precinct	
	Blackwattle Bay sub-precinct	
	Wentworth Park sub-precinct	
l	Light rail stop	
G	Ferry stop	
	Large format warehouse and industrial building typologies	
	Other buildings within the Darling Island sub-precinct	

# ΙF



48 Conversion of older wharf and marine buildings at the water's edge along the foreshore.



**49** Contemporary large format commercial buildings sit side by side with other conversions of existing wharf buildings along the foreshore.



53 Larger scale warehouse conversions are also part of the Pyrmont Village sub-precinct fabric and sit side by side with finer scale buildings along Harris Street.



**50** Continuous street frontage along Pyrmont Street



**51** Conversion of warehouses in the area around the Pyrmont Village sub-precinct also provide a contrast to the fine grain that Pyrmont Village is most noted for.



52 Fine grain built form along Union Street.

# 3.9 Key Street + Built Form Interfaces

### **Union Street**

The section of Union Street related to The Star's property boundaries is part of a relatively modest street wall with taller development significantly set back from the street to reinforce the lower scale character.

The scale of development currently along Union Street ranges from 12-20 metres in height depending upon the elevation of the street and feature generally zero lot setbacks.

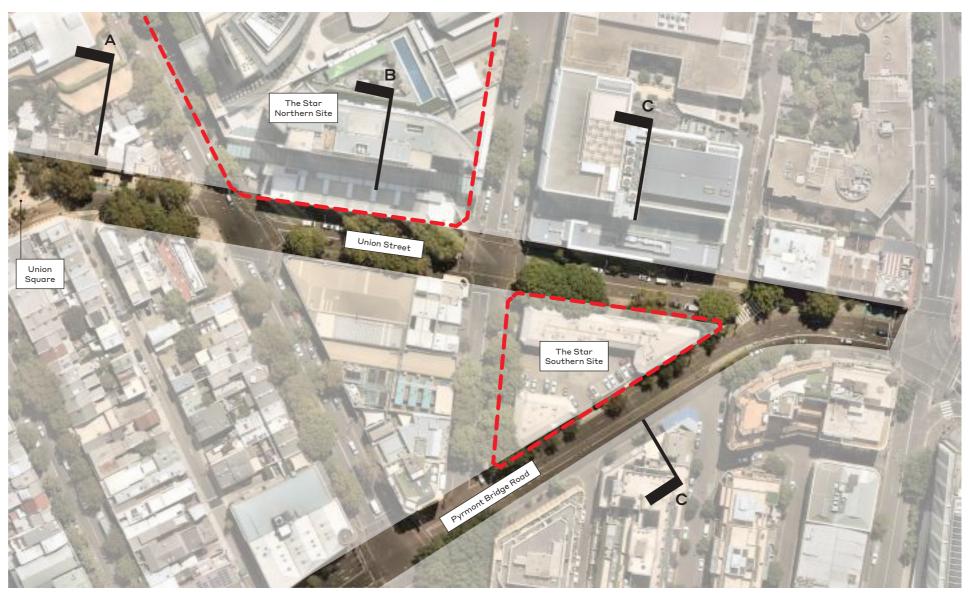
The change in elevation of the street is also consistent with the change in grain and character of the street's built form. This is marked by significantly more refined, lower scale development, part of the Pyrmont Village sub-precinct towards Union Square, which is the area's high point.

The need to mediate these changes in scale, particularly with future development, will be significant in enabling development to occur without detracting from the existing character of the street.

### Pyrmont Bridge Road

Pyrmont Bridge Road comprises heights up to 8 storeys, consisting mainly of residential frontages adapted from former warehouses and in-filled with newer perimeter block development. Given the role of Pyrmont Bridge Road as an east-west main road across the peninsula, a street wall of a similar scale on The Star Key Site to match the bulk and scale would not be inconsistent with the character of this street.

The lack of walkway amenity on this road is exacerbated by a lack of canopy cover in this area. The zero lot setback characteristic of development in the area also results in footpaths less than 1.5 metres in width.



54 An aerial view of Union Street and Pyrmont Bridge Road



55 Grain and scale of lower density uses towards Union Square



 ${\bf 56}\,$  Lower scale street wall with development set back from the street along Union Street



**57** The lower scale podium is consistent across both street blocks along Union Street

#### **Key Findings**

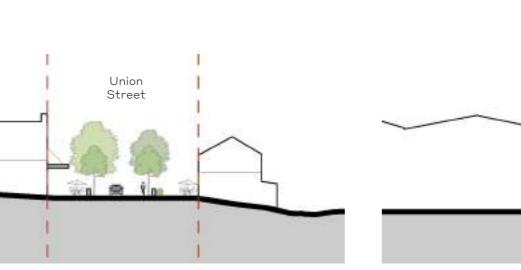
- A transitional built form element (such as a podium) should be introduced to ensure any future development on the site can successfully mediate the scale of existing built form around Union Street and Pyrmont Bridge Road. Future development along Union Street should avoid point tower forms that do not provide a transition to the ground plane.
- Where possible, podiums and setbacks to towers above podiums should be designed to minimise the visual impact from the street.
- Using Pyrmont Bridge Road as a datum for establishing a podium height for future development would not be unreasonable given the character and scale of residential apartments on the other side of the road.



**58** Narrow footpath to the southern boundary of The Star's southern site provides poor pedestrian amenity.

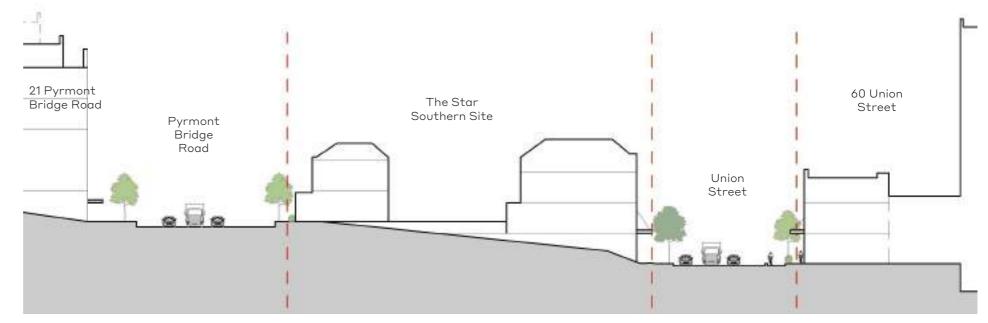


59 The intersection at Pyrmont Bridge Road and Pirrama Road

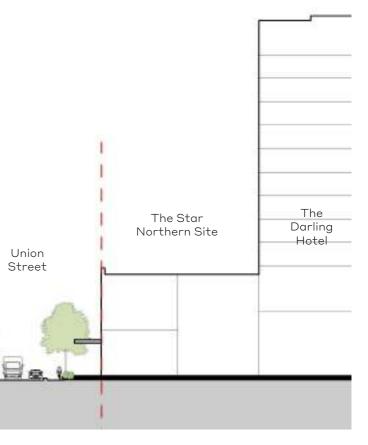


**61** Section A - Section through Union Street near Union Square, a much finer grain of development which is consistent with the heritage fabric in the sub-precinct.

**62** Section B - This section drawn through The Star and The Darling Hotel illustrate the setback of development from Union Street to preserve the existing street character.



**60** Section C - Joint section through Pyrmont Bridge Road and Union Street. A datum for the podium of any future development using the Pyrmont Bridge Road scale as a starting point would not be inconsistent with the built form character on Union Street.



