



Premise

Planning Proposal

249-271 RAILWAY TERRACE, SCHOFIELDS NSW 2762




Report No: 323135/PP01_Final

5 September 2023

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Document reference: 323135_PP01_Final

DOCUMENT AUTHORISATION					
Revision	Revision Date	Proposal Details			
Draft	11/08/23	Draft for client review			
DraftRev1	14/08/23	Draft for DP&E review			
Final	05/09/23	Final for issue to DP&E			
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INTRODUCTION & EXECUTIVE SUMMARY

This Planning Proposal (PP) is submitted to the NSW Department of Planning and Environment ('DP&E') in support of an amendment to Appendix 7 (Alex Avenue and Riverstone Precinct Plan) of State Environmental Planning Policy (Precincts – Central River City) 2021 ('Central River City SEPP'), on behalf of Provincial Investments (NSW) Pty Ltd. The land to which this PP relates is 249, 259 & 271 Railway Terrace, Schofields ('the site').

The purpose of this PP is to facilitate an increase in residential density and the delivery of a mix of social, affordable and build-to-rent housing on the site, acknowledging the strategic location of the site within the North West Growth Centre and its proximity to key infrastructure including Schofields Train Station and the Alex Avenue Town Centre.

The proposed amendments to the land use controls under the Central River City SEPP, to facilitate the residential density uplift, which will include a mix of social, affordable and build-to-rent housing, comprise:

- An increase to the maximum building height applicable to the site from 16 metres to 32 metres; and
- An increase to the maximum floor space ratio (FSR) from 1.75:1 to 3.5:1.

The Proposal has been prepared in accordance with Sections 3.33 and 9.1 of the *Environmental Planning and Assessment Act 1979* (EP&A Act) and the NSW Department of Planning, Industry and Environment's Local Environmental Plan Making Guideline (August 2023). The PP has also considered the key/site specific matters to be addressed as detailed in email correspondence received from DP&E dated 1 August 2023.

The Proposal is structured as follows:

- Section 1 – Site Analysis
- Section 2 – Planning Context
- Section 3 - Objectives and Intended Outcomes
- Section 4 – Explanation of Provisions
- Section 5 – Justification
- Section 6 - Maps
- Section 7 – Community Consultation
- Section 8 – Project Timeline
- Section 9 – Conclusion

This Report should be read in conjunction with the following specialist consultant reports and plans:

Table 1 Consultant Documentation

Discipline	Prepared By/Document Reference
Survey Plan	ChadwickCheng Consulting Surveyors, Ref: 41188/D-MGA-2d-LOT, dated 11/07/22.
Visual Analysis & Massing	Nordon Jago Architects, Ref: PR.01, dated August/September 2023.
PSI / DSI	Construction Sciences, Ref: 10791EV.P.323-R01, dated 8 November 2022.
RAP	Construction Sciences, Ref: 10791EV.P.323-R02, dated 9 November 2022.
Flooding & Stormwater	Infrastructure and Services Report, Lucas Consulting Engineers Pty Limited, Rev: A, dated: 4/08/23.
Aboriginal Heritage	Baker Archaeology, dated February 2023.

Discipline	Prepared By/Document Reference
Utilities	Infrastructure and Services Report, Lucas Consulting Engineers Pty Limited, Rev: A, dated: 4/08/23.
Traffic & Parking	The Traffic Planner, Rev: 1.2, dated 29/08/23.
Social & Community Infrastructure	Hill PDA, Ref: P24008, Version: 1.2, dated 30 August 2023.
Urban Design	Premise, Ref: 323135_UDR_001C, dated 5 September 2023.
Affordable Housing Statement	Dated 5 September 2023.

The PP is considered worthy of support for the following reasons:

- The proposed amendments to the maximum building height and maximum FSR would support the precinct vision by facilitating additional residential housing to meet the needs of the community in an accessible location, noting the proximity of the site to the Schofields Train Station and the Alex Avenue Town Centre. The PP will also contribute to the provision of affordable housing to meet the needs of the community.
- The PP maintains consistency with the applicable State, Regional and Local strategic planning framework.
- The PP is consistent with the relevant Ministerial Directions.
- The existing and future planned infrastructure and services have the capability to effectively service demand from development under this PP; and
- Overall, there are no anticipated adverse social or economic impacts which would arise from the proposal.

1. THE SITE & ITS CONTEXT

1.1 SITE DESCRIPTION

The land the subject of this PP ('the site') comprises three parcels of land:

- 249 Railway Terrace, Schofields Lot 4 DP 1268701
- 257 Railway Terrace, Schofields Lot 3 DP 1268701
- 271 Railway Terrace, Schofields Lot 5 DP 26987

The site is situated within the North West Growth Centre, within the Alex Avenue Precinct. It is irregular in shape and has a total area of 6.308 hectares. The site maintains a frontage to Railway Terrace (58.31 metres and 90.19 metres), a frontage to Pelican Road (246 metres) and a frontage to Bingham Street (approximately 131 metres). Existing improvements on the site include a dwelling and associated metal sheds in the north-western portion of Lot 4 and a dwelling and associated outbuilding in the south-western portion of Lot 5.



Figure 1: The Site (Source: Nearmap, 2023)

The central portion of the site bounds Lot 1DP 1268701 which was formerly part of the now Lot 3 DP 1268701. This portion of land has recently been dedicated to Council for drainage and open space purposes, noting that the lot in its entirety is identified in Contributions Plan No. 20 – Riverstone & Alex Avenue Precincts (contribution plan) as a 'local park including landscaping and fencing.' The contribution plan also identifies stormwater works on the site comprising a detention basin, bioretention works within the basin and a gross pollutant trap at the inlet to the basin. Council has prepared plans for the basin works, refer to **Attachment A**. It is understood that works are due to commence towards the end of this year and be completed around March 2024.

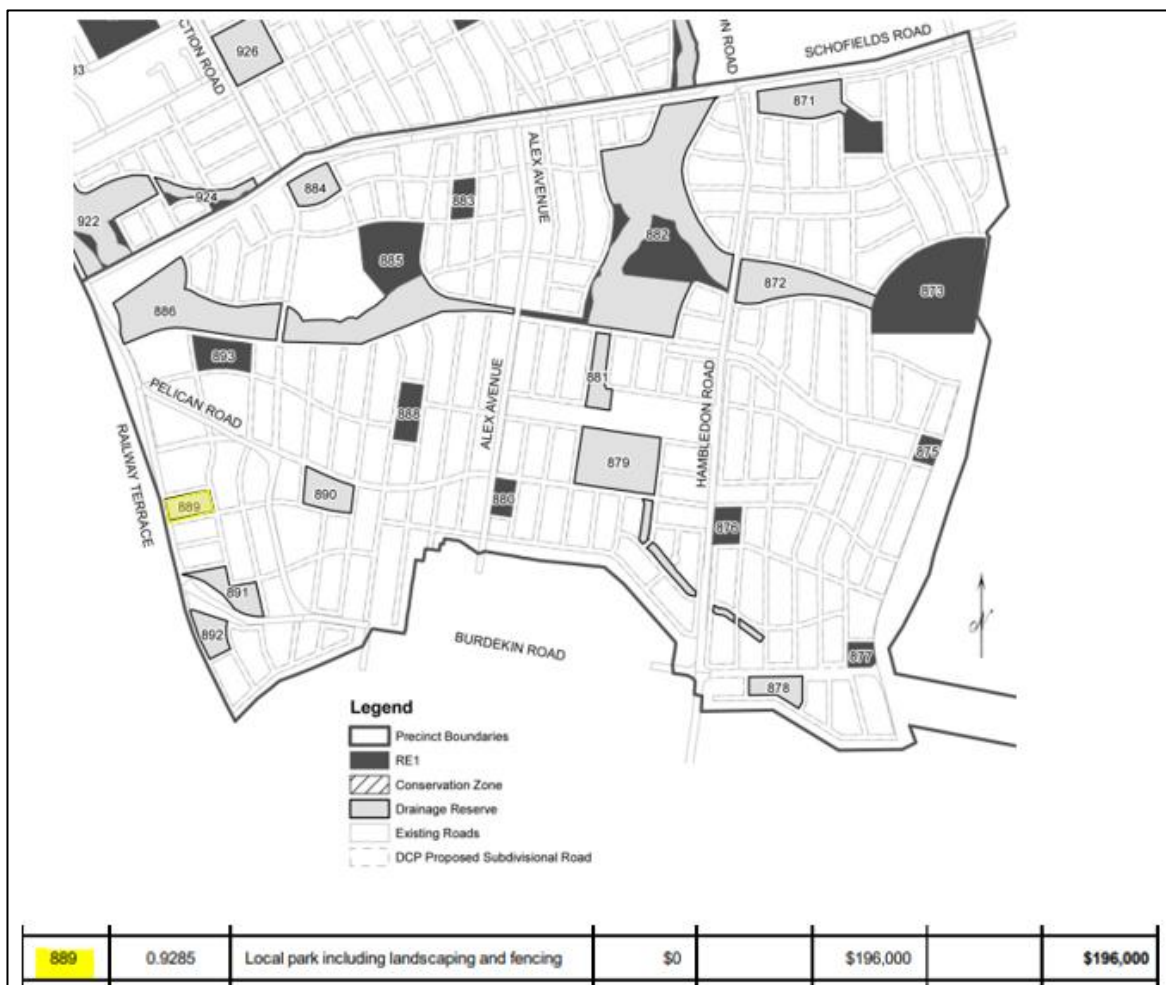


Figure 2: Contribution Plan extract (Source: BCC, July 2020)

Immediately to the north of the site is an existing rural residential property, with the Alex Avenue Town Centre beyond. To the east of the site, on the eastern side of Pelican Road is existing vacant land with a series of 4 to 5 storey residential flat buildings located to the north-east. Immediately to the south of the site, on the southern side of Bingham Street is a series of 5 storey residential flat building developments. The railway line is located on the western side of Railway Terrace, with residential development beyond.

The site in its entirety is subject to an Order to confer biodiversity certification on the State Environmental Planning Policy (Sydney Region Growth Centres) 2006 (the now Central River City SEPP), refer to the North West Growth Centre Biodiversity Certification Map at **Attachment B**.

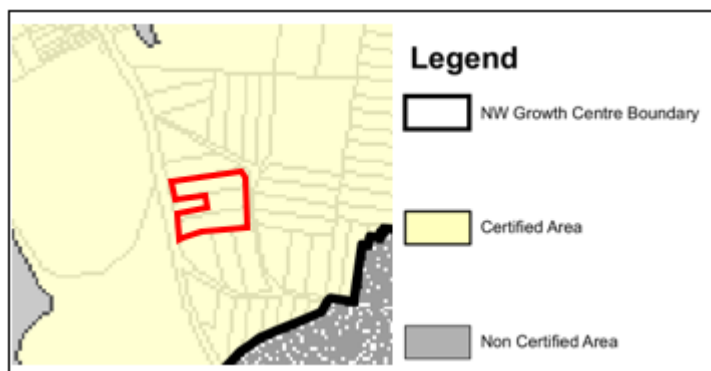


Figure 3: Biodiversity Certification Map extract (Source: NSW Government, November 2007)

1.2 LOCAL & REGIONAL CONTEXT

The site is situated along the western fringe of the Alex Avenue Precinct of the North West Growth Centre which is located in the Blacktown Local Government Area ('LGA'). The Alex Avenue Precinct was rezoned in May 2010, along with the adjoining Riverstone Precinct immediately to the north. The Alex Avenue Precinct is also bound by the established The Ponds residential housing estate to the east, the established residential suburb of Quakers Hill to the south and the railway line and Schofields Precinct to the west.



Figure 4: Alex Avenue Precinct & the site denoted by green outline (Source: Nearmap, 2023)

The site is located within 200 metres of the Schofields Train Station (Blacktown/Richmond Rail line), to the north. In addition to the recent duplication of the tracks at Schofields station a 700 space commuter carpark has also been delivered. It is understood that there are future plans for Schofield station to be further upgraded to provide 4 tracks, which will reinforce the station as a key public transport hub. The existing North-West Metro is also proposed to be extended to Schofields and St Marys, through to the Western Sydney Aerotropolis, which will connect the site to additional employment opportunities.

The site is also situated within 80 metres of the Alex Avenue Town Centre (zoned B2 Local Centre, B4 Mixed Use & RE1 Public Recreation), to the north which includes the Schofields Village Shopping Centre. Schofields Village includes a Coles supermarket, McDonalds restaurant, various retail shops and restaurants and a child care centre. The Town Centre will also include a town park for public recreation.

The site is located within 30 minutes of key services including Schofields Train Station, the Alex Avenue Town Centre, Western Sydney Parklands, Western Sydney University Nirimba, Western Sydney Institute of TAFE, schools and early learning centres, including Galungara Public School, Schofields Public School and the Marsden Park Industrial Area.

2. LOCAL PLANNING CONTEXT

2.1 STATE ENVIRONMENTAL PLANNING POLICY (PRECINCTS – CENTRAL RIVER CITY) 2021

The Site is subject to the provisions of State Environmental Planning Policy (Precincts – Central River City) 2021 ('Central River City SEPP'), specifically Appendix 7 – Alex Avenue and Riverstone Precinct Plan, being the primary Environmental Planning Instrument.

Zoning and Permissibility

The Site is zoned R3 Medium Density Residential pursuant to Appendix 7 of the Central River City SEPP. The objectives of the zone are:

- *To provide for the housing needs of the community within a medium density residential environment.*
- *To provide a variety of housing types within a medium density residential environment.*
- *To enable other land uses that provide facilities or services to meet the day to day needs of residents.*
- *To support the well being of the community, by enabling educational, recreational, community, and other activities where compatible with the amenity of a medium density residential environment.*

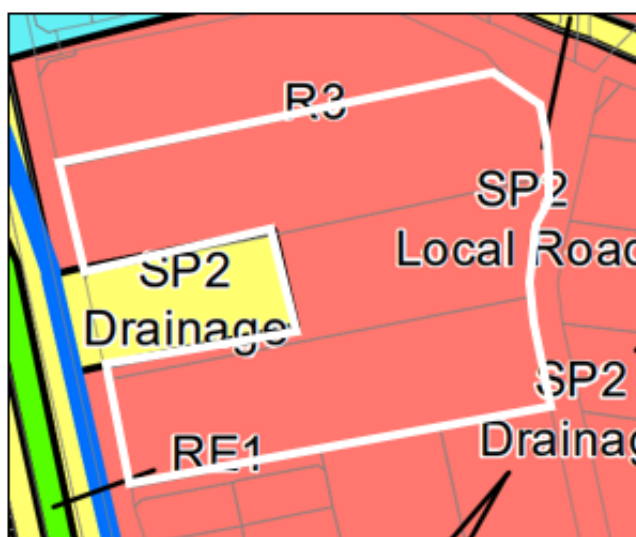


Figure 5: Extract of North West Growth Centre Land Zoning Map -Sheet LZN_005

The PP will facilitate additional housing to meet the needs of the community, including an affordable housing component, in an accessible location that is readily serviced by public transport infrastructure and social infrastructure.

The principal development standards contained in Appendix 7 of the Central River City SEPP, that are applicable to the site, are summarised as follows:

Clause 4.1AB Minimum lot sizes for residential development in Zone R2 Low Density Residential and Zone R3 Medium Density Residential

The minimum lot size for a residential flat building on Lots 3 & 4 DP 1268701 is 1,000m² and the minimum lot size for a residential flat building on Lot 5 DP 26987 is 2,000m².

Clause 4.1B Residential Density

A minimum residential density of 45 dwellings per hectare is applicable to Lots 3 & 4 DP 1268701 and a minimum residential density of 25 dwellings per hectare is applicable to Lot 5 DP 26987.

The development generated by the PP would exceed these minimum residential density provisions.

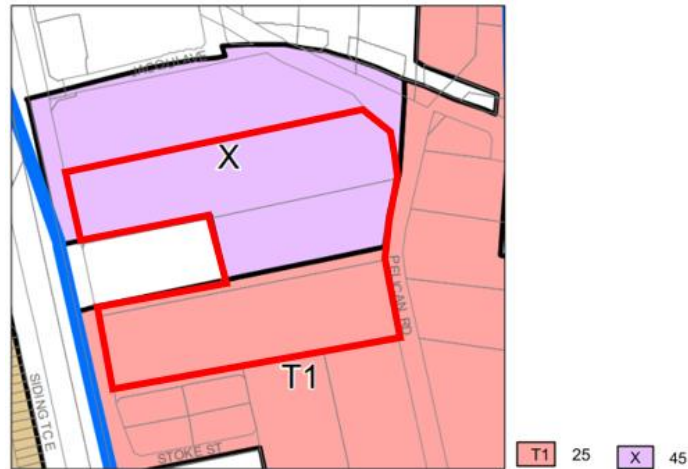


Figure 6: Extract of North West Growth Centre Residential Density Map - Sheet RDN_005

Clause 4.3 Height of buildings

The site is subject to a maximum building height of 16 metres.

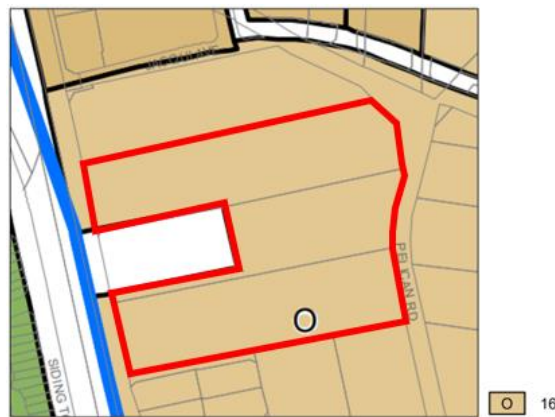


Figure 7: Extract of North West Growth Centre Height of Buildings Map - Sheet HOB_005

Clause 4.4 Floor Space Ratio

The site is subject to a maximum floor space ratio of 1.75:1.

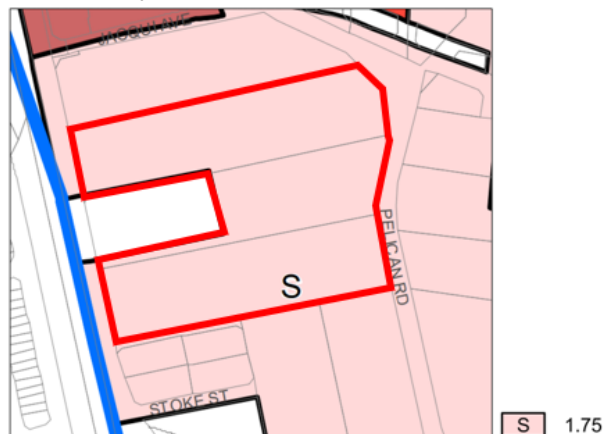


Figure 8: Extract of North West Growth Centre Floor Space Ratio Map - Sheet FSR_005

2.3 BLACKTOWN CITY COUNCIL GROWTH CENTRE PRECINCTS DEVELOPMENT CONTROL PLAN 2010

The Blacktown City Council Growth Centre Precincts Development Control Plan 2010 (Growth Centre DCP) is applicable to the site.

Schedule 1 (Alex Avenue Precinct) of the Growth Centre DCP contains controls for development within the Alex Avenue Precinct, which includes the site. Schedule 1 provides the following vision for the Precinct:

The vision for the Alex Avenue Precinct is that a range of housing types will develop to meet the needs of a diverse community, supported by local services, infrastructure, facilities and employment, in an environmentally sustainable manner.

The local centre will be the main focus of activity and daily life for the Precinct, providing for community interaction and delivering services and facilities to meet the needs of all residents.

The Precinct will be an integral part of the Blacktown local government area and the North West Growth Centre. It will be linked to surrounding suburbs and to major regional destinations such as Rouse Hill Regional Centre and Blacktown City Centre.

The proposed amendments to the maximum building height and maximum FSR would support the precinct vision by facilitating additional residential housing to meet the needs of the community in an accessible location, noting the proximity of the site to the Schofields Train Station and the Alex Avenue Town Centre. The PP will also contribute to the provision of affordable housing to meet the needs of the community.

At the time of establishing the residential densities for the Alex Avenue Precinct, the Department acknowledged that the minimum dwelling densities were established to provide flexibility to developers to respond to market demand.

This PP is seeking to facilitate the delivery of higher density residential development, consistent with the Alex Avenue Indicative Layout Plan and Residential Structure Plan, which identify the site for 'Medium to High Density Residential/Mixed Use' development.

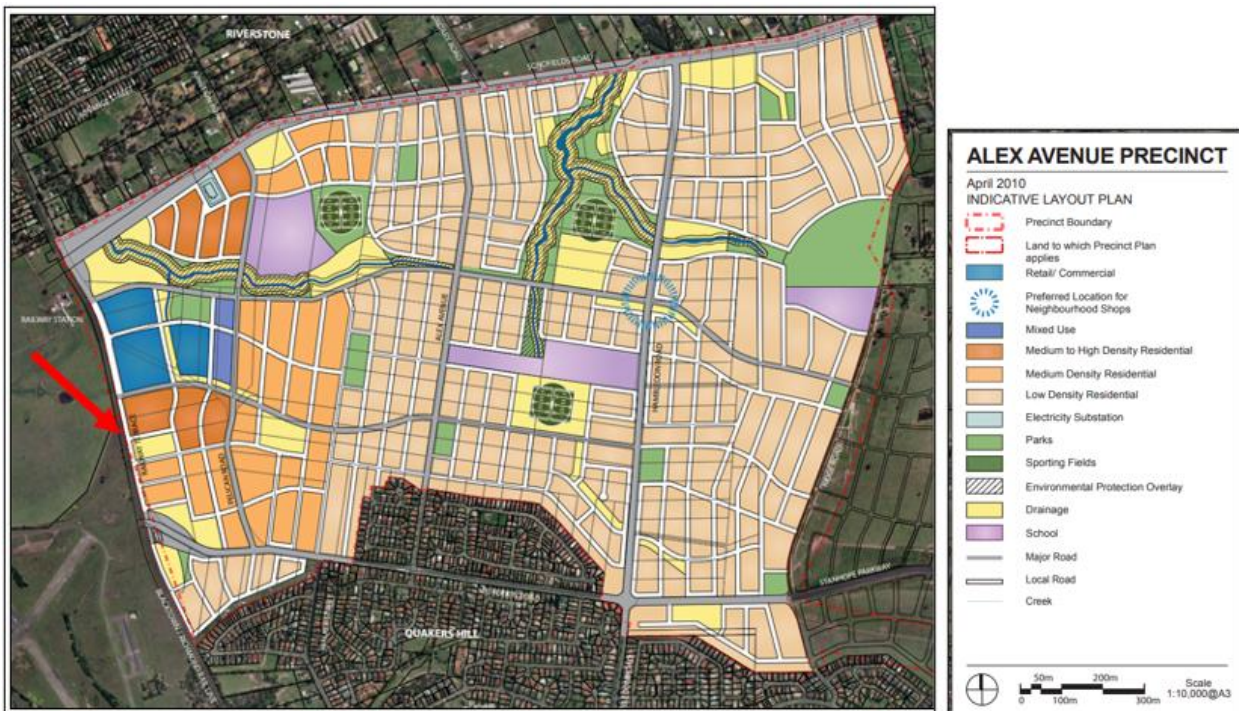


Figure 9: Extract of Alex Avenue ILP Map (Source: NSW DPIE, 2010)

3. OBJECTIVES AND INTENDED OUTCOMES

The key objective of the PP is to amend Appendix 7 of the Central River City SEPP to facilitate an increase in residential density and the delivery of a significant proportion of affordable housing on the site, acknowledging the strategic location of the site within the North West Growth Centre and its proximity to key infrastructure including Schofields Train Station and the Alex Avenue Town Centre.

The PP recognises that the context of the Alex Avenue Precinct has changed since the 2010 studies to inform the rezoning of the precinct, notably, the announcement of the planned extension of the North-West Metro to Schofields and St Marys through to the Western Sydney Aerotropolis. The proposed amendments to the maximum building height and FSR recognise the opportunity to provide an increased residential density that aligns with the strategic location of the site and its proximity to public transport, the town centre, open space and employment and education facilities.

Of the anticipated dwelling yield generated by the proposed increase to the maximum building height and FSR; in the order of 1,751 dwellings, 33 percent of this yield (578 dwellings) will comprise a mix of build-to-rent housing, social and affordable housing to respond to the housing needs of the community.

4. EXPLANATION OF PROVISIONS

4.1 OVERVIEW

The amendments sought to the Central River City SEPP relate to an increase to the maximum building height and maximum FSR applicable to the site.

The proposal does not seek to amend any other provisions of the Central River City SEPP.

4.2 AMENDMENT TO THE MAXIMUM BUILDING HEIGHT

The objectives of this PP would be achieved through amending the current 16 metre maximum building height applicable to the site to permit a maximum building height of 32 metres.

4.3 AMENDMENT TO THE MAXIMUM FLOOR SPACE RATIO

In amending the maximum building height from 16 metres to 32 metres, a proportional increase to the current 1.75:1 FSR is required. It is proposed to amend the current FSR to permit a maximum FSR of 3.5:1.

The amended FSR will ensure that the site is capable of achieving a dwelling yield in the order of 1,751 dwellings; 33 percent of which would comprise a mix of build-to-rent, social and affordable housing.

5. JUSTIFICATION

5.1 NEED FOR THE PLANNING PROPOSAL

IS THE PLANNING PROPOSAL A RESULT OF ANY STRATEGIC STUDY OR REPORT?

The Proposal responds to the framework established by the Blacktown Local Strategic Planning Statement 2020 and the Blacktown Housing Strategy 2020, which both aim to promote diverse and affordable housing for the current and future community, supported by essential infrastructure in the right locations at the right time.

Overall, the intended outcomes and objectives are consistent with State, Regional and Local strategic planning frameworks, which are outlined at Section 5.2 below.

IS THE PLANNING PROPOSAL THE BEST MEANS OF ACHIEVING THE OBJECTIVES OR INTENDED OUTCOMES OR IS THERE A BETTER WAY?

It is considered the PP is the best means of achieving the objectives / intended outcomes. Under the current Central River City SEPP, the maximum building height and FSR provisions do not reflect the strategic location of the site, noting its proximity to public transport, the town centre, employment opportunities and regional open space.

The proposed amendments are the most effective means of delivering a range of housing types in an accessible location.

5.2 RELATIONSHIP TO STRATEGIC PLANNING FRAMEWORK

IS THE PLANNING PROPOSAL CONSISTENT WITH THE OBJECTIVES AND ACTIONS CONTAINED WITHIN THE APPLICABLE REGIONAL OR SUBREGIONAL STRATEGY (INCLUDING THE SYDNEY METROPOLITAN PLAN AND EXHIBITED DRAFT STRATEGIES)?

5.2.1.1 Greater Sydney Region Plan, A Metropolis of Three Cities

The Greater Sydney Region Plan, *A Metropolis of Three Cities* (Sydney Region Plan) is built on a vision of *three cities where most residents live 30 minutes of their jobs, education and health facilities, services and great places*. The Sydney Region Plan acknowledges that to meet the needs of a growing and changing population, the vision seeks to transform Greater Sydney into a metropolis of three cities:

- The Western Parkland City;
- The Central River City; and
- The Eastern Harbour City.

The site is situated in the Central River City, an area that the Sydney Region Plan acknowledges will grow substantially capitalising on its location close to the geographic centre of Greater Sydney and the unprecedented public and private investment that is contributing to new transport and other infrastructure.

The Sydney Region Plan identifies 10 directions for the metropolis of three cities, these being:

Infrastructure and Collaboration

- A city supported by infrastructure *Infrastructure supporting new developments*
- A collaborative city *Working together to grow a Greater Sydney*

Liveability

- A city for people *Celebrating diversity and putting people at the heart of planning*
- Housing the city *Giving people housing choices*
- A city of great places *Designing places for people*

Productivity

- A well-connected city *Developing a more accessible and walkable city*
- Jobs and skills for the city *Creating the conditions for a stronger economy*

Sustainability

- A city in its landscape *Valuing green spaces and landscape*
- An efficient city *Using resources wisely*
- A resilient city *Adapting to a changing world*

Of key relevance to the PP is the 'Liveability' direction of Housing the city, in particular Objective 10 (Greater housing supply) and Objective 11 (Housing is more diverse and affordable).

Objective 10 – Greater housing supply

This objective identifies the need for ensuring a steady supply of market housing in locations that are well supported by existing or planned services and amenity, with an emphasis on public transport access.

The NSW Government has identified that 725,000 additional homes will be needed by 2036 to meet demand based on current population projections. By 2056, it is anticipated that significant further housing supply will be required to meet Greater Sydney's continued strong population growth.

As part of this unprecedented level of supply, a range of housing types, tenures and price points will be needed to meet demand. The Sydney Region Plan acknowledges that *good strategic planning can link the delivery of new homes in the right locations with local infrastructure* and that *developers also play an important role in supporting housing outcomes. The development industry needs to continually provide new housing and translate the development capacity created by the planning system into approvals and supply.*

The PP acknowledges the need for additional housing within an accessible location. The site is located within 200 metres of the Schofields Train Station (Blacktown/Richmond Rail line) and the proposed density increase will facilitate the delivery of more housing in an accessible location, where the benefits gained from the proximity of the site to public transport can be maximised.

In addition to the recent duplication of the tracks at Schofields station, a 700-space commuter carpark has been delivered. It is understood that there are future plans for Schofields station to be further upgraded to provide 4 tracks, which will reinforce the station as a key public transport hub.

The existing North-West Metro is also proposed to be extended to Schofields and St Marys, through to the Western Sydney Aerotropolis, which will connect the site to additional employment opportunities.

Objective 11 – Housing is more diverse and affordable

This objective acknowledges that across Greater Sydney, both home renters and purchasers face housing affordability challenges. Greater Sydney has been measured as being one of the least affordable housing markets globally¹² and is the least affordable Australian city. This has been exacerbated in the past five years by rapid home price growth.

Factors that contribute to rental and purchasing affordability challenges include the limited availability of smaller dwellings to meet the growing proportion of small households as well as the growing distance between areas where housing is affordable and the location of employment and education opportunities. An examination of housing needs reaffirms the critical importance of providing a diversity of housing across the housing continuum in Greater Sydney.

The housing continuum recognises the fundamental importance of household income on the ability to access housing of different types, cost and tenure (refer to Figure 19). Households on moderate, low or very low incomes, who spend more than 30 per cent of their income on housing, are impacted in their ability to pay for essential items like food, clothing, transport and utilities. Lower income households (earning up to approximately \$67,600 per annum) without other financial support cannot afford the average rental cost for even more moderately priced areas of Greater Sydney, which are generally on the outskirts of Greater Sydney (refer to Figure 20). Cities require a range of workers to be close to centres and jobs. An absence of affordable housing often results in workers having to commute for long distances. A range of housing choices, including

affordable rental housing reduces the need for people to go into social housing and also supports a pathway for people to move out of social housing.

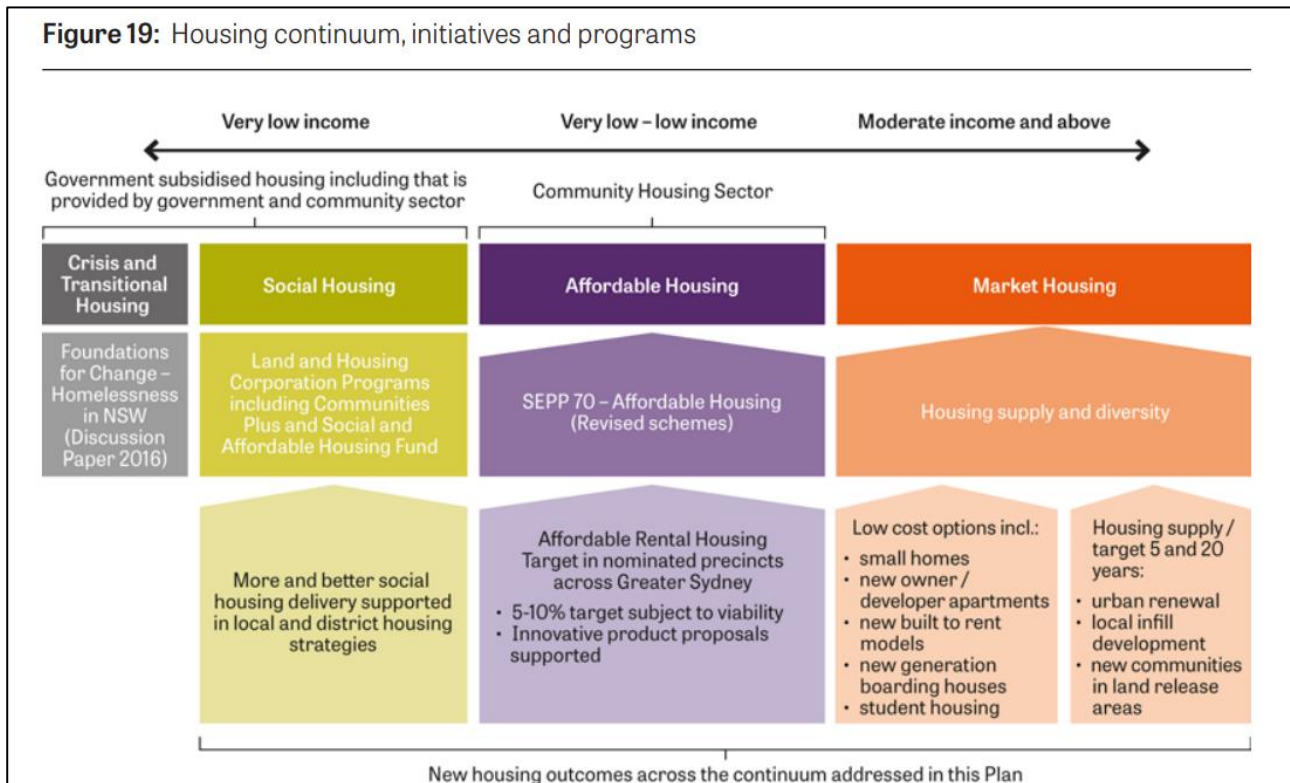


Figure 10: Extract of the Housing Continuum (Source: GCC, 2018)

This objective relevantly provides that other initiatives to support housing diversity and affordability are market led. These include innovative purchase and rental models, small/medium scale owner-developer apartment projects that employ flexible design and delivery outcomes, and institutionally delivered and managed rental accommodation in purpose designed rental buildings – referred to as build to rent. These forms of rental accommodation need to be delivered close to public transport and centres and offer the opportunity to include Affordable Rental Housing Schemes, if viable.

The Social and Community Infrastructure Assessment, prepared by Hill PDA to accompany this PP relevantly provides that it is important to note that the PP is proposed to incorporate 33 percent build-to-rent, social and affordable housing, which meet the benchmarks identified by the former Greater Sydney Commission for 5-10 per cent of the uplift value in high growth areas to be allocated to affordable housing. This would meet Blacktown City Council’s vision by adding to the supply of affordable housing in the area and contributing to improved housing diversity within the region.

5.2.1.2 Central City District Plan

The Central City District is the central and major component of the Central River City, as identified in the Sydney Region Plan. The Central City District Plan (District Plan) is a 20 year plan to manage growth in the context of economic, social and environmental matters to achieve the 40 year vision of Greater Sydney. It is a guide for implementing the Sydney Region Plan at a district level; providing a bridge between regional and local planning.

The District Plan informs LSPS and LEPs, the assessment of planning proposals as well as community strategic plans and policies. The District Plan also assists councils to plan for and support growth and change and align their local planning strategies to place-based outcomes. As has been mentioned previously, a key driver for the proposed amendments to building height and FSR are the site’s context within an accessible location in proximity to facilities to meet the day-to-day needs of residents having regard to the Alex Avenue Town Centre.

Of relevance is the direction for liveability, as detailed in the following **Table 2**.

Table 2 Directions for liveability

Direction	Response
<p>Housing the city</p> <p><i>Planning Priority C5</i></p> <p>Providing housing supply, choice and affordability with access to jobs, services and public transport</p>	<p>The PP will facilitate the delivery of a mix of market, build-to-rent, social and affordable housing stock that will cater to the diverse and varied needs of the community.</p> <p>The site is located within 200 metres of the Schofields Train Station (Blacktown/Richmond Rail line), to the north. In addition to the recent duplication of the tracks at Schofields station a 700 space commuter carpark has also been delivered. It is understood that there are future plans for Schofield station to be further upgraded to provide 4 tracks, which will reinforce the station as a key public transport hub.</p> <p>The site is also situated within 80 metres of the Alex Avenue Town Centre to the north which includes the Schofields Village Shopping Centre. Schofields Village includes a Coles supermarket, McDonalds restaurant, various retail shops and restaurants and a child care centre. The Town Centre will also include a town park for public recreation.</p>

5.2.1.3 Housing 2041 NSW Housing Strategy

Housing 2041 – NSW Housing Strategy (Housing 2041) represents a 20-year vision for housing in NSW. It embodies the government’s goals and ambitions to deliver better housing outcomes by 2041— housing in the right locations, housing that suits diverse needs and housing that feels like home.

By implementing Housing 2041 the NSW Government aims to achieve the following overarching objectives, all of which are integrated and aligned with the four key pillars of housing needs; supply, diversity, affordability and resilience. The following **Table 2** identifies the relevant objectives and how the PP is responding to each:

Table 3 Objectives of Housing 2041

Objective	Response
<i>To deliver housing supply in the right locations at the right time</i>	The proposed amendments to the maximum building height and maximum FSR would support the precinct vision by facilitating additional residential housing to meet the needs of the community in an accessible location, noting the proximity of the site to the Schofields Train Station and the Alex Avenue Town Centre. The PP will also contribute to the provision of affordable housing to meet the needs of the community.
<i>To provide housing that is diverse and meets varied and changing needs</i>	The PP will facilitate the delivery of a mix of market, build-to-rent, social and affordable housing stock that will cater to the diverse and varied needs of the community.
<i>To provide housing that is affordable and secure</i>	Of the additional yield generated by the PP, 33 percent will comprise a mix build-to-rent, social and affordable housing for the community.
<i>To deliver enduring and resilient housing</i>	The proposed mix of housing and its strategic location will ensure the ongoing viability of the development generated by the PP.

The key forms of housing identified in Housing 2041 which the PP will facilitate are:

- Affordable rental housing/affordable housing for very low to moderate income households.
- Build-to-rent housing, a housing product that refers to the construction of a property for rental purposes. It offers increased security of tenure, more choice, property management and high quality places to live for renters.

IS THE PLANNING PROPOSAL CONSISTENT WITH LOCAL COUNCIL’S COMMUNITY STRATEGIC PLAN OR OTHER LOCAL PLANS?

5.2.1.4 Blacktown Local Strategic Planning Statement 2020

The Blacktown Local Strategic Planning Statement 2020 (LSPS) sets out a 20-year vision for the future of Blacktown City as it grows and changes. The 20-year vision of the LSPS is for *a planned city of sustainable growth, supported by essential infrastructure, efficient transport, a prosperous economy and equitable access to a vibrant, healthy lifestyle*. The LSPS acknowledges that the population of the Blacktown LGA is forecast to hit 612,000 people by 2041, generating the need for 223,100 dwellings to cater for the projected population.

The LSPS divides the Blacktown LGA into four (4) precincts, with the site located in the Riverstone Precinct, which is bounded to the south by the M7 Motorway, to the west by Eastern Creek and to the east and north by boundaries with the Hawkesbury and The Hills LGAs.

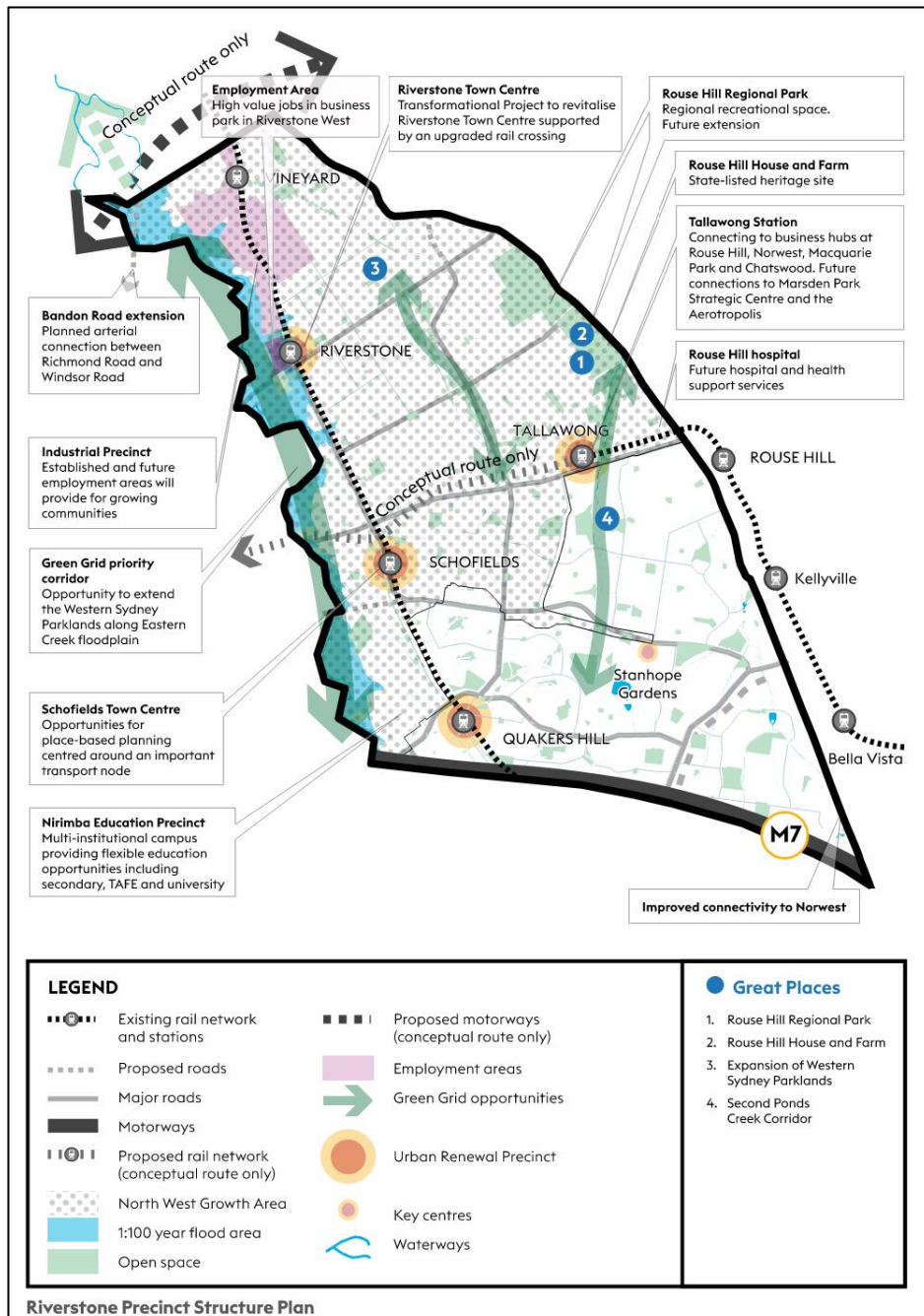


Figure 11: Extract of Riverstone Precinct Structure Plan (Source: BCC, 2020)

The LSPS acknowledges that new housing will be developed in new communities within the North West Growth Area and that the Sydney Metro will connect Tallawong Station to Schofields Station. A duplicated Richmond Rail Line, beyond Schofields Station, will also be completed. Improvements to public transport will mean more people will be able to access Rouse Hill Strategic Centre within 30 minutes by public transport.

The Western Sydney Parklands will be extended along Eastern Creek to South Creek in public ownership. Together with protecting precious environmental lands, this will create the opportunity to provide walking and cycling paths that link into the broader Greater Sydney Green Grid.