#### Pirrama Road

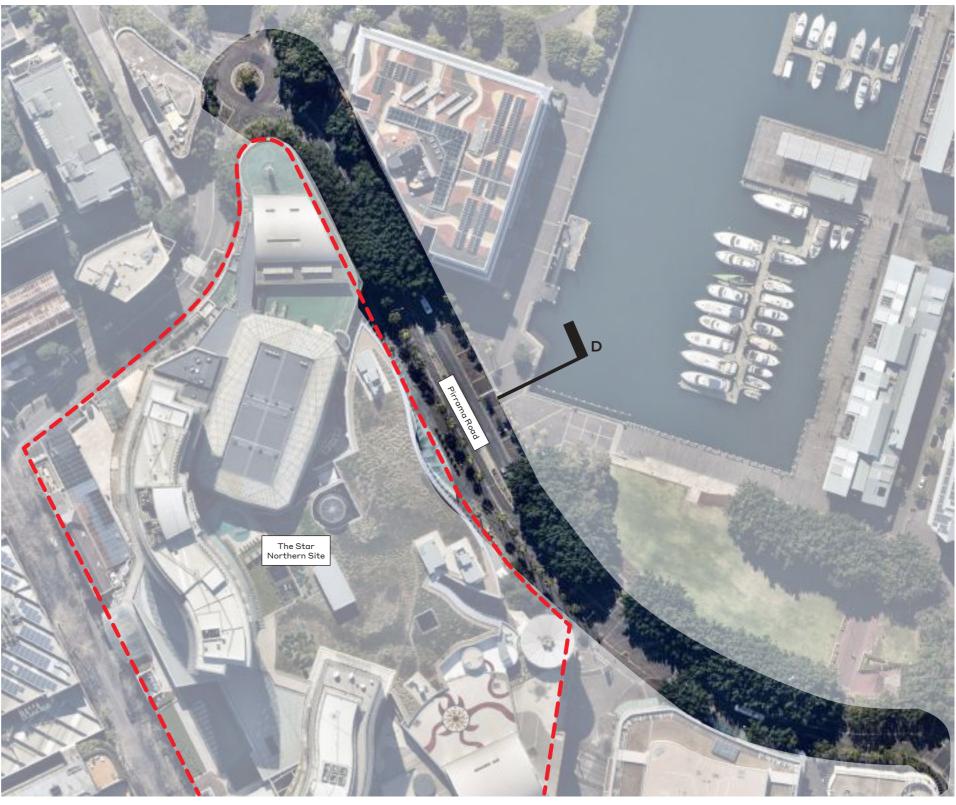
Pirrama Road has been identified under the PPPS as a priority active transport corridor and provides a key access point to the foreshore walk. In retaining most of the existing functions within the existing 6 storey podium, The Star will not look to add to the existing podium, except for a tower at the northern end of the site at the corner of Pirrama and Jones Bay Roads.

The positioning of any new development above the podium will need to ensure the form is set back from the podium edge to provide a visual break from the street wall and minimise the impact of prevailing winds from the south and northeast. Setbacks will assist in providing a foil for downdraft winds.

At the street level, priority will also be given to exploring adjustments to the existing entry on the ground floor to provide a more open and active edge, which opens up the light rail entry and street edge to the foreshore while seeking opportunities to improve define active transport routes.



63 Pirrama Road frontage



64 An aerial view of Pirrama Road

2200827 The Star Key Site - Urban Design Report

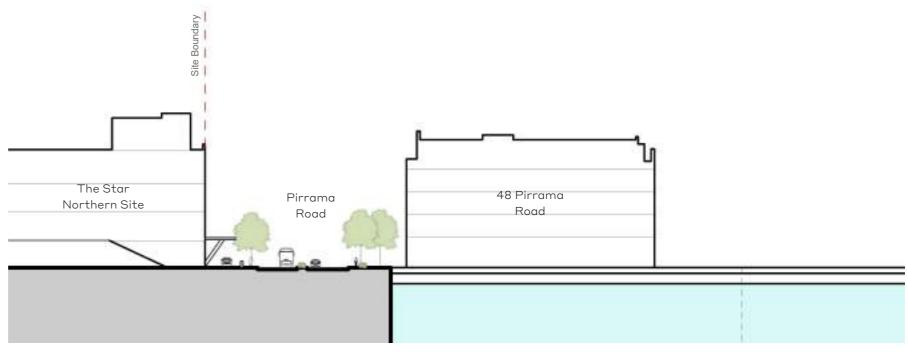
#### **Key Findings**

- As indicated in the PPPS considerations for Key Sites, a tower (RL110) will be positioned at the northern end of Pirrama Road of the main Star site.
- The position of the tower does not anticipate any changes to the datum of the existing podium. However, it will need to ensure a setback above the podium which is sufficient to mitigate the effects of wind and provide a clear articulation between the two forms.
- Additional considerations will also need to be made at street level to improve pedestrian amenity to enable better access to the light rail and foreshore.



65 Light rail signage along Pirrama Road is poorly located, undersized and difficult to identify.

66 Existing podium at the corner of Pirrama Road and Jones Bay Road.



67 Section D - Section through Pirrama Road. The position of the North Tower does not anticipate a change in datum of the existing podium but a setback above the podium will need to be provided to ensure clear built form articulation and to mitigate wind impacts to the street.



#### Edward Street

Edward Street falls from the southwest to the northeast towards the foreshore and is only one of two streets that lead to the water, running along the eastern edge of The Star's northern site.

The street's character is the consequence of its role as a service road that serves as the crossing point for the light rail while also enabling car park and services access to The Star and the 60 Union Street development.

The resultant outcome is a built form on both sides of the street, which offer little pedestrian amenity or activation. As a dead-end street, cut off by the light rail, the built form comprise largely blanks walls up to 8 storeys in height, with car park and service entry portals for vehicles.

Given it is only one of two streets connecting to the foreshore within the Darling Island sub-precinct, The Star is open to working with TfNSW and associated stakeholders to remove the existing barriers to the light rail and provide an improved built form and public domain outcome which seeks to prioritise pedestrians and reinstate the link to the foreshore.



68 Car park and service access along Edward Street



69 Light rail crossing at the end of Edward Street

#### Pyrmont Street

The northern end of Pyrmont Street, which forms the western edge of the Darling Island sub-precinct, is identified by large warehouse and utility buildings in front of Harris Street.

The imposing nature of the built form along this street is a response to the topographic dip in Pyrmont Street, which is a low point in the area, resulting in building heights of up to 8 storeys along a continuous stretch from north to south. This scale is on both sides of the street where the corresponding street wall created by The Star is of equal height.

The street itself sees little footfall given it is the 'back' of the buildings which front Harris Street, and its corresponding side on The Star side is used as the porte-cochere entry to the hotel and a layover for taxis.



70 SELS building at corner of Pyrmont Street and Jones Bay Road



71 Porte-cochere exit on Pyrmont Street

#### Jones Bay Road

Jones Bay Road is located at the northern end of the site and serves The Star as access for service and loading. The north elevation generally forms the backside of the Star main site, which is reflected by the nature of the facade treatment to the existing building.

While no changes are anticipated to the loading and porte-cochere entries, a revised treatment to the podium to improve visual permeability would improve built form outcomes. The public domain level should consider widened footpaths to improve pedestrian amenity in anticipation of the future throughsite link to the west from Harris Street.



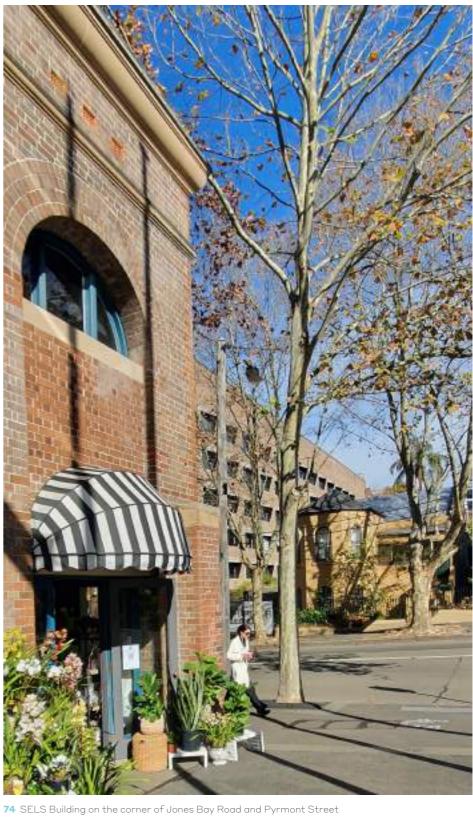
**72** The datum of the existing podium is unlikely to change significantly in height. The Jones Bay Road elevation will need to consider a more open facade to increase site permeability.

#### **Key Findings**

- Future tower development on both the northern and southern sites will need to consider setbacks above podium level to minimise visual and wind impact to the street.
- Edward Street will need to be prioritised as an important potential connection to the foreshore from Union Street and the potential Metro station.



**73** Services and loading access from Jones Bay Road will likely remain untouched, however widening of the footpath to Jones Bay Road will improve access to the foreshore.



## 3.10 Active Uses + Vitality

A measure of activity along the streets surrounding The Star Key Site has been undertaken to gain a better understanding of the level of active uses and vitality of the local area. These street frontages have been categorised into the following levels of street vitality:

#### Active frontages

• Narrower shops, high visual connectivity between the street and the internal activity and good levels of variation in design, materials and retail offer.



• There is usually a higher level public domain comfort with shade, wind and rain protection often provided to pedestrians by awnings and verandahs.

#### Passive frontages

• Long lengths frontage, low visual connectivity to street, less variation and permeability with potential to improve activity through new activity or design modifications, such as awnings and new doorways.

#### Inactive frontages

- Service areas, car parks, blank frontages • with poor or no visual connectivity, no variation or permeability.
- Primarily service, car parking or stairs beyond facade with little visible human activity possible from the street.