The following **Table 3** identifies the applicable Local Planning Priorities of the LSPS and how the PP responds:

Table 4 LSPS Local Planning Priorities

Planning Priority	Statement of Consistency
<p>Local Planning Priority 5: <i>Providing housing supply, choice and affordability with access to jobs, services and public transport</i></p>	<p>The PP will facilitate the delivery of additional housing supply in an accessible location. The development generated by the PP will deliver in the order of 1,751 new dwellings, 33 percent of which will comprise a mix of build-to-rent, social and affordable housing to promote housing choice and affordability.</p> <p>The site is situated within 200 metres of the Schofields Train Station (Blacktown/Richmond Rail line) and the proposed density increase will facilitate the delivery of more housing in an accessible location, where the proximity of the site to public transport can be maximised.</p> <p>In addition to the recent duplication of the tracks at Schofields station, a 700 space commuter carpark has been delivered. It is understood that there are future plans for Schofields station to be further upgraded to provide 4 tracks, which will reinforce the station as a key public transport hub.</p> <p>The existing North-West Metro is also proposed to be extended to Schofields and St Marys, through to the Western Sydney Aerotropolis, which will connect the site to additional employment opportunities.</p>
<p>Local Planning Priority 7: <i>Delivering integrated land use and transport planning and a 30-minute city</i></p>	<p>The PP will facilitate additional residential development within an accessible location. The amendments sought through the PP will facilitate development that will get the full benefit of the site's proximity to the Schofields Train Station and the Alex Avenue Town Centre.</p>

5.2.1.5 Our Blacktown 2041 Community Strategic Plan

The Out Blacktown 2041 Community Strategic Plan ('CSP') has 6 strategic directions, with each strategic direction containing strategies that identify how Council, along with partners and stakeholders, will work towards achieving the directions. The development of the CSP has been guided by and is based on the following principles:

- Sustainability;
- Leadership;
- Equity;
- Partnership; and
- Excellence.

Of the 6 strategic directions, the most relevant to this PP is 'A growing city supported by accessible infrastructure.' An assessment of the PP against this strategic direction is provided in the following **Table 4:**

Table 5 Assessment against the relevant CSP Direction

CSP Direction	Statement of Consistency
A growing city supported by accessible infrastructure	
<p><i>Focus areas</i></p> <ul style="list-style-type: none"> • A strategic urban planning framework which encourages sustainable growth • Delivery of infrastructure to support the growing City • Housing supply supports affordability, diversity of choice and liveability 	<p>The PP will facilitate the delivery of additional housing supply in an accessible location. The development generated by the PP will deliver in the order of 1,751 new dwellings, 33 percent of which will comprise a mix of build-to-rent, social and affordable housing to promote housing choice and affordability.</p>

5.2.1.6 Blacktown Housing Strategy 2020

The Blacktown Housing Strategy 2020 (Housing Strategy) supports the LSPS and sets out a 20-year housing vision for Blacktown City as it grows and changes. The Housing Strategy acknowledges that *the market is shifting to more medium and high density developments, concentrated in greenfield areas and Urban Renewal Precincts*. The Housing Strategy *aims to concentrate housing growth in established areas around key Strategic Centres and transport nodes that provide significant opportunities for urban renewal, and in new release areas in the North West Growth Area*.

The Housing Strategy notes that *there are long wait times for social housing (5–10 years and up to 10–15 years for larger dwellings), which is indicative of undersupply. There are currently 2,300 households on the waiting list for social and community housing*. The Housing Strategy further states that *a decline in social housing stock over the last decade has exacerbated undersupply. As at 2016, it was estimated that a minimum of 9,200 additional affordable housing dwellings could be utilised if they were available, by eligible households. This demand could escalate to 15,000–20,000 more affordable homes required in the LGA by 2036*.

Of the estimated 1,751 dwellings that the PP would facilitate, 33 percent will be delivered as build-to-rent, social and affordable housing. This will make a significant contribution to easing the current undersupply of social housing within the Blacktown LGA.

The proximity of the site to the Schofields Train Station and its location within a rezoned precinct (Alex Avenue) of the North West Growth Area make it a prime site to achieve the aims of the Housing Strategy. The Housing Strategy provides revised housing and population forecasts for the North West Growth Area, noting that the original Precinct Plan for the Alex Avenue Precinct envisaged demand for 6,240 lots/dwellings to accommodate the forecast population growth of 18,000 residents, across the 420 gross hectare precinct area. The housing strategy has provided a revised forecast which identifies the need for 8,706 lots/dwellings for a population increase of 25,325 people within the Alex Avenue Precinct; an increase of 2,466 lots/dwellings from the original precinct plan.

The additional dwellings that would be facilitated through the PP will make a significant contribution to delivering the additional dwellings and accommodating the population increase forecast in the Housing Strategy.

The following **Table 6** provides details of the Housing Strategy Priorities and how the PP responds to each:

Table 6 Assessment against the Housing Strategy Priorities

Housing Strategy Priority	Statement of Consistency
<i>1. Plan for housing supply to meet population growth</i>	The additional dwellings generated by the PP will contribute to the provision of additional housing supply to support the growing population of the Blacktown LGA.
<i>2. Plan for housing supported by infrastructure</i>	This PP is accompanied by an Infrastructure and Services Report prepared by Lucas Consulting Engineers Pty Limited which advises that the development facilitated by the PP is capable of being adequately serviced.
<i>3. Plan for appropriate housing in suitable location</i>	The PP will facilitate additional residential development within an accessible location. The amendments sought through the PP will facilitate development that will get the full benefit of the site's proximity to the Schofields Train Station and the Alex Avenue Town Centre.
<i>4. Plan for diversity and choice in housing</i>	The PP will facilitate the delivery of a mix of market, build-to-rent, social and affordable housing that will promote housing diversity and choice.
<i>5. Plan to improve housing affordability</i>	Of the estimated 1,751 dwellings that the PP would facilitate, 33 percent will be delivered as build-to-rent, social and affordable housing. This will make a significant contribution to easing the current undersupply of social housing within the Blacktown LGA.
<i>6. Promote excellence in housing design</i>	The detailed design of the development facilitated by the PP will be subject to a State Significant Development assessment.
<i>7. Improve housing resilience and sustainability</i>	The detailed design of the development facilitated by the PP will be subject to a State Significant Development assessment.

5.2.1.7 North West Priority Growth Area Land Use and Infrastructure Implementation Plan

The North West Priority Growth Area Land Use and Infrastructure Implementation Plan (Implementation Plan) outlines plans for the growing North West Priority Growth Area and the infrastructure needed to support that growth. The Implementation Plan's development is guided by the NSW Government's 20-year metropolitan plan 'A Plan for Growing Sydney.'

The site forms part of the North West Priority Growth Area, which is close to a number of key transport nodes and established areas of employment, including:

- Rouse Hill and Norwest Business Park
- state and regional road network connections, including Windsor Road, Richmond Road, The Northern Road and M7 Motorway, providing connections to the M4 Motorway to Blacktown, Penrith and Parramatta
- Sydney Metro Northwest connecting the North West Priority Growth Area to Chatswood via Epping with the construction of eight new stations
- the town centres of Rouse Hill, Blacktown, Penrith and Castle Hill

- to the southwest and east which provide retail, commercial and
- industrial uses, and associated employment
- a public transport corridor from the Sydney Metro Northwest towards Marsden Park.

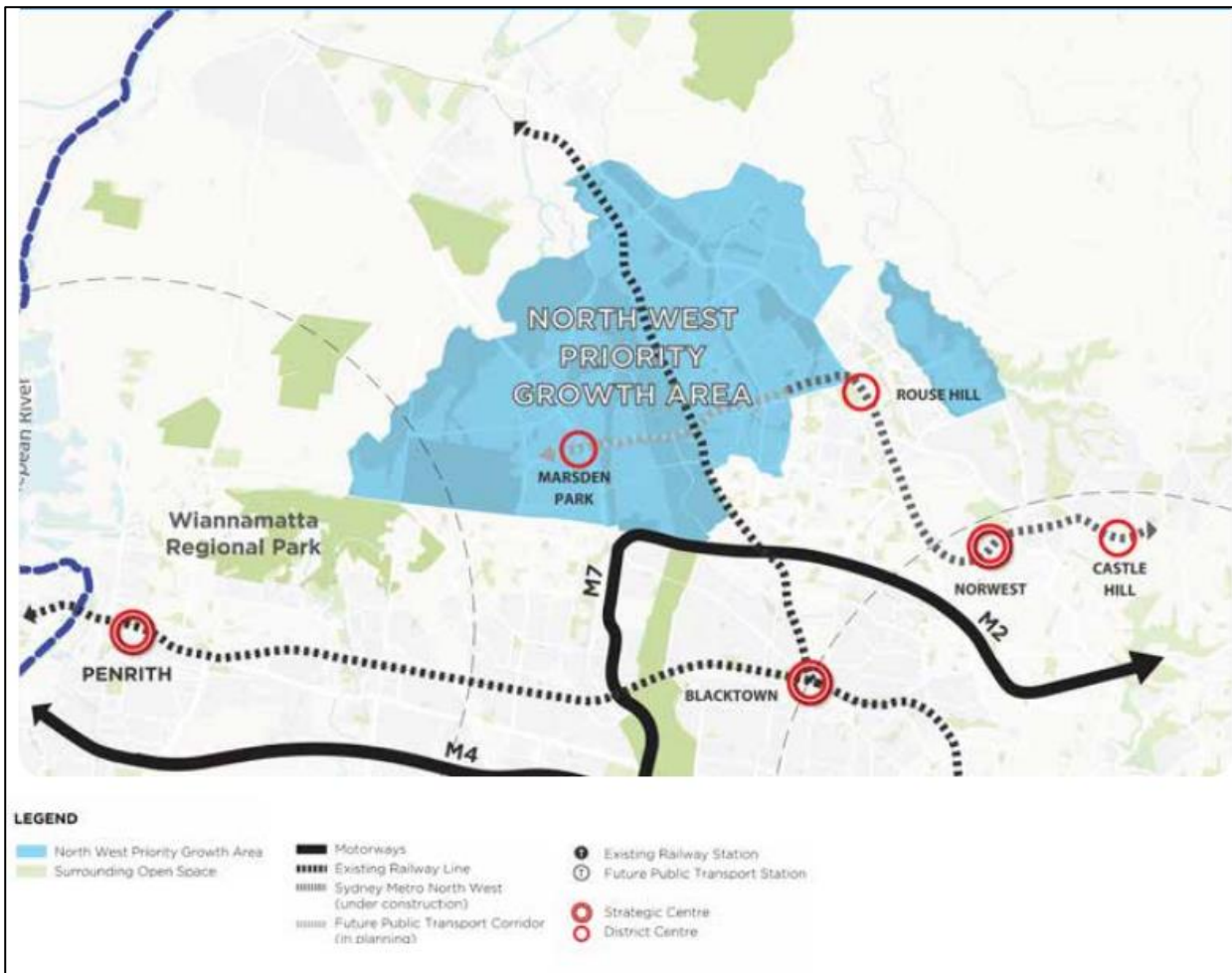


Figure 12: North West Priority Growth Area (Source: NSW Government, 2017)

The Infrastructure Plan notes that precincts are rezoned following Precinct Planning Process, which includes the preparation of an Indicative Layout Plan to guide the future urban structure and a development control plan that details planning controls to enable defined development outcomes. The site is located in the rezoned Alex Avenue Precinct and is subject to the provisions of the Growth Centre DCP.

Section 5 (Key considerations) of the Implementation Plan identifies 'affordable housing and housing affordability' as a continuing challenge for many households. In response to the Department's 2014 Housing Diversity Package, developers are providing a broader range of housing types, including small lot housing and apartments.

The Implementation Plan acknowledges that housing affordability needs to be addressed through exploring opportunities within the North West Priority Growth Area such as:

- independently assessing need and viability of the development feasibility of a nominated affordable rental housing target;
- support councils to achieve additional affordable housing;
- undertake broad approaches to facilitate affordable housing, such as through planning approaches that support the community housing sector to better leverage housing affordability supply outcomes, and more cost effective and innovative building approaches
- support social housing.

Of the estimated 1,751 dwellings that the PP would facilitate, 33 percent will be delivered as build-to-rent, social and affordable housing. This will make a significant contribution to easing the current undersupply of social housing within the Blacktown LGA.

Section 6 (Land use and infrastructure) of the Implementation Plan identifies a new growth corridor along Schofields Road from Rouse Hill to Marsden Park as shown in the following Figure 13.



Figure 13: Schofields Road Corridor (Source: NSW Government, 2017)

The Schofields Station Interchange is identified as a key transport node which has the potential to support higher densities. The PP acknowledges the proximity of the site to the Schofields Station Interchange and its ability to facilitate a higher density.

IS THE PLANNING PROPOSAL CONSISTENT WITH APPLICABLE STATE ENVIRONMENTAL PLANNING POLICIES?

State Environmental Planning Policies (SEPP's) considered relevant to the Proposal along with an assessment against them is provided in **Table 7** below.

Table 7 Assessment against current SEPPs

Name of SEPP	Comment	Consistency
<i>SEPP (Biodiversity and Conservation) 2021</i>	<p>The site is situated within the Hawkesbury-Nepean Catchment and the provisions of Chapter 6 (Water catchments) of the Biodiversity and Conservation SEPP are applicable to the site, specifically Part 6.2 (Development in regulated catchments).</p> <p>This PP is accompanied by an Infrastructure and Services Report which relevantly provides that the increase in density generated by the PP would not impact the stormwater design for the current residential density, as there would be no increase in paved areas and hence, no change to the volume of water leaving the site.</p>	Yes
<i>SEPP (Building Sustainability Index: BASIX) 2004</i>	The future built form generated by the PP would be capable of demonstrating compliance with BASIX provisions.	Yes
<i>SEPP (Housing) 2021</i>	<p>The aims of the policy encourage the provision of affordable housing and diverse housing types.</p> <p>The PP will facilitate the delivery of a mix of build-to-rent, social and affordable housing which will comprise 33 percent of the anticipated 1,751 dwelling yield.</p>	Yes
<i>SEPP 65 – Design Quality of Residential Apartment Development</i>	<p>SEPP 65 aims to improve the design quality of residential apartments.</p> <p>The architectural concept work undertaken to inform this PP, including the building footprints and massing, has been informed by the provisions of SEPP 65 as they relate to building separation and setbacks, landscape area, deep soil provision and communal open space.</p> <p>The concept work undertaken has sought to establish a foundation for the future detailed built form design work.</p>	Yes
<i>SEPP (Planning Systems) 2021</i>	Chapter 2 (State and Regional Development) identifies the thresholds for State Significant Development (SSD), it is anticipated that the development generated by the PP would form a SSD application.	Yes
<i>SEPP (Precincts – Central River City) 2021</i>	<p>This PP seeks to amend the maximum building height and maximum FSR provisions at Appendix 7 (Alex Avenue and Riverstone Precinct Plan) of SEPP (Precincts – Central River City) 2021.</p> <p>No other change is sought to the remaining provisions of the SEPP.</p>	Yes
<i>SEPP (Resilience and Hazards) 2021</i>	<p>Chapter 4 (Remediation of Land) aims to promote the remediation of contaminated land for the purpose of reducing risk to harm to human health.</p> <p>A Stage 1 Preliminary and Stage 2 Detailed Site Investigation (PSI/DSI) has been prepared by Construction Sciences to address the provisions of State Environmental</p>	Yes

Name of SEPP	Comment	Consistency
	<p>Planning Policy (Resilience and Hazards) 2021, by undertaking an assessment of the potential for contamination to be present on the site, arising from past and present land use activities.</p> <p>In accordance with the recommendations of the PSI/DSI, a Remedial Action Plan (RAP) has been prepared by Construction Sciences to address the identified contamination risks onsite and to address the identified data gaps onsite.</p>	
<i>SEPP (Transport and Infrastructure) 2021</i>	<p>Chapter 2 (Infrastructure) of the Transport and Infrastructure SEPP 2021 relevantly identifies criteria to be addressed for development in or adjacent to rail corridors and interim rail corridors.</p> <p>It is acknowledged that the site is opposite the rail corridor and the provisions of Section 2.98 (Development adjacent to rail corridors) and Section 2.100 (Impact of rail noise or vibration on non-rail development) may apply to the future built form generated by the PP.</p> <p>It is anticipated that Sydney Trains would be afforded the opportunity to provide comment on the PP at the public exhibition phase.</p>	Yes

IS THE PLANNING PROPOSAL CONSISTENT WITH THE APPLICABLE MINISTERIAL DIRECTIONS?

The current Ministerial Directions(as at 13 December 2022) along with an assessment is provided in **Table 8** below:

Table 8 Assessment against current Ministerial Directions.

Ministerial Principles	Direction	Commentary
1. Planning Systems		
1.1 Implementation of Regional Plans	(1) Planning proposals must be consistent with a Regional Plan released by the Minister for Planning and Public Spaces	The proposal has taken into consideration the Regional Plan applicable to the site.
1.2 Development of Aboriginal Land Council Land	(1) When preparing a planning proposal to which this direction applies, the planning proposal authority must take into account: (a) any applicable development delivery plan made under the chapter 3 of the State Environmental Planning Policy (Planning Systems) 2021; or (b) if no applicable development delivery plan has been published, the interim development delivery plan published on the Department’s website on the making of this direction	This Ministerial Direction is not relevant to the proposal.
1.3 Approval and Referral Requirements	(1) A planning proposal to which this direction applies must: (a) minimise the inclusion of provisions that require the concurrence, consultation or referral of development applications to a Minister or public authority, and (b) not contain provisions requiring concurrence, consultation or referral of a Minister or public authority unless the relevant planning authority has obtained the approval of: i. the appropriate Minister or public authority, and ii. the Planning Secretary (or an officer of the Department nominated by the Secretary), prior to undertaking community consultation in satisfaction of Schedule 1 to the EP&A Act, and (c) not identify development as designated development unless the relevant planning authority: i. can satisfy the Planning Secretary (or an officer of the Department nominated by the Secretary) that the class of development is likely to have a significant impact on the environment, and ii. has obtained the approval of the Planning Secretary (or an officer of the Department nominated by the Secretary) prior to undertaking community consultation in satisfaction of Schedule 1 to the EP&A Act	The proposal: - Would not increase the provision requiring concurrence or referral. - Does not contain provisions requiring concurrence unless approval is obtained. - is not identified as being designated development.
1.4 Site Specific Provisions	(1) A planning proposal that will amend another environmental planning instrument in order to allow particular development to be carried out must either: (a) allow that land use to be carried out in the zone the land is situated on, or	The proposal would not cause any unnecessarily restrictive site planning controls.

Ministerial Principles	Direction	Commentary
	<p>(b) rezone the site to an existing zone already in the environmental planning instrument that allows that land use without imposing any development standards or requirements in addition to those already contained in that zone, or</p> <p>(c) allow that land use on the relevant land without imposing any development standards or requirements in addition to those already contained in the principal environmental planning instrument being amended.</p> <p>(2) A planning proposal must not contain or refer to drawings that show details of the proposed development.</p>	
1.5 Parramatta Road Corridor Urban Transformation Strategy	<p>(1) A planning proposal that applies to land in the nominated local government areas within the Parramatta Road Corridor must:</p> <p>(a) give effect to the objectives of this direction,</p> <p>(b) be consistent with the Strategic Actions within the Parramatta Road Corridor Urban Transformation Strategy (November 2016),</p> <p>(c) be consistent with the Parramatta Road Corridor Planning and Design Guidelines (November 2016) and particularly the requirements set out in Section 3 Corridor-wide Guidelines and the relevant Precinct Guidelines,</p> <p>(d) be consistent with the staging and other identified thresholds for land use change identified in the Parramatta Road Corridor Implementation Plan 2016 – 2023 (November 2016), and the Parramatta Road Corridor Urban Transformation Implementation Update 2021, as applicable,</p> <p>(e) contain a requirement that development is not permitted until land is adequately serviced (or arrangements satisfactory to the relevant planning authority, or other appropriate authority, have been made to service it) consistent with the Parramatta Road Corridor Implementation Plan 2016 – 2023 (November 2016),</p> <p>(f) be consistent with the relevant District Plan.</p>	The site is not located within the Parramatta Road Corridor, therefore consideration of this Direction if not applicable.
1.6 Implementation of Northwest Priority Growth Area Land Use and Infrastructure Implementation Plan	(1) Planning proposals to which this direction applies shall be consistent with the Northwest Priority Growth Area Land Use and Infrastructure Strategy.	The site is not located within the Northwest Priority Growth Area. The PP is not inconsistent with the Northwest Priority Growth Area Land Use and Infrastructure Strategy.
1.7 Implementation of the Greater Parramatta Priority Growth Area Interim Land Use and Infrastructure Implementation Plan	(1) Planning proposals shall be consistent with the interim Plan published in July 2017.	The site is not located within the Greater Parramatta Priority Growth Area, therefore consideration of this Direction if not applicable.
1.8 Implementation of Wilton Priority Growth Area Interim Land Use and Infrastructure Implementation Plan	(1) A planning proposal is to be consistent with the Interim Land Use and Infrastructure Implementation Plan and Background Analysis, approved by the Minister for Planning and Public Spaces and as published on 5 August 2017 on the website of the Department of Planning, Industry and Environment (Implementation Plan).	The site is not located within the Wilton Priority Growth Area, therefore consideration of this Direction if not applicable.

Ministerial Principles	Direction	Commentary
1.9 Implementation of Glenfield to Macarthur Urban Renewal Corridor	(1) A planning proposal is to be consistent with the precinct plans approved by the Minister for Planning and Public Spaces and published on the Department's website on 22 December 2017	The site is not located within the Glenfield to Macarthur Urban Renewal Corridor, therefore consideration of this Direction if not applicable.
1.10 Implementation of the Western Sydney Aerotropolis Plan	(1) A planning proposal is to be consistent with the Western Sydney Aerotropolis Plan approved by the Minister for Planning and Public Spaces and as published on 10 September 2020 on the website of the Department of Planning, Industry and Environment.	This Ministerial Direction is not relevant to the proposal.
1.11 Implementation of Bayside West Precincts 2026 Plan	(1) A planning proposal authority must ensure that a planning proposal is consistent with the Bayside West Precincts 2036 Plan, approved by the Minister for Planning and Public Spaces and published on the Department of Planning, Industry and Environment website in September 2018	The site is not located within the Bayside West Precinct, therefore consideration of this Direction if not applicable.
1.12 Implementation of Planning Principles for the Cooks Cove Precinct	(1) A planning proposal authority must ensure that a planning proposal is consistent with the following principles: (a) Enable the environmental repair of the site and provide for new recreation opportunities. (b) Not compromise future transport links (such as the South-East Mass Transit link identified in Future Transport 2056 and the Greater Sydney Region Plan) that will include the consideration of the preserved surface infrastructure corridor, noting constraints, including the Cooks River, geology, Sydney Airport, and existing infrastructure will likely necessitate consideration of future sub-surface solutions and potential surface support uses. (c) Create a highly liveable community that provides choice for the needs of residents, workers, and visitors to Cooks Cove. (d) Ensure best practice design and a high-quality amenity with reference to the NSW design policy Better Placed. (e) Deliver an enhanced, attractive, connected and publicly accessible foreshore and public open space network and protect and enhance the existing market garden. (f) Safeguard the ongoing operation of Sydney Airport. (g) Enhance walking and cycling connectivity and the use of public transport to encourage and support a healthy and diverse community and help deliver a 30-minute city. (h) Deliver a safe road network that balances movement and place, provides connections to the immediate and surrounding areas, and is cognisant of the traffic conditions in this area; and (i) Enhance the environmental attributes of the site, including protected flora and fauna, riparian areas and wetlands and heritage	The site is not located within the Cooks Cove Precinct, therefore consideration of this Direction if not applicable.
1.13 Implementation of St Leonards and Crows Nest 2036 Plan	(1) A planning proposal authority must ensure that a planning proposal is consistent with the St Leonards and Crows Nest 2036 Plan, approved by the Minister for Planning and Public Spaces and published on the Department of Planning, Industry and Environment website on 29 August 2020.	This Ministerial Direction is not relevant to the proposal.
1.14 Implementation of Greater Macarthur 2040	(1) A planning proposal authority must ensure that a planning proposal is consistent with Greater Macarthur 2040, approved by the Minister for Planning and Public Spaces and as	This Ministerial Direction is not relevant to the proposal.

Ministerial Principles	Direction	Commentary
	published on 19 November 2018 on the website of the Department of Planning, Industry and Environment.	
1.15 Implementation of the Pymont Peninsula Place Strategy	<p>(1) A planning proposal authority must ensure that a planning proposal is consistent with the Pymont Peninsula Place Strategy, approved by the Minister for Planning and Public Spaces and published on the Department of Planning, Industry and Environment website on 11 December 2020, including that it:</p> <p>(a) gives effect to the objectives of this direction and the Vision (Part 5) of the Pymont Peninsula Place Strategy,</p> <p>(b) is consistent with the 10 directions (Part 6) and Structure Plan (Part 8) in the Pymont Peninsula Place Strategy,</p> <p>(c) delivers on envisaged future character for sub-precincts (Part 9), including relevant place priorities in the Pymont Peninsula Place Strategy, and</p> <p>(d) supports the delivery of the Big Moves (Part 7) in the Pymont Peninsula Place Strategy</p>	This Ministerial Direction is not relevant to the proposal.
1.16 North West Rail Link Corridor Strategy	<p>(1) A planning proposal that applies to land located within the NWRL Corridor must:</p> <p>(a) give effect to the objectives of this direction</p> <p>(b) be consistent with the proposals of the NWRL Corridor Strategy, including the growth projections and proposed future character for each of the NWRL precincts</p> <p>(c) promote the principles of transit-oriented development (TOD) of the NWRL Corridor Strategy</p>	This Ministerial Direction is not relevant to the proposal.
1.17 Implementation of the Bays west Place Strategy	<p>(1) A planning proposal authority must ensure that a planning proposal is consistent with the Bays West Place Strategy, approved by the Minister for Planning and published on the Department of Planning and Environment website on 15 November 2021, including that it:</p> <p>(a) gives effect to the objectives of this Direction and the Vision of the Bays West Place Strategy,</p> <p>(b) is consistent with the 14 Directions and Structure Plan(s) in the Bays West Place Strategy,</p> <p>(c) delivers on envisaged future character for sub-precincts, and</p> <p>(d) supports the delivery of the Big Moves in the Bays West Place Strategy</p>	This Ministerial Direction is not relevant to the proposal.
1.18 Implementation of the Macquarie Park Innovation Precinct	<p>(1) A planning proposal authority must ensure that a planning proposal is consistent with the Place Strategy (August 2022) and Master Plan (August 2022), approved by the Minister for Planning and Minister for Homes and published on the Department of Planning and Environment website on 30 September 2022.</p> <p>(2) A planning proposal must also:</p> <p>(a) ensure the rezoning of land is aligned with the delivery of infrastructure, requiring any proposal to rezone land within the Macquarie Park Investigation Area to:</p> <p>i. incorporate the outcomes of the Macquarie Park Innovation Precinct Detailed Transport Study;</p> <p>ii. incorporate and address requirements of the Infrastructure Delivery Plan;</p>	This Ministerial Direction is not relevant to the proposal.

Ministerial Principles	Direction	Commentary
	<p>iii. ensure funding mechanisms are in place for the delivery of infrastructure required to support the growth proposed by the planning proposal, prior to rezoning, including local and regional infrastructure requirements.</p> <p>(b) ensure ‘Out of Sequence planning’ landowner led planning proposals informed by detailed neighbourhood master plans are only progressed where full landowner agreement is achieved.</p> <p>(c) ensure implementation aligns with the intent of the Waterloo Road Master Plan, prepared by Hassell, Version 3 date 28.8.2020, for City of Ryde and published by the City of Ryde council website https://www.ryde.nsw.gov.au/files/assets/public/development/waterloo-road-active-street-master-plan-final-master-plan.pdf.</p>	
1.19 Implementation of the Westmead Place Strategy	(1) A planning proposal authority must ensure that a planning proposal is consistent with the Westmead Place Strategy, approved by the Minister for Planning and published on the Department of Planning and Environment.	This Ministerial Direction is not relevant to the proposal.
1.20 Implementation of the Camellia-Rosehill Place Strategy	(1) A planning proposal authority must ensure that a planning proposal to which this Direction applies is consistent with the Camellia-Rosehill Place Strategy, approved by the Minister for Planning and published on the Department of Planning and Environment website.	This Ministerial Direction is not relevant to the proposal.
1.21 Implementation of South West Growth Area Structure Plan	(1) A planning proposal authority must ensure that a planning proposal is consistent with the Structure Plan and Guide approved by the Minister for Planning and as published in December 2022 on the website of the Department of Planning and Environment.	This Ministerial Direction is not relevant to the proposal.
1.22 Implementation of the Cherrybrook Station Place Strategy	(1) A planning proposal authority must ensure that a planning proposal is consistent with the Cherrybrook Station Precinct Place Strategy, approved by the Minister for Planning and published on the Department of Planning and Environment website.	This Ministerial Direction is not relevant to the proposal.
Biodiversity and Conservation		
3.1 Conservation Zones	<p>(1) A planning proposal must include provisions that facilitate the protection and conservation of environmentally sensitive areas.</p> <p>(2) A planning proposal that applies to land within a conservation zone or land otherwise identified for environment conservation/protection purposes in a LEP must not reduce the conservation standards that apply to the land (including by modifying development standards that apply to the land). This requirement does not apply to a change to a development standard for minimum lot size for a dwelling in accordance with Direction 9.3 (2) of “Rural Lands”.</p>	This Ministerial Direction is not relevant to the proposal.
3.2 Heritage Conservation	<p>(1) A planning proposal must contain provisions that facilitate the conservation of:</p> <p>(a) items, places, buildings, works, relics, moveable objects or precincts of environmental heritage significance to an area, in relation to the historical, scientific, cultural, social, archaeological, architectural, natural or aesthetic value of the item, area, object or place, identified in a study of the environmental heritage of the area,</p> <p>(b) Aboriginal objects or Aboriginal places that are protected under the National Parks and Wildlife Act 1974, and</p> <p>(c) Aboriginal areas, Aboriginal objects, Aboriginal places or landscapes identified by an Aboriginal heritage survey prepared by or on behalf of an Aboriginal Land Council, Aboriginal</p>	This Ministerial Direction is not relevant to the proposal.

Ministerial Principles	Direction	Commentary
	body or public authority and provided to the relevant planning authority, which identifies the area, object, place or landscape as being of heritage significance to Aboriginal culture and people.	
3.3 Sydney Drinking Water Catchments	<p>(1) A planning proposal must be prepared in accordance with the general principle that water quality within the Sydney drinking water catchment must be protected, and in accordance with the following specific principles:</p> <p>(a) new development within the Sydney drinking water catchment must have a neutral or beneficial effect on water quality (including groundwater), and</p> <p>(b) future land use in the Sydney drinking water catchment should be matched to land and water capability, and</p> <p>(c) the ecological values of land within a Special Area should be maintained.</p> <p>(2) When preparing a planning proposal, the planning proposal authority must:</p> <p>(a) consult with Water NSW, describing the means by which the planning proposal gives effect to the water quality protection principles set out in paragraph (1) of this direction, and</p> <p>(b) ensure that the proposal is consistent with Part 6.5 of Chapter 6 of the State Environmental Planning Policy (Biodiversity and Conservation) 2021, and</p> <p>(c) identify any existing water quality (including groundwater) risks to any waterway occurring on, or adjacent to the site, and</p> <p>(d) give consideration to the outcomes of the Strategic Land and Water Capability Assessment prepared by WaterNSW, and</p> <p>(e) zone land within the Special Areas</p> <p>(f) include a copy of any information received from WaterNSW as a result of the consultation process in its planning proposal prior to the issuing of a gateway determination under section 3.34 of the EP&A Act.</p>	This Ministerial Direction is not relevant to the proposal.
3.4 Application of C2 and C3 Zones and Environmental Overlays in Far North Coast LEPS	(1) A planning proposal that introduces or alters an C2 Environmental Conservation or C3 Environmental Management zone or an overlay and associated clause must apply that proposed C2 Environmental Conservation or C3 Environmental Management zone, or the overlay and associated clause, in line with the Northern Councils C Zone Review Final Recommendations.	This Ministerial Direction is not relevant to the proposal.
3.5 Recreation Vehicle Areas	<p>(1) A planning proposal must not enable land to be developed for the purpose of a recreation vehicle area (within the meaning of the Recreation Vehicles Act 1983):</p> <p>(a) where the land is within a conservation zone,</p> <p>(b) where the land comprises a beach or a dune adjacent to or adjoining a beach,</p> <p>(c) where the land is not within an area or zone referred to in paragraphs (a) or (b) unless the relevant planning authority has taken into consideration:</p> <p>i. the provisions of the guidelines entitled Guidelines for Selection, Establishment and Maintenance of Recreation Vehicle Areas, Soil Conservation Service of New South Wales, September 1985, and</p>	This Ministerial Direction is not relevant to the proposal.

Ministerial Principles	Direction	Commentary
	<p>ii. the provisions of the guidelines entitled Recreation Vehicles Act 1983, Guidelines for Selection, Design, and Operation of Recreation Vehicle Areas, State Pollution Control Commission, September 1985.</p>	
<p>3.6 Strategic Conservation Planning</p>	<p>(1) A planning proposal authority must be satisfied that a planning proposal that applies to avoided land identified under the State Environmental Planning Policy (Biodiversity and Conservation) 2021 demonstrates that it is consistent with:</p> <ul style="list-style-type: none"> (a) the protection or enhancement of native vegetation, (b) the protection or enhancement of riparian corridors, including native vegetation and water quality, (c) the protection of threatened ecological communities, threatened species and their habitats, (d) the protection or enhancement of koala habitat and corridors, and (e) the protection of matters of national environmental significance. <p>(2) A planning proposal authority must be satisfied that a planning proposal that applies to a strategic conservation area identified under the State Environmental Planning Policy (Biodiversity and Conservation) 2021 demonstrates that it is consistent with:</p> <ul style="list-style-type: none"> (a) the protection or enhancement of native vegetation, (b) the minimisation of impacts on areas of regionally significant biodiversity, including threatened ecological communities, threatened species and their habitats, (c) the protection or enhancement of koala habitat and corridors, including habitat connectivity and fauna movement, and links to ecological restoration areas, and (d) the maintenance or enhancement of ecological function. <p>(3) A planning proposal must not rezone land identified as avoided land in the State Environmental Planning Policy (Biodiversity and Conservation) 2021 to:</p> <ul style="list-style-type: none"> (a) a rural, residential, employment, mixed use, SP1 Special Activities, SP2 Infrastructure, SP3 Tourist, SP4 Enterprise, SP5 Metropolitan Centre, RE2 Private Recreation, W4 Working Waterfront or equivalent zone. <p>(4) A planning proposal must not rezone land identified as a strategic conservation area in the State Environmental Planning Policy (Biodiversity and Conservation) 2021 to:</p> <ul style="list-style-type: none"> (a) RU4, RU5, RU6, residential, employment, mixed use, SP1 Special Activities, SP2 Infrastructure, SP3 Tourist, SP4 Enterprise, SP5 Metropolitan Centre, RE2 Private Recreation, W4 Working Waterfront or equivalent zone. 	<p>This Ministerial Direction is not relevant to the proposal.</p>
<p>3.7 Public Bushland</p>	<p>(1) When preparing a planning proposal, the planning proposal authority must be satisfied that the planning proposal:</p> <p>is consistent with the objectives of this direction, and</p>	<p>This Ministerial Direction is not relevant to the proposal.</p>

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	<p>(b) gives priority to retaining public bushland, unless the planning proposal authority is satisfied that significant environmental, economic or social benefits will arise that outweigh the value of the public bushland.</p>	
3.8 Willandra Lakes Region	<p>(1) When preparing a planning proposal, the planning proposal authority must:</p> <p>(a) consult with the World Heritage Advisory Committee by:</p> <ul style="list-style-type: none"> i. sending written notice to the Committee that asks it to comment, within the consultation period, on the impact of the proposed local environmental plan (as set out in the planning proposal) on the World Heritage Property, including its outstanding universal values, and ii. providing the Committee with a copy of the planning proposal with the written notice, and iii. as far as is practicable, giving effect to any recommendations of the Committee made within the consultation period about the proposed local environmental plan, and iv. considering any other comments made by the Committee within the consultation period in relation to the proposed local environmental plan, and <p>(b) be satisfied the planning proposal is consistent with the following:</p> <ul style="list-style-type: none"> i. the objectives of this direction, and ii. the plan of management prepared for Mungo National Park under the National Parks and Wildlife Act 1974, if applicable, and iii. a relevant strategic plan of management, and iv. a relevant individual property plan. <p>(2) The World Heritage Advisory Committee may extend the 60-day consultation period by up to a further 60 days by advising the planning proposal authority in writing of the extension.</p>	This Ministerial Direction is not relevant to the proposal.
3.9 Sydney Harbour Foreshores and Waterways Area	<p>(1) When preparing a planning proposal, the planning proposal authority must consider whether the planning proposal:</p> <p>(a) is consistent with the following principles:</p> <ul style="list-style-type: none"> i. Sydney Harbour is a public resource, owned by the public, to be protected for the public good, and ii. the public good has precedence over the private good, and iii. the protection of the natural assets of Sydney Harbour has precedence over all other interests, and <p>(b) enables the following:</p> <ul style="list-style-type: none"> i. the protection, maintenance and enhancement of the natural assets and unique environmental, visual, and cultural qualities of Sydney Harbour and its islands and foreshores, and ii. increased, maintained, or improved public access to and along the foreshore and to and from the waterways for public recreational purposes, while minimising impact on watercourses, wetlands, riparian lands and remnant vegetation, and iii. the retention of foreshore land to meet existing and future demand for working harbour uses, and 	This Ministerial Direction is not relevant to the proposal.

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	<ul style="list-style-type: none"> iv. public access along foreshore land that does not interfere with industrial or commercial maritime uses, and v. foreshore land adjacent to land used for industrial or commercial maritime purposes is used for compatible purposes, and vi. links between water-based public transport and land-based public transport at appropriate public spaces along the waterfront, and vii. the provision and use of public boating facilities along the waterfront, and (c) recognises and protects Sydney Harbour and its islands and foreshores as places of exceptional heritage significance by: <ul style="list-style-type: none"> i. acknowledging the significance of Sydney Harbour for local Aboriginal communities and the role it played in European occupation, and ii. conserving the significant fabric, settings, relics and views associated with the heritage significance of heritage items, and iii. protecting the natural, scenic, environmental and cultural qualities of the Foreshores and Waterways Area, and iv. conserving archaeological sites and places of Aboriginal heritage significance. (d) meets the targets set out in the NSW Water Quality and River Flow Objectives: Sydney Harbour and Parramatta River, as published by the Department of Planning and Environment. 	
3.10 Water Catchment Protection	<p>(1) When preparing a planning proposal, the planning proposal authority must be satisfied that the planning proposal achieves the following:</p> <ul style="list-style-type: none"> (a) is consistent with the objectives of this direction, (b) is consistent with the Australian and New Zealand Guidelines for Fresh and Marine Water Quality, as published by Water Quality Australia, and any water quality management plan prepared in accordance with those guidelines, (c) includes documentation, prepared by a suitably qualified person(s), indicating whether the planning proposal: <ul style="list-style-type: none"> i. is likely to have an adverse direct, indirect or cumulative impact on terrestrial, aquatic or migratory animals or vegetation, and any steps taken to minimise such impacts ii. is likely to have an impact on periodic flooding that may affect wetlands and other riverine ecosystems iii. is likely to have an adverse impact on recreational land uses within the regulated catchment (d) identifies and considers the cumulative impact of the planning proposal on water quality (including groundwater) and flows of natural waterbodies and on the environment more generally, including on land adjacent to or downstream of the area to which this direction applies, (e) identifies how the planning proposal will: <ul style="list-style-type: none"> i. protect and improve environmental values, having regard to maintaining biodiversity, and protecting native vegetation, cultural heritage and water resources (including groundwater), 	<p>The PP is accompanied by an Infrastructure and Services Report prepared by Lucas Consulting Engineers Pty Limited which has considered stormwater and flooding, refer to the discussion at the ‘Stormwater and Flooding’ section of this Report.</p>

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	<ul style="list-style-type: none"> ii. impact the scenic quality of the natural waterbodies and the social, economic and environmental interests of the community, iii. protect and rehabilitate land from current and future urban salinity, and prevent or restore land degradation, (f) considers any feasible alternatives to the planning proposal. (2) When preparing a planning proposal, the planning proposal authority must: <ul style="list-style-type: none"> (a) consult with the councils of adjacent or downstream local government areas where the planning proposal is likely to have an adverse environmental impact on land in that local government area, and (b) as far as is practicable, give effect to any requests of the adjacent or downstream council. 	
Resilience and Hazards		
4.1 Flooding	<ul style="list-style-type: none"> (1) A planning proposal must include provisions that give effect to and are consistent with: <ul style="list-style-type: none"> (a) the NSW Flood Prone Land Policy, (b) the principles of the Floodplain Development Manual 2005, (c) the Considering flooding in land use planning guideline 2021, and (d) any adopted flood study and/or floodplain risk management plan prepared in accordance with the principles of the Floodplain Development Manual 2005 and adopted by the relevant council. (2) A planning proposal must not rezone land within the flood planning area from Recreation, Rural, Special Purpose or Conservation Zones to a Residential, Employment, Mixed Use, W4 Working Waterfront or Special Purpose Zones. (3) A planning proposal must not contain provisions that apply to the flood planning area which: <ul style="list-style-type: none"> (a) permit development in floodway areas, (b) permit development that will result in significant flood impacts to other properties, (c) permit development for the purposes of residential accommodation in high hazard areas, (d) permit a significant increase in the development and/or dwelling density of that land, (e) permit development for the purpose of centre-based childcare facilities, hostels, boarding houses, group homes, hospitals, residential care facilities, respite day care centres and seniors housing in areas where the occupants of the development cannot effectively evacuate, (f) permit development to be carried out without development consent except for the purposes of exempt development or agriculture. Dams, drainage canals, levees, still require development consent, (g) are likely to result in a significantly increased requirement for government spending on emergency management services, flood mitigation and emergency response measures, which can include but are not limited to the provision of road infrastructure, flood mitigation infrastructure and utilities, or (h) permit hazardous industries or hazardous storage establishments where hazardous materials cannot be effectively contained during the occurrence of a flood event. 	<p>The PP is accompanied by an Infrastructure and Services Report prepared by Lucas Consulting Engineers Pty Limited which has considered stormwater and flooding, refer to the discussion at the ‘Stormwater and Flooding’ section of this Report.</p>

Ministerial Principles	Direction	Commentary
	<p>(4) A planning proposal must not contain provisions that apply to areas between the flood planning area and probable maximum flood to which Special Flood Considerations apply which:</p> <ul style="list-style-type: none"> (a) permit development in floodway areas, (b) permit development that will result in significant flood impacts to other properties, (c) permit a significant increase in the dwelling density of that land, (d) permit the development of centre-based childcare facilities, hostels, boarding houses, group homes, hospitals, residential care facilities, respite day care centres and seniors housing in areas where the occupants of the development cannot effectively evacuate, (e) are likely to affect the safe occupation of and efficient evacuation of the lot, or (f) are likely to result in a significantly increased requirement for government spending on emergency management services, and flood mitigation and emergency response measures, which can include but not limited to road infrastructure, flood mitigation infrastructure and utilities. <p>(5) For the purposes of preparing a planning proposal, the flood planning area must be consistent with the principles of the Floodplain Development Manual 2005 or as otherwise determined by a Floodplain Risk Management Study or Plan adopted by the relevant council.</p>	
4.2 Coastal Management	<p>(1) A planning proposal must include provisions that give effect to and are consistent with:</p> <ul style="list-style-type: none"> (a) the objects of the Coastal Management Act 2016 and the objectives of the relevant coastal management areas. (b) the NSW Coastal Management Manual and associated Toolkit. (c) NSW Coastal Design Guidelines 2003; and (d) any relevant Coastal Management Program that has been certified by the Minister, or any Coastal Zone Management Plan under the Coastal Protection Act 1979 that continues to have effect under clause 4 of Schedule 3 to the Coastal Management Act 2016, that applies to the land. <p>(2) A planning proposal must not rezone land which would enable increased development or more intensive land-use on land:</p> <ul style="list-style-type: none"> (a) within a coastal vulnerability area identified by the State Environmental Planning Policy (Coastal Management) 2018; or (b) that has been identified as land affected by a current or future coastal hazard in a local environmental plan or development control plan, or a study or assessment undertaken: <ul style="list-style-type: none"> i. by or on behalf of the relevant planning authority and the planning proposal authority, or ii. by or on behalf of a public authority and provided to the relevant planning authority and the planning proposal authority. <p>(3) A planning proposal must not rezone land which would enable increased development or more intensive land-use on land within a coastal wetlands and littoral rainforests area identified by chapter 3 of the State Environmental Planning Policy (Biodiversity and Conservation) 2021.</p>	This Ministerial Direction is not relevant to the proposal.

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	<p>(4) A planning proposal for a local environmental plan may propose to amend the following maps, including increasing or decreasing the land within these maps, under the State Environmental Planning Policy (Coastal Management) 2018: (a) Coastal wetlands and littoral rainforests area map.</p> <p>(b) Coastal vulnerability area map.</p> <p>(c) Coastal environment area map; and</p> <p>(d) Coastal use area map. Such a planning proposal must be supported by evidence in a relevant Coastal Management Program that has been certified by the Minister, or by a Coastal Zone Management Plan under the Coastal Protection Act 1979 that continues to have effect under clause 4 of Schedule 3 to the Coastal Management Act 2016.</p>	
4.3 Planning for Bushfire Protection	<p>(1) In the preparation of a planning proposal the relevant planning authority must consult with the Commissioner of the NSW Rural Fire Service following receipt of a gateway determination under section 3.34 of the Act, and prior to undertaking community consultation in satisfaction of clause 4, Schedule 1 to the EP&A Act, and take into account any comments so made.</p> <p>(2) A planning proposal must:</p> <p>(a) have regard to Planning for Bushfire Protection 2019,</p> <p>(b) introduce controls that avoid placing inappropriate developments in hazardous areas, and</p> <p>(c) ensure that bushfire hazard reduction is not prohibited within the Asset Protection Zone (APZ).</p> <p>(3) A planning proposal must, where development is proposed, comply with the following provisions, as appropriate:</p> <p>(a) provide an Asset Protection Zone (APZ) incorporating at a minimum:</p> <p>i. an Inner Protection Area bounded by a perimeter road or reserve which circumscribes the hazard side of the land intended for development and has a building line consistent with the incorporation of an APZ, within the property, and</p> <p>ii. an Outer Protection Area managed for hazard reduction and located on the bushland side of the perimeter road,</p> <p>(b) for infill development (that is development within an already subdivided area), where an appropriate APZ cannot be achieved, provide for an appropriate performance standard, in consultation with the NSW Rural Fire Service. If the provisions of the planning proposal permit Special Fire Protection Purposes (as defined under section 100B of the Rural Fires Act 1997), the APZ provisions must be complied with,</p> <p>(c) contain provisions for two-way access roads which links to perimeter roads and/or to fire trail networks,</p> <p>(d) contain provisions for adequate water supply for firefighting purposes,</p> <p>(e) minimise the perimeter of the area of land interfacing the hazard which may be developed,</p> <p>(f) introduce controls on the placement of combustible materials in the Inner Protection Area</p>	This Ministerial Direction is not relevant to the proposal.

Ministerial Principles	Direction	Commentary
4.4 Remediation of Contaminated Land	<p>(1) A planning proposal authority must not include in a particular zone (within the meaning of the local environmental plan) any land to which this direction applies if the inclusion of the land in that zone would permit a change of use of the land, unless:</p> <p>(a) the planning proposal authority has considered whether the land is contaminated, and</p> <p>(b) if the land is contaminated, the planning proposal authority is satisfied that the land is suitable in its contaminated state (or will be suitable, after remediation) for all the purposes for which land in the zone concerned is permitted to be used, and</p> <p>(c) if the land requires remediation to be made suitable for any purpose for which land in that zone is permitted to be used, the planning proposal authority is satisfied that the land will be so remediated before the land is used for that purpose. In order to satisfy itself as to paragraph 1(c), the planning proposal authority may need to include certain provisions in the local environmental plan.</p> <p>(2) Before including any land to which this direction applies in a particular zone, the planning proposal authority is to obtain and have regard to a report specifying the findings of a preliminary investigation of the land carried out in accordance with the contaminated land planning guidelines.</p>	<p>In accordance with the recommendations of the PSI/DSI, a Remedial Action Plan (RAP) has been prepared by Construction Sciences to address the identified contamination risks onsite and to address the identified data gaps onsite.</p>
4.5 Acid Sulfate Soils	<p>(1) The relevant planning authority must consider the Acid Sulfate Soils Planning Guidelines adopted by the Planning Secretary when preparing a planning proposal that applies to any land identified on the Acid Sulfate Soils Planning Maps as having a probability of acid sulfate soils being present.</p> <p>(2) When a relevant planning authority is preparing a planning proposal to introduce provisions to regulate works in acid sulfate soils, those provisions must be consistent with:</p> <p>(a) the Acid Sulfate Soils Model LEP in the Acid Sulfate Soils Planning Guidelines adopted by the Planning Secretary, or</p> <p>(b) other such provisions provided by the Planning Secretary that are consistent with the Acid Sulfate Soils Planning Guidelines.</p> <p>(3) A relevant planning authority must not prepare a planning proposal that proposes an intensification of land uses on land identified as having a probability of containing acid sulfate soils on the Acid Sulfate Soils Planning Maps unless the relevant planning authority has considered an acid sulfate soils study assessing the appropriateness of the change of land use given the presence of acid sulfate soils. The relevant planning authority must provide a copy of any such study to the Planning Secretary prior to undertaking community consultation in satisfaction of clause 4 of Schedule 1 to the Act.</p> <p>(4) Where provisions referred to under 2(a) and 2(b) above of this direction have not been introduced and the relevant planning authority is preparing a planning proposal that proposes an intensification of land uses on land identified as having a probability of acid sulfate soils on the Acid Sulfate Soils Planning Maps, the planning proposal must contain provisions consistent with 2(a) and 2(b).</p>	<p>This Ministerial Direction is not relevant to the proposal.</p>

Ministerial Principles	Direction	Commentary
4.6 Mine Subsidence and Unstable Land	<p>(1) When preparing a planning proposal that would permit development on land that is within a declared mine subsidence district, a relevant planning authority must:</p> <p>(a) consult Subsidence Advisory NSW to ascertain:</p> <p>i. if Subsidence Advisory NSW has any objection to the draft local environmental plan, and the reason for such an objection, and ii. the scale, density and type of development that is appropriate for the potential level of subsidence, and</p> <p>(b) incorporate provisions into the draft Local Environmental Plan that are consistent with the recommended scale, density and type of development recommended under 1(a)(ii), and</p> <p>(c) include a copy of any information received from Subsidence Advisory NSW with the statement to the Planning Secretary (or an officer of the Department nominated by the Secretary prior to undertaking community consultation in satisfaction of Schedule 1 to the Act.</p> <p>(2) A planning proposal must not permit development on land that has been identified as unstable as referred to in the application section of this direction.</p>	This Ministerial Direction is not relevant to the proposal.
Transport and Infrastructure		
5.1 Integrating Land Use and Transport	<p>(1) A planning proposal must locate zones for urban purposes and include provisions that give effect to and are consistent with the aims, objectives and principles of:</p> <p>(a) Improving Transport Choice – Guidelines for planning and development (DUAP 2001), and</p> <p>(b) The Right Place for Business and Services – Planning Policy (DUAP 2001).</p>	This Ministerial Direction is not relevant to the proposal.
5.2 Reserving Land for Public Purposes	<p>(1) A planning proposal must not create, alter or reduce existing zonings or reservations of land for public purposes without the approval of the relevant public authority and the Planning Secretary (or an officer of the Department nominated by the Secretary).</p> <p>(2) When a Minister or public authority requests a relevant planning authority to reserve land for a public purpose in a planning proposal and the land would be required to be acquired under Division 3 of Part 2 of the Land Acquisition (Just Terms Compensation) Act 1991, the relevant planning authority must:</p> <p>(a) reserve the land in accordance with the request, and</p> <p>(b) include the land in a zone appropriate to its intended future use or a zone advised by the Planning Secretary (or an officer of the Department nominated by the Secretary), and</p> <p>(c) identify the relevant acquiring authority for the land.</p> <p>(3) When a Minister or public authority requests a relevant planning authority to include provisions in a planning proposal relating to the use of any land reserved for a public purpose before that land is acquired, the relevant planning authority must:</p> <p>(a) include the requested provisions, or</p> <p>(b) take such other action as advised by the Planning Secretary (or an officer of the Department nominated by the Secretary) with respect to the use of the land before it is acquired.</p> <p>(4) When a Minister or public authority requests a relevant planning authority to include provisions in a planning proposal to rezone and/or remove a reservation of any land that is reserved for public purposes because the land is no longer designated by that public authority</p>	This Ministerial Direction is not relevant to the proposal.

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	for acquisition, the relevant planning authority must rezone and/or remove the relevant reservation in accordance with the request.	
5.3 Development Near Regulated Airports and Defence Airfields	<p>1) In the preparation of a planning proposal that sets controls for development of land near a regulated airport, the relevant planning authority must:</p> <ul style="list-style-type: none"> (a) consult with the lessee/operator of that airport. (b) take into consideration the operational airspace and any advice from the lessee/operator of that airport. (c) for land affected by the operational airspace, prepare appropriate development standards, such as height controls. (d) not allow development types that are incompatible with the current and future operation of that airport. <p>(2) In the preparation of a planning proposal that sets controls for development of land near a core regulated airport, the relevant planning authority must:</p> <ul style="list-style-type: none"> (a) consult with the Department of the Commonwealth responsible for airports and the lessee/operator of that airport. (b) for land affected by the prescribed airspace (as defined in clause 6(1) of the Airports (Protection of Airspace) Regulation 1996, prepare appropriate development standards, such as height controls. (c) not allow development types that are incompatible with the current and future operation of that airport. (d) obtain permission from that Department of the Commonwealth, or their delegate, where a planning proposal seeks to allow, as permissible with consent, development that would constitute a controlled activity as defined in section 182 of the Airports Act 1996. This permission must be obtained prior to undertaking community consultation in satisfaction of Schedule 1 to the EP&A Act. <p>(3) In the preparation of a planning proposal that sets controls for the development of land near a defence airfield, the relevant planning authority must:</p> <ul style="list-style-type: none"> (a) consult with the Department of Defence if: <ul style="list-style-type: none"> i. the planning proposal seeks to exceed the height provisions contained in the Defence Regulations 2016 – Defence Aviation Areas for that airfield; or ii. no height provisions exist in the Defence Regulations 2016 – Defence Aviation Areas for the airfield and the proposal is within 15km of the airfield. (b) for land affected by the operational airspace, prepare appropriate development standards, such as height controls. (c) not allow development types that are incompatible with the current and future operation of that airfield. 	This Ministerial Direction is not relevant to the proposal.

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	<p>(4) A planning proposal must include a provision to ensure that development meets Australian Standard 2021 – 2015, Acoustic- Aircraft Noise Intrusion – Building siting and construction with respect to interior noise levels, if the proposal seeks to rezone land:</p> <p>(a) for residential purposes or to increase residential densities in areas where the Australian Noise Exposure Forecast (ANEF) is between 20 and 25; or</p> <p>(b) for hotels, motels, offices or public buildings where the ANEF is between 25 and 30; or</p> <p>(c) for commercial or industrial purposes where the ANEF is above 30.</p> <p>(5) A planning proposal must not contain provisions for residential development or to increase residential densities within the 20 Australian Noise Exposure Concept (ANEC)/ANEF contour for Western Sydney Airport.</p>	
5.4 Shooting Ranges	<p>(4) A planning proposal must include a provision to ensure that development meets Australian Standard 2021 – 2015, Acoustic- Aircraft Noise Intrusion – Building siting and construction with respect to interior noise levels, if the proposal seeks to rezone land:</p> <p>(a) for residential purposes or to increase residential densities in areas where the Australian Noise Exposure Forecast (ANEF) is between 20 and 25; or</p> <p>(b) for hotels, motels, offices or public buildings where the ANEF is between 25 and 30; or</p> <p>(c) for commercial or industrial purposes where the ANEF is above 30.</p> <p>(5) A planning proposal must not contain provisions for residential development or to increase residential densities within the 20 Australian Noise Exposure Concept (ANEC)/ANEF contour for Western Sydney Airport.</p>	This Ministerial Direction is not relevant to the proposal.
Housing		
6.1 Residential Zones	<p>(1) A planning proposal must include provisions that encourage the provision of housing that will:</p> <p>(a) broaden the choice of building types and locations available in the housing market, and</p> <p>(b) make more efficient use of existing infrastructure and services, and</p> <p>(c) reduce the consumption of land for housing and associated urban development on the urban fringe, and</p> <p>(d) be of good design.</p> <p>(2) A planning proposal must, in relation to land to which this direction applies:</p> <p>(a) contain a requirement that residential development is not permitted until land is adequately serviced (or arrangements satisfactory to the council, or other appropriate authority, have been made to service it), and</p> <p>(b) not contain provisions which will reduce the permissible residential density of land.</p>	<p>The PP will facilitate the delivery of a mix of market, build-to-rent, social and affordable housing to provide housing choice in the market.</p> <p>The site is strategically located in close proximity to the Schofields Train Station and the Alex Avenue Town Centre.</p> <p>This PP is accompanied by an Infrastructure and Services Report prepared by Lucas Consulting Engineers Pty Limited which advises that the development facilitated by the PP is capable of being adequately serviced.</p> <p>The PP will facilitate an increase in residential density for the site.</p>

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6.2 Caravan Parks and Manufactured Home Estates	<p>(1) In identifying suitable zones, locations and provisions for caravan parks in a planning proposal, the relevant planning authority must:</p> <p>(a) retain provisions that permit development for the purposes of a caravan park to be carried out on land, and</p> <p>(b) retain the zonings of existing caravan parks, or in the case of a new principal LEP zone the land in accordance with an appropriate zone under the Standard Instrument (Local Environmental Plans) Order 2006 that would facilitate the retention of the existing caravan park.</p> <p>(2) In identifying suitable zones, locations and provisions for manufactured home estates (MHEs) in a planning proposal, the relevant planning authority must:</p> <p>(a) take into account the categories of land set out in Schedule 6 of State Environmental Planning Policy (Housing) 2021 as to where MHEs should not be located,</p> <p>(b) take into account the principles listed in clause 125 of State Environmental Planning Policy (Housing) 2021 (which relevant planning authorities are required to consider when assessing and determining the development and subdivision proposals), and</p> <p>(c) include provisions that the subdivision of MHEs by long term lease of up to 20 years or under the Community Land Development Act 1989 be permissible with consent</p>	This Ministerial Direction is not relevant to the proposal.
Industry and Employment		
7.1 Employment Zones	<p>(1) A planning proposal must:</p> <p>(a) give effect to the objectives of this direction,</p> <p>(b) retain the areas and locations of Employment zones,</p> <p>(c) not reduce the total potential floor space area for employment uses and related public services in Employment Zones.</p> <p>(d) not reduce the total potential floor space area for industrial uses in E4, E5 and W4 zones, and</p> <p>(e) ensure that proposed employment areas are in accordance with a strategy that is approved by the Planning Secretary.</p>	This Ministerial Direction is not relevant to the proposal.
7.2 Reduction in non-hosted short-term rented accommodation period	<p>(1) The council must include provisions which give effect to the following principles in a planning proposal to which this direction applies:</p> <p>(a) non-hosted short term rental accommodation periods must not be reduced to be less than 90 days</p> <p>(b) the reasons for changing the non-hosted short-term rental accommodation period should be clearly articulated</p> <p>(c) the impact of reducing the non-hosted short-term rental accommodation period should be analysed and explained, including social and economic impacts for the community in general, and impacted property owners specifically.</p>	This Ministerial Direction is not relevant to the proposal.

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7.3 Commercial and Retail Development along the Pacific Highway, North Coast	<p>(1) A planning proposal that applies to land located on “within town” segments of the Pacific Highway must provide that:</p> <p>(a) new commercial or retail development must be concentrated within distinct centres rather than spread along the highway.</p> <p>(b) development with frontage to the Pacific Highway must consider impact the development has on the safety and efficiency of the highway; and</p> <p>(c) for the purposes of this paragraph, “within town” means areas which, prior to the draft local environmental plan, have an urban zone (e.g.: “village”, “residential”, “tourist”, “commercial”, “industrial”, etc) and where the Pacific Highway speed limit is less than 80km/hour.</p> <p>(2) A planning proposal that applies to land located on “out-of-town” segments of the Pacific Highway must provide that:</p> <p>(a) new commercial or retail development must not be established near the Pacific Highway if this proximity would be inconsistent with the objectives of this direction.</p> <p>(b) development with frontage to the Pacific Highway must consider the impact the development has on the safety and efficiency of the highway; and</p> <p>(c) for the purposes of this paragraph, “out-of-town” means areas which, prior to the draft local environmental plan, do not have an urban zone (e.g.: “village”, “residential”, “tourist”, “commercial”, “industrial”, etc) or are in areas where the Pacific Highway speed limit is 80km/hour or greater.</p> <p>(3) Notwithstanding the requirements of paragraphs (1) and (2), the establishment of highway service centres may be permitted at the localities listed in Table 1, provided that Roads and Maritime Services is satisfied that the highway service centre(s) can be safely and efficiently integrated into the Highway interchange(s) at those localities. For the purposes of this paragraph, a highway service centre has the same meaning as is contained in the Standard Instrument (Local Environmental Plans) Order 2006.</p>	This Ministerial Direction is not relevant to the proposal.
Resources and Energy		
8.1 Mining, Petroleum Production and Extractive Industries	<p>(1) In the preparation of a planning proposal affected by this direction, the relevant planning authority must:</p> <p>(a) consult the Secretary of the Department of Primary Industries (DPI) to identify any:</p> <p>i. resources of coal, other minerals, petroleum or extractive material that are of either State or regional significance, and</p> <p>ii. existing mines, petroleum production operations or extractive industries occurring in the area subject to the planning proposal, and</p> <p>(b) seek advice from the Secretary of DPI on the development potential of resources identified under (1)(a)(i), and</p> <p>(c) identify and take into consideration issues likely to lead to land use conflict between other land uses and:</p>	This Ministerial Direction is not relevant to the proposal.

Ministerial Principles	Direction	Commentary
	<ul style="list-style-type: none"> i. development of resources identified under (1)(a)(i), or ii. existing development identified under (1)(a)(ii). <p>(2) Where a planning proposal prohibits or restricts development of resources identified under (1)(a)(i), or proposes land uses that may create land use conflicts identified under (1)(c), the relevant planning authority must:</p> <ul style="list-style-type: none"> (a) provide the Secretary of DPI with a copy of the planning proposal and notification of the relevant provisions, (b) allow the Secretary of DPI a period of 40 days from the date of notification to provide in writing any objections to the terms of the planning proposal, and (c) include a copy of any objection and supporting information received from the Secretary of DPI with the statement to the Planning Secretary (or an officer of the Department nominated by the Secretary before undertaking community consultation in satisfaction of Schedule 1 to the Act. 	
9.0 Primary Production		
9.1 Rural Zones	<p>(1) A planning proposal must:</p> <ul style="list-style-type: none"> (a) not rezone land from a rural zone to a residential, employment, mixed use, SP4 Enterprise, SP5 Metropolitan Centre, W4 Working Waterfront, village or tourist zone. (b) not contain provisions that will increase the permissible density of land within a rural zone (other than land within an existing town or village). 	This Ministerial Direction is not relevant to the proposal.
9.2 Rural Lands	<p>(1) A planning proposal must:</p> <ul style="list-style-type: none"> (a) be consistent with any applicable strategic plan, including regional and district plans endorsed by the Planning Secretary, and any applicable local strategic planning statement (b) consider the significance of agriculture and primary production to the State and rural communities (c) identify and protect environmental values, including but not limited to, maintaining biodiversity, the protection of native vegetation, cultural heritage, and the importance of water resources (d) consider the natural and physical constraints of the land, including but not limited to, topography, size, location, water availability and ground and soil conditions (e) promote opportunities for investment in productive, diversified, innovative and sustainable rural economic activities (f) support farmers in exercising their right to farm (g) prioritise efforts and consider measures to minimise the fragmentation of rural land and reduce the risk of land use conflict, particularly between residential land uses and other rural land use (h) consider State significant agricultural land identified in chapter 2 of the State Environmental Planning Policy (Primary Production) 2021 for the purpose of ensuring the ongoing viability of this land 	This Ministerial Direction is not relevant to the proposal.

Ministerial Principles	Direction	Commentary
	<p>consider the social, economic and environmental interests of the community.</p> <p>(2) A planning proposal that changes the existing minimum lot size on land within a rural or conservation zone must demonstrate that it:</p> <p>(a) is consistent with the priority of minimising rural land fragmentation and land use conflict, particularly between residential and other rural land uses</p> <p>(b) will not adversely affect the operation and viability of existing and future rural land uses and related enterprises, including supporting infrastructure and facilities that are essential to rural industries or supply chains</p> <p>(c) where it is for rural residential purposes:</p> <p>i. is appropriately located taking account of the availability of human services, utility infrastructure, transport and proximity to existing centres</p> <p>ii. is necessary taking account of existing and future demand and supply of rural residential land. Note: where a planning authority seeks to vary an existing minimum lot size within a rural or conservation zone, it must also do so in accordance with the Rural Subdivision Principles in clause 5.16 of the relevant Local Environmental Plan.</p>	
9.3 Oyster Aquacultural	<p>(1) In the preparation of a planning proposal the relevant planning authority must:</p> <p>(a) identify any 'Priority Oyster Aquaculture Areas' and oyster aquaculture leases outside such an area, as shown the maps to the Strategy, to which the planning proposal would apply,</p> <p>(b) identify any proposed land uses which could result in any adverse impact on a 'Priority Oyster Aquaculture Area' or oyster aquaculture leases outside such an area,</p> <p>(c) identify and take into consideration any issues likely to lead to an incompatible use of land between oyster aquaculture and other land uses and identify and evaluate measures to avoid or minimise such land use in compatibility,</p> <p>(d) consult with the Secretary of the Department of Primary Industries (DPI) of the proposed changes in the preparation of the planning proposal, and</p> <p>(e) ensure the planning proposal is consistent with the Strategy.</p> <p>(2) Where a planning proposal proposes land uses that may result in adverse impacts identified under (1)(b) and (1)(c), relevant planning authority must:</p> <p>(a) provide the Secretary of DPI with a copy of the planning proposal and notification of the relevant provisions,</p> <p>(b) allow the Secretary of DPI a period of 40 days from the date of notification to provide in writing any objections to the terms of the planning proposal, and</p> <p>(c) include a copy of any objection and supporting information received from the Secretary of DPI with the statement to the Planning Secretary before undertaking community consultation in satisfaction of Schedule 1 to the EP&A Act.</p>	This Ministerial Direction is not relevant to the proposal.
9.4 Farmland of State and Regional Significance on the NSW Far North Coast	<p>(1) A planning proposal must not:</p> <p>(a) rezone land identified as "State Significant Farmland" for urban or rural residential purposes.</p>	This Ministerial Direction is not relevant to the proposal.

Ministerial Principles	Direction	Commentary
	(b) rezone land identified as “Regionally Significant Farmland” for urban or rural residential purposes. (c) rezone land identified as “significant non-contiguous farmland” for urban or rural residential purposes.	

5.3 ENVIRONMENTAL, SOCIAL OR ECONOMIC IMPACT

IS THERE ANY LIKELIHOOD THAT CRITICAL HABITAT OF THREATENED SPECIES, POPULATION OR ECOLOGICAL COMMUNITIES, OR THEIR HABITATS WILL BE ADVERSELY AFFECTED AS A RESULT OF THE PROPOSAL?

Part 7 (Biodiversity assessment and approvals under the Planning Act) of the *Biodiversity Conservation Act 2016* provides at Section 7.2 that development or an activity likely to significantly affect threatened species if—

- (a) *it is likely to significantly affect threatened species or ecological communities, or their habitats, according to the test in section 7.3, or*
- (b) *the development exceeds the biodiversity offsets scheme threshold if the biodiversity offsets scheme applies to the impacts of the development on biodiversity values, or*
- (c) *it is carried out in a declared area of outstanding biodiversity value.*

Section 7.6 (Part does not apply to biodiversity certified land) acknowledges that Part 7 does not apply to biodiversity certified land that has been certified under Part 8 (Biodiversity certification of land).

The site in its entirety is subject to an Order to confer biodiversity certification on the State Environmental Planning Policy (Sydney Region Growth Centres) 2006 (the now Central River City SEPP) for the purpose of the *Threatened Species Conservation Act 1995* (now repealed and replaced with the *Biodiversity Conservation Act 2016*). Refer to the discussion at Section 1.1 (Site Description) of this Report.

ARE THERE ANY OTHER LIKELY ENVIRONMENTAL EFFECTS AS A RESULT OF THE PLANNING PROPOSAL AND HOW ARE THEY PROPOSED TO BE MANAGED?

This section outlines relevant environmental considerations resulting from the PP and how they have been addressed.

CONTAMINATION

A Stage 1 Preliminary and Stage 2 Detailed Site Investigation (PSI/DSI) has been prepared by Construction Sciences to address the provisions of State Environmental Planning Policy (Resilience and Hazards) 2021, by undertaking an assessment of the potential for contamination to be present on the site, arising from past and present land use activities.

The PSI/DSI relevantly makes the following conclusions:

- There is a potential for contamination to be present at the site, arising from the past land use activities, specifically:
 - The presence of bonded asbestos within AEC06, AEC08, AEC20, AEC24, AEC25 and AEC28;
 - The presence of friable asbestos within AEC13 and AEC24; and
 - Elevated concentrations of microbes in AEC24.
- The presence of large amount of construction and demolition waste as well as the presence of disused cars presents an aesthetics impact;
- There are data gaps associated with the contamination status of soils underneath the buildings and driveways onsite as well as the presence of septic tanks onsite; and
- The site is not yet considered to be suitable for land use scenario comprising residential with minimal opportunities for soil access including dwellings with fully and permanently paved yard space such as high rise buildings and flats.

In accordance with the recommendations of the PSI/DSI, a Remedial Action Plan (RAP) has been prepared by Construction Sciences to address the identified contamination risks onsite and to address the identified data gaps onsite.

ABORIGINAL HERITAGE

An Aboriginal Heritage Due Diligence Assessment has been prepared by Baker Archaeology which has undertaken a due diligence assessment in accordance with the 'Due Diligence Code of Practice for the Protection of Aboriginal Objects in New South Wales' (DECCW, 2010), to identify whether physical evidence of Aboriginal objects are present or likely to occur on the site.

The AHDDA relevantly concludes that no Aboriginal objects are known to be present on the land nor are Aboriginal objects anticipated to occur. The landform, lack of archaeological sensitivity and history of disturbance makes the discovery of any Aboriginal objects improbable. No impacts to Aboriginal objects are anticipated to occur.

STORMWATER & FLOODING

An Infrastructure and Services Report has been prepared by Lucas Consulting Engineers Pty Limited which has considered stormwater and flooding.

Stormwater infrastructure associated with the buildings and internal open space areas will be collected by a piped system and connected through to the nearest available pit in the new roads.

Street drainage in the internal roads will be provided to collect the stormwater from the buildings and the internal roads in accordance with Blacktown Council's guidelines and requirements.

Council has previously advised that on-site detention will not be required for the site; however, it is proposed to implement stormwater reuse and quality improvement devices for each building. This will be carried out by providing underground storage tanks under the external paved areas.

Stormwater will be collected from the roof of the buildings and other 'clean' areas where no pre-treatment will be required for reuse in the irrigation systems.

Street water will be collected and directed through a gross pollutant trap prior to discharging into Council's proposed detention basin known as 'Basin SP2'. The street system and reuse scheme will be sized to cater for the 20-year ARI flow. Provision for flows more than the 20-year ARI and up to the 100-year ARI will be treated as overland flow and contained within the street kerb and gutter.

Overflow pipes will be provided from the storage tanks to the nearest street pipe if the tanks are full and cannot take any more water.

Civil designs for the stormwater are currently with Council for review as part of a Development Application for the subdivision of the site into 6 superlots and the construction of roads (DA-23-00676). The increase in the number of units will not impact on the current designs as there is no increase in paved areas, hence no change to the volume of water leaving the site.

SERVICES & UTILITIES

An Infrastructure and Services Report has been prepared by Lucas Consulting Engineers Pty Limited which has investigated the availability of services associated with the PP. Utility services are available to the site from Railway Terrace as detailed below:

Water

There are existing water mains in Railway Terrace servicing the area. These comprise a 250mm DICL, 500mm CICL and 250mm CICL main owned and maintained by Sydney Water.

New water mains will need to be constructed off the existing DN250 DICL main in Railway Terrace. These will be located in the footpath verge of the new internal roads.

In accordance with the Water Supply Code of Australia (WSA 03), Sydney Water Edition, Table SW 3.0, it will be necessary to provide 200mm mains for buildings exceeding 8 storeys in height within the new internal roads.

The extent of works associated with the water mains will be the subject of the Section 73 Notice of Requirements (NOR) from Sydney Water for the proposed development. An application for this has been submitted to Sydney Water under Case No. 208163. A Feasibility Application and Growth Data Form has also been submitted to Sydney Water under case No. 209297. Both applications are currently under review by Sydney Water, with further advice to be provided.

Sewer

A 225mm diameter gravity sewer main exists at the front of the site adjacent to Railway Terrace.

The sewer then drains towards the southeast to a DN300 main in Burdekin Road approximately 230m away.

New sewer mains will need to be constructed off the existing DN225 main in Railway Terrace. The new sewer lines for the internal roads will be a 225mm gravity sewer. There will be 3 connection points to the existing sewer along Railway Terrace.

As a result of the proposed increase in density, it may be necessary to amplify the existing DN225 sewer in Railway Terrace to a DN300 sewer. This will need to be provided from the subject site to the existing DN300 main located in Burdekin Road 230m away.

The extent of these works will also be the subject of the Section 73 Notice of Requirements (NOR) from Sydney Water for the proposed development. As advised above an application for this has been submitted to Sydney Water under Case 208163. It is currently under review by Sydney Water with the NOR expected to be issued in 6 to 8 weeks' time.

Electricity

Electricity is available in the area and will be extended to the site. Details of the proposed electrical services have been investigated with Endeavour Energy who confirm that:

Our preliminary network assessment determines that the required load of the proposed apartment development at above location will be 7.3 MVA (ADMD 3.5kVA/unit x 1,800units + 1.0MVA for other loads).

At present, there is sufficient capacity at Schofields ZS to supply this development.

Gas

There is an existing 110mm PE 210kPa gas main located in Railway Terrace which will service the as required. Jemena typically requires a 32mm PE main to service the new developments however given the size of the development this may need to be 50mm.

Jemena's requirements will be detailed when a formal application is made to them for the development however gas is available to the site.

Telecommunications

Telstra currently has underground conduits and cables in the footpath area of Railway Terrace passing along the frontage of the site. The conduits also contain Optic Fibre cables belonging to AARNet. Plans indicate that services are available to the site and will be extended as underground services along the proposed access roads to facilitate distribution to the proposed buildings.

TRAFFIC & PARKING

A Traffic Assessment has been prepared by The Traffic Planner which has assessed the traffic related impacts of the PP and relevantly concludes the following:

- The proposed amendments to the building height and floor space ratio and the potential development will have no negative (or even noticeable) impacts on the existing road network operation.
- There will be no requirement for road or intersection upgrades as a result of the proposed development. No charges are proposed to the Indicative Road Layout.

- The site is very well serviced by the existing train and bus network, with future expansion of the Metro network servicing the area.
- The proposed development should be supported in relating to the impacts on traffic and the public transport network.
- Access from the proposed development site by pedestrians, cyclists and public transport users is of the highest priority, especially to and within the local centres. These centres are most easily accessed from the proposed development by walking and cycling. Shared pedestrian and cyclist footpaths are partially constructed on Railway Terrace and once completed will connect the proposed development to the local centre.
- The site is located an approximate 400 metre bicycle ride from the Schofields Train Station and bicycle storage facilities are located at the Schofields Train Station.

As a guide, the car parking requirements outlined in the Blacktown Development Control Plan 2015 have been considered (1 space per 1/2 bedroom dwelling, 2 spaces per 3+ bedroom dwelling) and it has been determined that the development would generate the need for approximately 1,711 resident car parking spaces and 700 visitor car parking spaces.

The above rates of parking are considered adequate and can be accommodated in the site footprint. The parking requirements will be assessed at the detailed design stage of the project and accommodations can be made if necessary, such as multi-level basement car parking.

It is acknowledged that this indicative car parking requirement does not factor in the car parking rates applicable to build-to-rent, social and affordable housing – with these rates to be applied as part of the detailed design stage of the project. It is anticipated that the application of these rates would further reduce the above figures.

SOCIAL INFRASTRUCTURE

A Social and Community Infrastructure Assessment (SCIA) has been prepared by Hill PDA which has used benchmarks identified in the 'City of Parramatta's Community Infrastructure Strategy', Blacktown City Council's 'Recreation and Open Space Strategy', and the 'Growth Centres Commission's Development Code: Precinct Development Parameters' to determine provisioning rates for the Local catchment.

The Local catchment has been defined as an area which encompasses an approximate 800 metre radius from the site. The population of the Local catchment, as recorded in the 2021 Census, is used as a baseline population. This review provides a comparison between the baseline population and the total forecasted population inclusive of what is expected to be generated from the PP.

The SCIA has considered infrastructure need at three catchment levels, being (from smallest to largest):

- Local: Approx. 800 metre radius of the site
- District: Blacktown – North (SA3)
- Region: Blacktown LGA.

The PP assumes approximately 1,751 dwellings, with the projected future population arising from this yield being in the order of 2,903 residents. This population will contribute to a projected population of approximately 11,712 with the PP.

Supply has been calculated for local, district and region serving infrastructure types across the following categories:

- Open space and recreation facilities: Open spaces, playing fields and courts, play spaces, aquatic facilities and indoor recreation
- Community facilities: Libraries, community spaces, schools, health care, child care, affordable housing.

An audit of these services has been carried out within each catchment. The audit has drawn upon relevant Council and NSW Government data for current service location and capacity, as well as infrastructure strategies to predict future wider service demand and planned service capacity development. A summary of key findings is provided below.

Table 16: Infrastructure provisioning gaps in 2041

Current provision vs need		
Infrastructure type	Baseline	With Planning Proposal
Open space		
Open space	90 per cent of Local catchment within 400 metre walking distance of an open space.	95 per cent of Local catchment within 400 metre walking distance of an open space following introduction of new open space adjacent to the site.
	Playing fields: Met	Playing fields: Met
Playing fields and courts	Tennis courts: Met	Tennis courts: Met
	Netball/basketball courts: Met	Netball/basketball courts: Met
Playgrounds	Local playgrounds: 1 playground recommended.	Local playgrounds: 1 playground recommended.
	Neighbourhood playground: Met	Neighbourhood playground: Met
Aquatic facilities	Met	Met
Indoor recreation	Indoor courts: 0-1 courts across the District catchment Indoor sports centre: Met	Indoor courts: 0-1 courts across the District catchment Indoor sports centre: Met
Community facilities		
Libraries (central library)	Met	Met
Libraries (district library)	Met	Met
Community spaces	Met (Consider introducing a community space for the Local catchment)	Met (Consider introducing a community space for the Local catchment)
Child care	Long day care: -80 places Outside school hours care: -52 places (Recommended to provide additional LDC and OSHC places)	Long day care: -179 places Outside school hours care: -185 places (Recommended to provide additional LDC and OSHC places)
Schools	Met	Met
Healthcare	Community health facility recommended to service the District catchment	Community health facility recommended to service the District catchment

Figure 14: Table 16 extract from Social and Community Infrastructure Assessment (Source: Hill PDA, 2023)

The SCIA has relevantly provided the following conclusions:

Open space and recreation facilities

Open space and recreation facilities were considered using population benchmarks at the relevant catchments.

The benchmarks used in the Blacktown City Council ROSS identify that all residents should be within 400-500 metres of an open space, and open spaces should be a minimum of 0.3 ha in size. The Local catchment currently provides open space access for approximately 90 per cent of its site area. This review identified that approximately three quarters of the open spaces in the Local catchment are 0.3 hectares or greater in size.

It is also understood that land adjoining to the west of the site was recently acquired by Council for drainage and open space purposes and will be developed into a new reserve known as Basin E3.2. The provision of this new open space will improve access to open spaces across the Local catchment, though it is recommended that further opportunities for the provision of open space are explored in the vicinity of the site.

Community Facilities

The review identified that the Local catchment has sufficient access to District and Regional level community spaces. However, there is a shortage of local-level community spaces within the Local catchment. It is therefore recommended that opportunities for the provision of local-level community spaces are explored. The existing provision of library facilities is considered sufficient to meet the needs of the population (with or without the PP).

In relation to the provision of childcare facilities, the assessment has identified that there is a baseline shortfall in the provision of 80 long daycare (LDC) places and 52 out of school hours care (OSHC) places across the Local catchment. It has been estimated that the increase in population from the PP would result in a shortfall of an additional 137 LDC places and 184 OSHC places.

It is understood that the PP is capable of facilitating approximately 2,000m² of floor area for non-residential development which could potentially incorporate a new childcare centre. The provision of this childcare facility is considered to be essential in meeting demand for additional childcare places resulting from the PP.

It is also important to note that the PP is proposed to incorporate 33 percent build-to-rent, social and affordable housing, which meet the benchmarks identified by the former Greater Sydney Commission for 5-10 per cent of the uplift value in high growth areas to be allocated to affordable housing. This would meet Blacktown City Council's vision by adding to the supply of affordable housing in the area and contributing to improved housing diversity within the region.

URBAN DESIGN

An Urban Design Report has been prepared by Premise which has considered the PP's contextual fit for the site through considering key urban design elements that will shape the vision for the site.

HOW HAS THE PLANNING PROPOSAL ADEQUATELY ADDRESSED SOCIAL AND ECONOMIC EFFECTS?

The Proposal will contribute to the social and economic benefit of the wider precinct. The provision of additional residential development, which will include a mix of market, build-to-rent, social and affordable housing responds to the applicable State and local strategic documents; which all identify the need for the delivery of a range of diverse housing types to meet the needs of a growing population whilst also addressing the current issue of housing affordability.

5.4 STATE AND COMMONWEALTH INTEREST

IS THERE ADEQUATE PUBLIC INFRASTRUCTURE FOR THE PLANNING PROPOSAL

The future development of the site as a result of this PP would marginally increase the demand for public infrastructure. Services infrastructure demand has been addressed previously in this report (see 'Services & Utilities' discussion). Other public infrastructure considerations would be managed through local developer contributions, proposed State Infrastructure Contributions or under possible VPA arrangements.

WHAT ARE THE VIEWS OF STATE AND COMMONWEALTH PUBLIC AUTHORITIES CONSULTED IN ACCORDANCE WITH GATEWAY DETERMINATION?

Consultation has been commenced with the following public authorities:

Transport for NSW (TfNSW)

Meeting facilitated by DP&E held 25 August 2023 to discuss the PP. TfNSW have advised that a key consideration for the PP will be pedestrian permeability, with the need for a Travel Demand Management (TDM) plan to be prepared. TfNSW have also requested a SIDRA modelling analysis. In this regard, there are on-going discussions with TfNSW to inform the preparation of the SIDRA analysis.

Sydney Water

Meeting facilitated by DP&E held 29 August 2023 to discuss the PP. Sydney Water have advised that there is the need to consider the current dwelling yield figures within the North West Growth Centre to

undertake an analysis of the available servicing capacity. DP&E have advised that Sydney Water should have these figures around October/November 2023. Sydney Water are currently in the process of reviewing the submitted Feasibility Application and Growth Data Form submitted under Case no. 209297.

Endeavour Energy

To inform the preparation of the Infrastructure and Services report, Endeavour Energy have been consulted and have advised that *at present, there is sufficient capacity at Schofields ZS to supply this development.*

It is anticipated that further consultation with the relevant agencies will be undertaken post-Gateway Determination.

6. MAPS

The PP is accompanied by the following relevant maps:

- Existing Height of Buildings Map;
- Proposed Height of Buildings Map;
- Existing Floor Space Ratio Map; and
- Proposed Floor Space Ratio Map.

Refer to the maps at **Attachment C** to this Report.

7. COMMUNITY CONSULTATION

In accordance with Section 3.34 of the EP&A Act requires the relevant planning authority to consult with the community in accordance with the Gateway Determination. It is anticipated the PP will be required to be publicly exhibited for a minimum of 28 days in accordance with the requirements of the NSW Department of Planning, Industry and Environment's Local Environmental Plan Making Guideline (August 2023). It is anticipated that the public exhibition would be notified by way of:

- On the Planning Portal.
- A notice on the Blacktown City Council website.
- Written correspondence to adjoining and surrounding landowners (where practical).

The Gateway determination, PP and any further specialist studies required would be publicly exhibited at Council's offices and any other locations considered appropriate to provide interested parties with the opportunity to view the submitted documentation.

8. PROJECT TIMELINE

The following project timeline has been issued by the DP&E in email correspondence dated 3 August 2023:

Dates/Timeframe	Actions
14 July 2023	Letter inviting Proponent to lodge Planning Proposal
Week commencing 7 August 2023	DP&E fortnightly meetings
Week commencing 14 August 2023	Draft Planning Proposal package submitted to DP&E for pre-lodgement review
14 September 2023	Lodgement of Planning Proposal package to DP&E
Target 20 working days to assess the proposal	
13 October 2023	Issue Gateway Determination
Target 20 working days to update Planning Proposal	
Week commencing 13 November 2023	Public exhibition for 28 days
Week commencing 8 January 2024	Finalisation process
Respond to submissions & post exhibition amendments; Finalisation report and legal drafting	
Week commencing 18 March 2024	Approvals and Notification

9. CONCLUSION AND RECOMMENDATIONS

The PP is considered worthy of support for the following reasons:

- The proposed amendments to the maximum building height and maximum FSR would support the precinct vision by facilitating additional residential housing to meet the needs of the community in an accessible location, noting the proximity of the site to the Schofields Train Station and the Alex Avenue Town Centre. The PP will also contribute to the provision of affordable housing to meet the needs of the community.
- The PP maintains consistency with the applicable State, Regional and Local strategic planning framework.
- The PP is consistent with the relevant Ministerial Directions.
- The existing and future planned infrastructure and services have the capability to effectively service demand from development under this PP; and
- Overall, there are no anticipated adverse social or economic impacts which would arise from the proposal.

ATTACHMENT A

Blacktown City Council Basin Plans

BLACKTOWN CITY COUNCIL

FILE NUMBER : F16/646
PLAN NUMBER : E39-15V

NW GROWTH CENTRE - ALEX AVENUE PRECINCT RAILWAY TERRACE FUTURE ROAD DESIGN RAILWAY TERRACE FUTURE ROAD DESIGN SCHOFIELDS



LOCALITY SKETCH
N.T.S.

- WARNING !**
UNDERGROUND TELECOM SERVICES IN VICINITY OF WORKS. EXERCISE EXTREME CAUTION DURING EXCAVATION.
- WARNING !**
UNDERGROUND WATER SERVICES IN VICINITY OF WORKS. EXERCISE EXTREME CAUTION DURING EXCAVATION.
- WARNING !**
UNDERGROUND GAS SERVICES IN VICINITY OF WORKS. HIGH FIRE HAZARD. EXERCISE EXTREME CAUTION DURING EXCAVATION.
- WARNING !**
UNDERGROUND OPTICAL CABLE SERVICE IN VICINITY OF WORKS. EXERCISE EXTREME CAUTION DURING EXCAVATION.
- WARNING !**
UNDERGROUND ELECTRICITY SERVICES IN VICINITY OF WORKS. HIGH HAZARD. EXERCISE EXTREME CAUTION DURING EXCAVATION.
- WARNING !**
OVERHEAD AERIAL SERVICES PRESENT. EXERCISE EXTREME CAUTION WHEN USING CRANE/EXCAVATOR.
- WARNING !**
SHORING / BENCHING WILL BE REQUIRED FOR TRENCH EXCAVATION WORKS.
- WARNING !**
POSSIBLE DANGER TO PEDESTRIANS DURING CONSTRUCTION STAGE. PROVIDE & MAINTAIN FENCING & SAFE PEDESTRIAN ACCESS AROUND WORK.
- WARNING !**
CONFINED SPACES IDENTIFIED IN EXISTING AND PROPOSED PIT AND PIPE NETWORK.
- WARNING !**
POSSIBLE DANGER TO VEHICULAR TRAFFIC. PROVIDE & MAINTAIN BARRICADES DURING CONSTRUCTION.
- WARNING !**
MINIMUM CLEARANCES IN PLAN AND ELEVATION NEED TO BE CHECKED WITH RELEVANT SERVICE UTILITY AUTHORITY.
- WARNING !**
PROVIDE SIGNS WARNING OF POSSIBLE DROWNING HAZARD WHEN IN FLOOD.
- WARNING !**
DANGEROUS UNDERGROUND SERVICES EXIST WITHIN AREA OF WORKS. COPY OF ALL SERVICE UTILITY DIAGRAMS REQUIRED ON SITE.
- WARNING !**
INTERIM FLOOD HAZARD WORKS AND STAGING REQUIRED DURING CONSTRUCTION.
- WARNING !**
CONSTRUCTION CREW TO MANAGE AND STAGE ANY INTERIM FLOOD HAZARD WORKS.