#### LEGEND

**[ ]** Darling Island sub-precinct Zone of influence Light rail stop G Ferry stop Fine grain retail Active frontages Passive frontages Inactive frontages

#### **Key Findings**

#### Union Street

In line with the PPPS Structure Plan, the eastern end of Union Street will need to develop a mix of functions that reflect a greater intensity of use which may be both fine grain and community-focused on ensuring increased pedestrian amenity and safety, particularly if the Metro is located along Union Street.

#### Pirrama Road

The primary frontage along this edge is over 95 metres and is mainly inactive. The re-animation of this key frontage is critical to the success of the foreshore walk and Pyrmont Bay Park. The integration of smaller, more intimate single or double-storey retail and restaurant opportunities at street level need to be developed to focus on activating the street rather than reading as part of The Star itself.

#### Edward Street

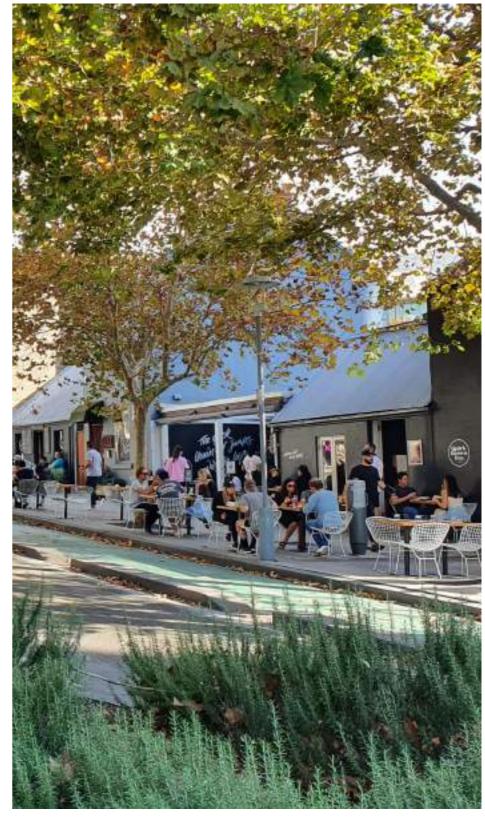
Despite its very ordinary activation level, it is still only one of two connections to the foreshore within the Darling Island sub-precinct. Over time, with the consolidation of public transport, its importance as a movement corridor will be vital to the sub-precinct's success.

#### Pyrmont Street

The northern end of Pyrmont Street is not identified as a priority in the PPPS, and it has a relatively low level of use. The street should be afforded upgrades to lighting and passive surveillance measures to ensure the safety of pedestrians.

#### Jones Bay Road

More can be done to lift the streetscape, particularly given it is only one of two connections within the Darling Island sub-precinct to the foreshore. This will be even more significant with the future through-site link from Harris Street indicated in the PPPS (subject to future development). Effort should be made to provide a more walkable street to include widened footpaths, increased tree canopy cover and where appropriate, new active frontages.



76 Active frontages along western end of Union Street towards Union Square

## 3.11 Public Open Space

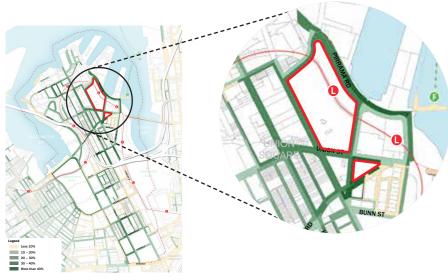
The PPPS identifies a number of open spaces and key streets on the peninsula, of which solar amenity needs to be protected. Refer to **Section 3.12 - Solar** Access to Open Spaces for further information.

The focus of the Master Plan is on improving the streets around The Star Key Site as well as connections to the foreshore walk. While these improvements will have an influence on the accessibility of the surrounding open spaces, the Master Plan will not propose any direct changes or upgrades to Pyrmont Bay Park, Metcalfe Park and the foreshore walk.

The Darling Island sub-precinct has an average of 40% tree canopy across the peninsula, which is significantly higher when compared with the other subprecincts.

#### **Key Findings**

- The Master Plan should aim to retain the majority of the existing tree canopy where possible and if any trees are proposed to be removed, and replaced to offset losses, to be agreed with the CoS and other stakeholders.
- A focus on native species should form the basis of any replacement strategy to be discussed with the CoS and other stakeholders.
- A clearer understanding will need to be sought regarding tree canopy and deep soil targets in line with the Darling Island sub-precinct plan currently being prepared by the DPIE.



77 Existing tree canopy cover Source: PPPS (Volume I - Project Analysis)



LEGEND

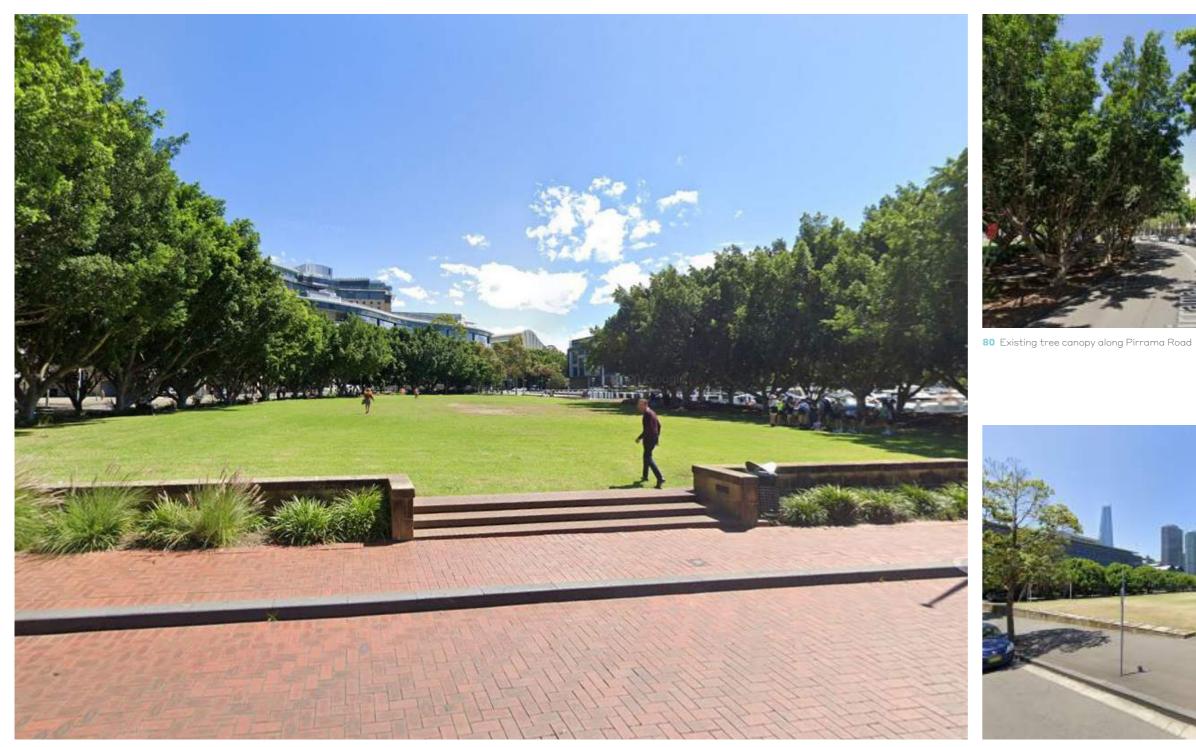


The Star Key Site

- Darling Island sub-precinct
- Zone of influence
- Light rail stop
- Ferry stop
- Open spaces
- Harris Street
- Harbour foreshore walk
- ■■■ Incomplete harbour foreshore walk



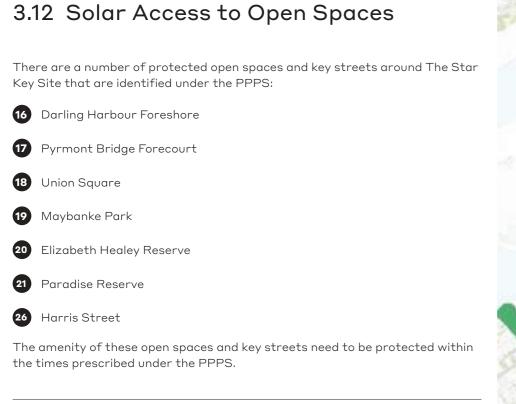
Secondary streets around The Star Key Site to be improved as part of the Master Plan