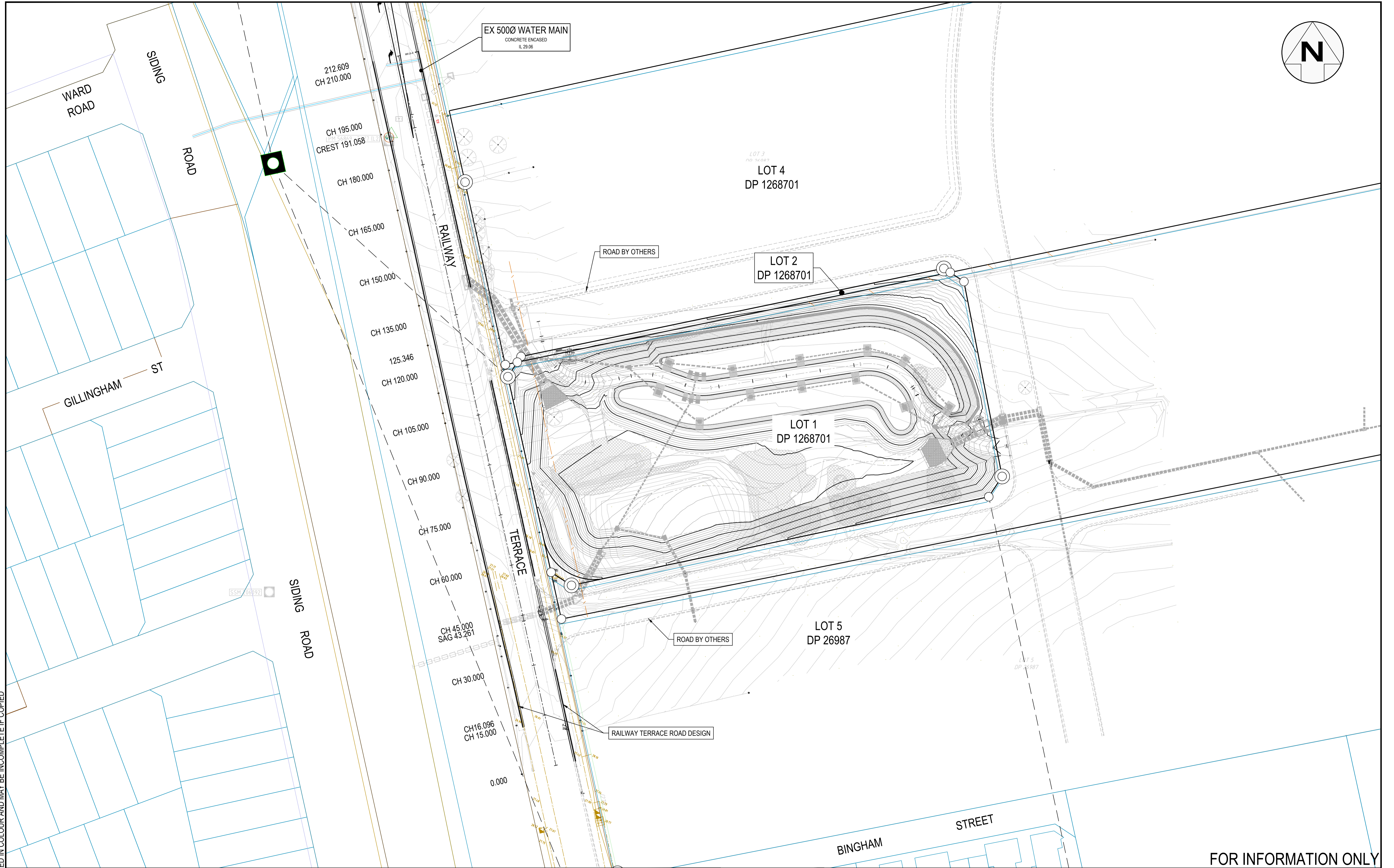
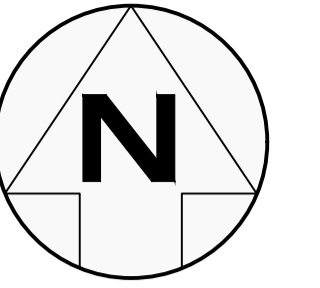
Sheet List Table	
Sheet Number	Sheet Title
GENERAL	
1	DRAWING SCHEDULE & LOCALITY SKETCH
ENGINEERING PLAN AND SECTIONS	
2	ROAD DESIGN PLAN
3	RAILWAY TERRACE LONGITUDINAL SECTION
4	RAILWAY TERRACE CROSS SECTIONS CH 0.0 - CH 212.609

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REV	DATE	DESCRIPTION	CHECKED	APPROVED	DATE	SCALE	DO NOT SCALE FROM DRAWING	DRAWINGS / DESIGN PREPARED BY	TITLE	NAME	DATE	PROJECT NW GROWTH CENTRE - ALEX AVENUE PRECINCT RAILWAY TERRACE FUTURE ROAD DESIGN SCHOFIELDS	TITLE DRAWING SCHEDULE & LOCALITY SKETCH	SHEET 1 OF 4			
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DRAWING FILE LOCATION / NAME I:\ID\Drainage - NW Growth Centre\CP20 - Alex Ave and Riverstone Precincts\Basins Package 3 E1 & E3\AutoCAD\Basin E3.2\E39-15V ROAD TITLE 001.dwg			PLOT DATE / TIME 16.02.2022 / 4:10 PM			PLOT BY mcbri1		CO-ORDINATE SYSTEM GDA 94 / MGA ZONE 56	HEIGHT DATUM AHD	DRAWINGS VERIFIED BY		COUNCIL CHAMBERS: 62 FLUSHCROBE ROAD, BLACKTOWN, NSW 2148 ALL MAIL - GENERAL MANAGER, PO BOX 63, BLACKTOWN, NSW 2148 TELEPHONE: (02) 9839 6000 FAX: (02) 9831 1961 DX: 8117 BLACKTOWN		CAD FILE E39-15V ROAD TITLE 001.dwg	FILE No F16/646	PLAN No E39-15V	REV 0





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SCALE	DO NOT SCALE FROM DRAWING

DRAWINGS / DESIGN PREPARED BY	TITLE	NAME	DATE
	SURVEYED	B.C.C.	
	DESIGNED	DY	
	DRAWN	LMCB	
	CHECKED		
	ACCEPTED - BCC CPEng NER Certified		

DRAWINGS VERIFIED BY	TITLE	NAME	DATE



PROJECT		SHEET	
NW GROWTH CENTRE - ALEX AVENUE PRECINCT		A1	
RAILWAY TERRACE FUTURE ROAD DESIGN SCHOFIELDS		2	
TITLE		OF	
ROAD DESIGN PLAN		4	
CAD FILE	FILE No	PLAN No	REV
E39-15V ROAD PLAN.dwg	F16/646	E39-15V	0

DRAWING FILE LOCATION / NAME
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PLOT DATE / TIME
16.02.2022 / 4:11 PM

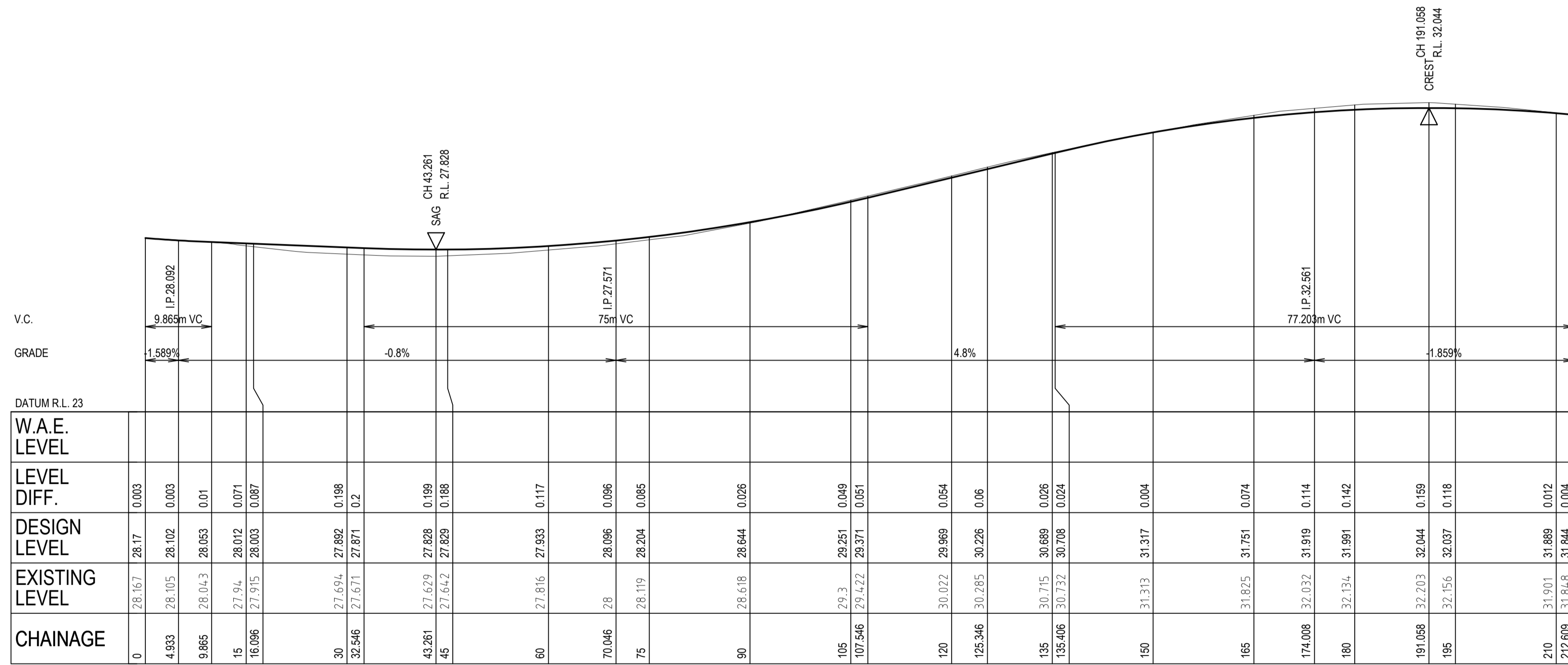
PLOT BY
mcbri1

CO-ORDINATE SYSTEM
GDA 94 / MGA ZONE 56

HEIGHT DATUM
AHD

COUNCIL CHAMBERS: 62 FLUSHCROBE ROAD, BLACKTOWN, NSW 2148
 ALL MAIL: GENERAL MANAGER, PO BOX 63, BLACKTOWN, NSW 2149
 TELEPHONE: (02) 9839 6000 FAX: (02) 9831 1961 DX: 8117 BLACKTOWN

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LONGITUDINAL SECTION - FUTURE RAILWAY TERRACE (CENTRELINE)

SCALE 1:500 (H)
SCALE 1:100 (V)

FOR INFORMATION ONLY

REV	DATE	DESCRIPTION	CHECKED	APPROVED	DATE
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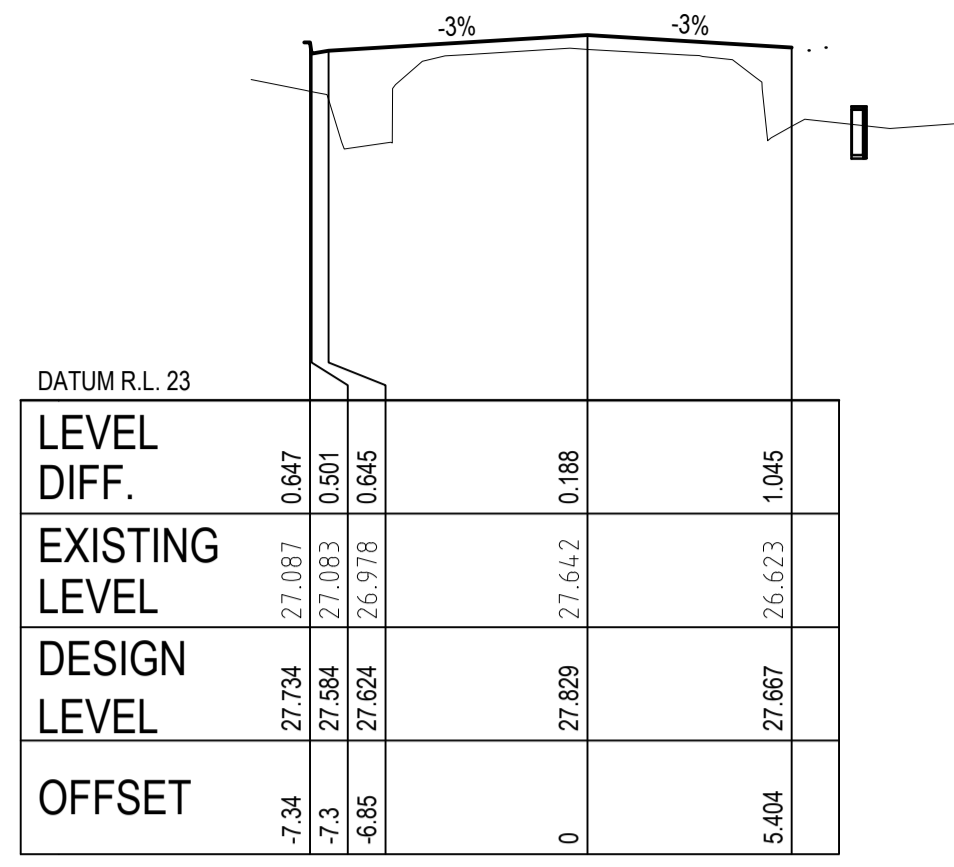
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0 1 2 3 4 5 6 7 8	SCALE 1:200 (A3)
0 1 2 3 4 5 6 7 8	SCALE 1:500 (A3)

DRAWINGS / DESIGN PREPARED BY	TITLE	NAME	DATE
DRAWINGS VERIFIED BY	SURVEYED	B.C.C.	
	DESIGNED	DY	
	DRAWN	LMCB	
	CHECKED		
	ACCEPTED - BCC CPEng NER Certified		

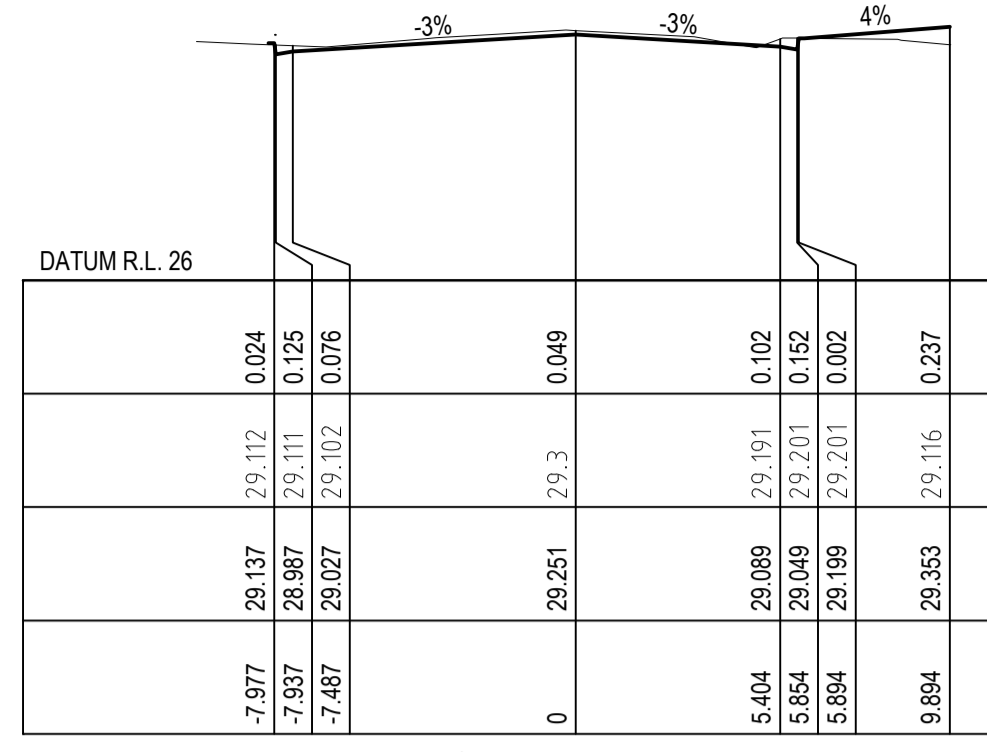
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GDA 94 / MGA ZONE 56	AHD



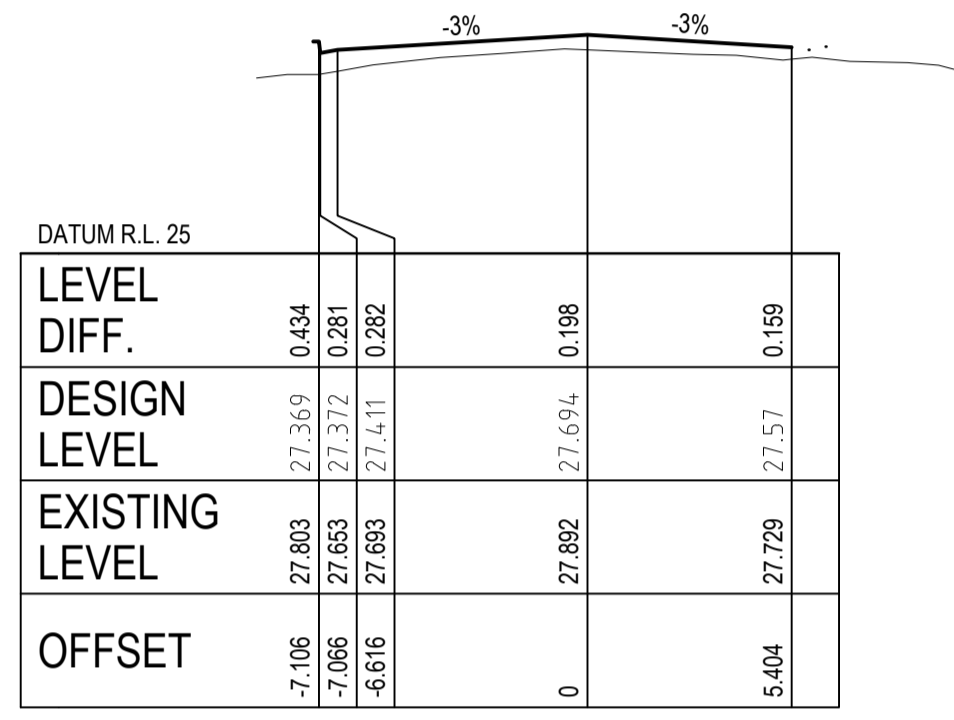
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NW GROWTH CENTRE - ALEX AVENUE PRECINCT RAILWAY TERRACE FUTURE ROAD DESIGN SCHOFIELDS	E39-15V RT ROAD SECTS.dwg	F16/646	E39-15V	0
TITLE	RAILWAY TERRACE LONGITUDINAL SECTION			SHEET
				3
				OF
				4



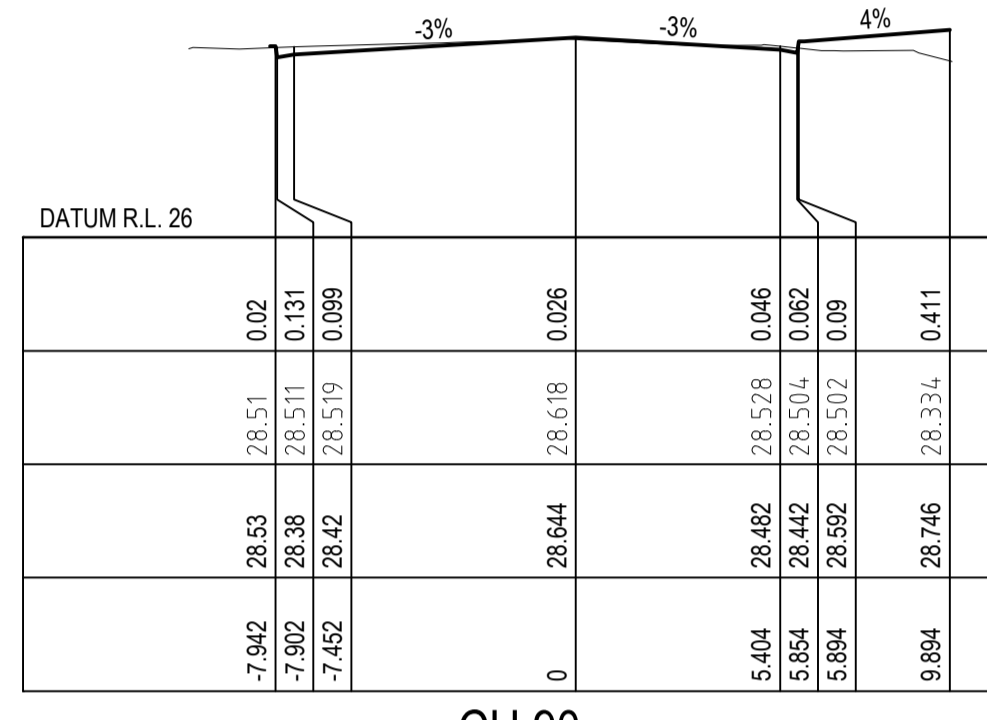
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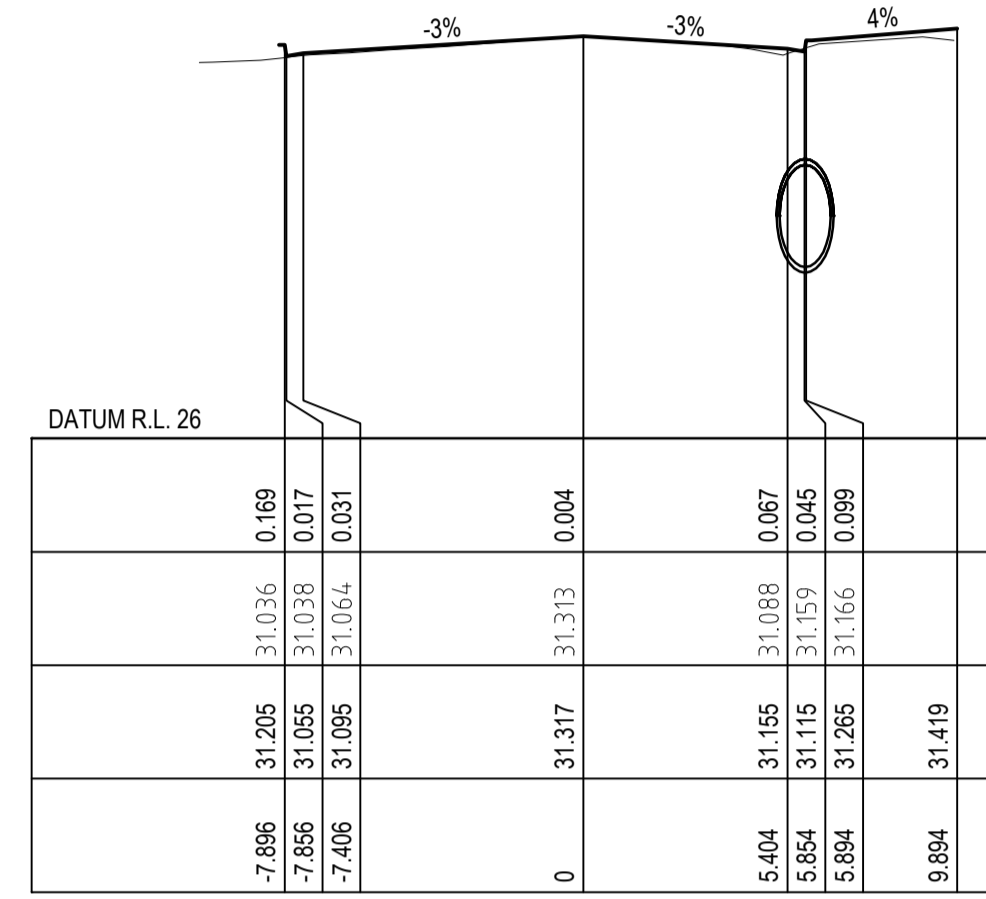
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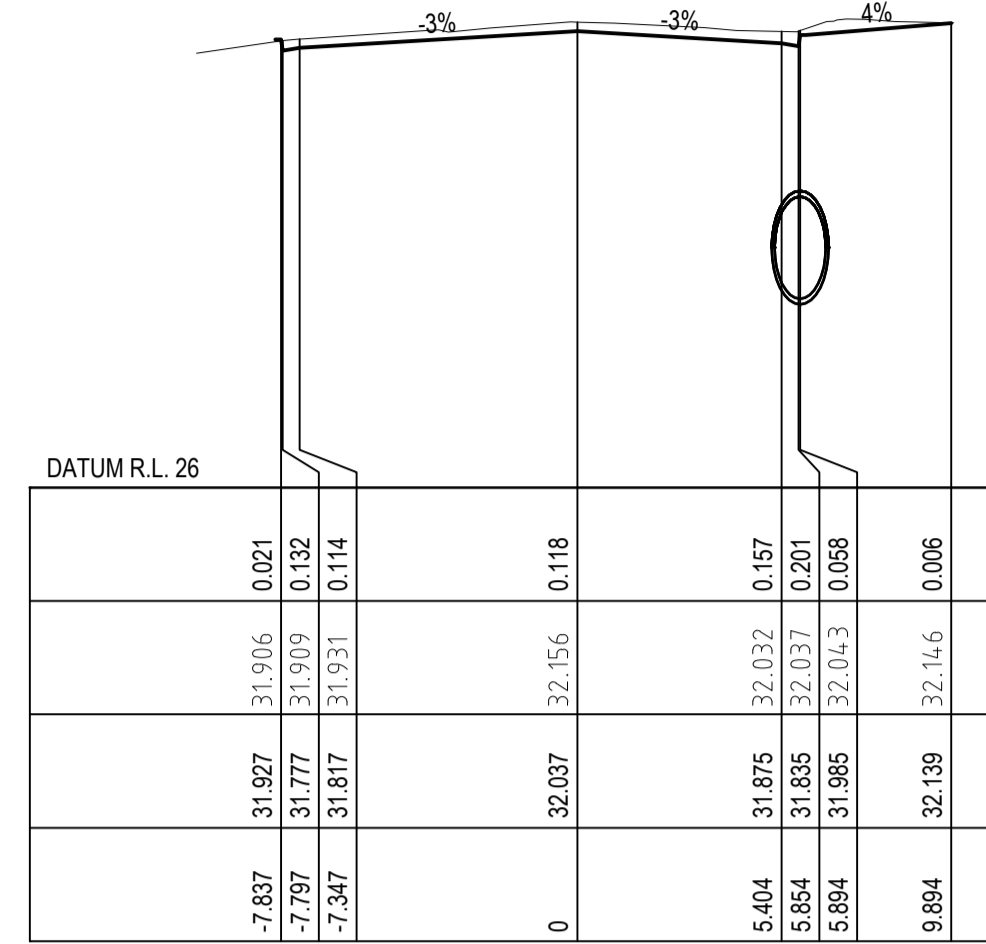
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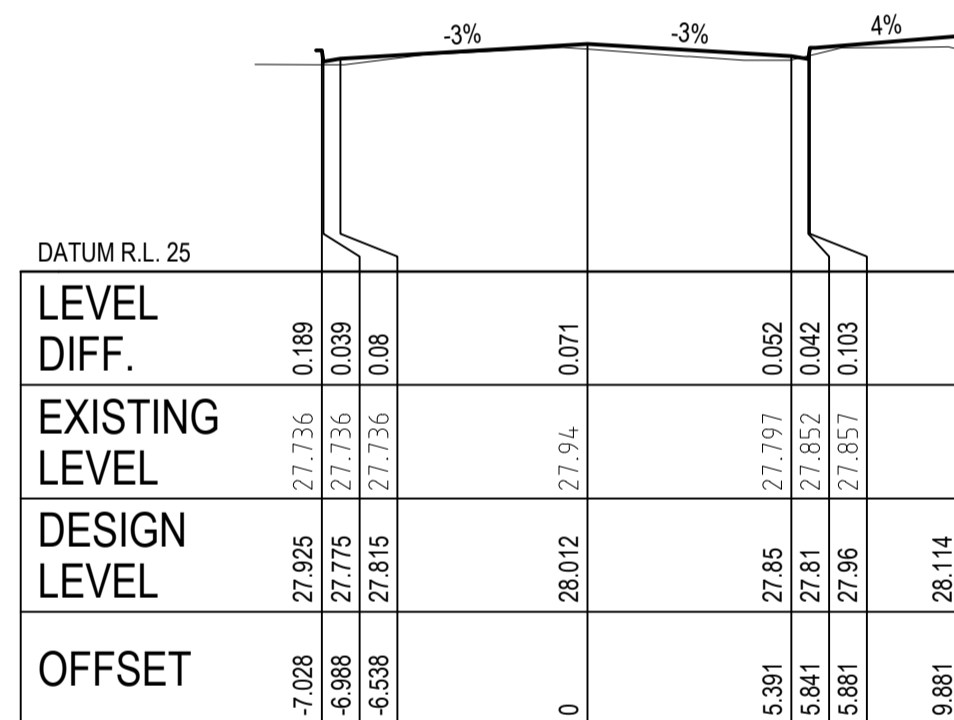
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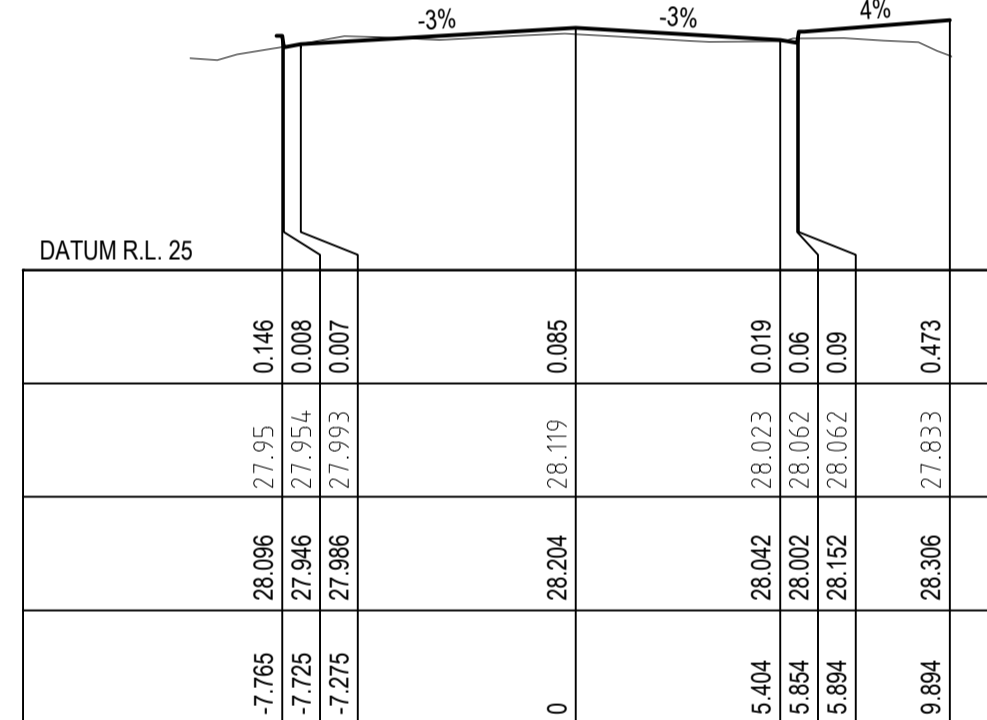
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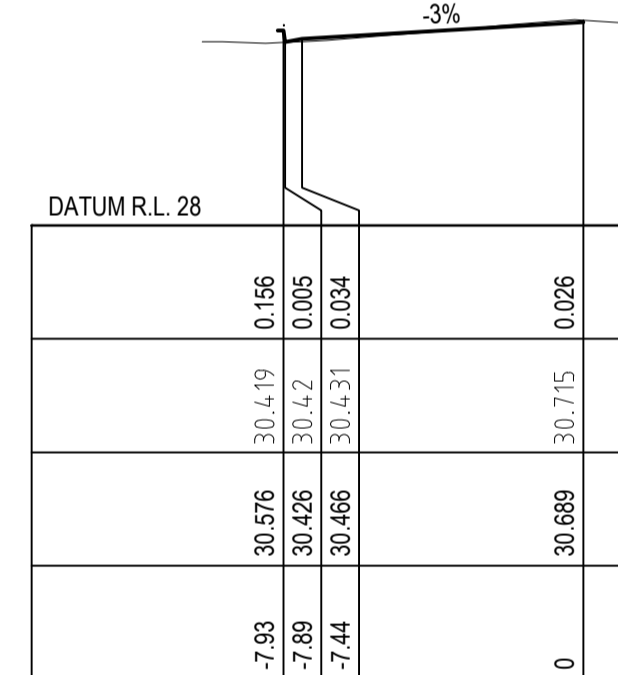
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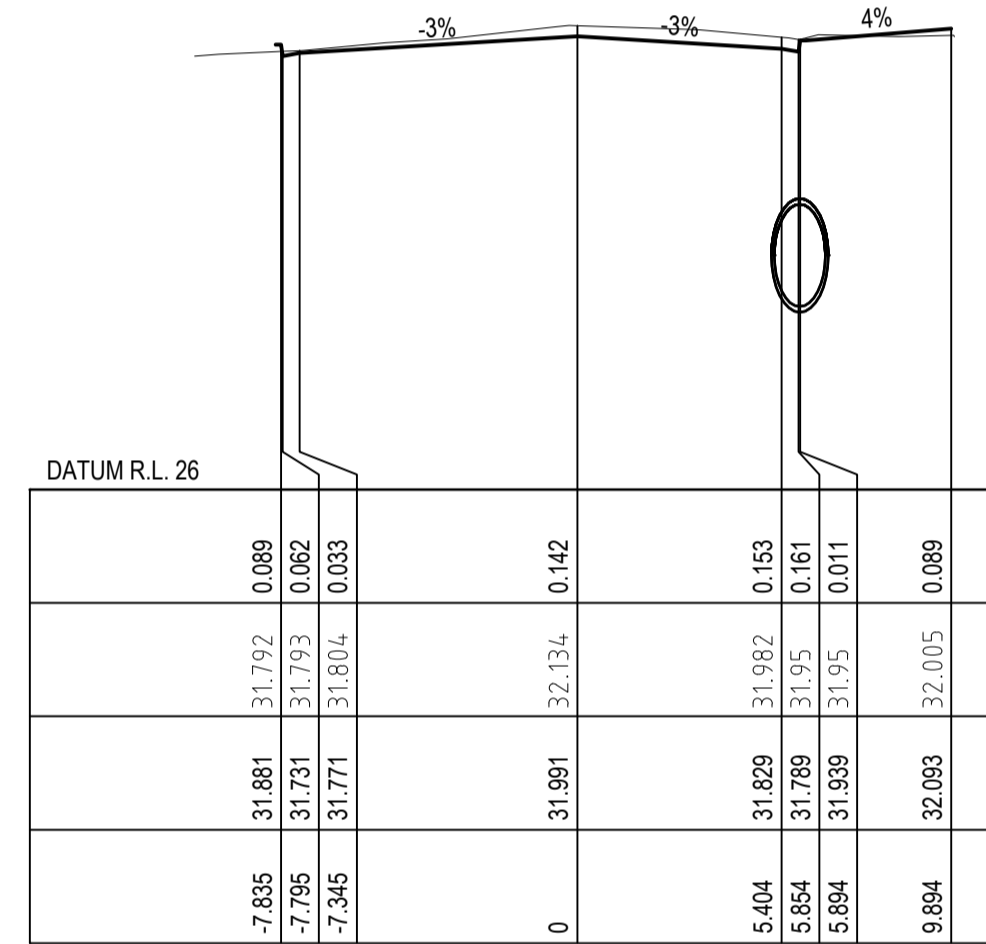
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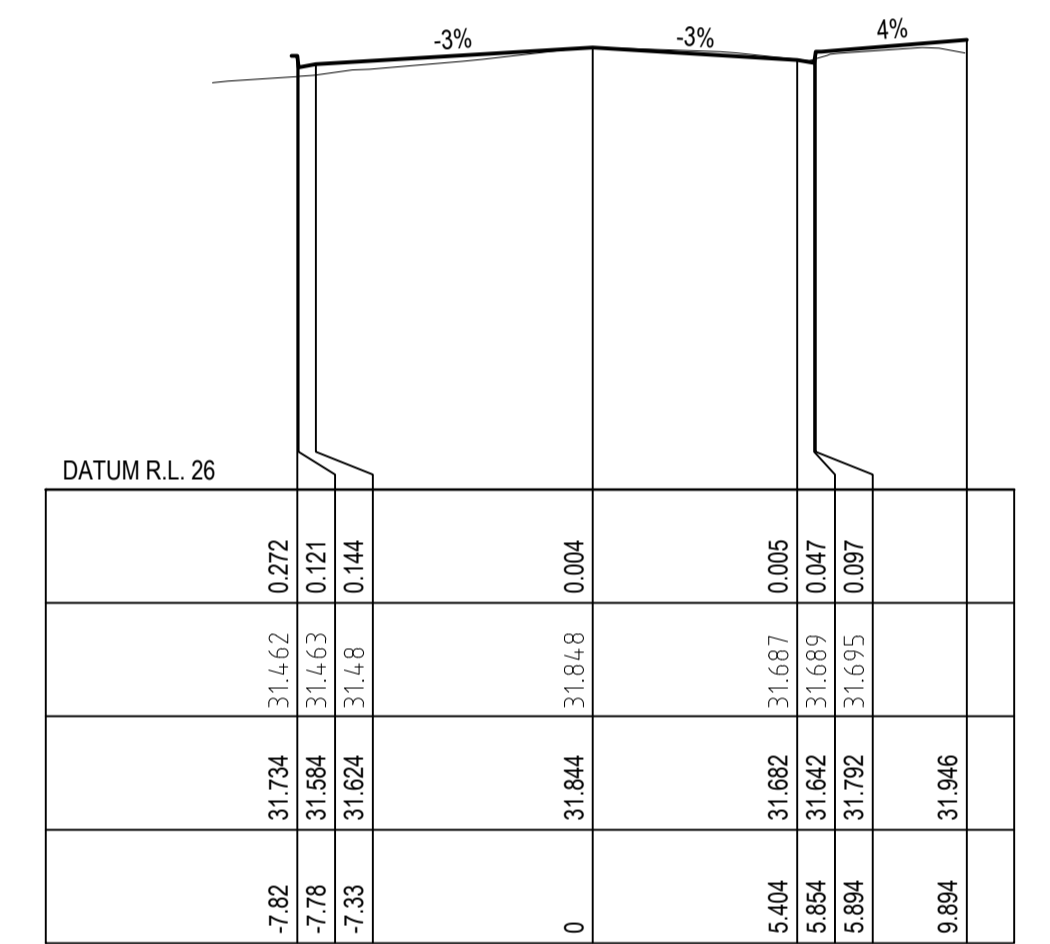
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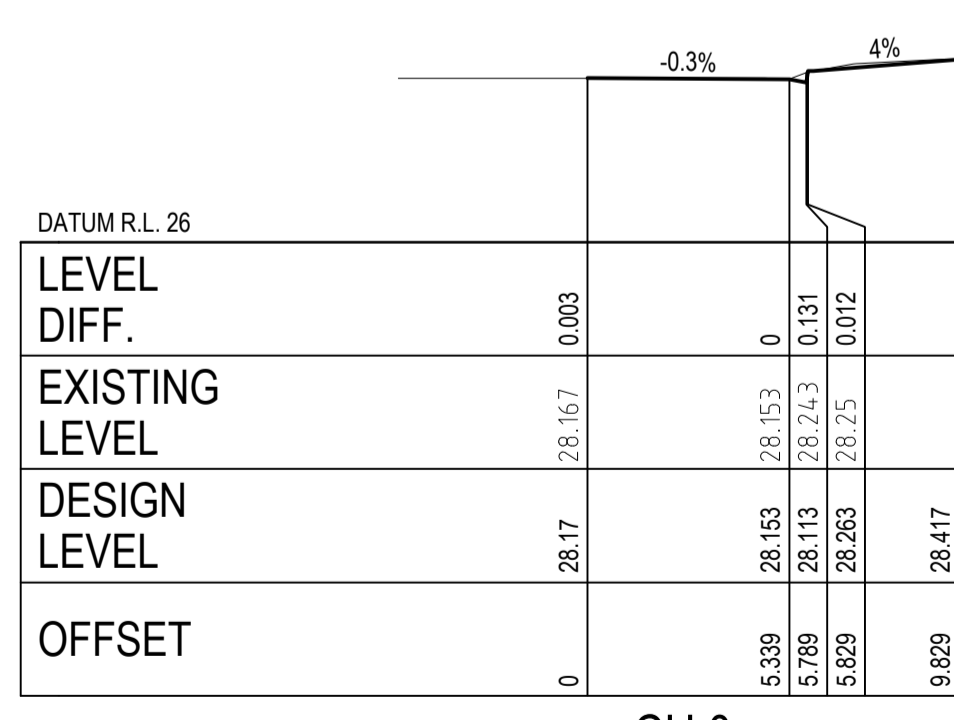
CH 135