**79** Pyrmont Bay Park

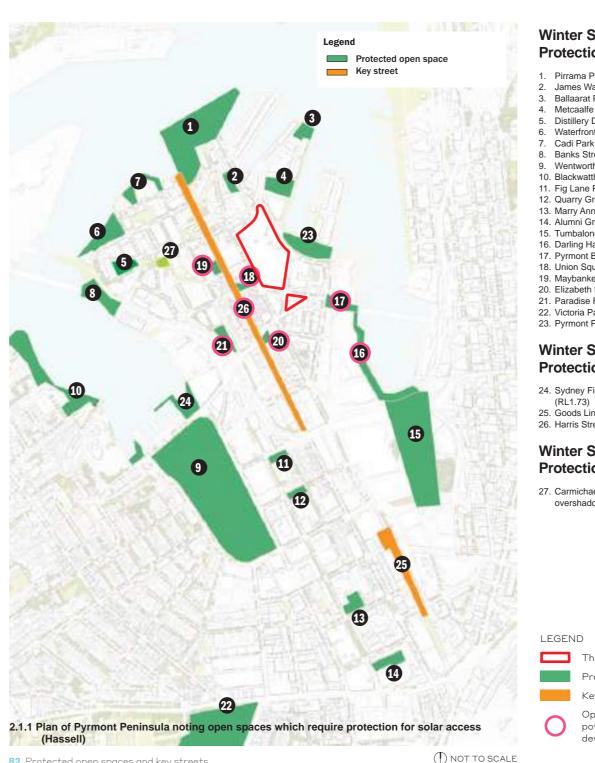






#### **Key Findings**

• Future built form on The Star Key Site will need to ensure that solar access to the open spaces and key streets listed above is protected during the Winter Solstice hours as directed by the PPPS. The Master Plan does not consider any potential future CoS amendments or requirements around solar access to these open spaces and key streets.





#### Winter Solstice Solar Protection 10-2pm

- Pirrama Park and Gibs Park (RL10.08)
  James Watkinson Reserve (RL21.56)
- Ballaarat Park (RL2.0)
- 4. Metcaalfe Park (RL3.67)
- 5. Distillery Drive Park/The Knoll (RL36.0) 6. Waterfront Park (RL9.81)
- Cadi Park (RL5.04)
  Banks Street Park (RL4.27)
- 9. Wentworth Park (RL3.73)
- 10. Blackwattle Bay Park (RL4.32)
- 11. Fig Lane Park (RL20.77) 12. Quarry Green (RL20.43)
- 13. Marry Ann Street Park (RL14.45)
- 14. Alumni Green (RL12)
- 15. Tumbalong Park (RL2.2)
- Darling Harbour Foreshore (RL1.73)
  Pyrmont Bridge Forecourt (RL7.01)
- 18. Union Square (RL15.17)
- 19. Maybanke Park (RL23.75) 20. Elizabeth Healey Reserve (RL15.12)
- 21. Paradise Reserve (RL9.13)
- 22. Victoria Park (RL22.99) 23. Pyrmont Park (RL1.5)

#### Winter Solstice Solar Protection 12-2pm

- 24. Sydney Fish Markets Urban Park
- 25. Goods Line (RL6.5)
- 26. Harris Street (RL12.07)

#### Winter Solstice Solar Protection 10-12pm

27. Carmichael Park (RL23.6) - no additional overshadowing beyond 12pm



potentially impacted by future development on The Star Key Site



83 Elizabeth Healey Reserve

84 Union Square



85 Maybanke Park

86 Paradise Reserve



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# 4.0

# PUBLIC DOMAIN OPPORTUNITIES AND CONSTRAINTS

#### **Public Domain Constraints** 4.1

The following constraints for the public domain surrounding the The Star Key Site have been identified and will be addressed by the proposed Master Plan.

#### Pirrama Road

- Bus and coach layovers currently obstruct Pirrama Road making it 1 more challenging to provide good access to the foreshore.
- Crossing points are limited at intersections with large vehicular 2 entries. Find safer locations for pedestrians to access the foreshore.
- Limited lines of sight from the street to the light rail stop make public 3 transport hard to access.
- Vehicular cross-overs such as the existing taxi drop off clash with the 4 footpath and are ill-defined, posing a hazard to pedestrians.
- Sections of Pirrama Road are flood-prone. Mitigation measures to be 5 integrated into public domain improvements.

#### **Union Street**

- Union Street and Edward Street crossing point currently has limited 6 capacity to support increased pedestrian numbers with the potential arrival of the Metro.
- Northern and southern edges of the street, including The Star have 7 limited opportunities for activation and increased pedestrian capacity while maintaining the capacity of the bike lane.
- Wayfinding for public transport is poor, particularly from Union Street 8 as bus, light rail and ferry stops are not in any direct line of sight.

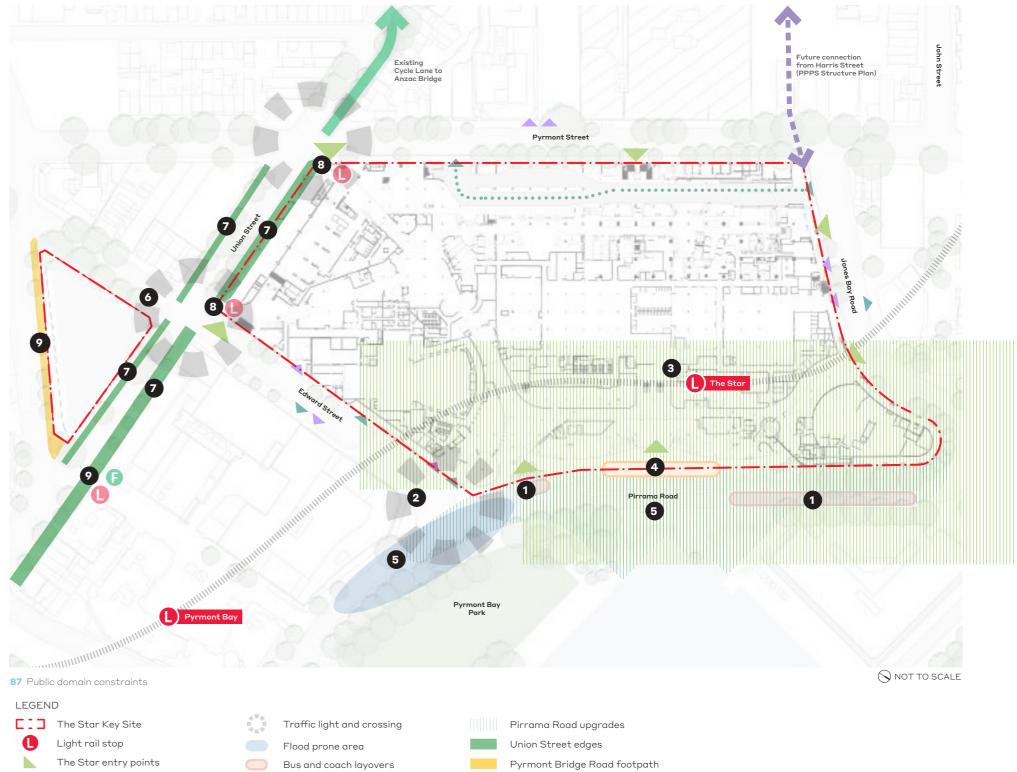
#### **Pyrmont Bridge Road**

Narrow footpath along Pyrmont Bridge Road will need to be 9 maximised to ensure amenity for commuters and explore possibility of bike lane along Pyrmont Bridge Road.

Loading and servicing points

Car park entry points

Vehicular cross-overs



#### Edward Street



Multiple loading and car park entries in close proximity to each other along the street increase risk of pedestrian vehicle conflicts.



Narrow footpaths along both sides of the street provide a poor pedestrian outcome, exacerbated by vehicle cross-overs.