CH 180



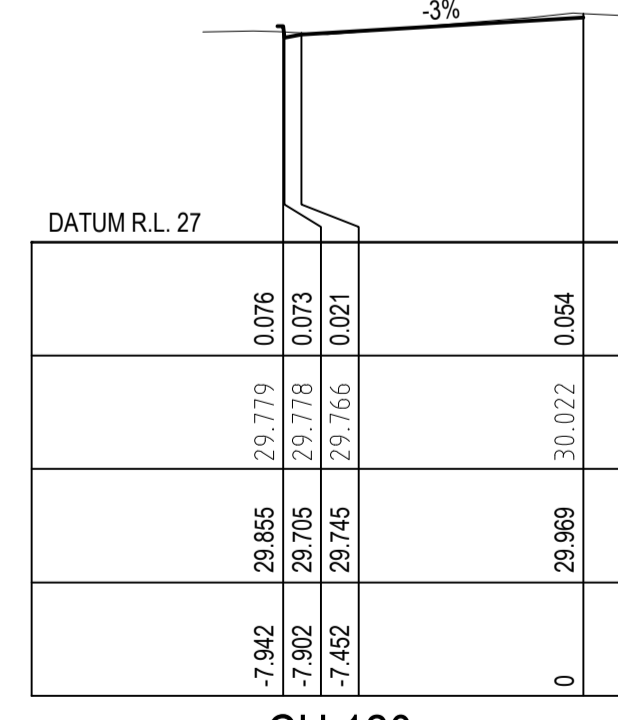
CH 212.609



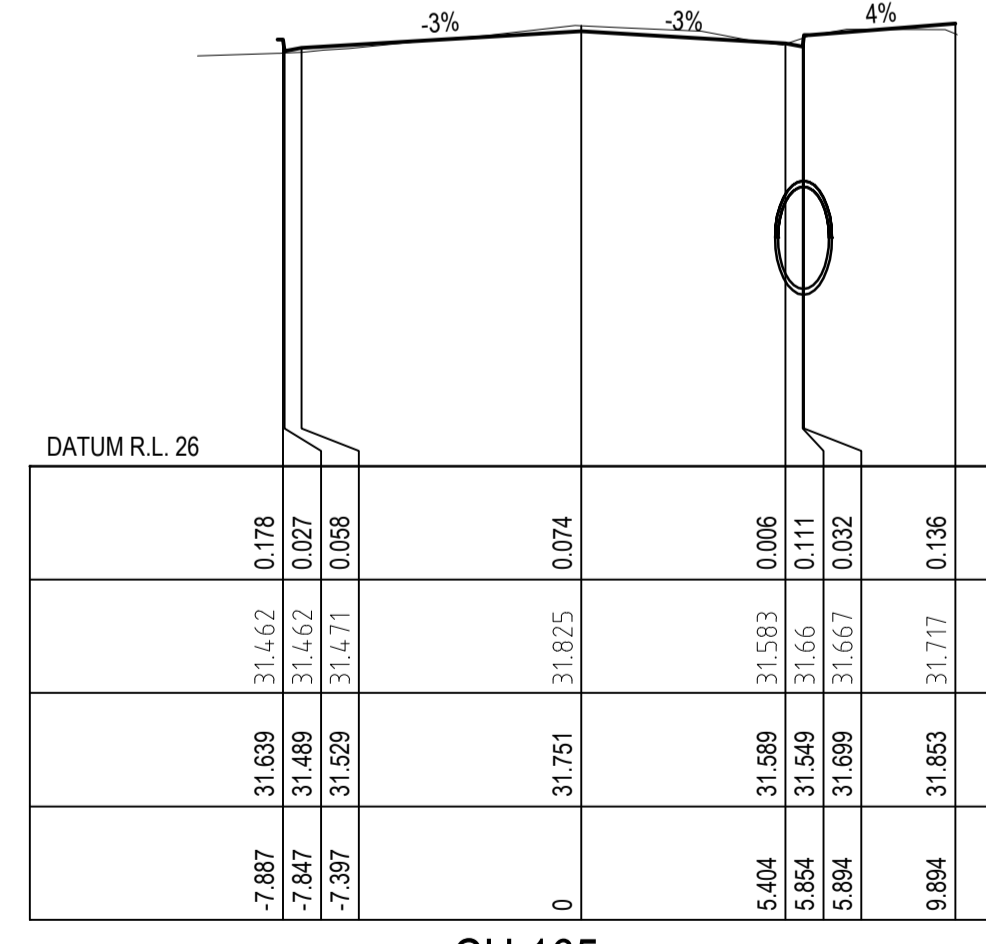
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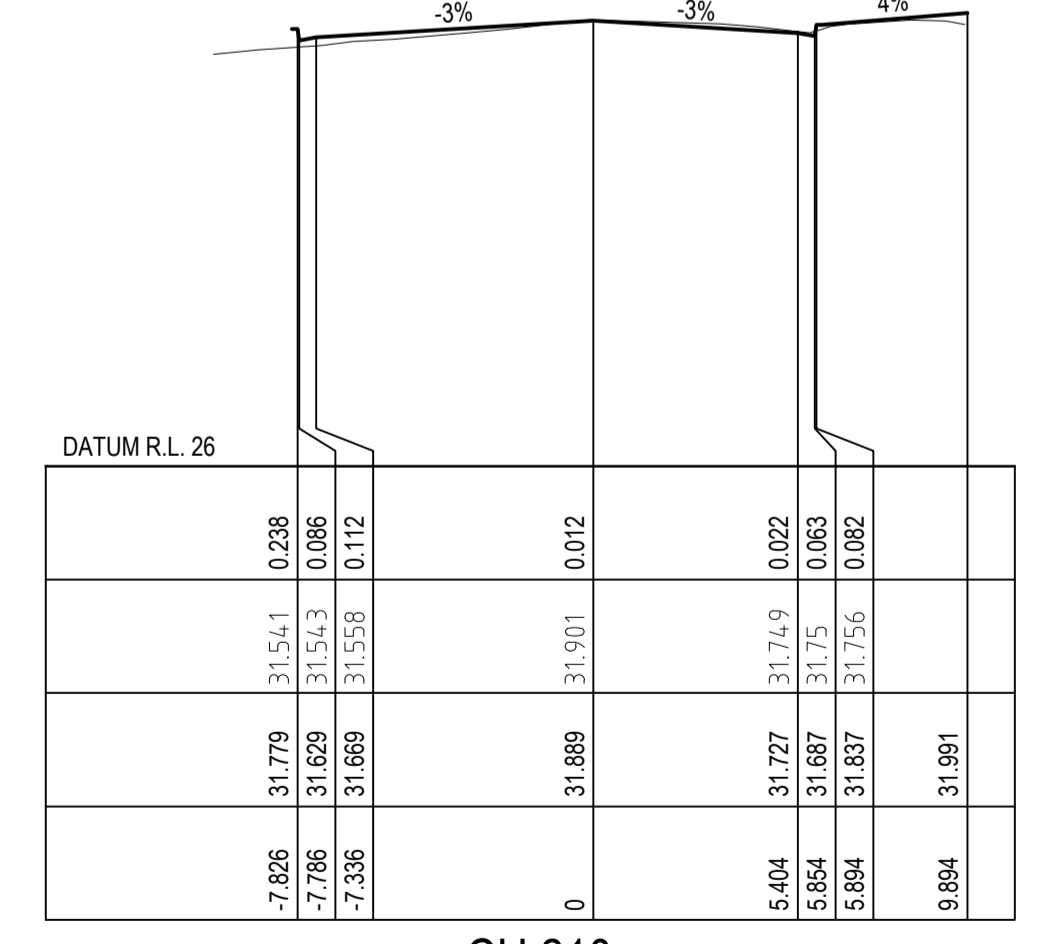
CH 60



CH 120



CH 165

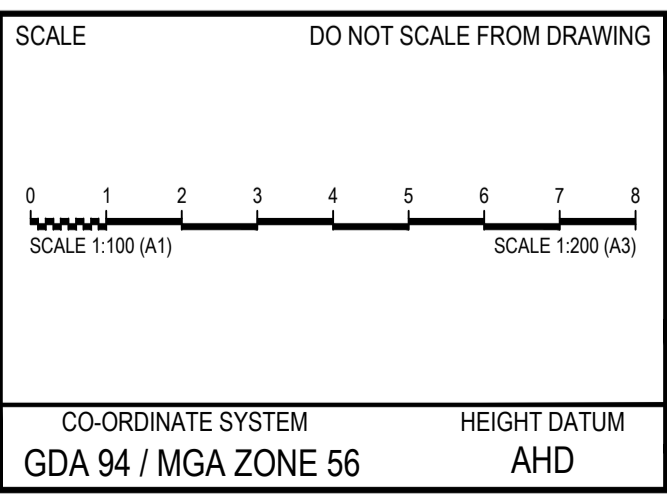


CH 210

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		DESIGNED	DY	
		DRAWN	LMCB	
		CHECKED		
		ACCEPTED	- BCC CPEng NER Certified	

CO-ORDINATE SYSTEM	HEIGHT DATUM
GDA 94 / MGA ZONE 56	AHD

Blacktown
City Council

COUNCIL CHAMBERS: 62 FLUSHCOMBE ROAD, BLACKTOWN, NSW 2148
ALL MAIL: GENERAL MANAGER, PO BOX 63, BLACKTOWN, NSW 2148
TELEPHONE: (02) 9839 6000 FAX: (02) 9831 9811 DX: 8117 BLACKTOWN

PROJECT		TITLE		REV
NW GROWTH CENTRE - ALEX AVENUE PRECINCT		RAILWAY TERRACE CROSS SECTIONS CH 0.0 - CH 212.609		A1
RAILWAY TERRACE FUTURE ROAD DESIGN SCHOFIELDS				SHEET
				4
				OF
				4
CAD FILE	FILE No	PLAN No		REV
E39-15V RT ROAD SECTS.dwg	F16/646	E39-15V		0

BLACKTOWN CITY COUNCIL

FILE NUMBER : F16/646
PLAN NUMBER : E39-15V

N/W GROWTH CENTRE - ALEX AVENUE PRECINCT CP 20 DETENTION BASIN E3.2 BASIN E3.2 RAILWAY TERRACE SCHOFIELDS



LOCALITY SKETCH
N.T.S.

IMPORTANT NOTE:

DESIGN LOCATION, DIMENSIONS AND LEVELS ARE CRITICAL TO THE HYDRAULIC PERFORMANCE OF THE SYSTEM AND SHALL NOT BE VARIED WITHOUT PRIOR APPROVAL OF COUNCILS MANAGER ASSET DESIGN. FULL WORK AS EXECUTED PLANS SHALL BE PROVIDED TO COUNCILS ASSET DESIGN MANAGER TO ALLOW ADOPTION OF REVISED FLOOD LEVELS.

WARNING !
UNDERGROUND GAS SERVICES IN VICINITY OF WORKS. HIGH FIRE HAZARD. EXERCISE EXTREME CAUTION DURING EXCAVATION.

WARNING !
UNDERGROUND TELECOM SERVICES IN VICINITY OF WORKS. EXERCISE EXTREME CAUTION DURING EXCAVATION.

WARNING !
UNDERGROUND WATER SERVICES IN VICINITY OF WORKS. EXERCISE EXTREME CAUTION DURING EXCAVATION.

WARNING !
UNDERGROUND OPTICAL CABLE SERVICE IN VICINITY OF WORKS. EXERCISE EXTREME CAUTION DURING EXCAVATION.

WARNING !
OVERHEAD AERIAL SERVICES PRESENT. EXERCISE EXTREME CAUTION WHEN USING CRANE/EXCAVATOR.

WARNING !
POSSIBLE DANGER TO PEDESTRIANS DURING CONSTRUCTION STAGE. PROVIDE & MAINTAIN FENCING & SAFE PEDESTRIAN ACCESS AROUND WORK.

WARNING !
POSSIBLE DANGER TO VEHICULAR TRAFFIC. PROVIDE & MAINTAIN BARRICADES DURING CONSTRUCTION.

WARNING !
PROVIDE SIGNS WARNING OF POSSIBLE DROWNING HAZARD WHEN IN FLOOD.

WARNING !
INTERIM FLOOD HAZARD WORKS AND STAGING REQUIRED DURING CONSTRUCTION.

CP20 ITEMS	
E2.1	CULVERT
E3.1	CULVERT
E3.2	BASIN
E3.3	BIORETENTION
E3.4	GPT

WARNING !
UNDERGROUND ELECTRICITY SERVICES IN VICINITY OF WORKS. HIGH HAZARD. EXERCISE EXTREME CAUTION DURING EXCAVATION.

WARNING !
SHORING / BENCHING WILL BE REQUIRED FOR TRENCH EXCAVATION WORKS.

WARNING !
CONFINED SPACES IDENTIFIED IN EXISTING AND PROPOSED PIT AND PIPE NETWORK.

WARNING !
MINIMUM CLEARANCES IN PLAN AND ELEVATION NEED TO BE CHECKED WITH RELEVANT SERVICE UTILITY AUTHORITY.

WARNING !
DANGEROUS UNDERGROUND SERVICES EXIST WITHIN AREA OF WORKS. COPY OF ALL SERVICE UTILITY DIAGRAMS REQUIRED ON SITE

WARNING !
CONSTRUCTION CREW TO MANAGE AND STAGE ANY INTERIM FLOOD HAZARD WORKS.



SHEET SCHEDULE	
Sheet Number	Sheet Title
GENERAL	
1	DRAWING SCHEDULE & LOCALITY SKETCH
2	GENERAL NOTES
3	SURVEY & UTILITIES PLAN
4	DEMOLITION PLAN
SOIL & WATER MANAGEMENT PLAN	
5	SOIL & WATER MANAGEMENT PLAN
6	SOIL & WATER DETAILS
BULK EARTHWORKS	
7	BULK EARTHWORKS
ENGINEERING PLANS	
8	BASIN E3.2 DETAIL PLAN
BASIN & BIORETENTION SECTIONS	
9	BIORETENTION ACCESS TRACK LONGITUDINAL SECTION
10	BASIN SECTIONS A-A & B-B
11	BASIN SECTIONS C-C & D-D
DRAINAGE	
12	LOCAL CATCHMENT PLAN
13	REGIONAL CATCHMENT PLAN
14	DRAINAGE LONGITUDINAL SECTIONS LINE Nos 1 & 2
15	DRAINAGE LONGITUDINAL SECTIONS LINE No 100
16	DRAINAGE LONGITUDINAL SECTIONS LINE Nos 106, & 3 - 6
RETAINING WALL SECTIONS & DETAILS	
17	RETAINING WALL SUBSOIL RIP RAP & STD FENCE DETAILS
STRUCTURAL DETAILS	
18	PITS 1-11, 1-13 & 1-14
19	HEADWALL 1-15 PLAN & ELEVATION
20	PIT 100-10, HEADWALL 100-12 PLAN & ELEVATION
LANDSCAPING	
21	LANDSCAPE PLAN

ISSUE 50% DESIGN

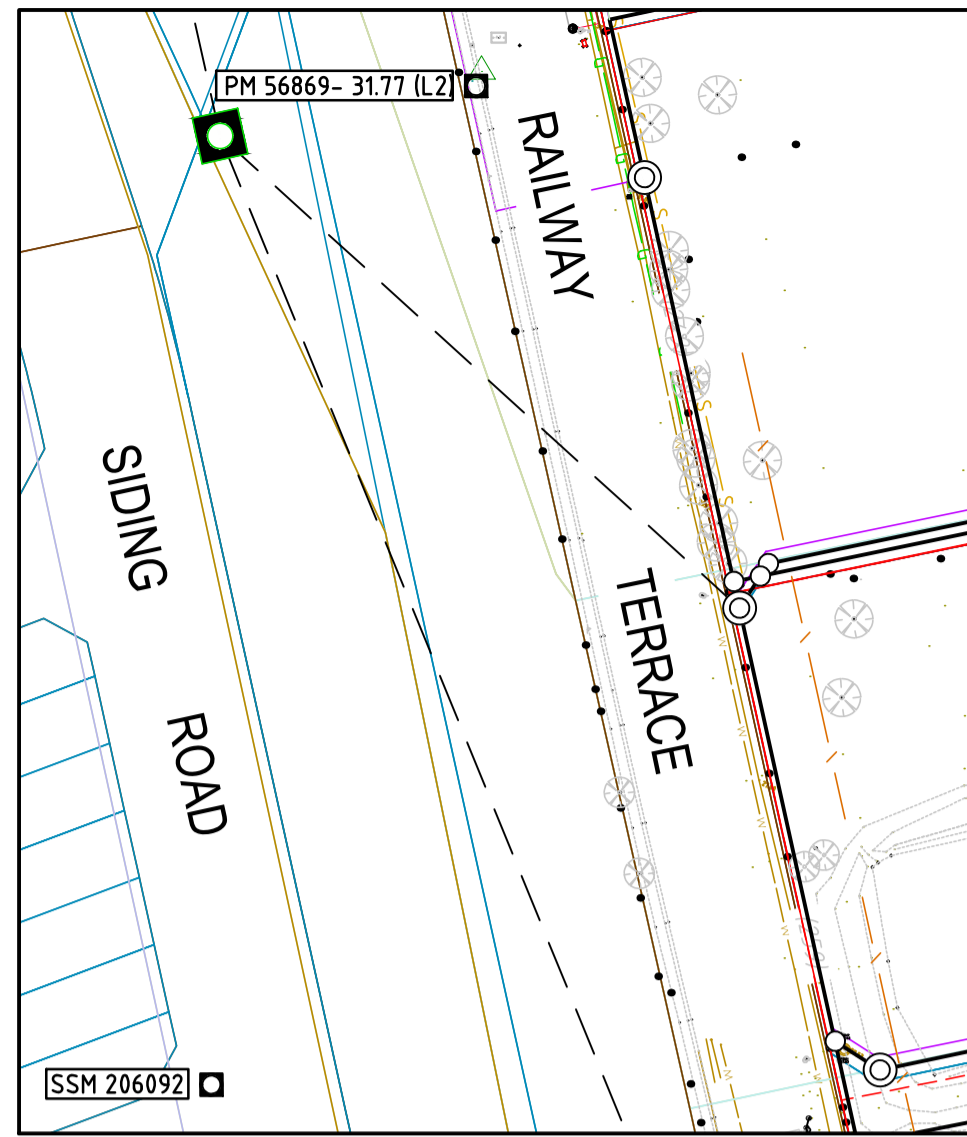
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0	02.11.21	ISSUED FOR DRP 50% REVIEW	DY	GE	02.11.21				DESIGNED	DY	
									DRAWN	LMCB	
									CHECKED		
									ACCEPTED - BCC CPEng NER Certified		

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 ISO 9001 Certified	 COUNCIL CHAMBERS: 62 FLUSHCROBE ROAD, BLACKTOWN, NSW 2148 ALL MAIL: GENERAL MANAGER, PO BOX 63, BLACKTOWN, NSW 2148 TELEPHONE: (02) 9839 6000 FAX: (02) 9831 1961 DX: 8117 BLACKTOWN
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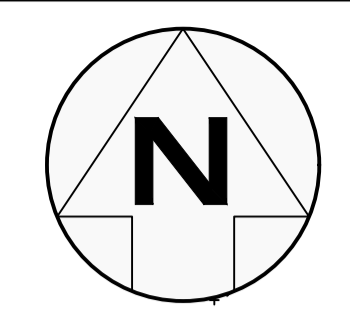
PROJECT N/W GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	A1 SHEET 1 OF 21
TITLE DRAWING SCHEDULE & LOCALITY SKETCH	
CAD FILE E39-15V 001.dwg	FILE No F16/646
PLAN No E39-15V	REV 1



PUBLIC UTILITIES LEGEND	
WATER MAINS	
RECYCLED WATER	
HYDRANT	
STOP VALVE	
WATER METER	
WATER TAP	
STORMWATER PIT	

SEWER	
SEWER LAMPHOLE	
SEWER MANHOLE	
GAS	
GAS VALVE	
ELECTRICITY	
ELECTRICITY PIT	
STAY POLE	

POWER POLE	
POWER LIGHT POLE	
LIGHT POLE	
TELEPHONE TWIN PIT	
STD MAIN PIT (1.1x1.1)	
TELSTRA	
TELSTRA PILLAR	
TELSTRA POLE	



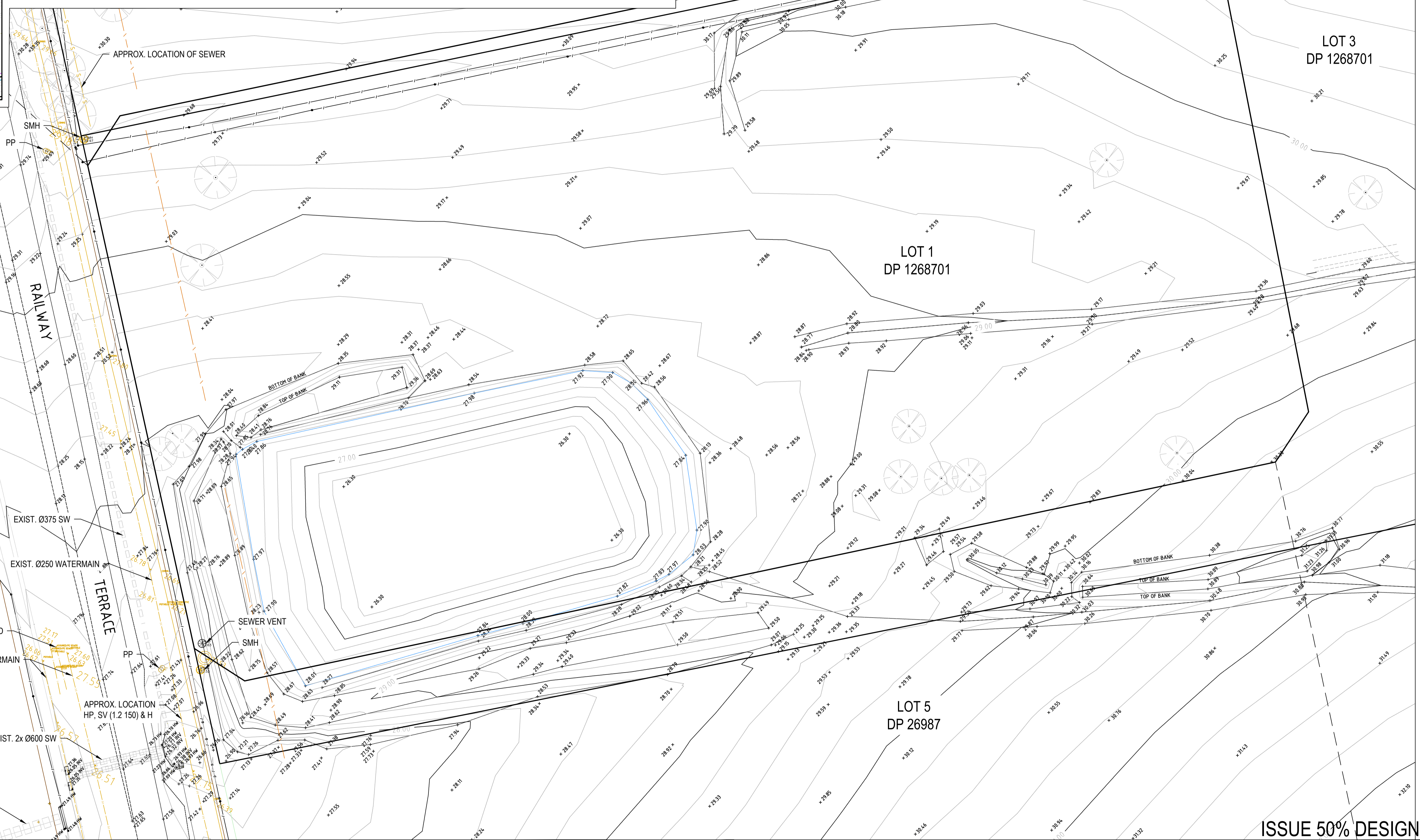
LEGEND	
	EXISTING SPOT LEVEL & CONTOUR
	EXISTING CADASTRAL BOUNDARY
	EXISTING VEGETATION
	PERMANENT MARK (PM) STATE SURVEY MARK

PLAN SCALE 1:1000

SURVEY COORDINATE TABLE

POINT No.	EASTING	NORTHING	RL
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SSM206292	303032.000	6268155.000	

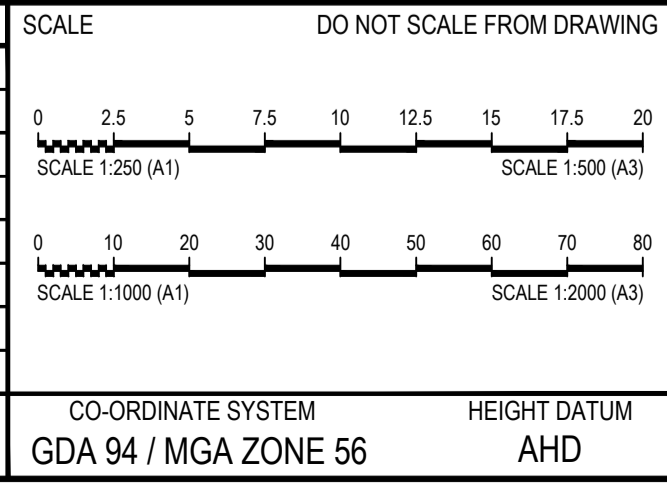
NOTES
 1. ALL COORDINATES TO GDA94 MGA ZONE 56.
 2. SITE SURVEY CARRIED OUT BY D HENRY FILE ?.dxf
 3. EXISTING CONTOURS COVERING SITE OF WORKS ARE DERIVED FROM BCC SURVEY (AS ABOVE).



ISSUE 50% DESIGN

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0	02.11.21	ISSUED FOR DRP 50% REVIEW	DY	GE	02.11.21



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DRAWINGS VERIFIED BY	

TITLE	NAME	DATE
SURVEYED	B.C.C.	
DESIGNED	DY	
DRAWN	LMCB	
CHECKED		
ACCEPTED - BCC CP/Eng NER Certified		



PROJECT NW GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS		A1
TITLE SURVEY & UTILITIES PLAN		SHEET
CAD FILE E39-15V 003.dwg		3
FILE No F16/646	PLAN No E39-15V	OF
		21
		REV 1

DRAWING FILE LOCATION / NAME
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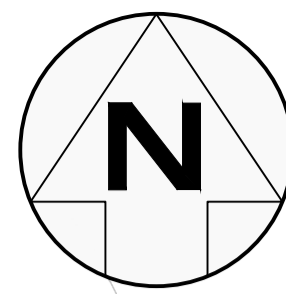
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PLOT BY
mcbri1

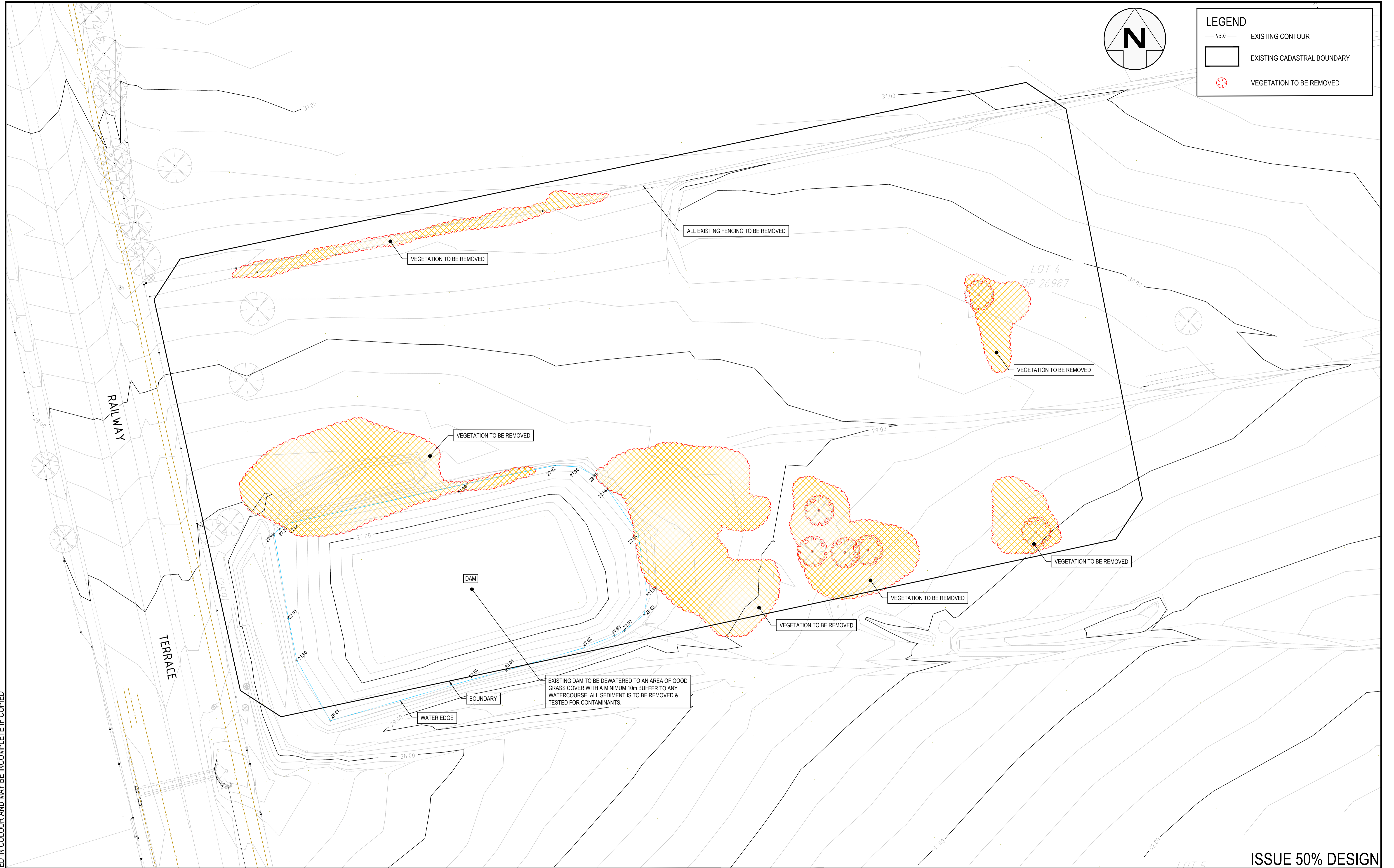
CO-ORDINATE SYSTEM
GDA 94 / MGA ZONE 56

HEIGHT DATUM
AHD

COUNCIL CHAMBERS: 62 FLUSHCROBE ROAD, BLACKTOWN, NSW 2148
 ALL MAIL: GENERAL MANAGER, PO BOX 63, BLACKTOWN, NSW 2149
 TELEPHONE: (02) 9839 6000 FAX: (02) 9831 1961 DX: 8117 BLACKTOWN



LEGEND	
	EXISTING CONTOUR
	EXISTING CADASTRAL BOUNDARY
	VEGETATION TO BE REMOVED



ISSUE 50% DESIGN

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REV	DATE	DESCRIPTION	CHECKED	APPROVED	DATE
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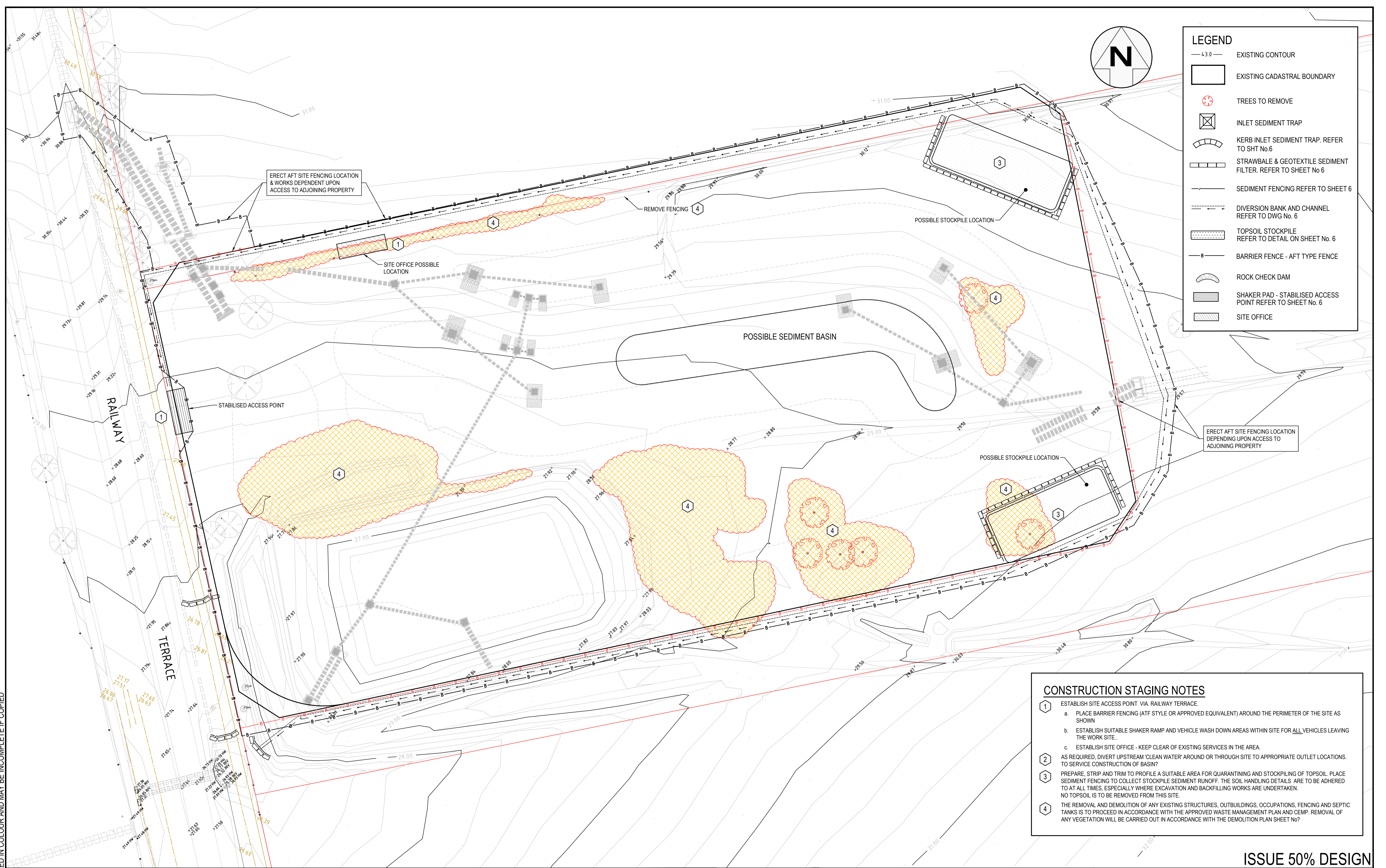
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DRAWINGS / DESIGN PREPARED BY	
DRAWINGS VERIFIED BY	

TITLE	NAME	DATE
SURVEYED	B.C.C.	
DESIGNED	DY	
DRAWN	LMCB	
CHECKED		
ACCEPTED - BCC CPEng NER Certified		



PROJECT NW GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS		A1 SHEET 4 OF 21
TITLE DEMOLITION PLAN		
CAD FILE E39-15V 004.dwg	FILE No F16/646	PLAN No E39-15V REV 1



LEGEND	
	EXISTING CONTOUR
	EXISTING CADASTRAL BOUNDARY
	TREES TO REMOVE
	INLET SEDIMENT TRAP
	KERB INLET SEDIMENT TRAP. REFER TO SHT No.6
	STRAWBALE & GEOTEXTILE SEDIMENT FILTER. REFER TO SHEET No.6
	SEDIMENT FENCING REFER TO SHEET 6
	DIVERSION BANK AND CHANNEL REFER TO DWG No. 6
	TOPSOIL STOCKPILE REFER TO DETAIL ON SHEET No. 6
	BARRIER FENCE - AFT TYPE FENCE
	ROCK CHECK DAM
	SHAKER PAD - STABILISED ACCESS POINT REFER TO SHEET No. 6
	SITE OFFICE

CONSTRUCTION STAGING NOTES	
1	ESTABLISH SITE ACCESS POINT VIA RAILWAY TERRACE. a. PLACE BARRIER FENCING (ATF STYLE OR APPROVED EQUIVALENT) AROUND THE PERIMETER OF THE SITE AS SHOWN b. ESTABLISH SUITABLE SHAKER RAMP AND VEHICLE WASH DOWN AREAS WITHIN SITE FOR ALL VEHICLES LEAVING THE WORK SITE. c. ESTABLISH SITE OFFICE - KEEP CLEAR OF EXISTING SERVICES IN THE AREA.
2	AS REQUIRED, DIVERT UPSTREAM 'CLEAN WATER' AROUND OR THROUGH SITE TO APPROPRIATE OUTLET LOCATIONS. TO SERVICE CONSTRUCTION OF BASIN?
3	PREPARE, STRIP AND TRIM TO PROFILE A SUITABLE AREA FOR QUARANTINING AND STOCKPILING OF TOPSOIL. PLACE SEDIMENT FENCING TO COLLECT STOCKPILE SEDIMENT RUNOFF. THE SOIL HANDLING DETAILS ARE TO BE ADHERED TO AT ALL TIMES, ESPECIALLY WHERE EXCAVATION AND BACKFILLING WORKS ARE UNDERTAKEN. NO TOPSOIL IS TO BE REMOVED FROM THIS SITE.
4	THE REMOVAL AND DEMOLITION OF ANY EXISTING STRUCTURES, OUTBUILDINGS, OCCUPATIONS, FENCING AND SEPTIC TANKS IS TO PROCEED IN ACCORDANCE WITH THE APPROVED WASTE MANAGEMENT PLAN AND CEMP. REMOVAL OF ANY VEGETATION WILL BE CARRIED OUT IN ACCORDANCE WITH THE DEMOLITION PLAN SHEET No.7

ISSUE 50% DESIGN

REV	DATE	DESCRIPTION	CHECKED	APPROVED	DATE
1	16.02.22	ISSUED FOR 50% DESIGN REVIEW	DY	GE	16.02.22
0	02.11.21	ISSUED FOR DRP 50% REVIEW	DY	GE	02.11.21

SCALE	DO NOT SCALE FROM DRAWING
<p>SCALE 1:250 (A1) SCALE 1:500 (A3)</p>	

DRAWINGS / DESIGN PREPARED BY	
DRAWINGS VERIFIED BY	

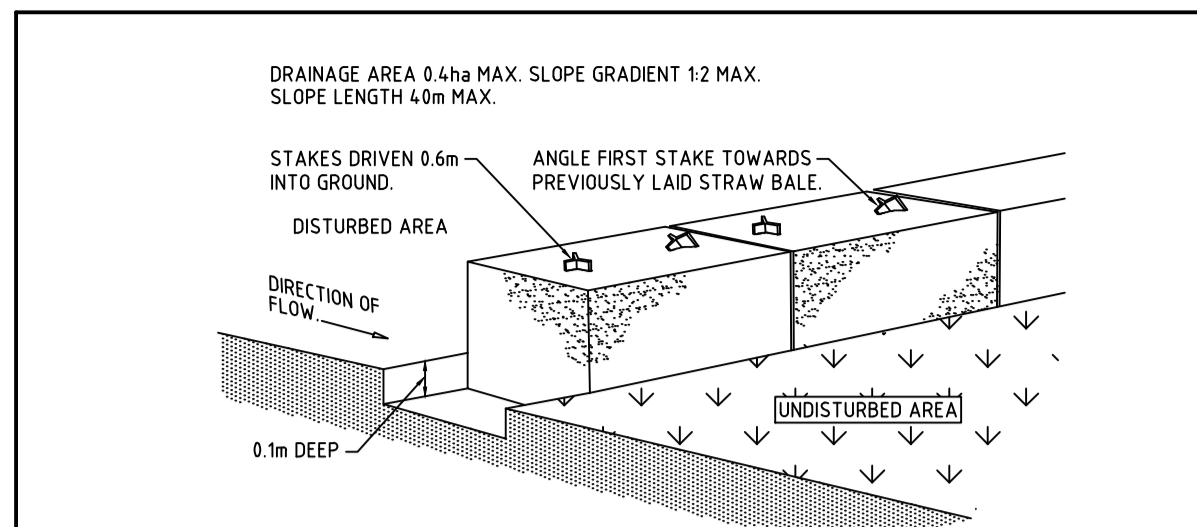
TITLE	NAME	DATE
SURVEYED	B.C.C.	
DESIGNED	DY	
DRAWN	LMCB	
CHECKED		
ACCEPTED - BCC CP/Eng NER Certified		



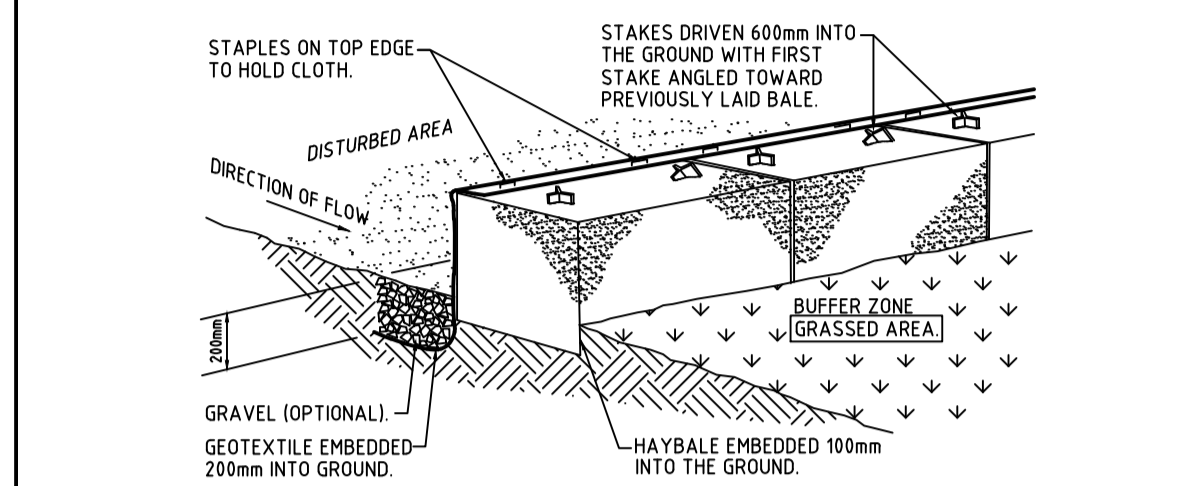
PROJECT	TITLE	CAD FILE	FILE No	PLAN No	REV
NW GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	SOIL & WATER MANAGEMENT PLAN	E39-15V SOIL & WATER.dwg	F16/646	E39-15V	1