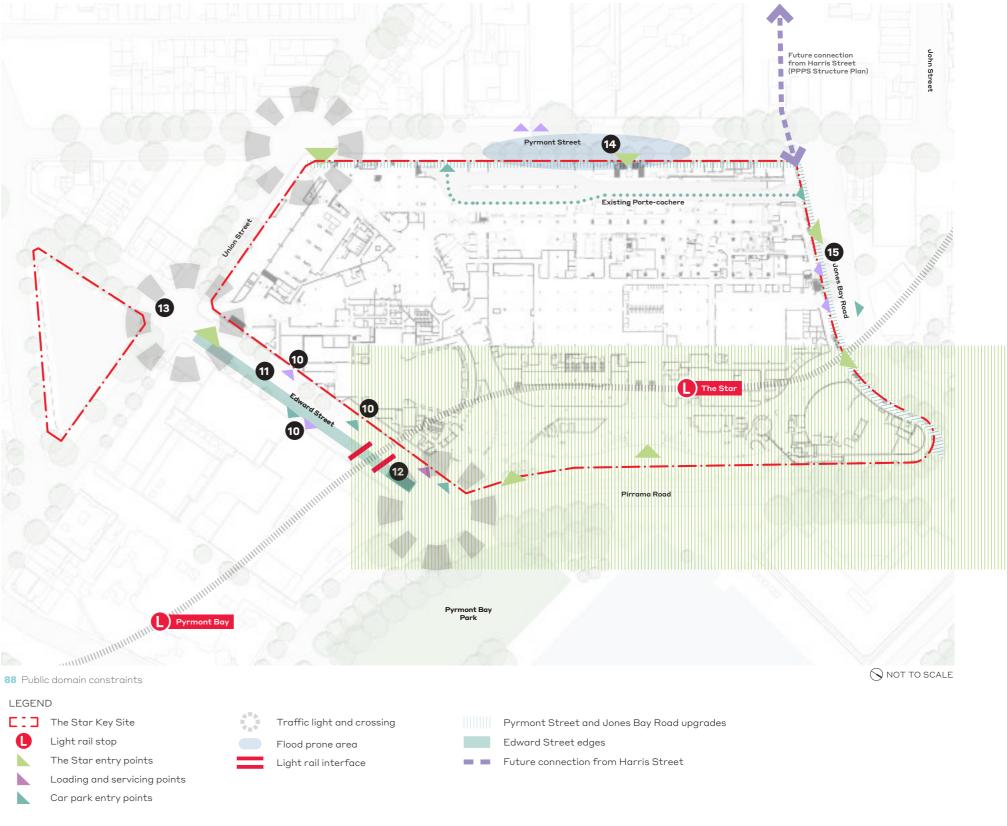
Current light rail interface does not enable a safe point of crossing to Pirrama Road.

Union Street and Edward Street crossing point currently has limited 13 capacity to support increased pedestrian numbers with the potential arrival of the Metro.

#### Pyrmont Street and Jones Bay Road

Sections of Pyrmont Street are flood prone. Mitigation measures to 14 be integrated into public domain improvements.

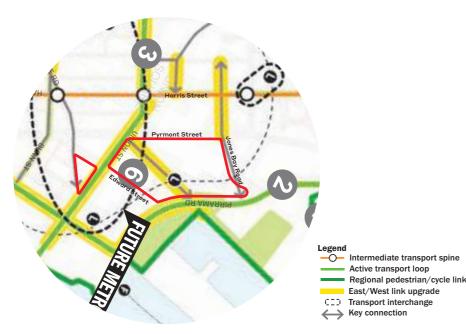
Multiple loading and car park entries in close proximity to each other 15 along the street increase risk of pedestrian vehicle conflicts.



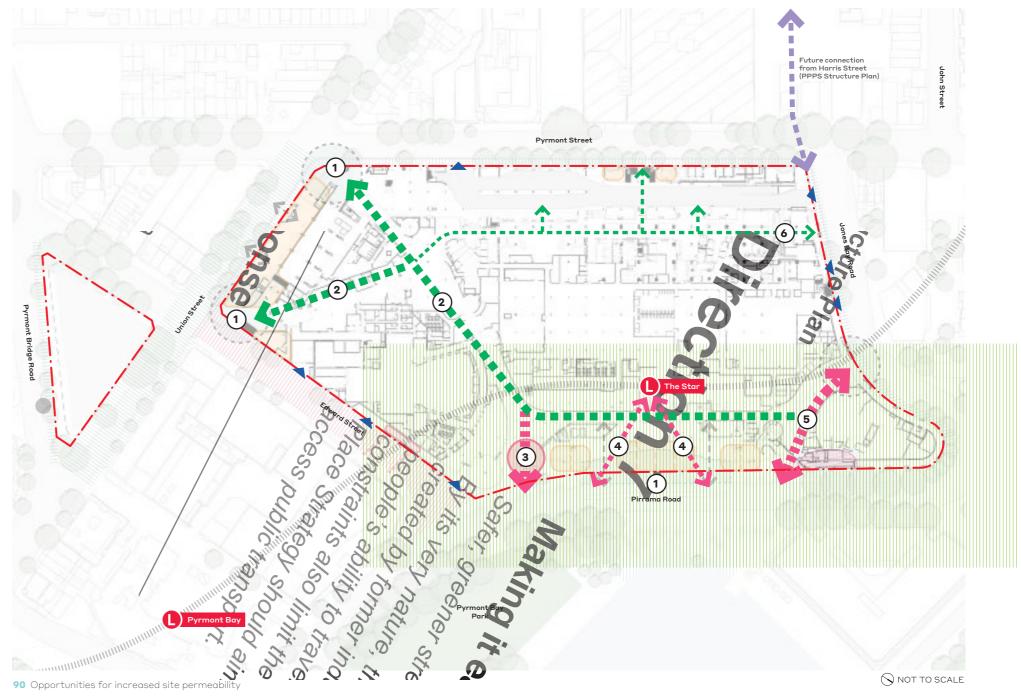
### 4.2 Site Permeability

The following opportunities to enhance and improve internal connections for the northern site seek to increase the site's permeability and improve walkability across the site to align with Direction 7 of the PPPS - Making it easier to move around.

- (1) Upgrades to existing wayfinding to public transport.
- Improved connectivity from key internal movement path down to 2 foreshore.
- (3) Revise existing entry stair to increase visibility.
- (4) Increased visibility to The Star light rail stop.
- Connection from Jones Bay Road to Pirrama Road to facilitate 5 improved movement to foreshore.
- Improve visibility and access to secondary entry adjacent to porte-(6) cochere.



**89** PPPS Structure Plan response to Direction 7 - Making it easier to move around Source: PPPS (Volume II - Strategic Framework)



90 Opportunities for increased site permeak

LEGEND

- The Star Key Site
- Light rail stop 0
- Vehicular access points
- Key internal movement path

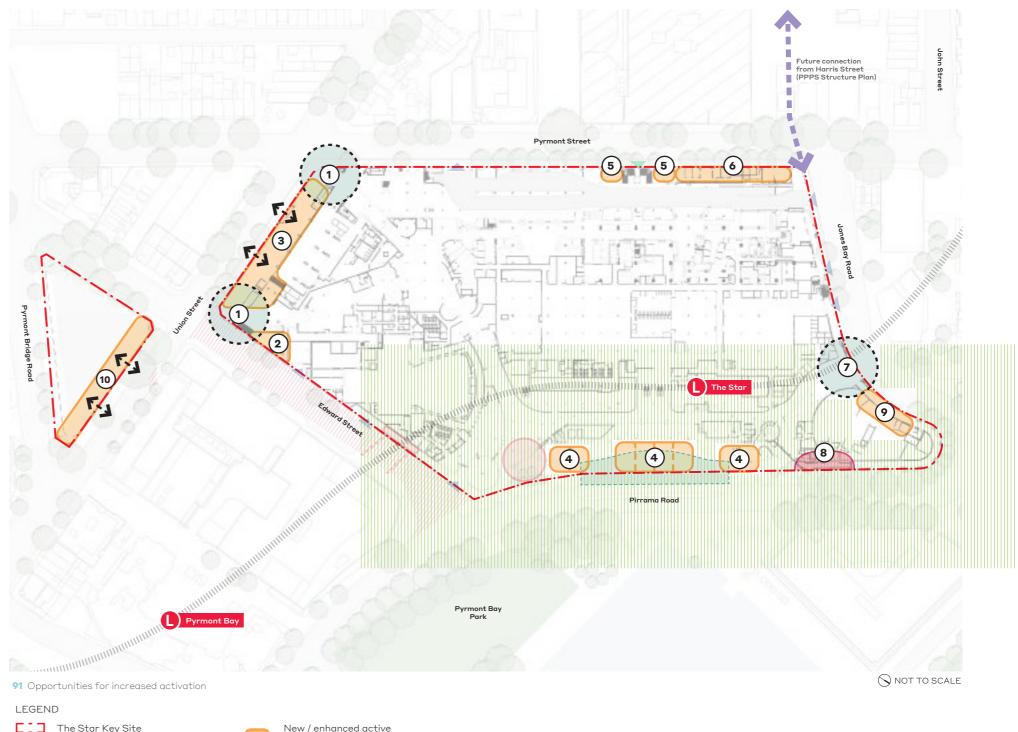
- Secondary internal movement path
- New connections (level change between key internal movement path to Pirrama Road)
- Future connection from Harris Street

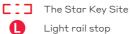
NOT TO SCALE

## 4.3 Activation

A number of opportunities to improve the streetscape amenity and uses around The Star Key Site through activation of the ground plane are identified below.