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A1
SHEET
5
OF
21

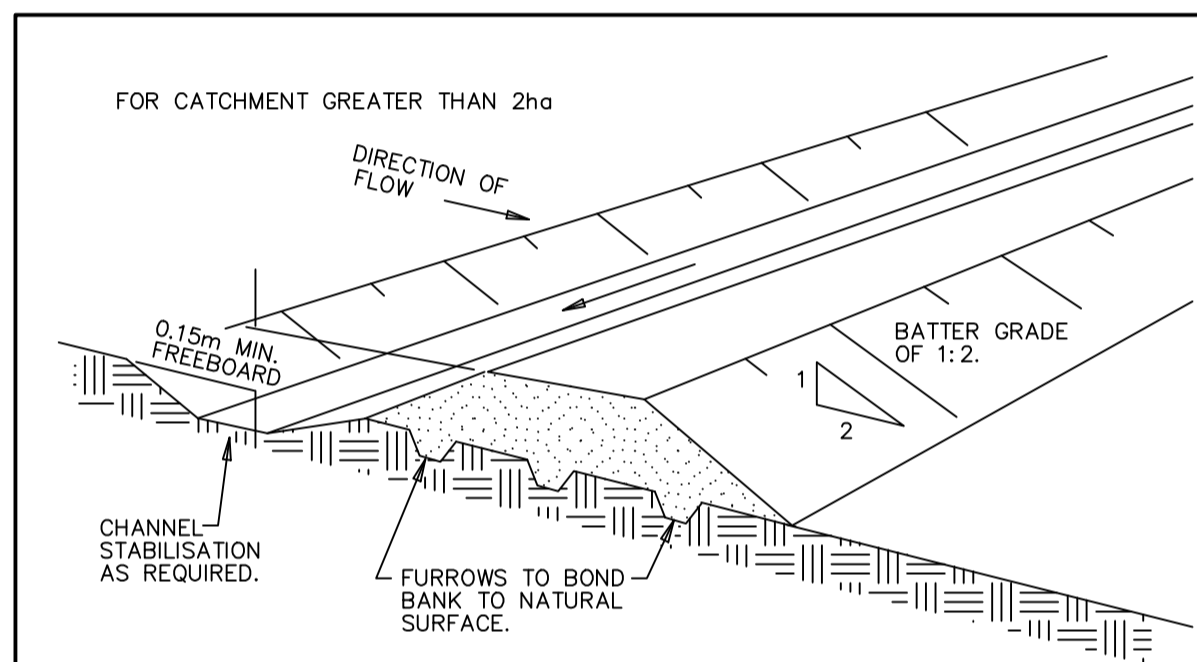


STRAW BALE SEDIMENT FILTER

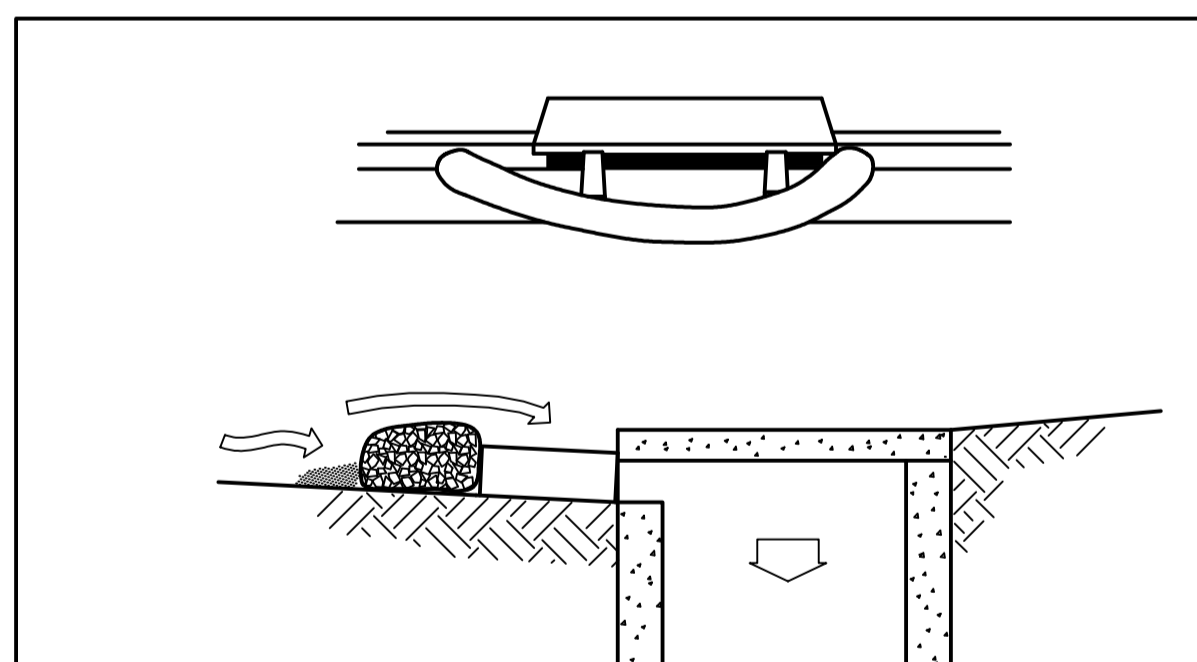


STRAW BALE AND GEOTEXTILE SEDIMENT FILTER

STRAWBALE SEDIMENT FILTER



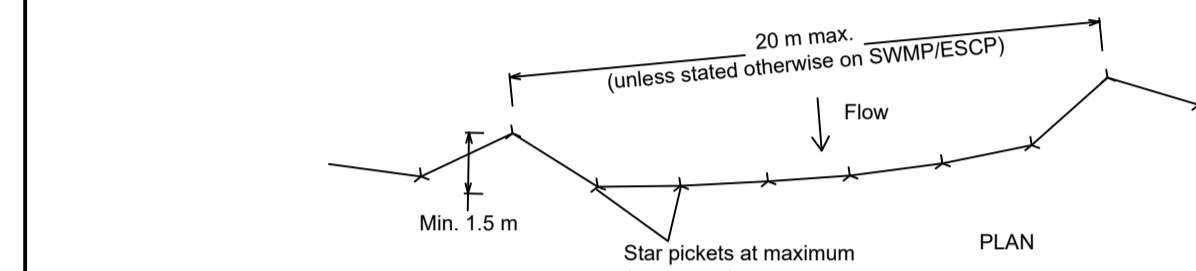
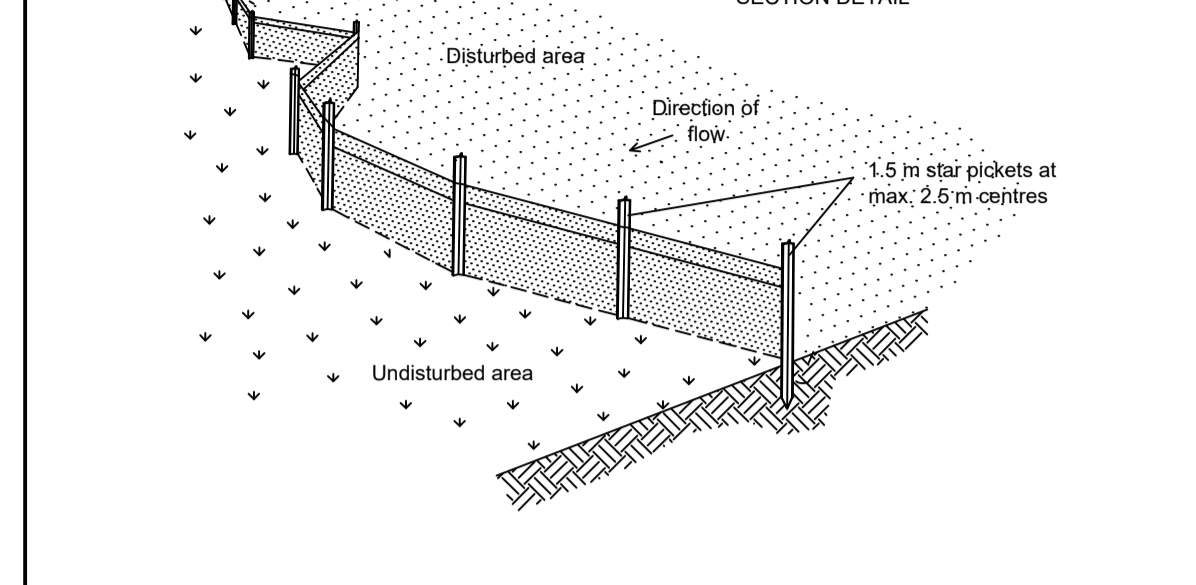
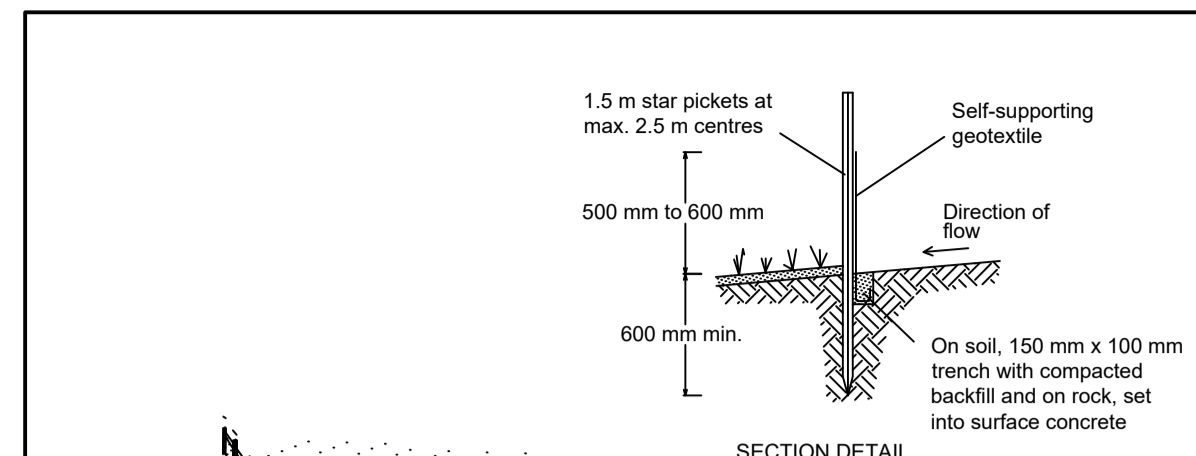
DIVERSION BANK AND CHANNEL



Construction Notes

1. Fabricate a sleeve made from geotextile or wire mesh longer than the length of the inlet pit and fill it with 25mm to 50 mm gravel.
2. Form an elliptical cross-section about 150mm high x 400 mm wide.
3. Place the filter at the opening leaving at least a 100-mm space between it and the kerb inlet. maintain the opening with spacer blocks.
4. Form a seal with the kerb to prevent sediment bypassing the filter.
5. Sandbags filled with gravel can substitute for the mesh or geotextile providing they are placed so that they firmly abut each other and sediment-laden waters cannot pass between.

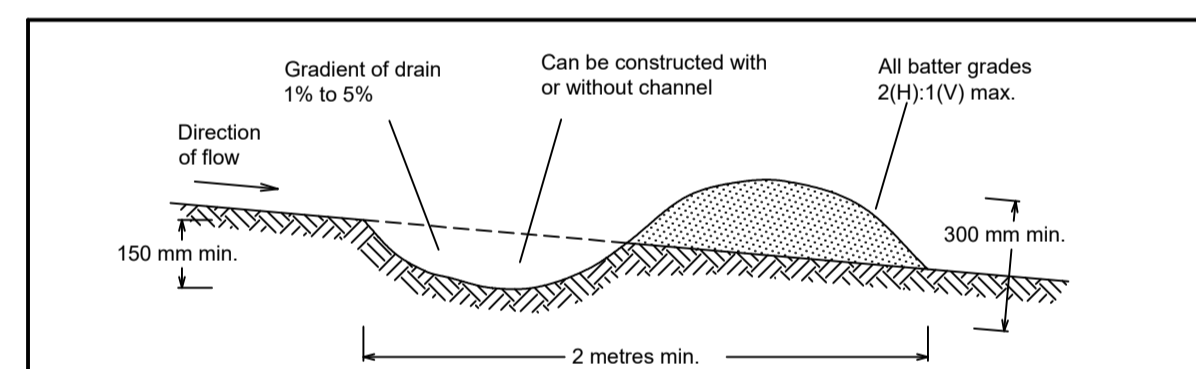
SANDBAG KERB INLET SEDIMENT TRAP DETAIL



Construction Notes

1. Construct sediment fences as close as possible to being parallel to the contours of the site, but with small returns as shown in the drawing to limit the catchment area of any one section. The catchment area should be small enough to limit water flow if concentrated at one point to 50 litres per second in the design storm event, usually the 10-year event.
2. Cut a 150-mm deep trench along the upslope line of the fence for the bottom of the fabric to be entrenched.
3. Drive 1.5 metre long star pickets into ground at 2.5 metre intervals (max) at the downslope edge of the trench. Ensure any star pickets are fitted with safety caps.
4. Fix self-supporting geotextile to the upslope side of the posts ensuring it goes to the base of the trench. Fix the geotextile with wire ties or as recommended by the manufacturer. Only use geotextile specifically produced for sediment fencing. The use of shade cloth for this purpose is not satisfactory.
5. Join sections of fabric at a support post with a 150-mm overlap.
6. Backfill the trench over the base of the fabric and compact it thoroughly over the geotextile.

SEDIMENT FENCE DETAIL

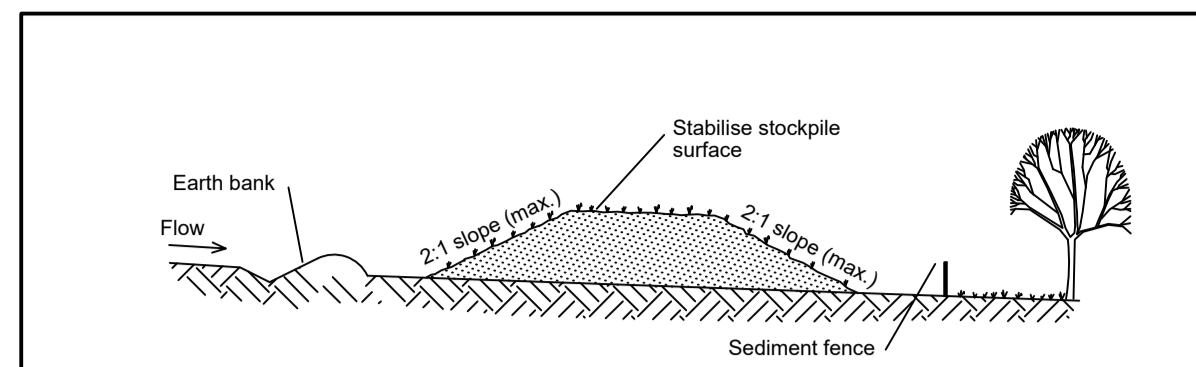


NOTE: Only to be used as temporary bank where maximum upslope length is 80 metres.

Construction Notes

1. Build with gradients between 1 percent and 5 percent.
2. Avoid removing trees and shrubs if possible - work around them.
3. Ensure the structures are free of projections or other irregularities that could impede water flow.
4. Build the drains with circular, parabolic or trapezoidal cross sections, not V shaped.
5. Ensure the banks are properly compacted to prevent failure.
6. Complete permanent or temporary stabilisation within 10 days of construction.

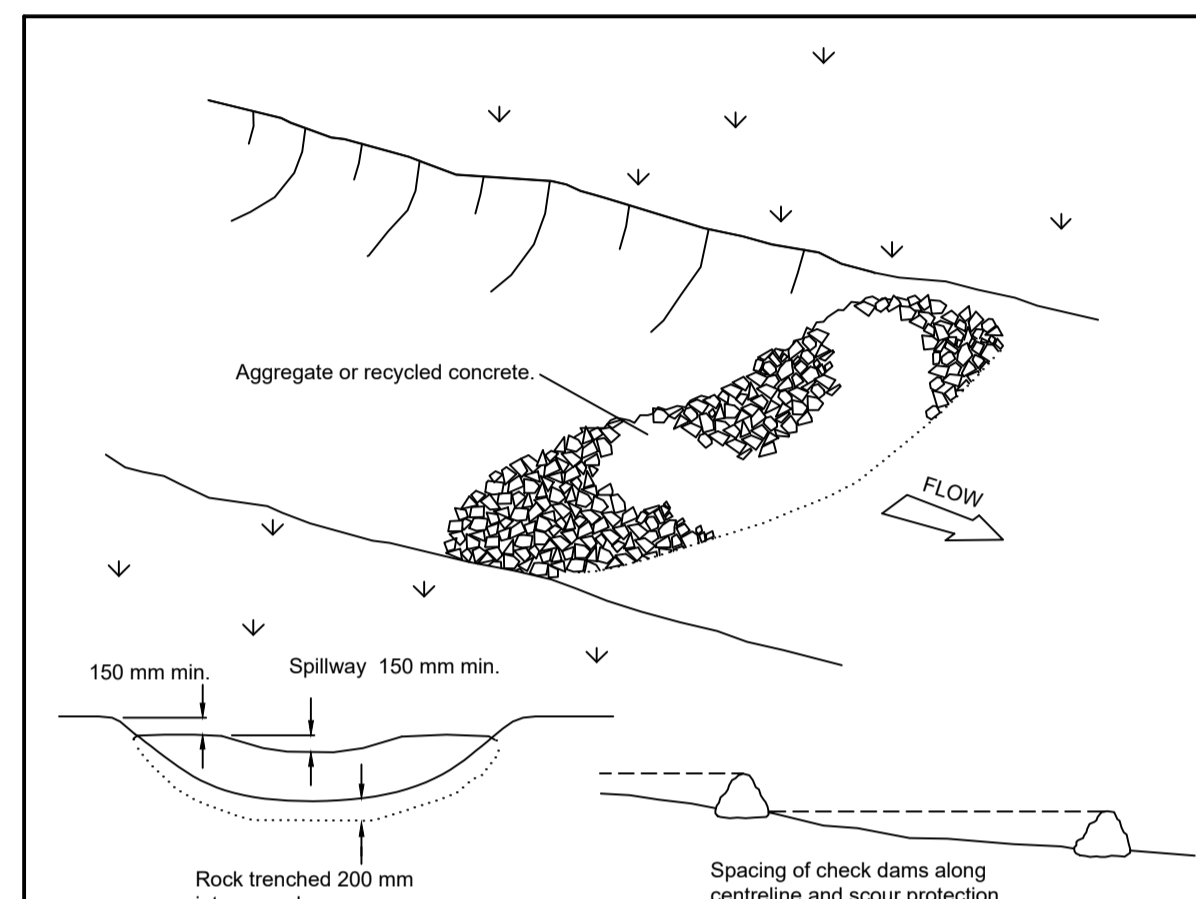
CATCH DRAIN DETAIL



Construction Notes

1. Place stockpiles more than 2 (preferably 5) metres from existing vegetation, concentrated water flow, roads and hazard areas.
2. Construct on the contour as low, flat, elongated mounds.
3. Where there is sufficient area, topsoil stockpiles shall be less than 2 metres in height.
4. Where they are to be in place for more than 10 days, stabilise following the approved ESCP or SWMP to reduce the C-factor to less than 0.10.
5. Construct earth banks (Standard Drawing 5-5) on the upslope side to divert water around stockpiles and sediment fences (Standard Drawing 6-8) 1 to 2 metres downslope.

STOCKPILE TREATMENT DETAIL



Construction Notes

1. Check dams can be built with various materials, including rocks, logs, sandbags and straw bales. The maintenance program should ensure their integrity is retained, especially where constructed with straw bales. In the case of bales, this might require their replacement each two to four months.
2. Trench the check dam 200 mm into the ground across its whole width. Where rock is used, fill the trenches to at least 100 mm above the ground surface to reduce the risk of undercutting.
3. Normally, their maximum height should not exceed 600 mm above the gully floor. The centre should act as a spillway, being at least 150 mm lower than the outer edges.
4. Space the dams so the toe of the upstream dam is level with the spillway of the next downstream dam.

ROCK CHECK DAM DETAIL SD 5-4

STABILISED ACCESS POINT - TYPE 1

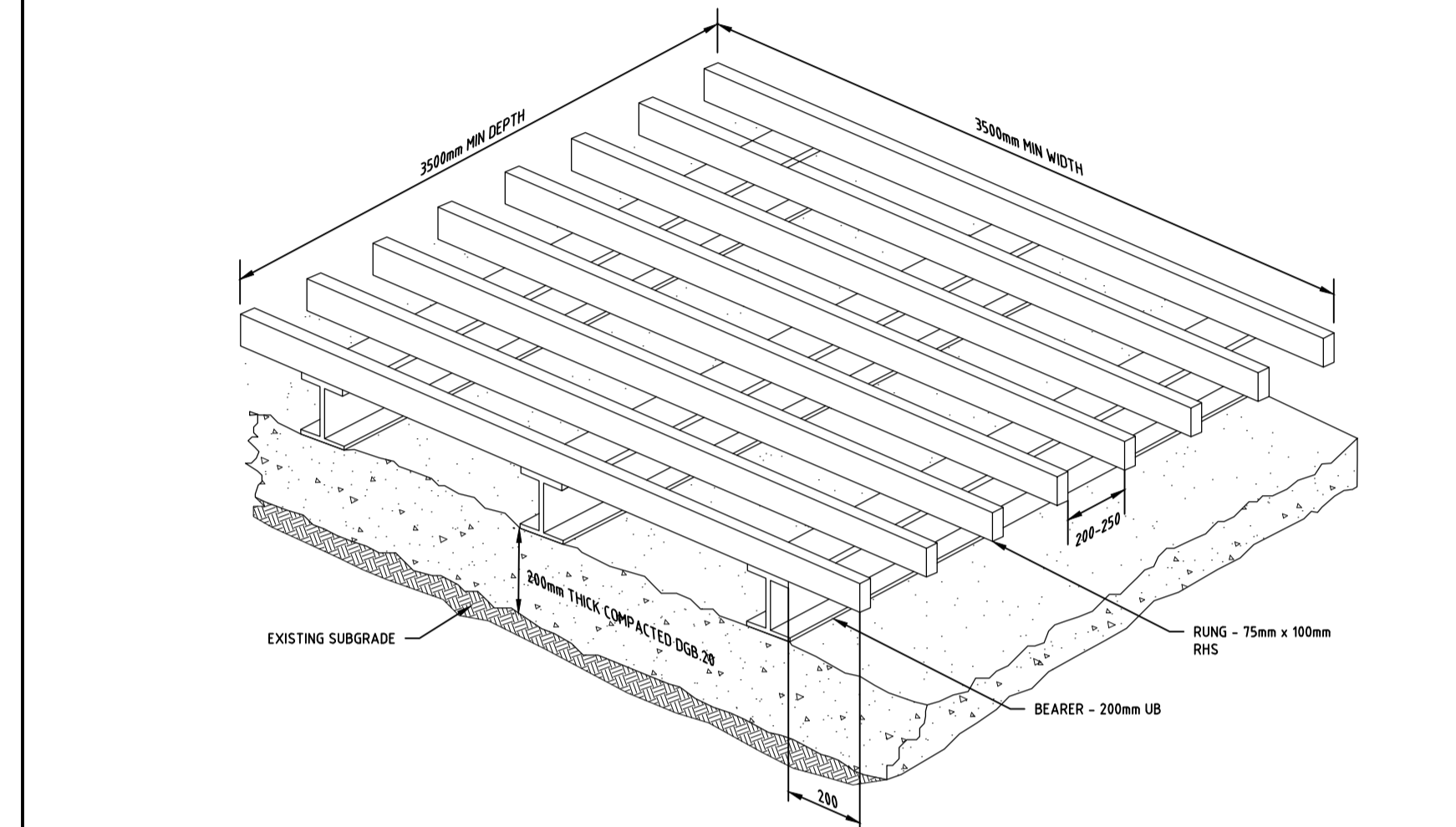
SHAKER PAD (CATTLE GRID)

A CORRECTLY DESIGNED AND INSTALLED SHAKER PAD WILL ASSIST IN PREVENTING SEDIMENT TRANSFER FROM A SITE. ANY STABILISED ACCESS POINT (SAP) CAN BE DESIGNED WITH A SHAKER PAD (COMPULSORY IN TYPE II SAP'S)

SHAKER PADS CAN BE DESIGNED AND CONSTRUCTED TO ENABLE RE-USE ON FUTURE PROJECTS.

- THE SHAKER PAD:**
- MUST BE DESIGNED AND CERTIFIED BY A PRACTICING STRUCTURAL ENGINEER. THE CERTIFIED DESIGN SHOULD BE SUBMITTED WITH THE RELEVANT APPLICATION.
 - CAN BE CONSTRUCTED FROM ANY SUITABLE MATERIAL.
 - MUST BE LOCATED ON A SUITABLY PREPARED AND COMPACTED SUB-GRADE/BASE MATERIAL.
 - MUST BE SITUATED SUCH THAT THE RUNGS OF THE SHAKER PAD ARE LEVEL WITH THE ADJOINING NATURAL SURFACE.
 - MUST BE A MINIMUM OF 3.5m IN LENGTH.
 - MUST BE A MINIMUM OF 3.5m IN WIDTH.
 - MUST HAVE CLEAR SPACING BETWEEN RUNGS OF 200 - 250mm.
 - RUNGS MUST HAVE A MAXIMUM WIDTH (BEARING AREA) OF 75mm.
 - MUST HAVE A MINIMUM CLEAR DEPTH OF 300mm IE FROM THE TOP OF THE RUNG TO THE FINISHED SUB-GRADE/BASE LEVEL.

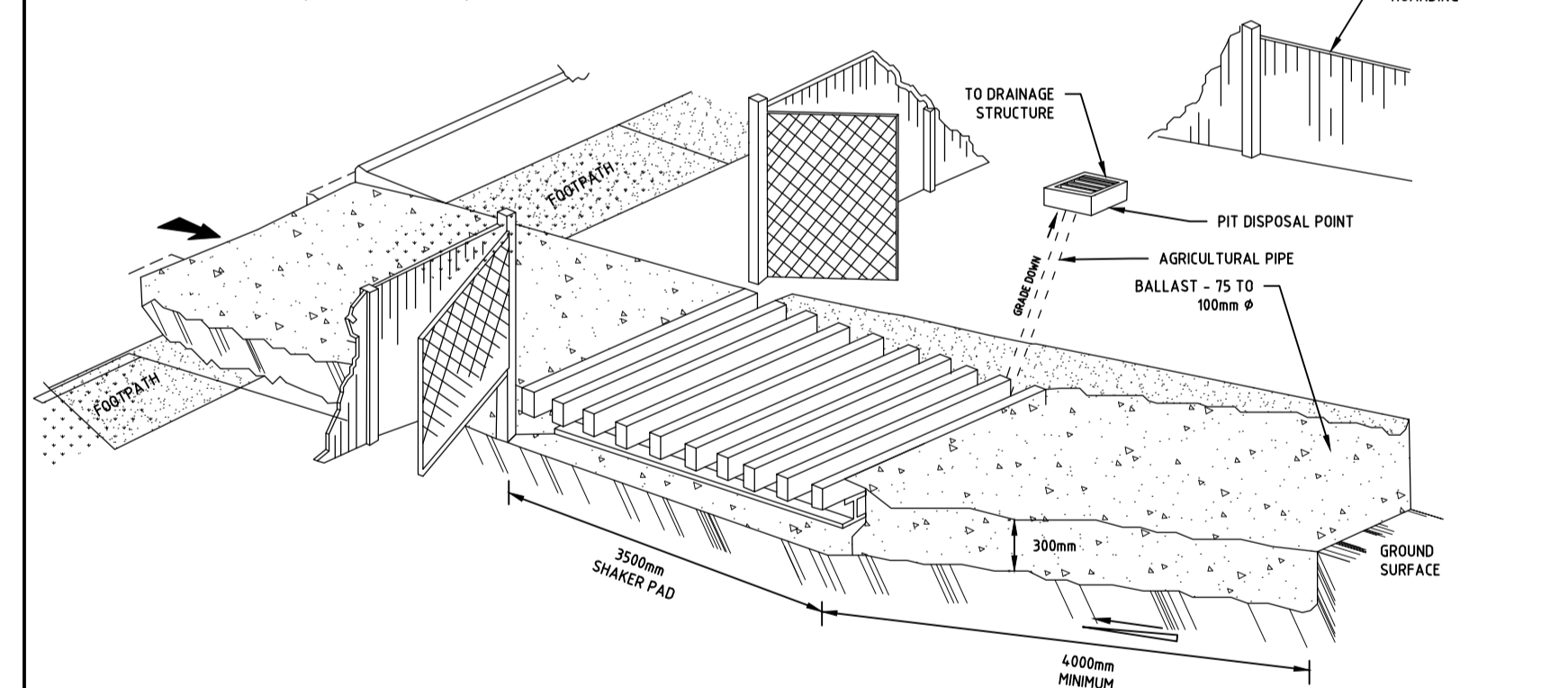
THE SHAKER PAD MUST BE PROVIDED WITH SUITABLE BARRIERS AT THE SIDES TO ENSURE THAT ALL TYRES OF VEHICLES LEAVING THE SITE TRAVERSE THE DEVICE.



STABILISED ACCESS POINT - TYPE 2

TYPE II SAP

THE TYPE II SAP DESIGN IS MORE DEFINED IN THAT IT REQUIRES AN AREA OF BALLAST WITHIN THE SITE COMBINED WITH A SHAKER PAD. ADJACENT THE SHAKER PAD AND IN THE PUBLIC WAY IS A TEMPORARY (CONCRETE) VEHICULAR CROSSING. (SEE DIAGRAM)



IN BOTH TYPE I AND TYPE II SAP'S, THE TEMPORARY VEHICULAR CROSSING MUST:

- CONNECT TO AN EXISTING GUTTER LAYBACK (WHERE THE KERB AND GUTTER EXIST). IF A GUTTER LAYBACK DOES NOT EXIST THEN THE CONNECTION MUST BE MADE TO THE GUTTER BY REMOVING THE ADJACENT KERB SECTION ONLY.
- CONNECT TO A DISH CROSSING (WHERE KERB AND GUTTER DOES NOT EXIST). IF A DISH CROSSING DOES NOT EXIST, THEN IT MUST BE CONSTRUCTED IN ACCORDANCE WITH DETAILS CONTAINED IN COUNCIL'S ISSUED FOOTPATH CROSSING LEVELS.

IT SHOULD BE NOTED THAT THESE TYPES OF SAPS ARE CONSIDERED TO BE APPLICABLE FOR THE MAJORITY OF ACTIVITIES HOWEVER SOME SITES MAY REQUIRE SPECIAL CONSIDERATION.

STABILISED ACCESS POINT

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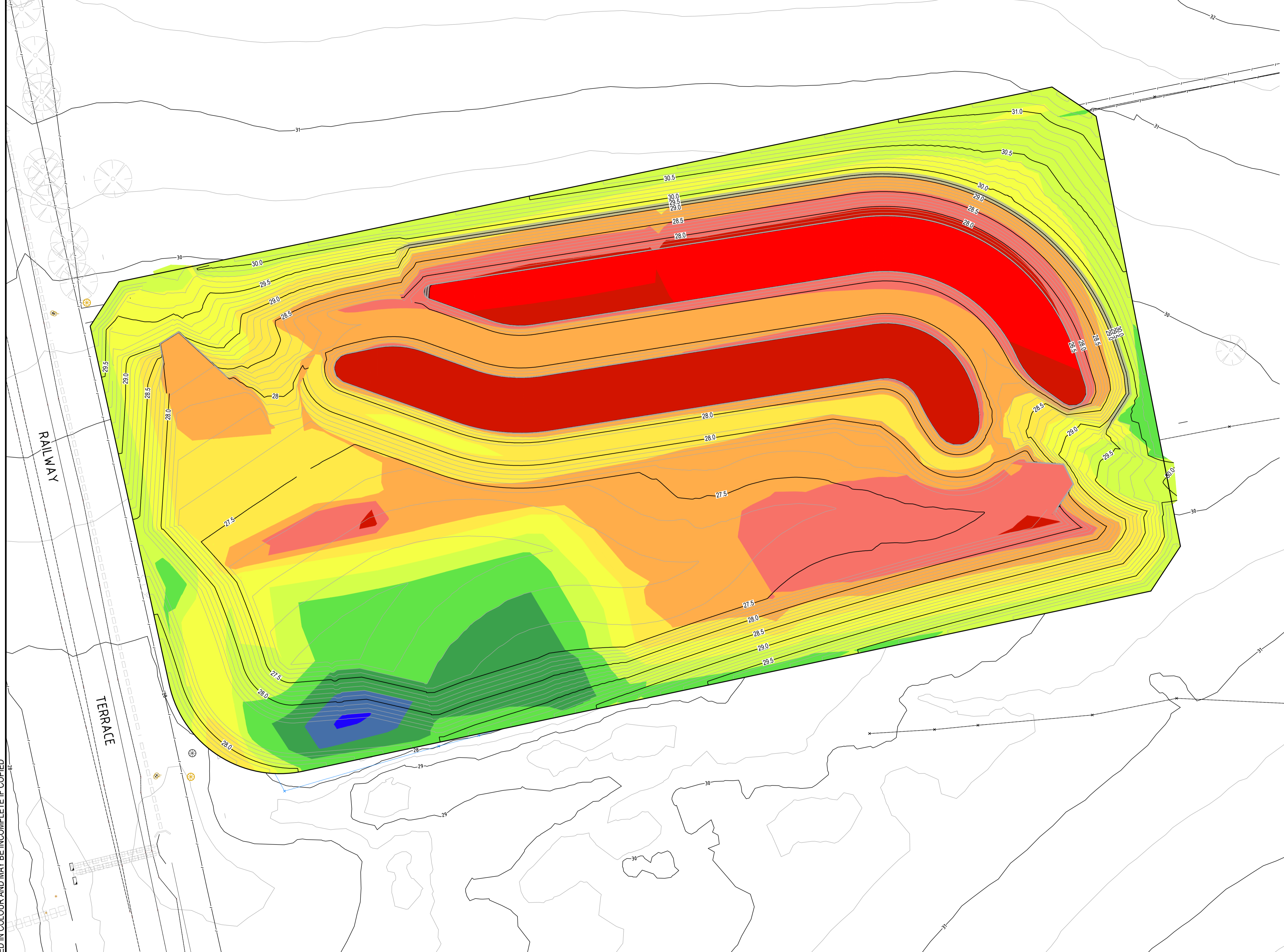
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TITLE	NAME	DATE
SURVEYED	B.C.C.	
DESIGNED	DY	
DRAWN	LMCB	
CHECKED		
ACCEPTED	BCC CPEng NER Certified	



PROJECT	TITLE	CAD FILE	FILE No	PLAN No	REV
NW GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	SOIL & WATER DETAILS	E39-15V SOIL & WATER.dwg	F16/646	E39-15V	A1 SHEET 6 OF 21



EARTHWORKS - DEPTH INDEX

Red	-4	TO	-3
Dark Red	-3	TO	-2
Light Red	-2	TO	-1.5
Orange	-1.5	TO	-1
Yellow	-1	TO	-0.5
Light Yellow	-0.5	TO	0
Light Green	0	TO	0.5
Green	0.5	TO	1
Dark Green	1	TO	1.5
Blue	1.5	TO	2
Light Blue	2	TO	3
Purple	3	TO	4

BULK EARTHWORKS VOLUMES

DESCRIPTION	VOLUMES m ³
BULK EARTHWORKS (INCLUDING TOPSOIL)	CUT 9485.696m ³ FILL 1220.454m ³
	CUT 8265.242m ³

NOTE:
1. DEPTH RANGES SHOW DIFFERENCE BETWEEN EXISTING GROUND & FINISHED SURFACE LEVELS.

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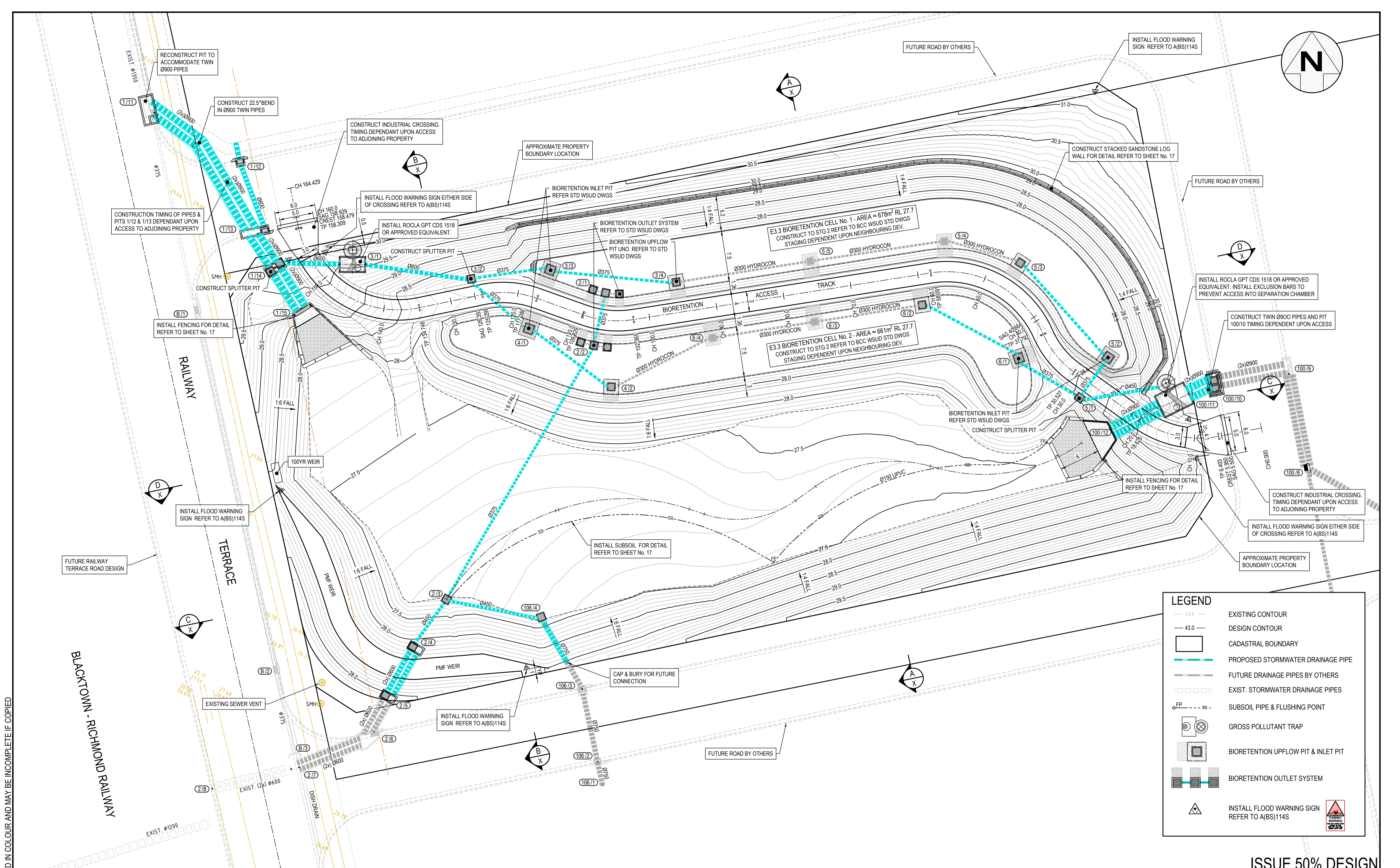
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	CHECKED		
	ACCEPTED	BCC CP/Eng NER Certified	

CO-ORDINATE SYSTEM	HEIGHT DATUM
GDA 94 / MGA ZONE 56	AHD



PROJECT	NW GROWTH CENTRE - ALEX AVENUE PRECINCT
BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	
TITLE	BULK EARTHWORKS
CAD FILE	E39-15V BULK EARTHWORKS.dwg
FILE No	F16/646
PLAN No	E39-15V
REV	1

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LEGEND

- EXISTING CONTOUR
- DESIGN CONTOUR
- CADASTRAL BOUNDARY
- PROPOSED STORMWATER DRAINAGE PIPE
- FUTURE DRAINAGE PIPES BY OTHERS
- EXIST. STORMWATER DRAINAGE PIPES
- SUBSOIL PIPE & FLUSHING POINT
- GROSS POLLUTANT TRAP
- BIORETENTION UPFLOW PIT & INLET PIT
- BIORETENTION OUTLET SYSTEM
- INSTALL FLOOD WARNING SIGN REFER TO A(BS)114S

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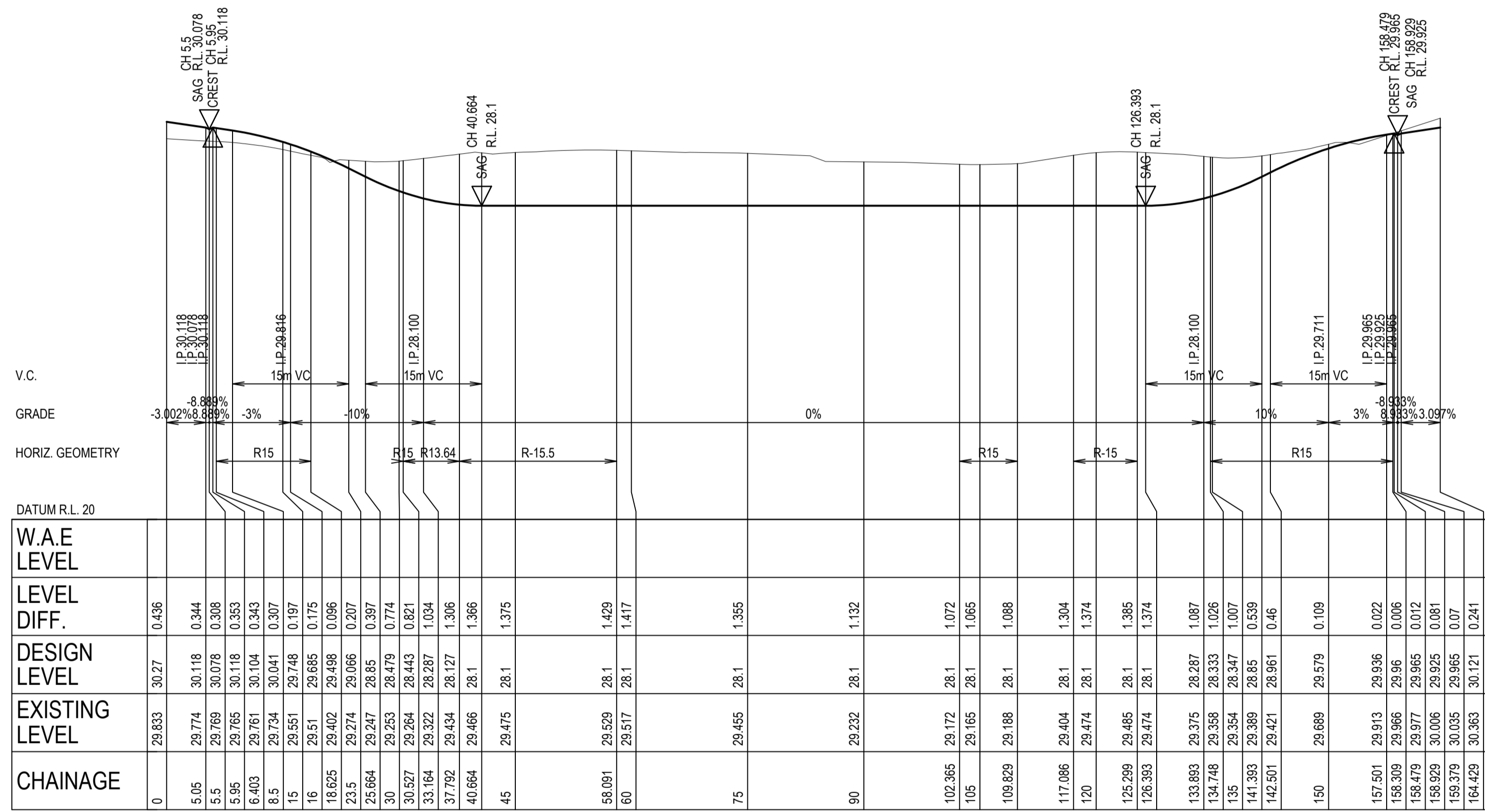
CO-ORDINATE SYSTEM	HEIGHT DATUM
GA 94 / MGA ZONE 56	AHD



PROJECT	NW GROWTH CENTRE - ALEX AVENUE PRECINCT	A1
	BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	SHEET
		8
		OF
		21
TITLE	BASIN E3.2 DETAIL PLAN	
CAD FILE	E39-15V DETAIL PLANS.dwg	FILE No
		F16/646
PLAN No	E39-15V	REV
		1

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LONGITUDINAL SECTION - E3.2 ACCESS PATH (CENTRELINE)

SCALE 1:500 (H)
SCALE 1:100 (V)

ISSUE 50% DESIGN

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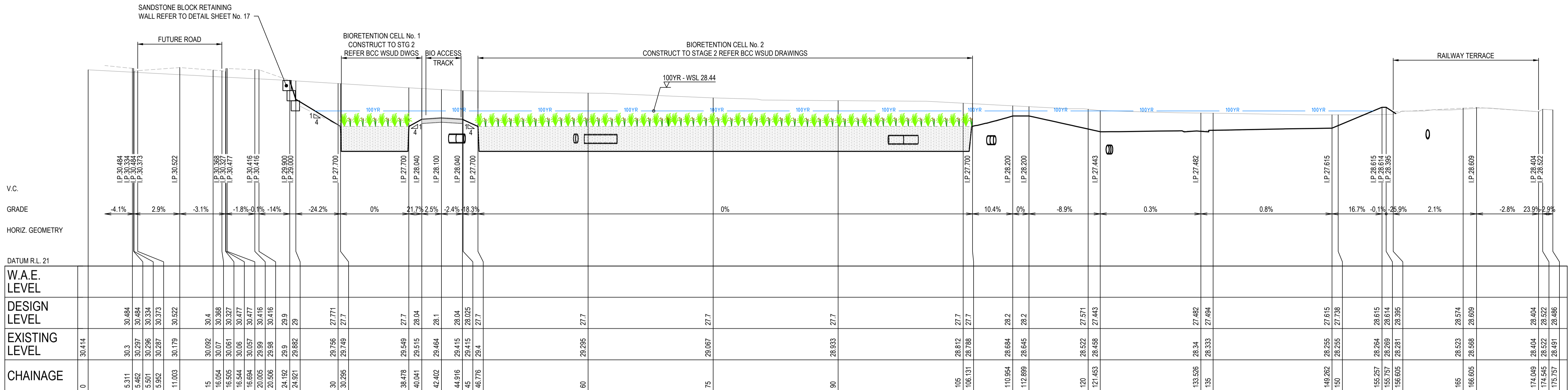
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CO-ORDINATE SYSTEM	HEIGHT DATUM
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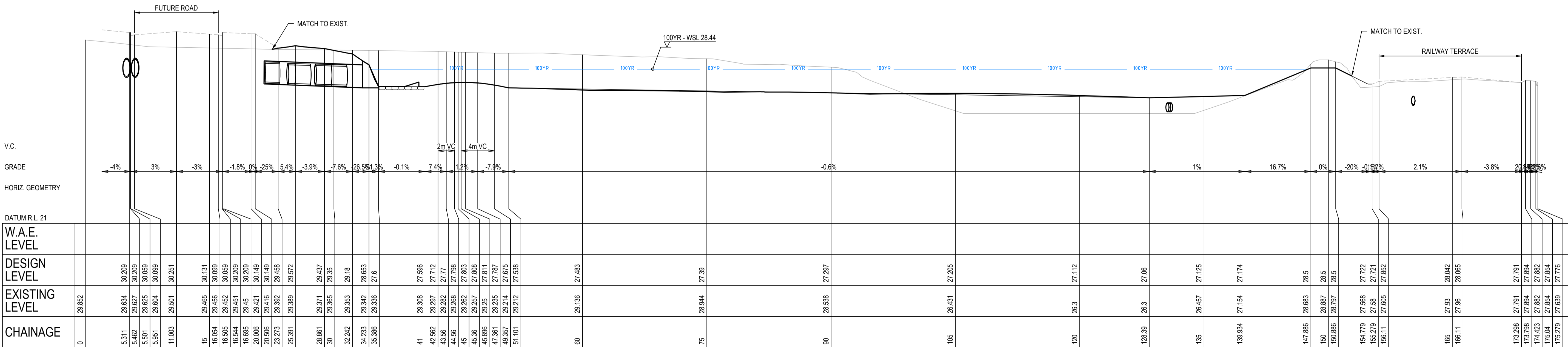


PROJECT	FILE No	PLAN No	REV
NW GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	F16/646	E39-15V	1
TITLE			
BIORETENTION ACCESS TRACK LONGITUDINAL SECTION			

A1	SHEET	9	OF	21
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BASIN - SECTION D-D
 SCALE 1:250 (H)
 SCALE 1:100 (V)



BASIN - SECTION C-C
 SCALE 1:250 (H)
 SCALE 1:100 (V)


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GLOBAL COMPLIANCE CERTIFICATION
ISO 9001 Certified

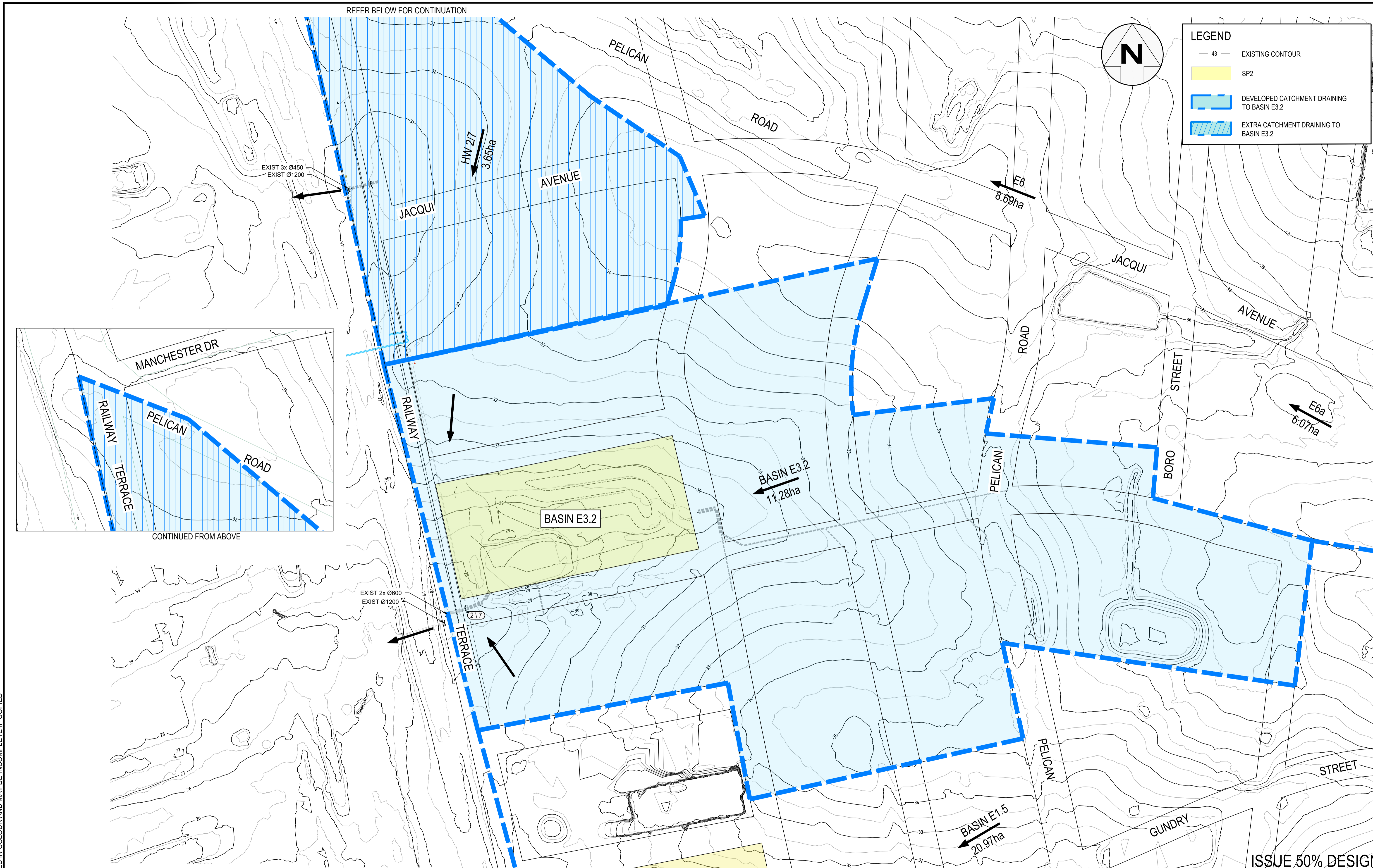


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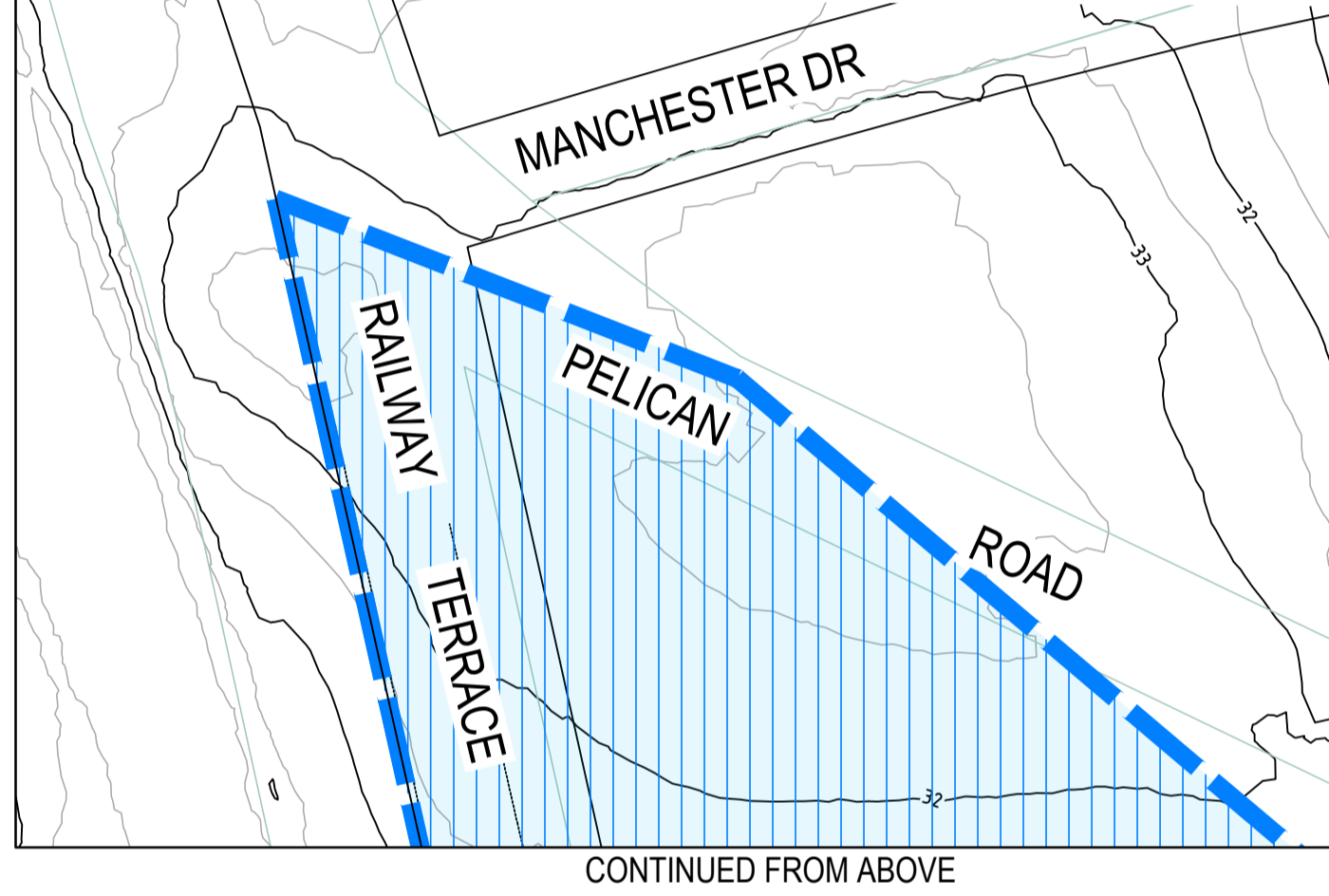
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NW GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	E39-15V 014 - 015.dwg	F16/646	E39-15V	1

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LEGEND

- 43 — EXISTING CONTOUR
- SP2
- DEVELOPED CATCHMENT DRAINING TO BASIN E3.2
- EXTRA CATCHMENT DRAINING TO BASIN E3.2



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REV	DATE	DESCRIPTION	CHECKED	APPROVED	DATE
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0	02.11.21	ISSUED FOR DRP 50% REVIEW	DY	GE	02.11.21

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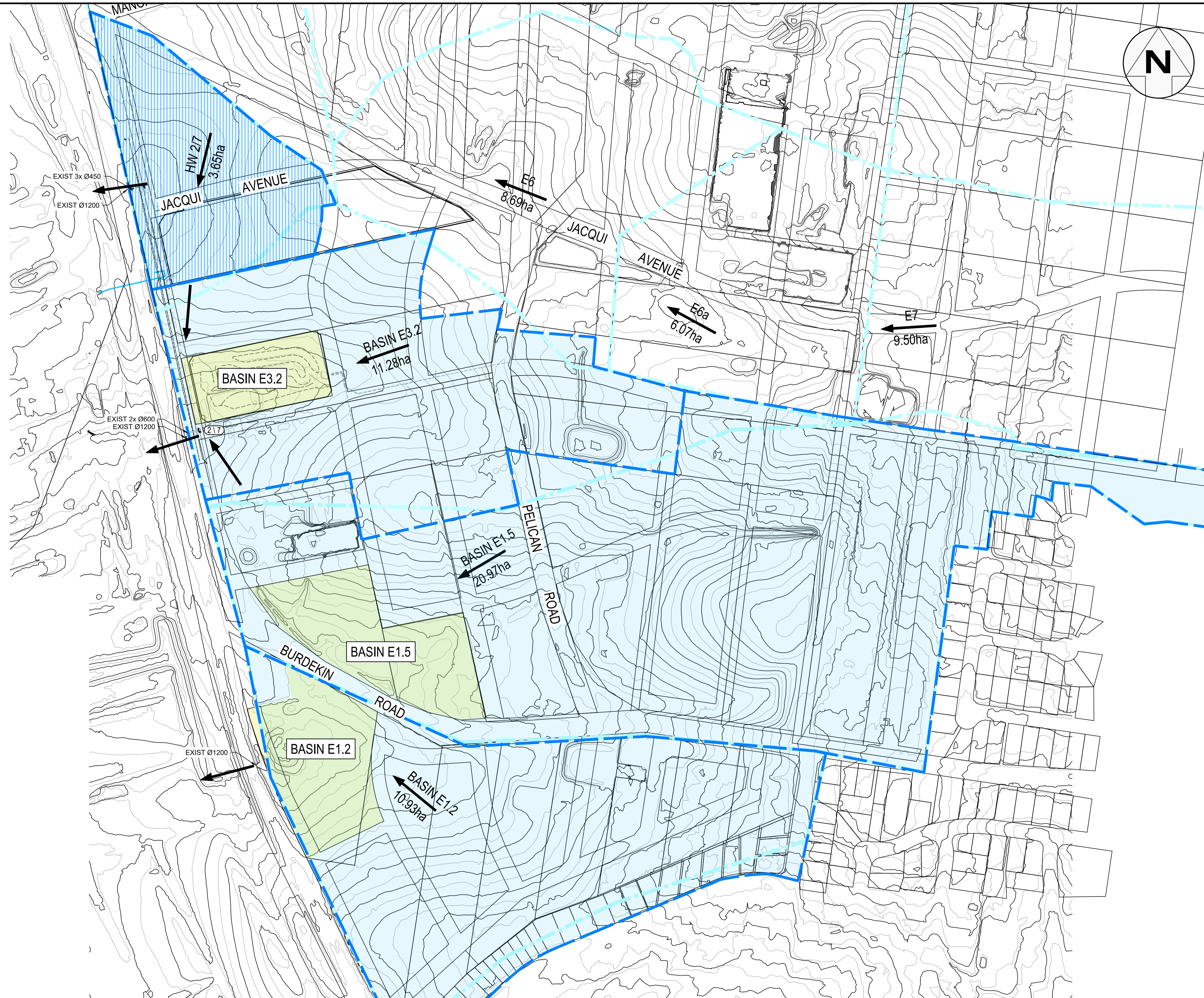
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	CHECKED		
	ACCEPTED	BCC CP/Eng NER Certified	

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PROJECT	CAD FILE	FILE No	PLAN No	REV
NW GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	E39-15V CATCHMENT PLAN.dwg	F16/646	E39-15V	1

A1
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12
OF
21



LEGEND

- 43 — EXISTING CONTOUR
- SP2
- EXISTING CATCHMENT
- DEVELOPED CATCHMENT DRAINING TO BASINS
- DEVELOPED CATCHMENT DIVERTED TO HW 2/7

ISSUE 50% DESIGN

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CO-ORDINATE SYSTEM	HEIGHT DATUM
GDA 94 / MGA ZONE 56	AHD

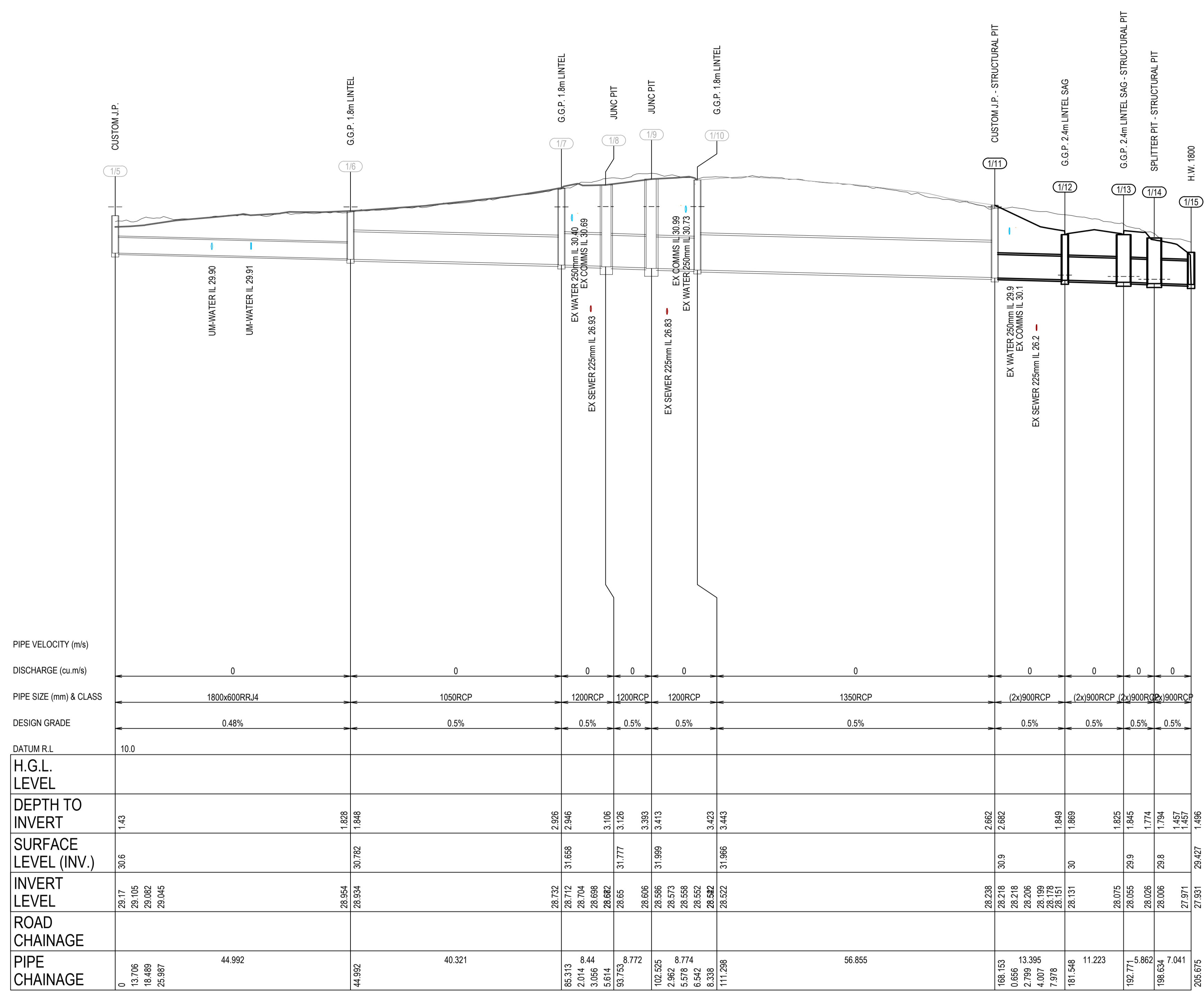


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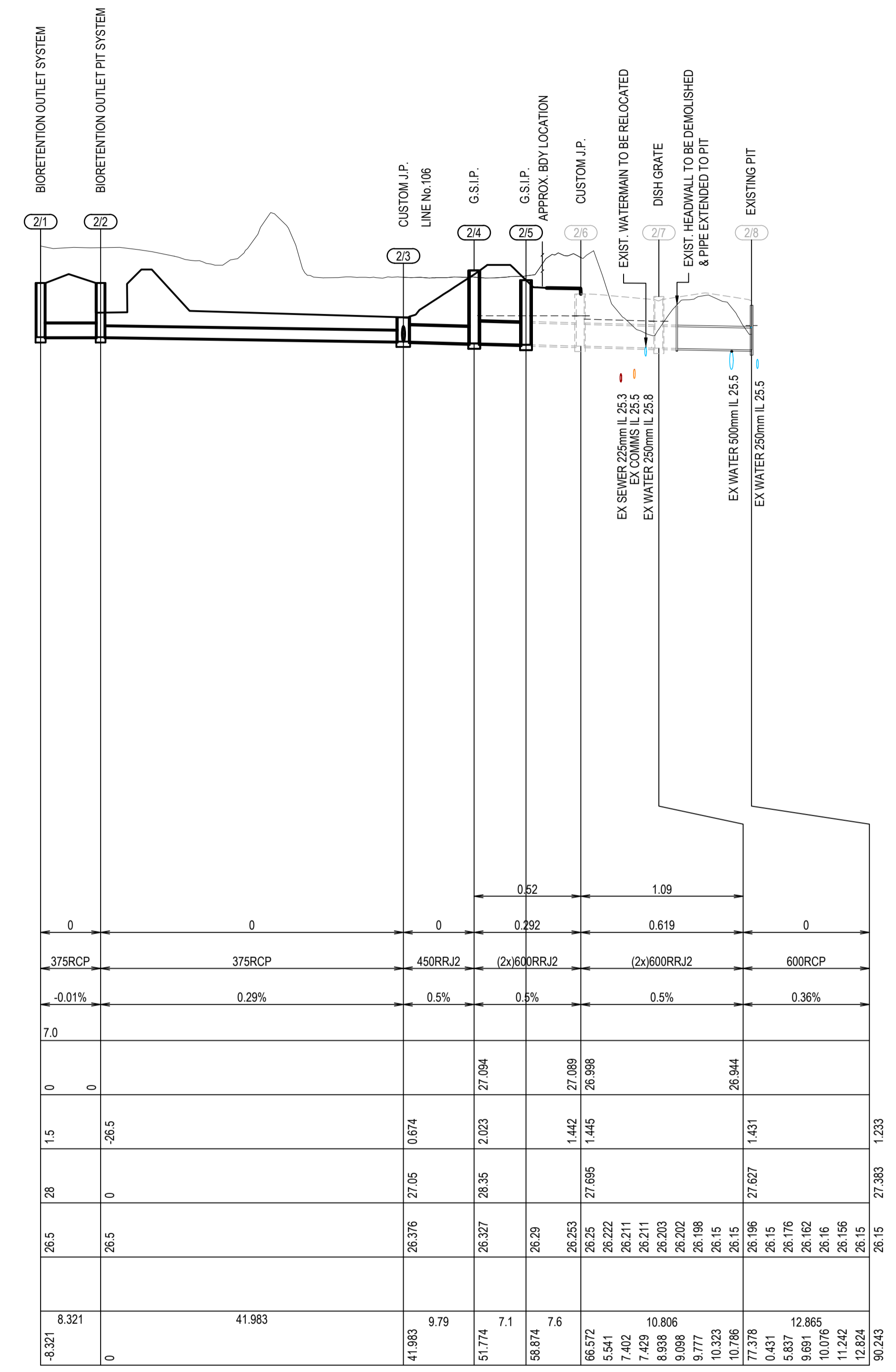
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PROJECT	NW GROWTH CENTRE - ALEX AVENUE PRECINCT	A1
	BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	SHEET
TITLE	REGIONAL CATCHMENT PLAN	13
		OF
		21
CAD FILE	FILE No	PLAN No
E39-15V CATCHMENT PLAN.dwg	F16/646	E39-15V
		REV
		1

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LINE No.1
RAILWAY TERRACE - BASIN E3.2



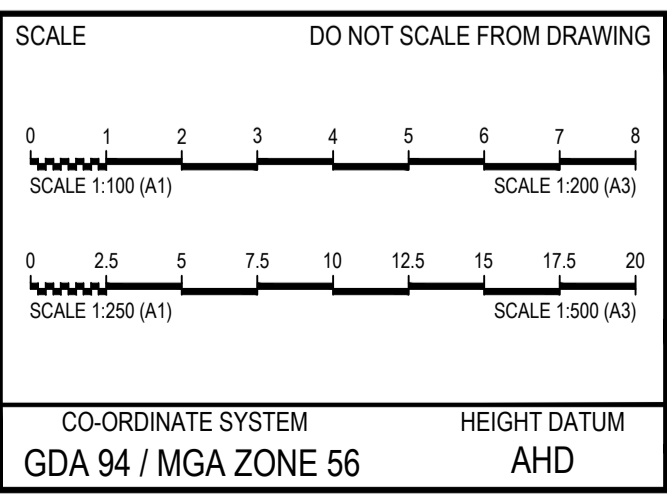
LINE No.2

PIPE VELOCITY (m/s)	DISCHARGE (cu.m/s)	PIPE SIZE (mm) & CLASS	DESIGN GRADE	DATUM R.L.	H.G.L. LEVEL	DEPTH TO INVERT	SURFACE LEVEL (INV.)	INVERT LEVEL	ROAD CHAINAGE	PIPE CHAINAGE
0	0	1800x600RRJ4	0.48%	10.0	30.6	1.43	29.17	29.105	0	13.706
0	0	1050RCP	0.5%	10.0	30.6	1.828	28.982	28.982	44.992	18.489
0	0	1200RCP	0.5%	10.0	30.6	2.926	28.945	28.945	44.992	25.997
0	0	1200RCP	0.5%	10.0	30.6	2.926	28.732	28.732	40.321	65.313
0	0	1200RCP	0.5%	10.0	30.6	2.926	28.712	28.712	40.321	2.014
0	0	1200RCP	0.5%	10.0	30.6	2.926	28.704	28.704	40.321	8.44
0	0	1200RCP	0.5%	10.0	30.6	2.926	28.698	28.698	40.321	3.056
0	0	1200RCP	0.5%	10.0	30.6	2.926	28.682	28.682	40.321	5.614
0	0	1200RCP	0.5%	10.0	30.6	2.926	28.65	28.65	40.321	93.753
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0	0	1200RCP	0.5%	10.0	30.6	2.926	28.573	28.573	40.321	2.962
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0	0	1200RCP	0.5%	10.0	30.6	2.926	28.522	28.522	40.321	8.338
0	0	1200RCP	0.5%	10.0	30.6	2.926	28.522	28.522	40.321	111.298
0	0	1350RCP	0.5%	10.0	30.6	2.662	28.238	28.238	56.855	168.153
0	0	(2x)900RCP	0.5%	10.0	30.6	2.662	28.216	28.216	56.855	0.656
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0	0	(2x)900RCP	0.5%	10.0	30.6	2.662	28.199	28.199	56.855	7.978
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0	0	(2x)900RCP	0.5%	10.0	30.6	2.662	28.075	28.075	56.855	192.771
0	0	(2x)900RCP	0.5%	10.0	30.6	2.662	28.055	28.055	56.855	5.862
0	0	(2x)900RCP	0.5%	10.0	30.6	2.662	28.026	28.026	56.855	198.634
0	0	(2x)900RCP	0.5%	10.0	30.6	2.662	28.005	28.005	56.855	7.041
0	0	(2x)900RCP	0.5%	10.0	30.6	2.662	27.971	27.971	56.855	206.675

PIPE VELOCITY (m/s)	DISCHARGE (cu.m/s)	PIPE SIZE (mm) & CLASS	DESIGN GRADE	DATUM R.L.	H.G.L. LEVEL	DEPTH TO INVERT	SURFACE LEVEL (INV.)	INVERT LEVEL	ROAD CHAINAGE	PIPE CHAINAGE
0	0	375RCP	-0.01%	7.0	26.5	1.5	26.5	26.5	8.321	0
0	0	375RCP	0.29%	7.0	26.5	0	26.5	26.5	41.983	0
0	0	450RRJ2	0.5%	7.0	26.5	0.674	27.05	27.05	41.983	9.79
0	0.292	(2x)600RRJ2	0.5%	7.0	26.5	2.023	28.35	28.327	41.983	51.774
0	0.619	(2x)600RRJ2	0.5%	7.0	26.5	1.442	27.084	27.084	41.983	7.1
0	0	600RCP	0.36%	7.0	26.5	1.431	26.998	26.998	41.983	58.874
0	0	600RCP	0.36%	7.0	26.5	1.431	27.695	27.695	41.983	7.6
0	0	600RCP	0.36%	7.0	26.5	1.431	26.25	26.25	41.983	66.572
0	0	600RCP	0.36%	7.0	26.5	1.431	26.222	26.222	41.983	5.541
0	0	600RCP	0.36%	7.0	26.5	1.431	26.211	26.211	41.983	7.402
0	0	600RCP	0.36%	7.0	26.5	1.431	26.203	26.203	41.983	10.806
0	0	600RCP	0.36%	7.0	26.5	1.431	26.202	26.202	41.983	9.098
0	0	600RCP	0.36%	7.0	26.5	1.431	26.198	26.198	41.983	9.777
0	0	600RCP	0.36%	7.0	26.5	1.431	26.15	26.15	41.983	10.323
0	0	600RCP	0.36%	7.0	26.5	1.431	26.15	26.15	41.983	10.786
0	0	600RCP	0.36%	7.0	26.5	1.431	26.196	26.196	41.983	77.378
0	0	600RCP	0.36%	7.0	26.5	1.431	26.15	26.15	41.983	0.431
0	0	600RCP	0.36%	7.0	26.5	1.431	26.176	26.176	41.983	5.837
0	0	600RCP	0.36%	7.0	26.5	1.431	26.162	26.162	41.983	12.865
0	0	600RCP	0.36%	7.0	26.5	1.431	26.16	26.16	41.983	10.076
0	0	600RCP	0.36%	7.0	26.5	1.431	26.156	26.156	41.983	11.242
0	0	600RCP	0.36%	7.0	26.5	1.431	26.15	26.15	41.983	12.824
0	0	600RCP	0.36%	7.0	26.5	1.431	26.15	26.15	41.983	90.243

ISSUE 50% DESIGN

REV	DATE	DESCRIPTION	CHECKED	APPROVED	DATE
1	16.02.22	ISSUED FOR 50% DESIGN REVIEW	DY	GE	16.02.22
0	02.11.21	ISSUED FOR DRP 50% REVIEW	DY	GE	02.11.21



DRAWINGS / DESIGN PREPARED BY	TITLE	NAME	DATE
DRAWINGS VERIFIED BY	SURVEYED	B.C.C.	
	DESIGNED	DY	
	DRAWN	LMCB	
	CHECKED		
	ACCEPTED	BCC CPEng NER Certified	

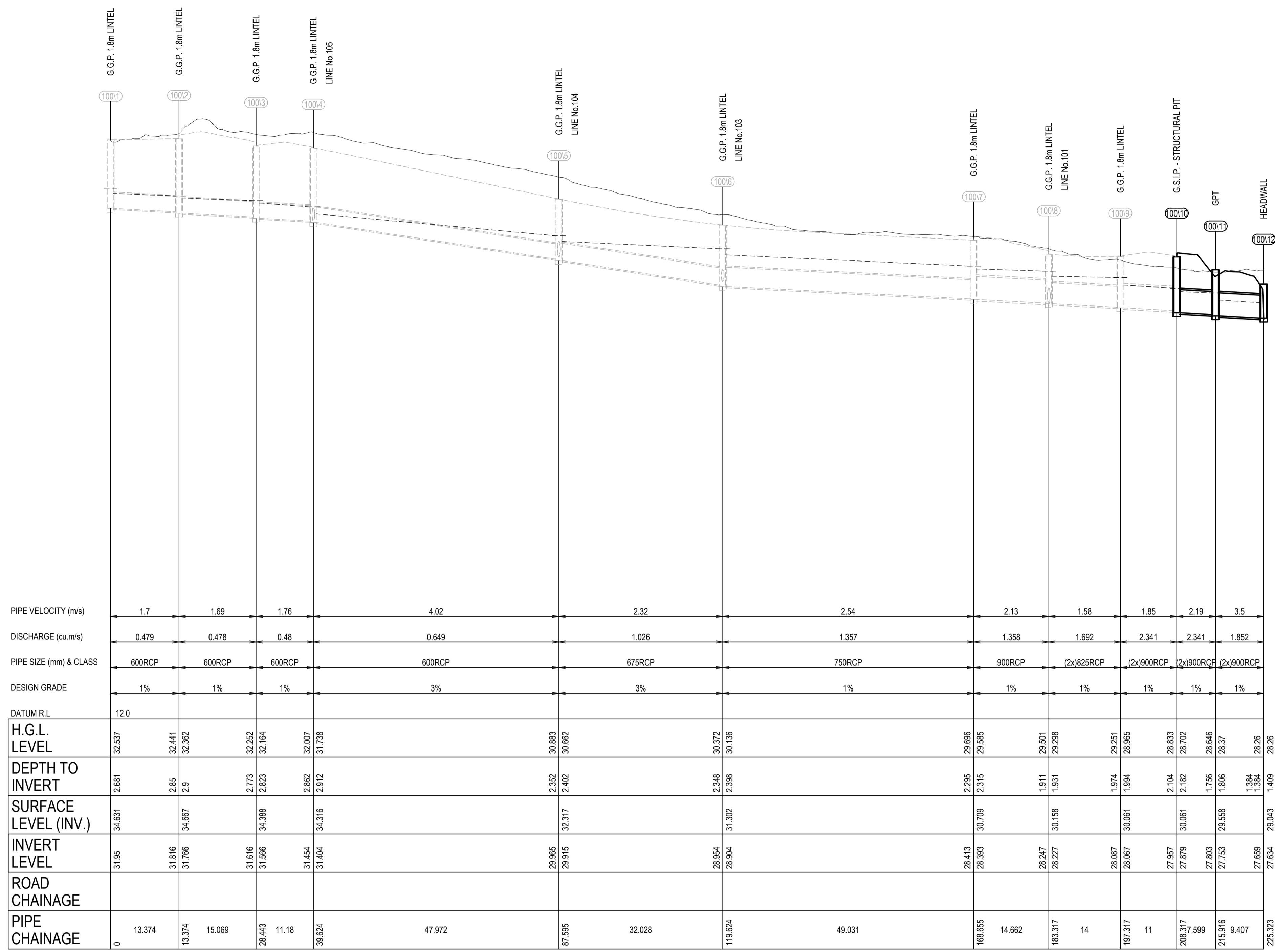
CO-ORDINATE SYSTEM	HEIGHT DATUM
GDA 94 / MGA ZONE 56	AHD

COUNCIL CHAMBERS: 62 FLUSHCOMBE ROAD, BLACKTOWN, NSW 2148
 ALL MAIL: GENERAL MANAGER, PO BOX 63, BLACKTOWN, NSW 2148
 TELEPHONE: (02) 9839 6000 FAX: (02) 9831 1961 DX: 8117 BLACKTOWN

PROJECT	CAD FILE	FILE No	PLAN No	REV
NW GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	E39-15V DRAINAGE LSECT.dwg	F16/646	E39-15V	1
TITLE	DRAINAGE LONGITUDINAL SECTIONS LINE Nos 1 & 2			

A1
SHEET
14
OF
21

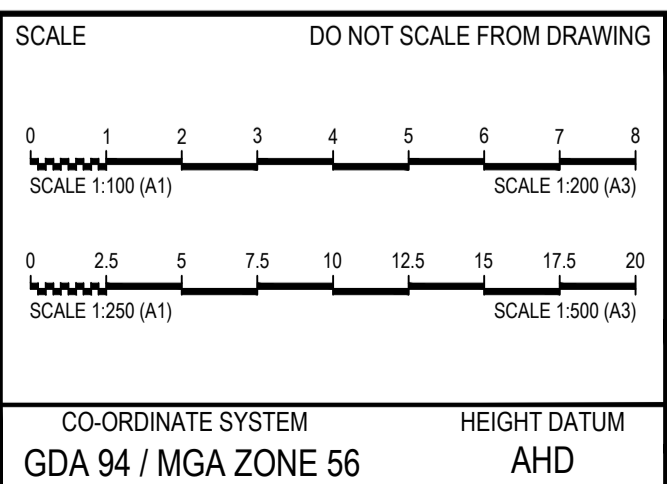
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LINE No.100

ISSUE 50% DESIGN

REV	DATE	DESCRIPTION	CHECKED	APPROVED	DATE
1	16.02.22	ISSUED FOR 50% DESIGN REVIEW	DY	GE	16.02.22
0	02.11.21	ISSUED FOR DRP 50% REVIEW	DY	GE	02.11.21



DRAWINGS / DESIGN PREPARED BY		TITLE	NAME	DATE
DRAWINGS VERIFIED BY		SURVEYED	B.C.C.	
		DESIGNED	DY	
		DRAWN	LMCB	
		CHECKED		
		ACCEPTED - BCC CPEng NER Certified		

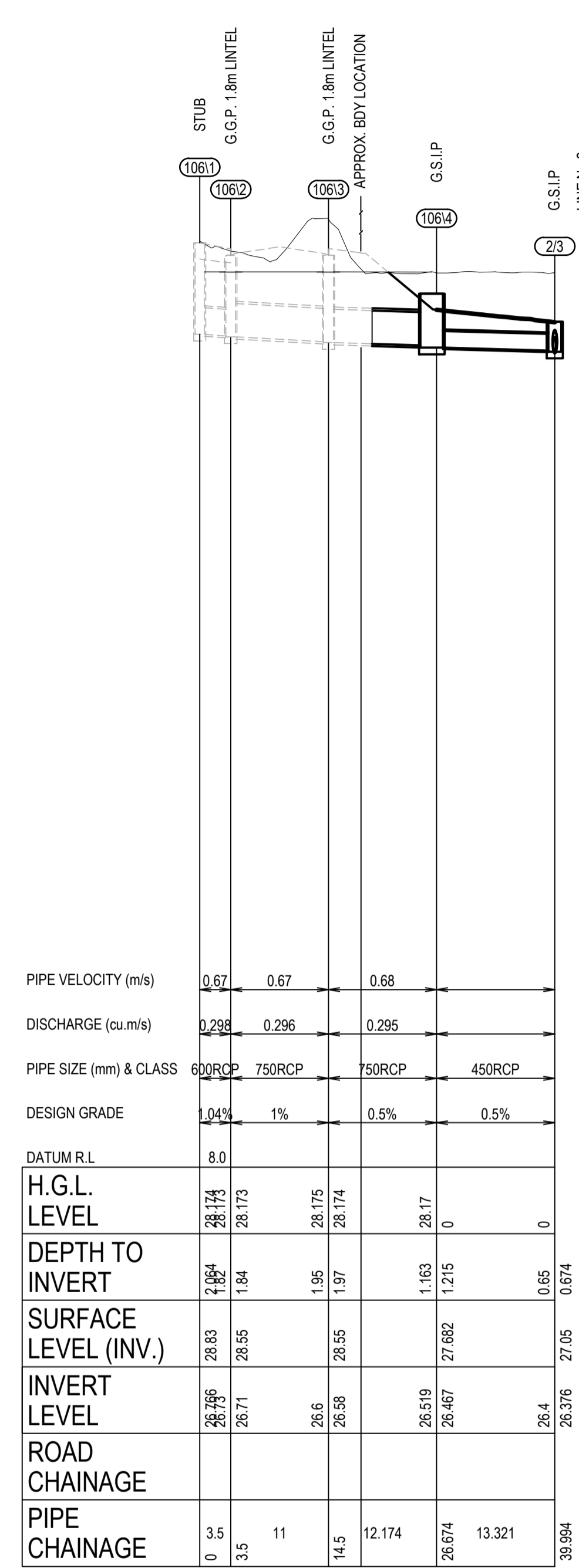
CO-ORDINATE SYSTEM	HEIGHT DATUM
GDA 94 / MGA ZONE 56	AHD



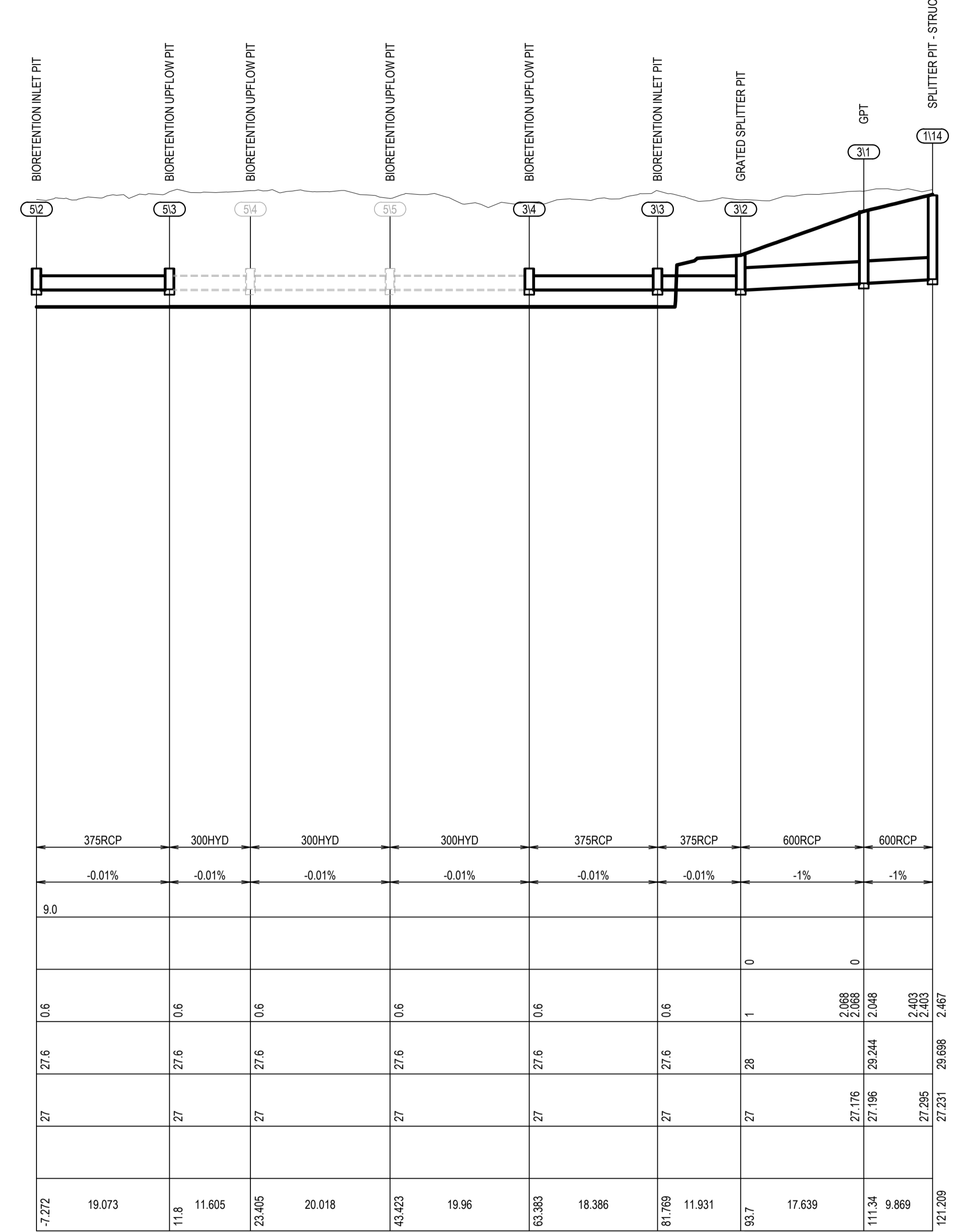
PROJECT	FILE No	PLAN No	REV
N/W GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	F16/646	E39-15V	1
TITLE	CAD FILE	FILE No	REV
DRAINAGE LONGITUDINAL SECTIONS LINE No 100	E39-15V DRAINAGE LSECT.dwg	F16/646	1

A1
SHEET
15
OF
21

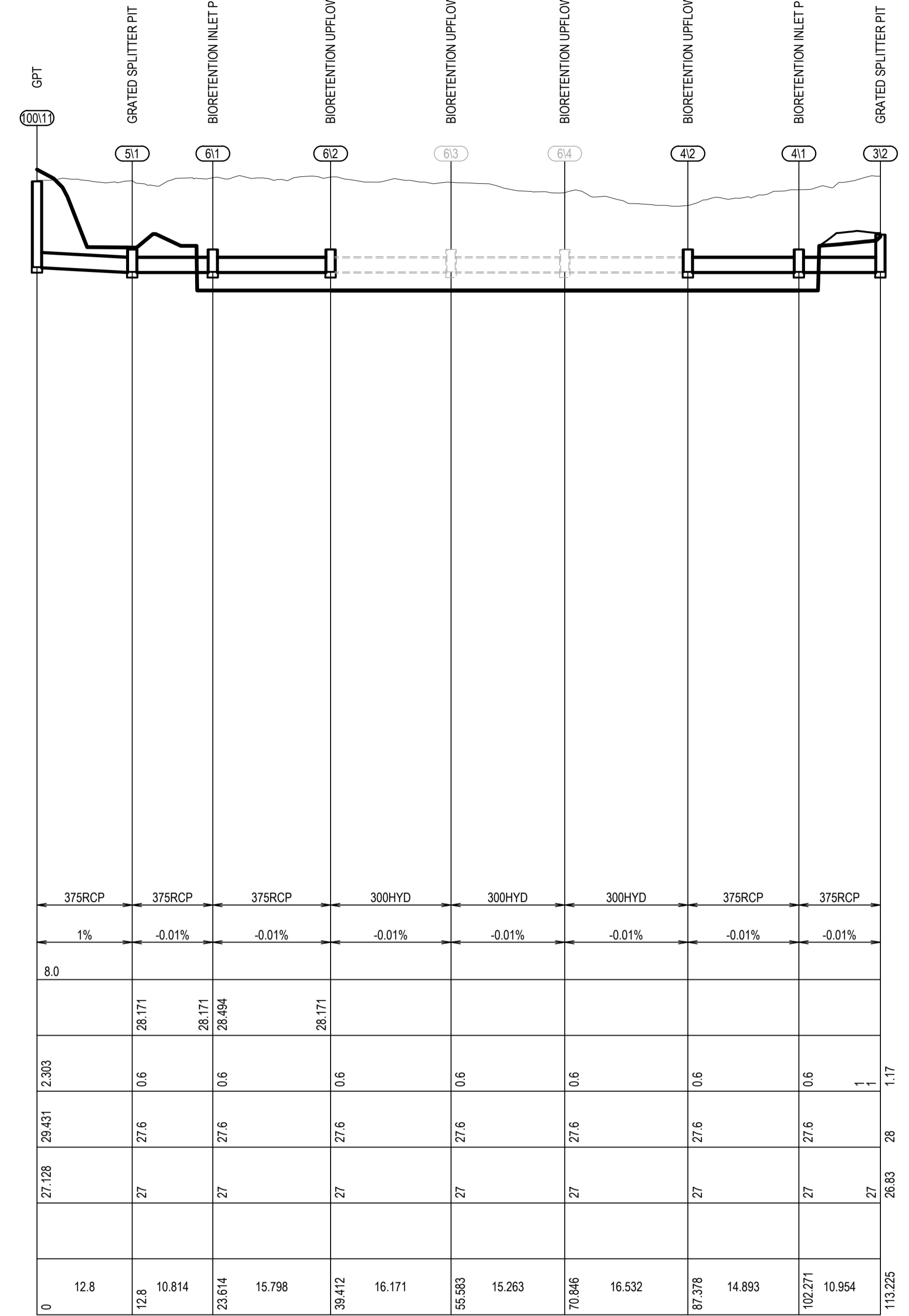
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LINE No.106