- Clearly defined corners to Union Street, Pyrmont Street and Pyrmont Bay Road, better legibility to the street. 1
- 2 Potential F&B venue at height over Edward Street.
- Enhanced activation along Union Street (potential for F&B venue to (3) open up more towards the street).
- (4) Reconfigure ground floor to smaller F&B venues along Pirrama Road.
- Suitable tenants in current vacant tenancies to provide activation (5) along Pyrmont Street.
- Suitable active uses in SELS building to enhance activation along 6 Pyrmont Street.
- Upgrade of existing through-site link on Jones Bay Road to accommodate better connection to Pirrama Road and internally through The Star.
- New porte-cochere located further north to minimise pedestrian 8 vehicular conflict, away from new low speed crossing to Pyrmont Bay Park.
- 9 Redevelopment of the North Tower podium and the new entry point will provide some opportunities for activation, associated with the hotel.
- Opportunity to consider active uses at ground floor and podium levels, (10) subject to building use and configuration at street level.





Improved / clear definition

of corner entries and better legibility to the street

Future connection from Harris Street

frontages and uses

-

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# PRECINCT VISION AND URBAN DESIGN PRINCIPLES



#### 5.1 Precinct Vision

The Star's vision is to create a World Class Integrated Resort within the Darling Island sub-precinct, offering the best variety of gaming, restaurants, bars, hospitality, tourism and entertainment experiences. The Star's Key Site Master Plan will provide much needed stimulus for growth in tourism investment and the creation of thousands of jobs, coinciding with the arrival of the new Metro set to connect Pyrmont to the innovation corridor to the west and Sydney CBD to the east.

As one of the largest employers in Pyrmont, The Star's continued growth and expansion will mean more economic and employment opportunities for Sydney and increase tourism revenue for New South Wales.

On the ground, the arrival of the Metro in Pyrmont will mean that the way people move around and access the peninsula will change. The anticipated reduction in the reliance on private vehicles to access destinations such as The Star will have a positive impact on how the public domain will be shaped to enable more pedestrian-focused places.

As the current home of Google in Sydney, there is opportunity for the Darling Island subprecinct to further its standing as a place for business and innovation. Its proximity to ICC is a significant factor in this ongoing evolution as a place of business and entertainment. Under the PPPS, there is a target to deliver 2,735 more jobs and dwellings to accommodate 600 more people by 2036. The Star will continue to focus on consolidating its position as a culture and entertainment destination while actively contributing to a more liveable and vibrant community.

#### An active role in Australia's premier and entertainment precinct

The emergence of Darling Harbour has coincided with the development of the ICC, Australia's largest entertainment, event and conference facility. The Star's vision is to enhance and further cultivate this by delivering world-class hotels and amenities, further boosting its reputation as a place of business and thought leadership.





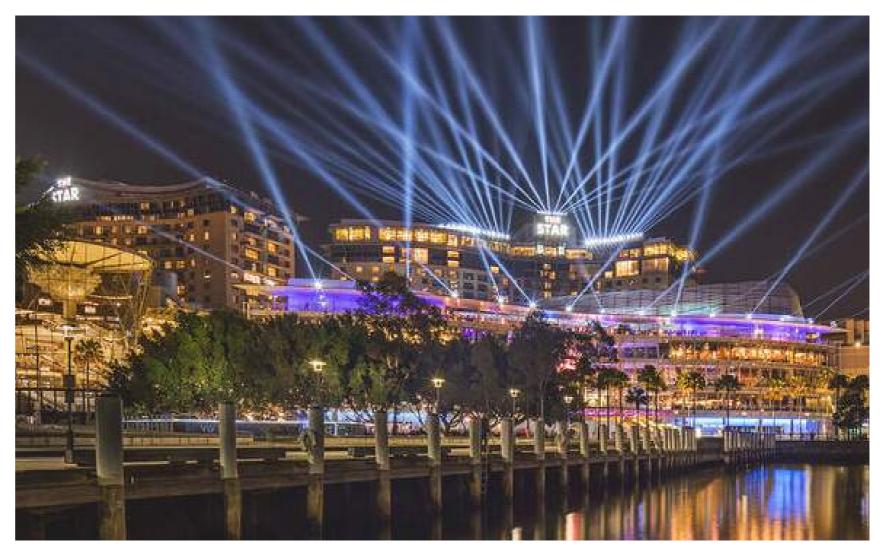
#### A greener neighbourhood, a place which is easy to navigate, but also easy to get into and out of using public transport

The common desire for people to move seamlessly between modes of public and active transport is a key priority. Preparing The Star and its front door as a place which is easily navigable, safe and prioritises walking, is not only in line with the PPPS but will provide for better liveability and improved access to our much loved foreshore.



# An integrated resort precinct at the heart of the world class foreshore

The future of the Darling Island subprecinct is about delivering entertainment and culture. At the heart of this will be a continually evolving resort precinct whose aim will be to attract international visitors to drive tourism in the long term.



#### An attraction not just for international tourists, but locals and visitors alike

The evolution of a world class foreshore has seen a single link to the harbour's various cultural and tourist attraction, which The Star has played an active role in delivering upon. These days, The Star isnt seen in isolation as one single place or destination, but one part of a broader collective of experiences which can be shared as part of the wider harbour experience.



# 5.2 Urban Design Principles -Public Domain and Ground Plane

Urban Design Principles around the public domain and ground plane have been developed to guide the development.

These principles are consistent with the strategies that are underlined in the development of both the envelope and subsequent ground plane around The Star Key Site.

# 1.

Union Street and the potential for Metro

The Star will work with TfNSW to prioritise Union Street as a critical pedestrian and active transport link to ensure sufficient capacity and amenity for visitors and residents without reducing existing traffic capacity and to ensure that the concept can accommodate Metro should it be delivered at the Union Street site.



2.

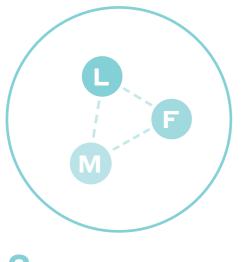
#### **Strategic Directions:**





**Prioritising Pirrama Road as a local** active transport corridor and key access point to the foreshore

Consistent with the objective of the PPPS, Pirrama Road will be prioritised as a key active transport corridor, with consideration being given over to providing a safer, slower environment for pedestrians and cyclists. The importance of these changes is also underlined by its importance as a key crossing point for many visitors to the foreshore walk.



# 3.

Improved access to public transport and interchange

Existing forms of public transport on the peninsula coupled with the potential arrival of the Metro in Pyrmont require better connections with one another to maximise their effectiveness. This includes consideration to providing a meaningful, safe and workable on-grade connection between transport modes via Edward Street, to enable access to buses and light rail on Pirrama Road and ferries at the wharf from Union Street.



4.

#### Streamlining visitor and service vehicle impacts around The Star

Due consideration to providing additional capacity for tourism and visitor related vehicles including coaches, mini buses, taxis and ride share within The Star's own site should be given to improve the amenity of existing streets. This also includes the relocation of existing vehicle accesses where feasible to areas with less foot traffic and include a reduction to on-street parking in anticipation of the potential arrival of the Metro.



5

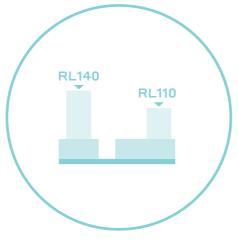
across the peninsula.

Prioritise activation in areas with greatest anticipated footfall and pedestrian movement

Union Street and Pirrama Road present the best opportunities to develop ground floor use strategies that are consistent with delivering increased activation to improve vitality and intensity of use along these edges. This approach will also reinforce their role as key active transport and movement corridors around and

# 5.3 Urban Design Principles -Built Form

Similarly Urban Design Principles specific to built form have been developed to guide the development, which seek to fulfil the strategic objectives defined under the PPPS.



# 1.

Working within clear height parameters established under the PPPS

As part of Darling Island sub-precinct's development as a cultural and entertainment destination focused on tourism, development on the northern site will be a six star hotel (RL110) while development on the southern site will be a mixed use tower (RL140) to balance the distribution of height and density between the two sections of the site.



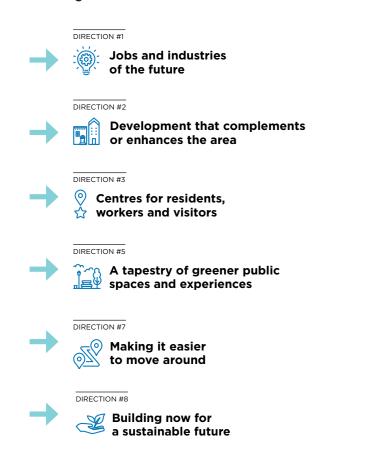
# 2.

#### Built form envelope design to mitigate wind effects and ensure built form consistency of the streetscape

To ensure the amenity of ground planes generally, the design of both the integrated tower and podium components will be developed to ensure they address the effects of wind, particularly downdrafts from towers using setbacks.

Podium datums will also be established to ensure they are compatible with prevailing wind conditions and work with the existing streetscape. The datum established particularly for the South Tower should mediate the existing built form scales on Pyrmont Bridge Road and Edward Street while providing a transition in scale to the finer grain of Union Square and the Pyrmont Village sub-precinct.

#### **Strategic Directions:**

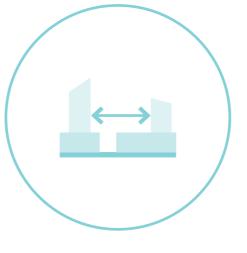




# 3.

Minimising overshadowing to open space and adjacent residential neighbourhoods

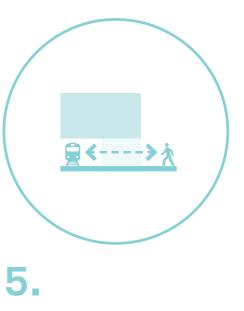
This principle is consistent with the PPPS considerations for the site, which includes not breaching the solar access planes to open spaces to ensure that the solar amenity of actively used public open spaces in the locality is preserved. The amenity of adjacent residential neighbourhoods will also be quantified as part of this process.



# 4.

Minimising bulk and scale, visual prominence

This will include consideration to building separation between proposed towers and existing building interfaces. The refinement of the tower envelopes' form will also be undertaken to minimise impacts from the water and surrounding public domain.



#### Working within the existing built form to develop a relatable interface at street level

Where possible The Star will look to identify opportunities to provide more open and active interfaces to the street enabling greater transparency into The Star, including existing entries on Union Street and Pirrama Road. Clear lines of sight consistent with movements to the existing light rail stop will be supported by improved wayfinding. Uses along Pirrama Road and Union Street will also be developed to complement their future roles as important corridors for pedestrian and active transport movement with the potential arrival of the Metro in Pyrmont. This page has been intentionally left blank.



# 6.0

# KEY MOVES

# 6.0 Key Moves

Informed by the our site analysis of the Darling Island sub-precinct and the Urban Design Principles around public domain, ground plane and built form established discussed in the previous sections of this report, the following Key Moves have been developed to guide the Master Plan and Landscape Design for The Star Key Site. They are:

- Key Move 1 Ground Plane and Movement
- Key Move 2 Site Permeability
- Key Move 3 Street Activation and Streetscape
- Key Move 4 Built Form

These key moves seek to align the Key Site Master Plan with the PPPS. However in the absence of further input regarding the Darling Island subprecinct design currently being completed by the DPIE, further development of these concepts may be required.

Key Move 1 - Ground Plane and 6.1 Movement





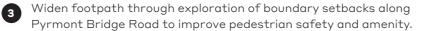
#### Priority Streets (Union Street + Pirrama Road)

(4)

Introduce traffic calming measures on Union Street to prioritise 1 pedestrians and active transport. Consider the removal of a parking lane and widening the northern footpath.

Transition Pirrama Road to a shared way and improve connection 2 between The Star, existing light rail stop and the foreshore without impairing current vehicular movement. Relocate existing porte-cochere and integrate with development on the northern site.

#### Secondary Streets (Pyrmont Bridge Road + Pyrmont Street)



Consider public domain improvements to improve pedestrian amenity along Pyrmont Street.

#### Widened Apertures + Improved Site Permeability (Key Corners)

foreshore walk.

#### Connections to the Water (Edward Street + Jones Bay Road)

- 2
- (3)

### 6.2 Key Move 2 - Site Permeability

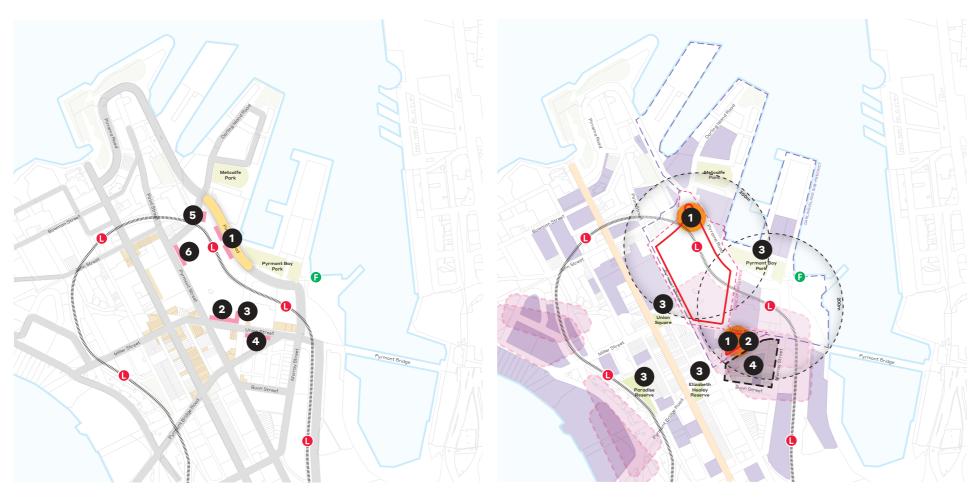


Provide clear and legible points of entry into The Star, supported by clearer wayfinding to improve navigation internally and to public transport and precinct-wide destinations such as open spaces and the

Investigate on-grade connection along Edward Street or dedicated light rail station by removing existing barriers created by the light rail crossing to connect the southern site and the potential Metro station to the foreshore, subject to discussion with TfNSW.

Improve pedestrian amenity along Jones Bay Road in anticipation of future connection from Harris Street, integrated with the new throughsite connection towards Pirrama Road and the foreshore.

# 6.3 Key Move 3 - Street Activation and 6.4 Key Move 4 - Built Form Streetscape



#### **Active Frontages**

5

- 1 Reconfigure ground floor to smaller F&B venues along new Pirrama Road shared way that allows for increased visibility towards the light rail stop.
- 2 Consider opening up more towards Union Street to provide enhanced street activation.
- 3 Investigate potential for an F&B venue at height over Edward Street.
- Consider active uses at ground floor and podium levels along Union Street, subject to building use and street level configuration.
  - Explore opportunities for street level activation associated with the new hotel, integrated with the new through-site link from Jones Bay Road.
- Introduce appropriate uses in the SELS building to enhance street activation along Pyrmont Street.

#### Height + Density Near Amenity

1 Locate development within areas with the greatest transport and open space amenity within the precinct.

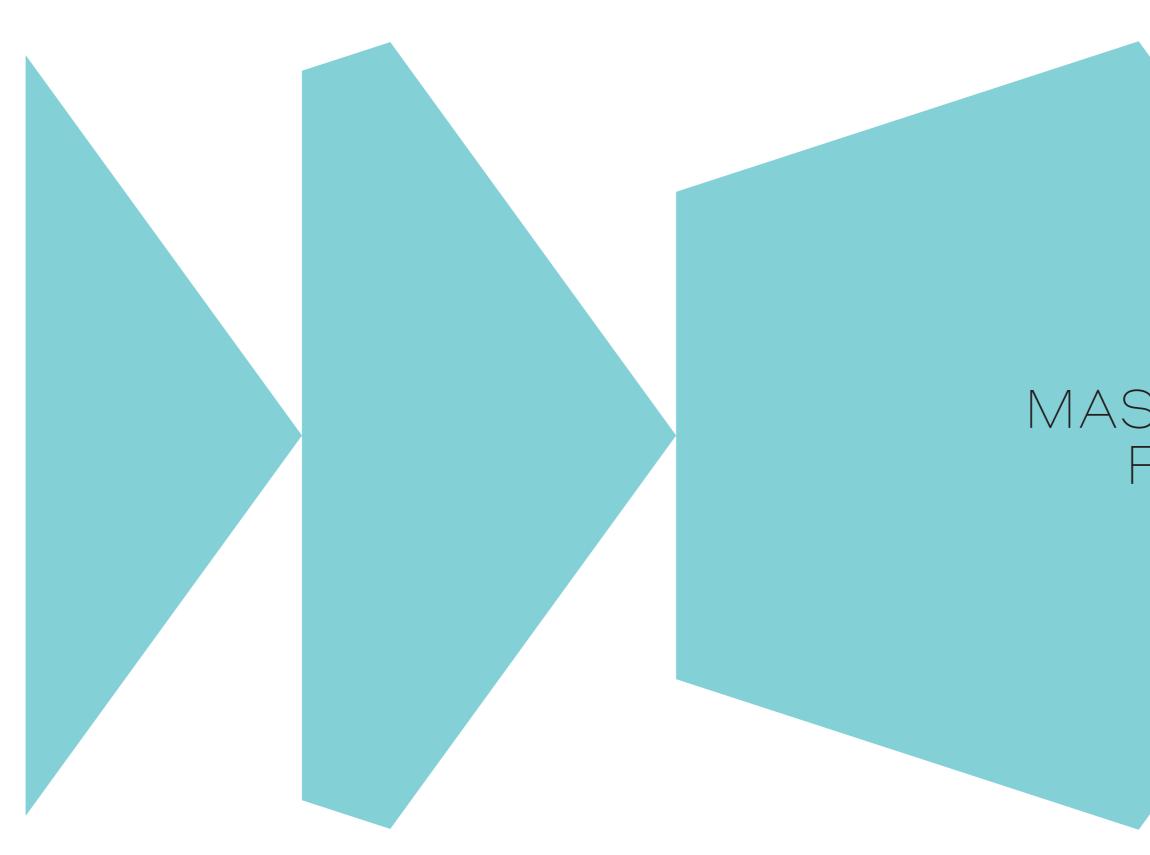
#### Podium Height + Setback

2 Ensure that podium height on the southern site respects and responds to the existing urban fabric. Provide suitable setbacks above podium level to mitigate wind impacts from the South Tower, consistent with wind advice.

#### Solar Access

- 3 Ensure that the proposed built form protects the solar amenity of open spaces and key streets identified under the PPPS.
- Develop and test assumptions around the potential future development of the residential cluster to the south to better understand and mitigate overshadowing impacts from the South Tower.

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# 7.0

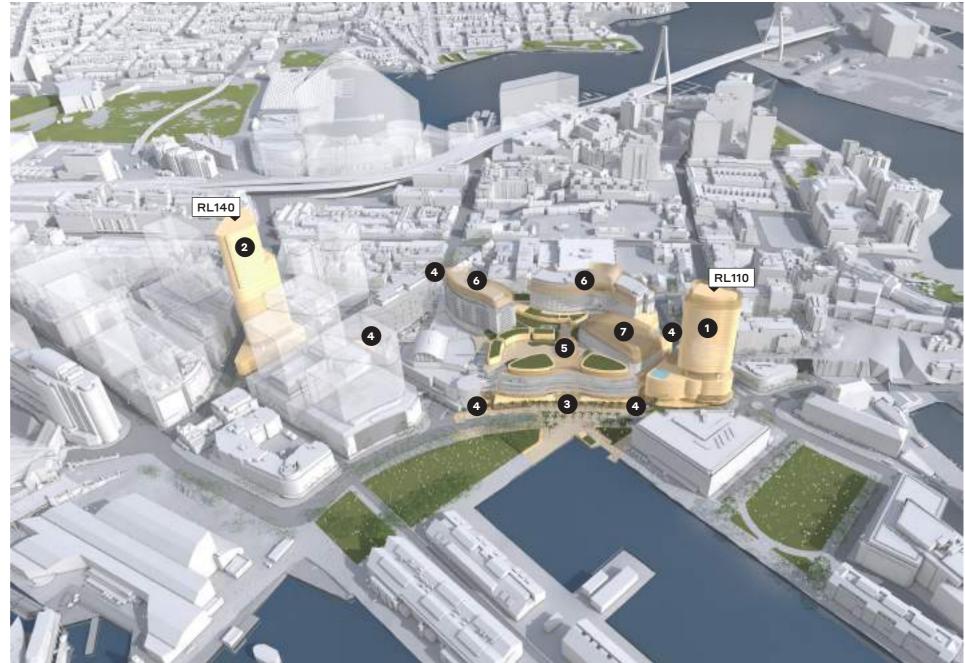
# MASTER PLAN PROPOSAL

# 7.0 Master Plan Proposal

## 7.1 Key Site Master Plan

Aligning with the principles and key moves discussed in this report, the Master Plan for The Star Key Site proposes the following: North Tower - A new 27 storey six star hotel tower (RL110) setback from 1 a 6 storey podium. South Tower - A new 37 storey mixed use tower (RL140) setback from a 2 5 storey podium. **Pirrama Road interface** - New smaller F&B venues on the ground level 3 with increased visibility towards the existing light rail stop. **Site permeability** - Increased site permeability and visibility towards 4 through-site connections at key corners of the northern site, including the re-configuration of stairs on Pirrama Road. Level 5 rooftop - Additional built form to facilitate indoor/outdoor dining 5 and events on rooftop of the main Star site. Facade upgrades - Facade upgrades to The Star Grand Hotel and 6 Residences. **MUEF modifications** - Refurbishment of the existing function space 7 associated with the MUEF

Please refer to the Key Site Master Plan Report by fjmt studio for further information.



**92** Aerial view of the proposed Master Plan for The Star Key Site Source: fjmt studio

2200827 The Star Key Site - Urban Design Report

# 7.0 Master Plan Proposal

### 7.2 Public Domain Master Plan

A Public Domain Master Plan has been prepared in support of the Key Site Master Plan. Proposed public domain improvements that seek to deliver a better public realm and the objectives of the PPPS include the following:

**Pyrmont Bay Plaza** - A revitalised foreshore plaza that expands beyond Pirrama Road to The Star's entry. The plaza includes a small water play zone adjacent to Pyrmont Bay Park, new green space and dense palm planting to provide additional shade while maintaining views to the foreshore.

**Pyrmont Bay Park** - Reconfigure edge to Pyrmont Bay Park to create a tiered edge to the Foreshore walk for respite and gathering.

Pirrama Road - A complete street including a table-top shared area, new tree planting, active frontages, outdoor dining and repositioned drop off to expand the plaza and better connect the light rail station with the Foreshore Walk.

**Edward Street** - Reconfigure Edward Street through kerb re-alignments that expand the public realm for additional landscape, seating and to enhance the connection from the potential Metro station to the foreshore.

**5** Union Street - Reconfigure Union Street through kerb re-alignments that expand the public realm for additional landscape, seating, outdoor dining and to enhance the pedestrian experience a from Pyrmont Bridge to Union Square.

**6 Pyrmont Street** - Reconfigure Pyrmont Street through kerb realignments that expand the public realm for additional landscape, seating, to enhance the pedestrian experience and improve connections through the site.

**Jones Bay Road** - Reconfigure Jones Bay Road through kerb realignments that expand the public realm for additional landscape to enhance the pedestrian experience and improve connections through the site.

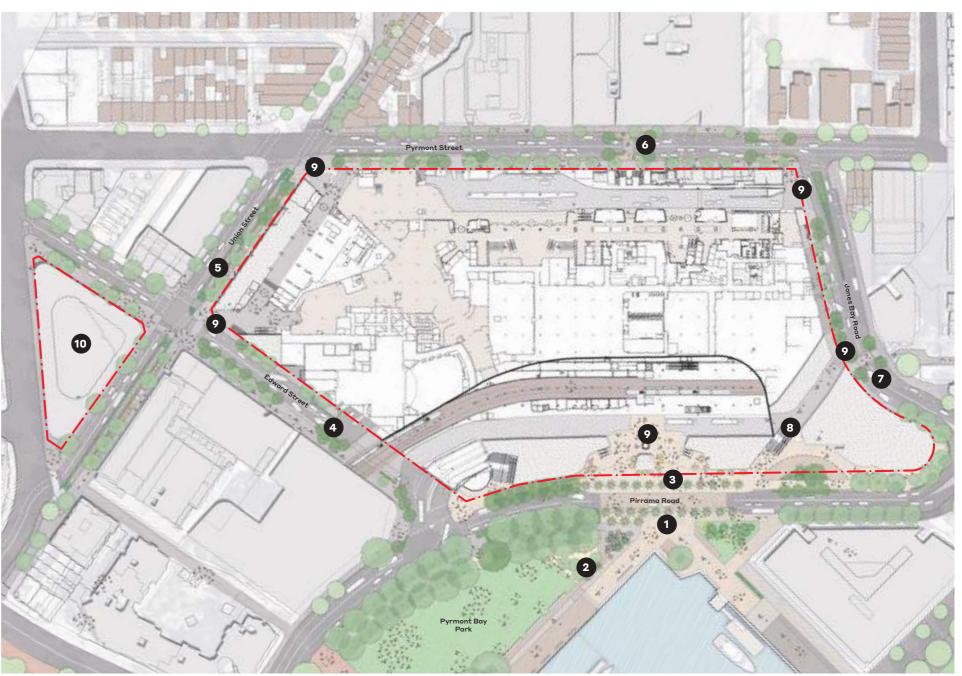
8 New through-site link - New activated link connecting people from Jones Bay Road to Pirrama Road.

**Enhanced connection and arrival gateway** - Public realm intervention to promote legibility and permeability across the site.

Promote legibility and permeability across the site. **Potential Metro** - Provision for public realm interventions to surrounding

streetscape to allow for pedestrian flow and strategic connections.

Please refer to the Public Domain Master Plan Report by Urbis for further information.



**93** Illustrative Public Domain Master Plan Source: Urbis

7

10

NOT TO SCALE

# E T H O S U R B A N