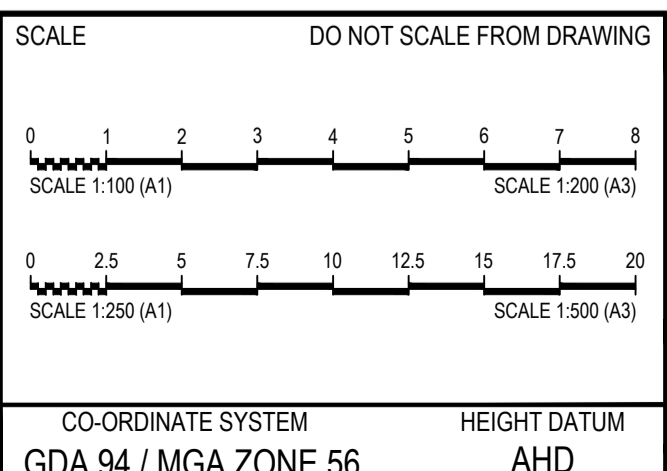
LINE No. 3 & 5



LINE No. 4 & 6

ISSUE 50% DESIGN

REV	DATE	DESCRIPTION	CHECKED	APPROVED	DATE
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0	02.11.21	ISSUED FOR DRP 50% REVIEW	DY	GE	02.11.21

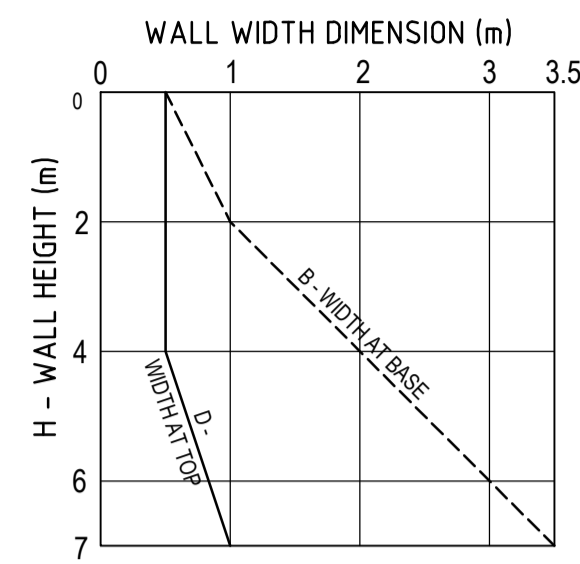


DRAWINGS / DESIGN PREPARED BY	TITLE	NAME	DATE
DRAWINGS VERIFIED BY	SURVEYED	B.C.C.	
	DESIGNED	DY	
	DRAWN	LMCB	
	CHECKED		
	ACCEPTED - BCC CPEng NER Certified		

CO-ORDINATE SYSTEM	HEIGHT DATUM
GDA 94 / MGA ZONE 56	AHD



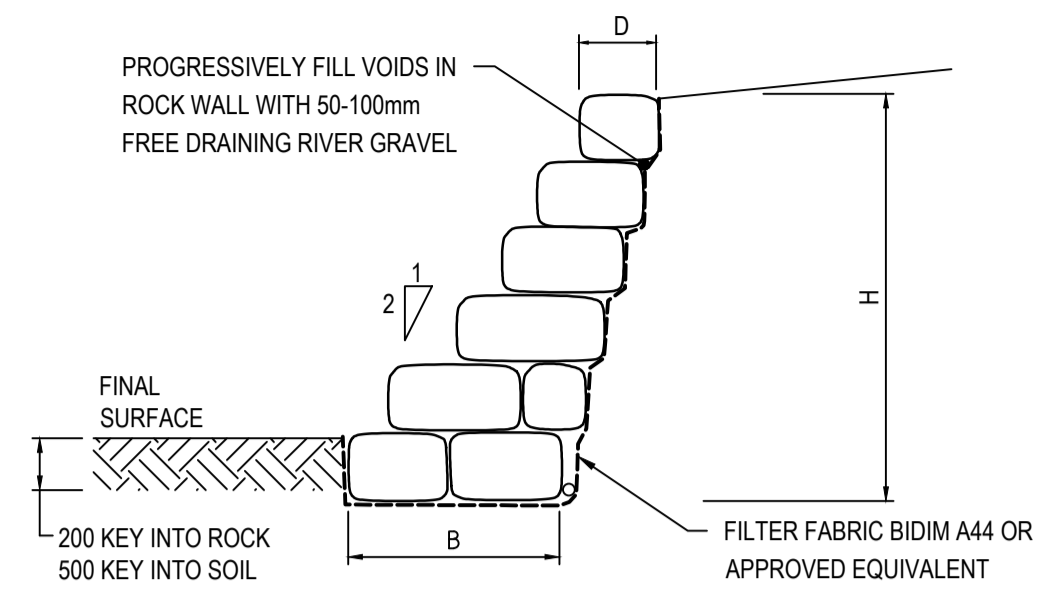
PROJECT	N/W GROWTH CENTRE - ALEX AVENUE PRECINCT	A1
	BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	SHEET
TITLE	DRAINAGE LONGITUDINAL SECTIONS LINE Nos 106, & 3 - 6	16
		OF
		21
CAD FILE	FILE No	PLAN No
E39-15V DRAINAGE LSECT.dwg	F16/646	E39-15V
		REV
		1



ROCK WALL DESIGN CHART

ROCK WALL NOTES

- BACKFILL IS TO BE GRANULAR, FREE DRAINING AND COMPACTED.
- FOUNDATION TO BE APPROVED FOR A SAFE BEARING CAPACITY OF 200kPa PRIOR TO CONSTRUCTION.
- WHERE THE SURFACE SLOPE OF RETAINED MATERIAL IS BETWEEN 1:10 AND 1:4 THE WALL BASE DIMENSION IS TO BE INCREASED BY 0.5m.
- ROCK IS TO BE SOUND DURABLE SANDSTONE OR OTHER APPROVED MATERIAL. REFER TO ROCK NOTES FOR SPECIFICATION.
- ROCKS SHALL BE PLACED IN SUCH A MANNER THAT THEY ARE STABLE AND INTERLOCKING, ONE LAID ROUGHLY COURSED AND BEDDED ON THEIR BROADEST BASE.
- ALL DIMENSIONS ARE IN MILLIMETRES UNLESS SHOWN OTHERWISE.

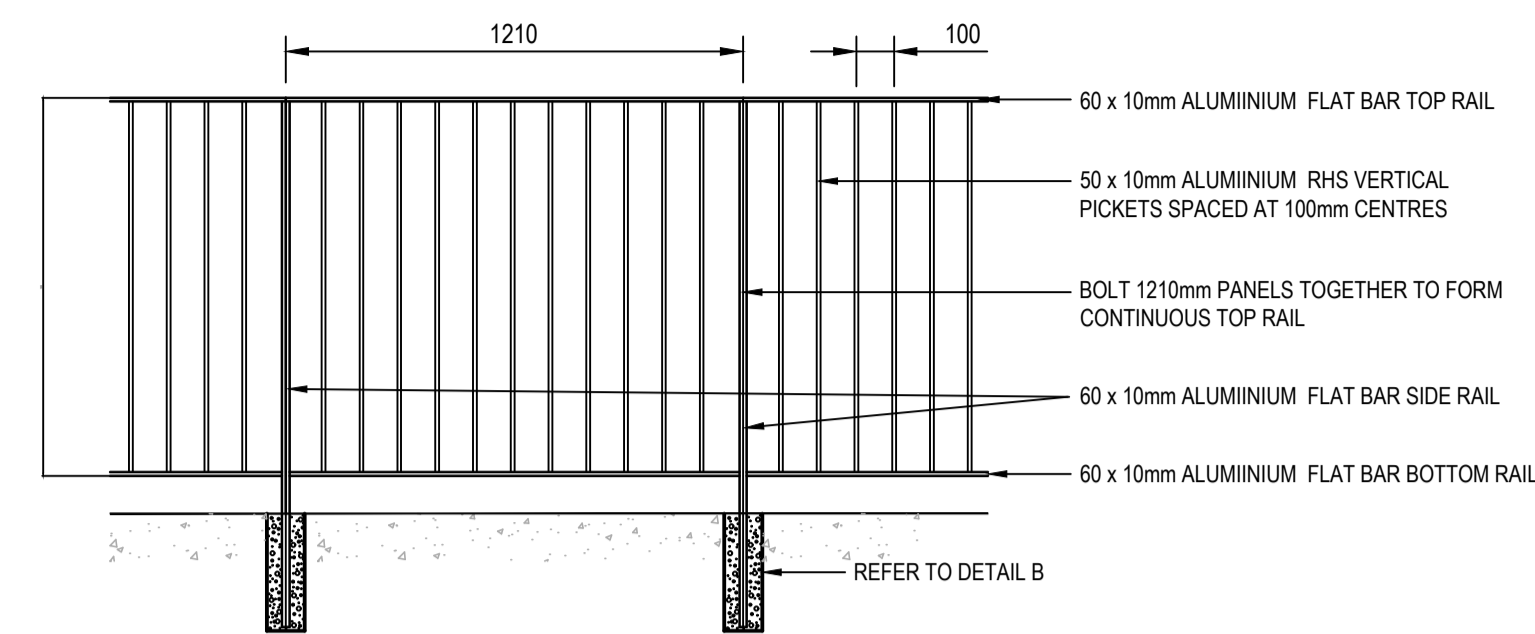


TYPICAL ROCK WALL GEOMETRY

NOTE: WHERE SURFACE SLOPE IS GREATER THAN 1:10 REFER NOTE 3.

ROCK NOTES

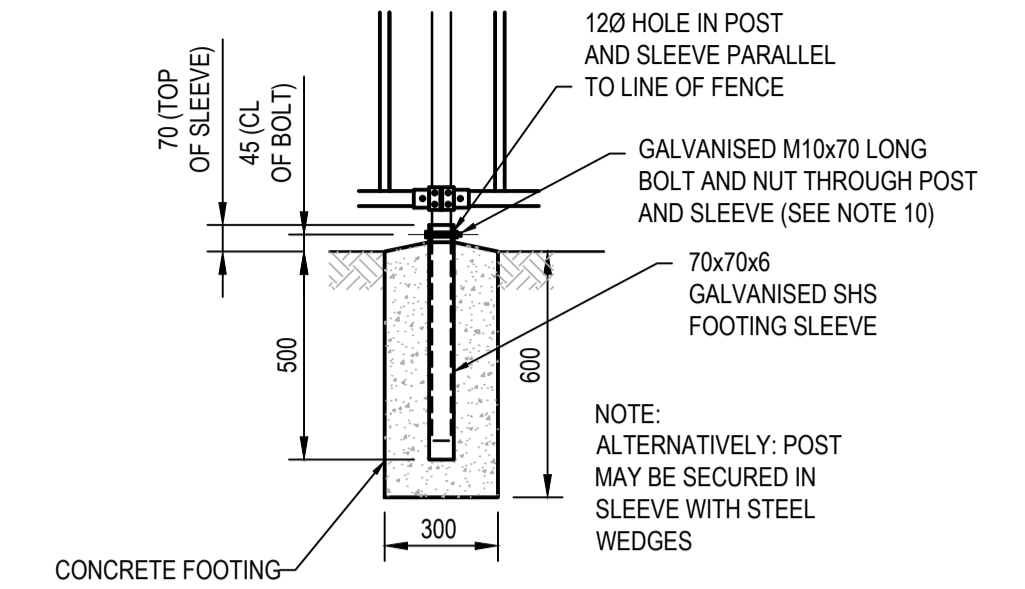
- ROCK IS TO BE SOUND, DURABLE 2200 Kg/m³ GENERALLY 1.0m² PLAN AREA AND 500 DEEP.
- INDIVIDUAL ROCKS SHALL BE FREE OF CRACKS, CLEAVAGE PLANES, SEAMS AND DEFECTS WHICH WOULD RESULT IN THE BREAKDOWN OF THE ROCK IN A FRESH WATER ENVIRONMENT.
- ROCK SHALL BE ROUGH AND ANGULAR.
- RATIO OF MAX. DIMENSION IN PLAN SHALL NOT EXCEED 2.5.
- ROCK RIP RAP SHALL COMPLY WITH 2 & 3 ABOVE AND WITH FOLLOWING GRADING SPECIFICATION:
MINIMUM DENSITY 2200 Kg/m³
M₅₀ = 60kg (NOMINAL 300mm DIA.)
M₁₅ = 25kg
M₁₀₀ = 60kg
- ROCKS ARE TO BE PLACED IN SUCH A MANNER THAT THEY ARE STABLE AND INTERLOCKING. LAID ROUGHLY COURSED AND BEDDED ON THEIR BROADEST BASE ON BIDIM A44.
- FOUNDATION TO BE APPROVED FOR A SAFE BEARING CAPACITY OF 200kPa PRIOR TO CONSTRUCTION.



**DETAIL A
TYPICAL BALUSTRADE BCC POOL FENCE DETAIL**

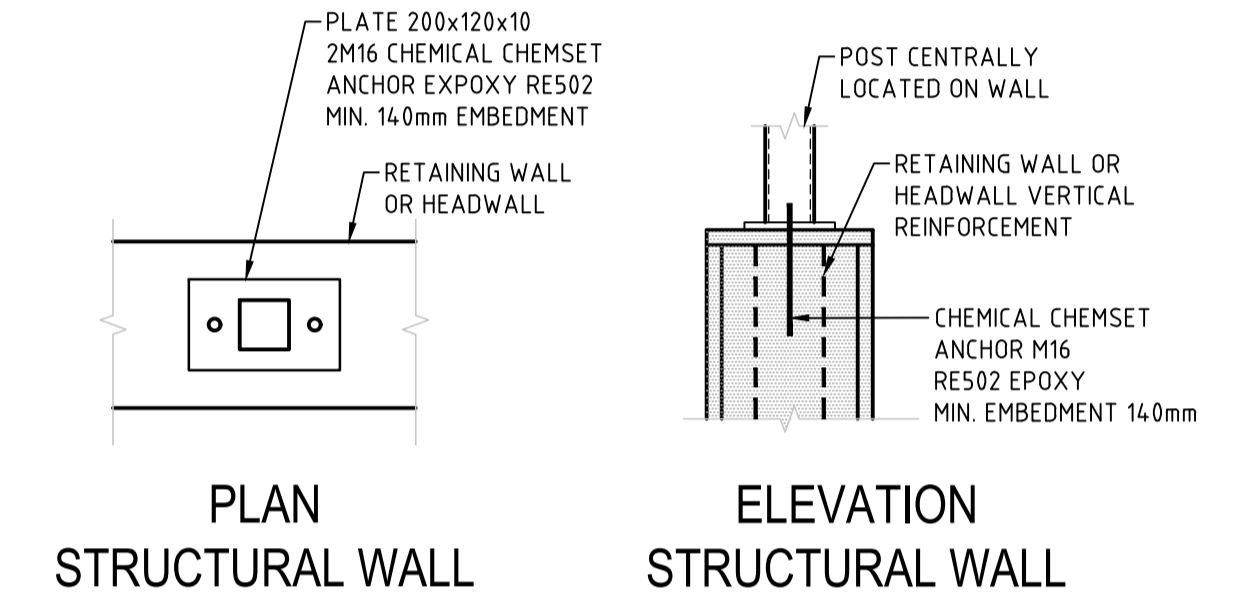
SCALE 1:20

- NOTE:**
- FENCE TO BE POWDER COATED AND COLOUR TO BE MONUMENT GREY
 - FENCE LOCATED IN GARDEN SPACES TO HAVE 400 X 300mm DIA. CONCRETE FOOTING, FINISHED FOOTING LEVEL TO BE 50mm BELOW FINISHED MULCH LEVELS



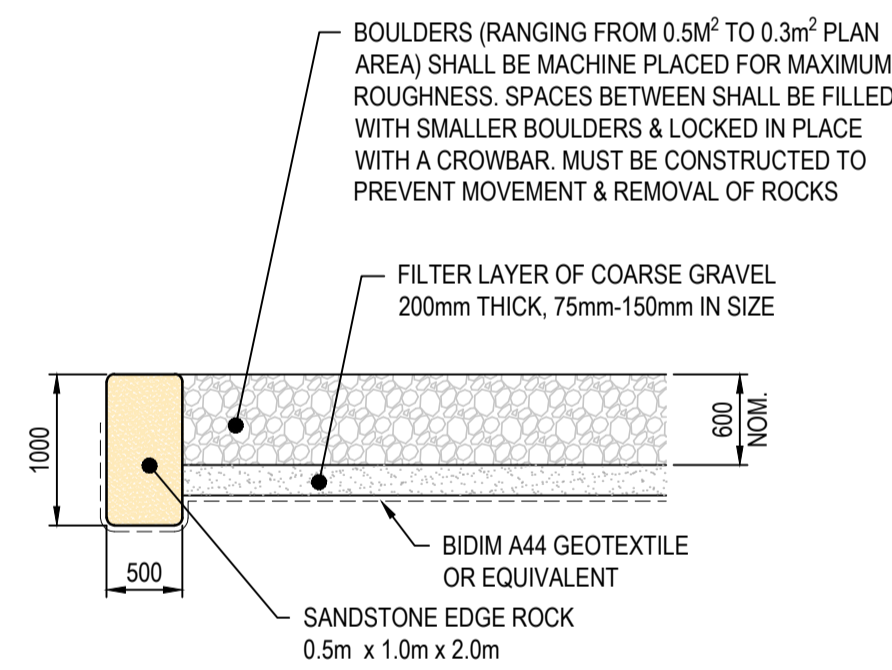
DETAIL B

- TO BE READ IN CONJUNCTION WITH NOTES ON DETAIL A & BCC A(BS)162M.
- REFER TO STRUCTURAL WALL FOOTING PLAN & ELEVATION.



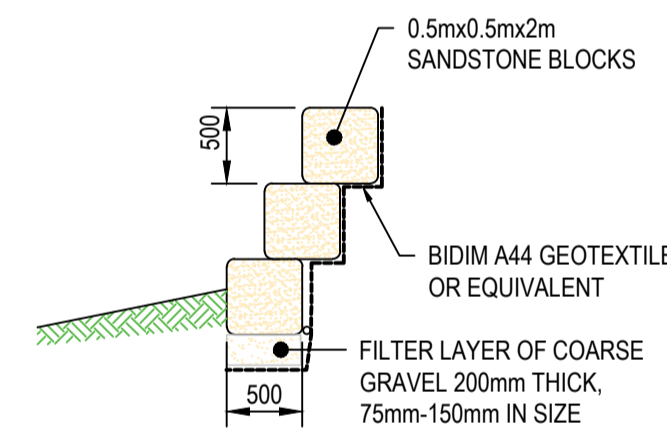
**PLAN
STRUCTURAL WALL**

**ELEVATION
STRUCTURAL WALL**



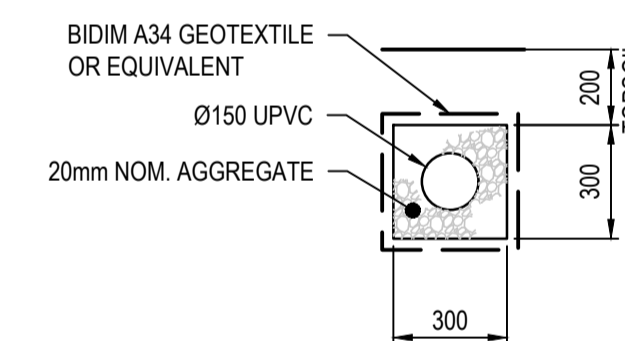
**DETAIL C
TYPICAL RIP RAP & EDGING BLOCK DETAIL**

SCALE 1:50



**SANDSTONE BLOCK
RETAINING WALL DETAIL**

SCALE 1:50



TYPICAL SUBSOIL DRAIN DETAIL

SCALE 1:20

THIS DRAWING MAY BE PREPARED IN COLOUR AND MAY BE INCOMPLETE IF COPIED

ISSUE 50% DESIGN

REV	DATE	DESCRIPTION	CHECKED	APPROVED	DATE
1	16.02.22	ISSUED FOR 50% DESIGN REVIEW	DY	GE	16.02.22
0	02.11.21	ISSUED FOR DRP 50% REVIEW	DY	GE	02.11.21

SCALE	DO NOT SCALE FROM DRAWING
SCALE 1:20 (A1)	SCALE 1:40 (A3)
SCALE 1:50 (A1)	SCALE 1:100 (A3)

DRAWINGS / DESIGN PREPARED BY	TITLE	NAME	DATE
DRAWINGS VERIFIED BY	SURVEYED	B.C.C.	
	DESIGNED	DY	
	DRAWN	LMCB	
	CHECKED		
	ACCEPTED - BCC CPEng NER Certified		

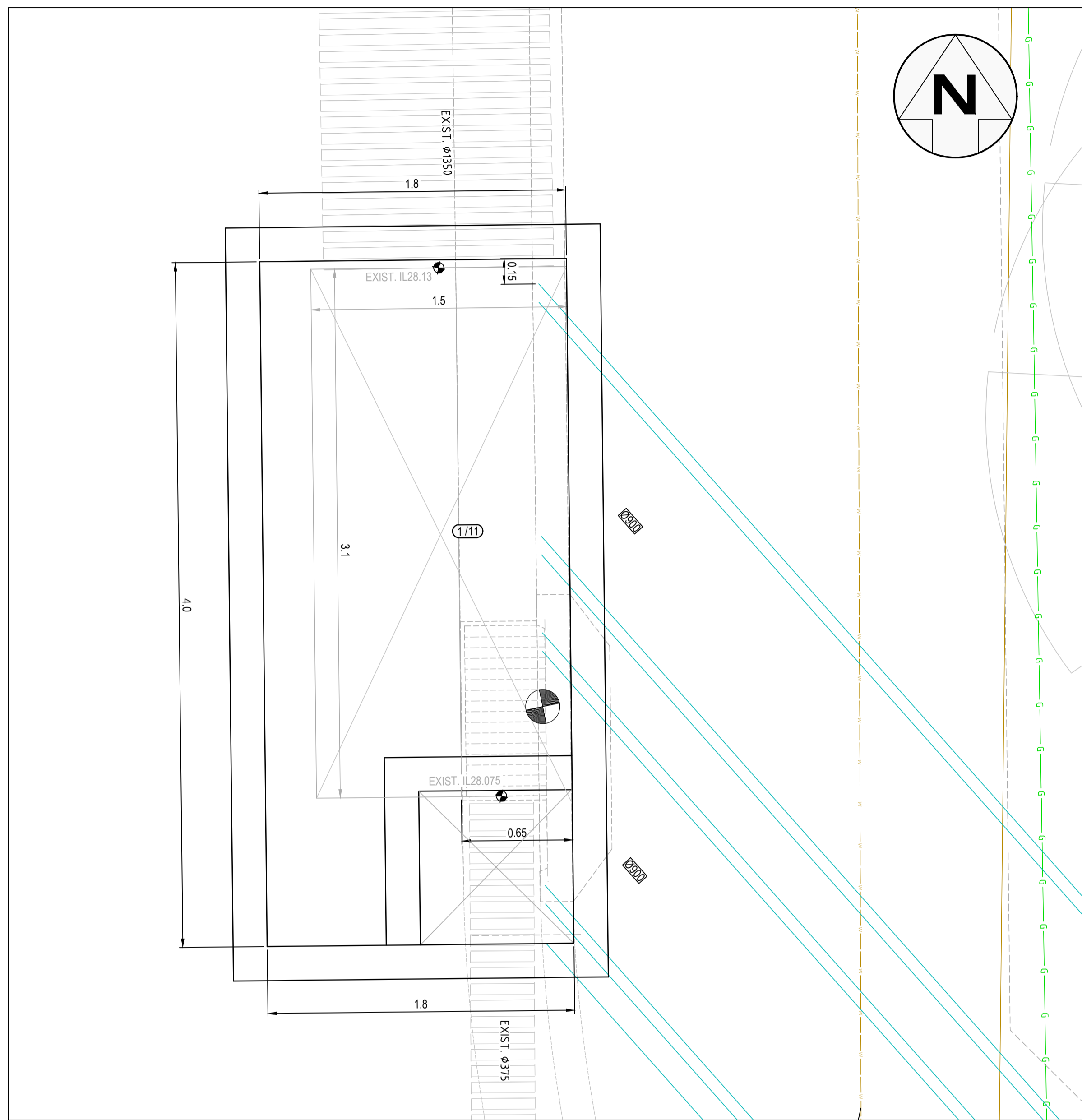
CO-ORDINATE SYSTEM	HEIGHT DATUM
GDA 94 / MGA ZONE 56	AHD



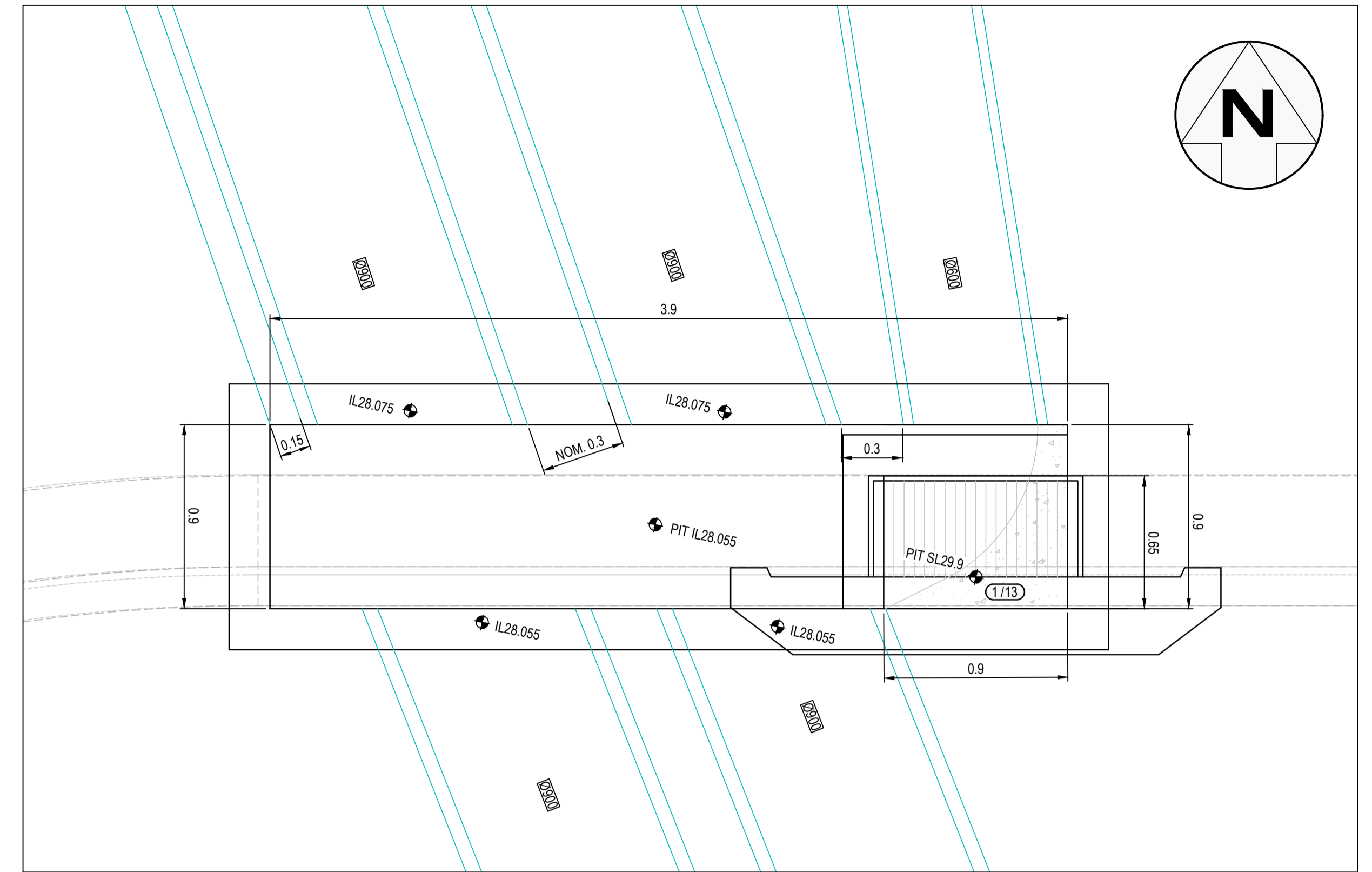
COUNCIL CHAMBERS: 62 FLUSHCOMBE ROAD, BLACKTOWN, NSW 2148
ALL MAIL: GENERAL MANAGER, PO BOX 63, BLACKTOWN, NSW 2148
TELEPHONE: (02) 9839 6000 FAX: (02) 9831 1961 DX: 8117 BLACKTOWN

PROJECT	FILE No	PLAN No	REV
N/W GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	F16/646	E39-15V	1
TITLE	RETAINING WALL SUBSOIL RIP RAP & STD FENCE DETAILS		
CAD FILE	E39-15V DETAILS.dwg		

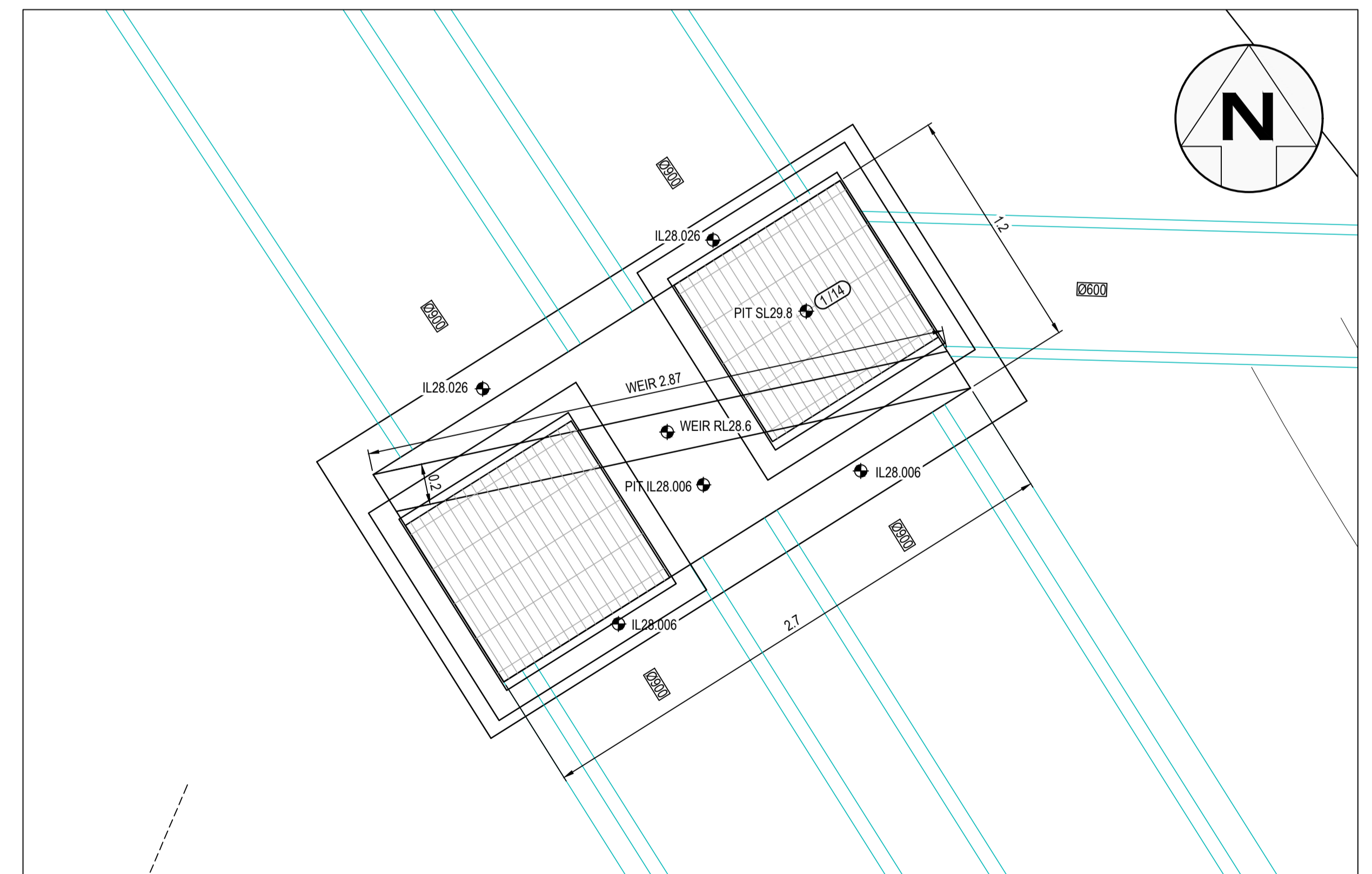
A1
SHEET
17
OF
21



PIT 1/11
SCALE 1:20



PIT 1/13
SCALE 1:20



PIT 1/14
SCALE 1:20

ISSUE 50% DESIGN

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REV	DATE	DESCRIPTION	CHECKED	APPROVED	DATE
1	16.02.22	ISSUED FOR 50% DESIGN REVIEW	DY	GE	16.02.22
0	02.11.21	ISSUED FOR DRP 50% REVIEW	DY	GE	02.11.21

SCALE	DO NOT SCALE FROM DRAWING
0 0.2 0.4 0.6 0.8 1.0 1.2 1.4 1.6	SCALE 1:20 (A1) SCALE 1:40 (A3)

DRAWINGS / DESIGN PREPARED BY	TITLE	NAME	DATE
DRAWINGS VERIFIED BY	SURVEYED	B.C.C.	
	DESIGNED	DY	
	DRAWN	LMCB	
	CHECKED		
	ACCEPTED	BCC CPEng NER Certified	

CO-ORDINATE SYSTEM	HEIGHT DATUM
GDA 94 / MGA ZONE 56	AHD

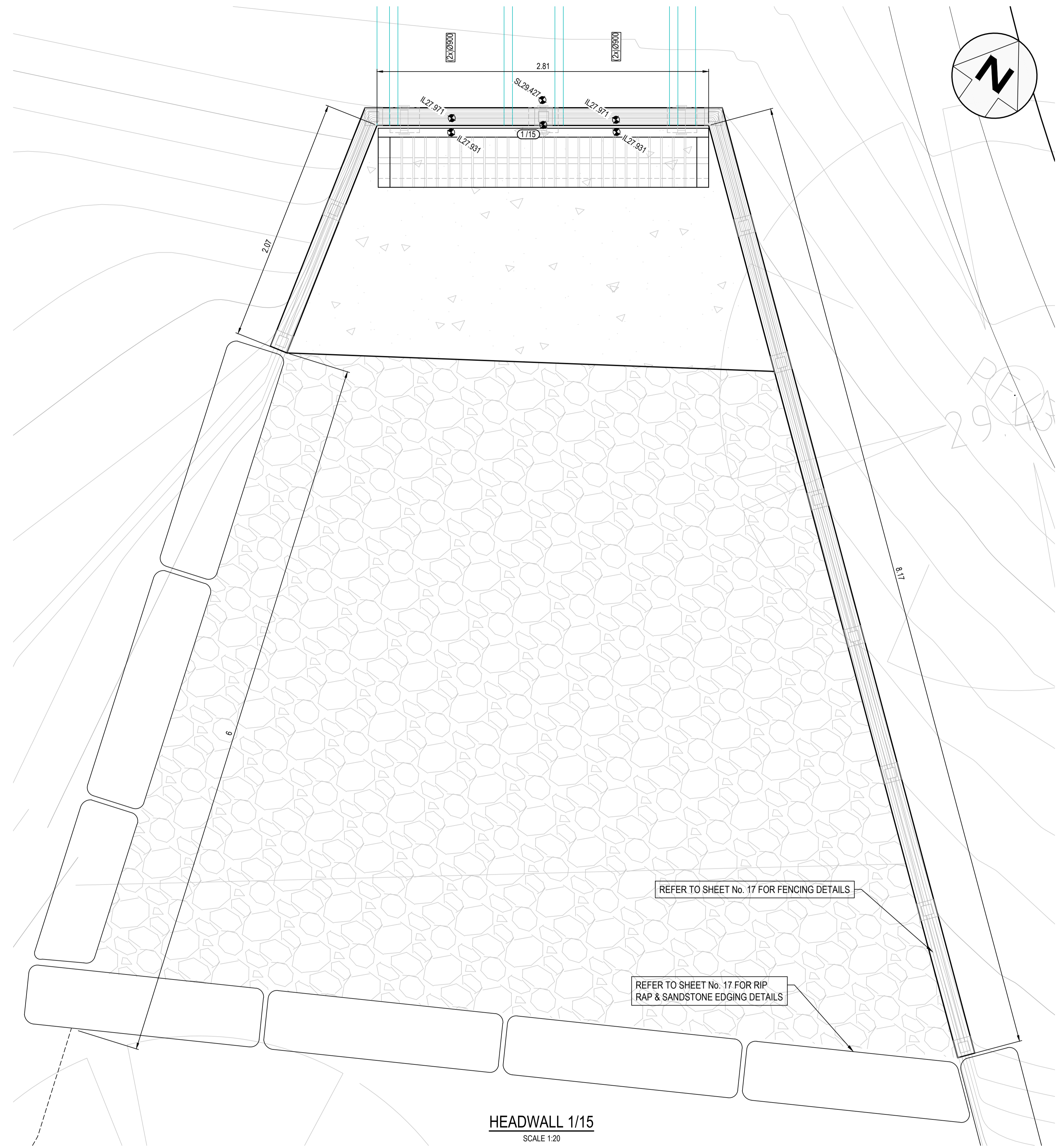


PROJECT	CAD FILE	FILE No	PLAN No	REV
N/W GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	E39-15V STRUCTURAL PITS.dwg	F16/646	E39-15V	1

A1
SHEET
18
OF
21

DATUM R.L. 21				
W.A.E				
LEVEL				
LEVEL DIFF.				
SURFACE-INVERT	0.45	1.15	1.15	0.45
DESIGN				
SURFACE LEVEL	28.4	29.1	29.1	28.3
DESIGN INVERT				
LEVEL	27.95	27.95	27.95	27.85
EXISTING				
LEVEL	29.28	29.4	29.47	29.13
LEVEL				
CHAINAGE	0	2.17	5.2	13.49

LONGITUDINAL SECTION - HEADWALL 1/15
 SCALE 1:100 (H)
 SCALE 1:100 (V)



HEADWALL 1/15
 SCALE 1:20

REFER TO SHEET No. 17 FOR FENCING DETAILS

REFER TO SHEET No. 17 FOR RIP RAP & SANDSTONE EDGING DETAILS

ISSUE 50% DESIGN

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REV	DATE	DESCRIPTION	CHECKED	APPROVED	DATE
1	16.02.22	ISSUED FOR 50% DESIGN REVIEW	DY	GE	16.02.22
0	02.11.21	ISSUED FOR DRP 50% REVIEW	DY	GE	02.11.21

SCALE	DO NOT SCALE FROM DRAWING
0 0.2 0.4 0.6 0.8 1.0 1.2 1.4 1.6 SCALE 1:20 (A1)	0 1 2 3 4 5 6 7 8 SCALE 1:100 (A1)
0 1 2 3 4 5 6 7 8 SCALE 1:200 (A3)	0 1 2 3 4 5 6 7 8 SCALE 1:200 (A3)

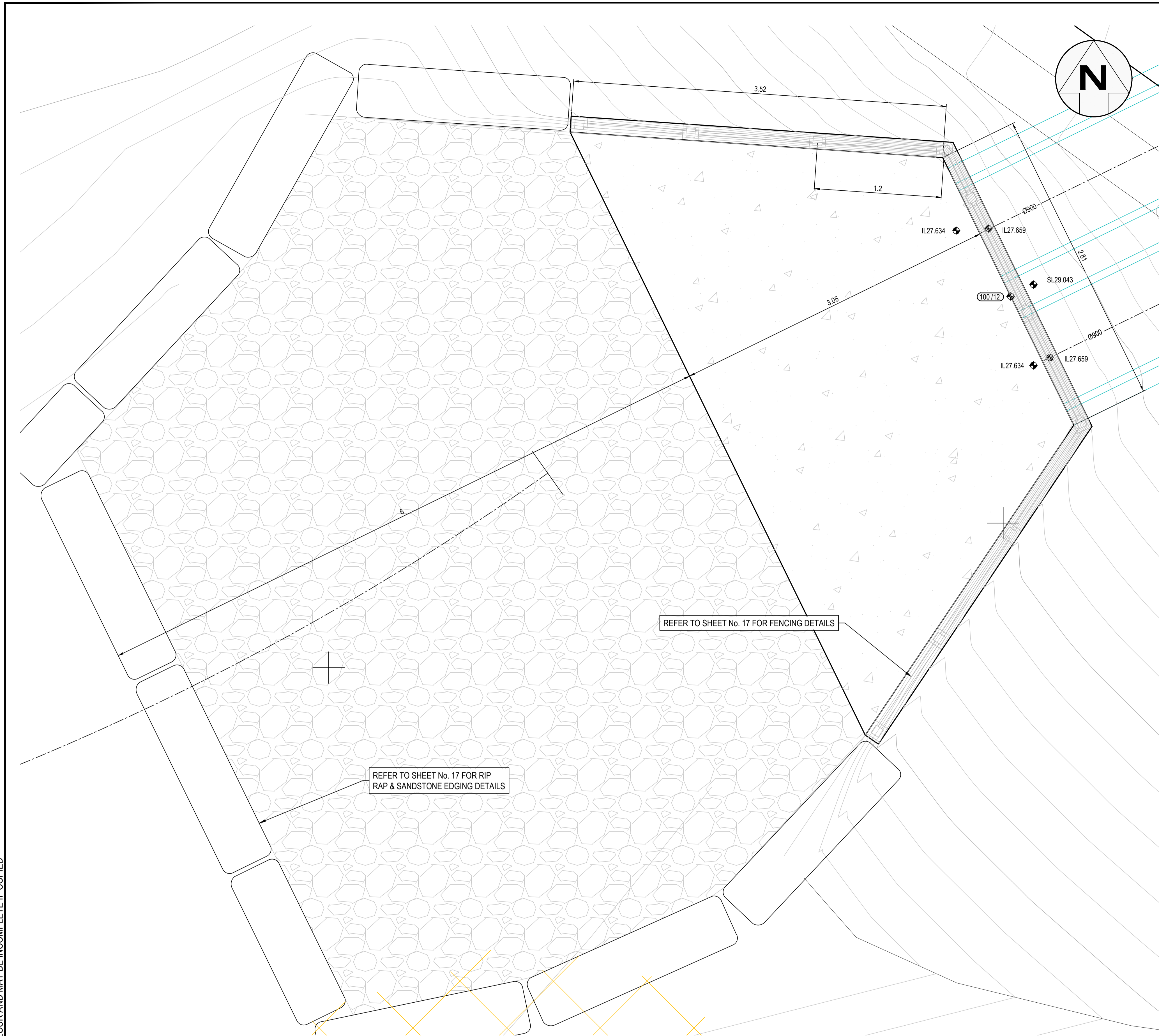
DRAWINGS / DESIGN PREPARED BY		TITLE	NAME	DATE
DRAWINGS VERIFIED BY		SURVEYED	B.C.C.	
		DESIGNED	DY	
		DRAWN	LMCB	
		CHECKED		
		ACCEPTED - BCC CPEng NER Certified		

CO-ORDINATE SYSTEM	HEIGHT DATUM
GDA 94 / MGA ZONE 56	AHD



PROJECT	FILE No	PLAN No	REV
NW GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	F16/646	E39-15V	1
TITLE	CAD FILE	FILE No	PLAN No
STRUCTURAL HEADWALL 1-15 PLAN & ELEVATION	E39-15V STRUCTURAL PITS.dwg	F16/646	E39-15V

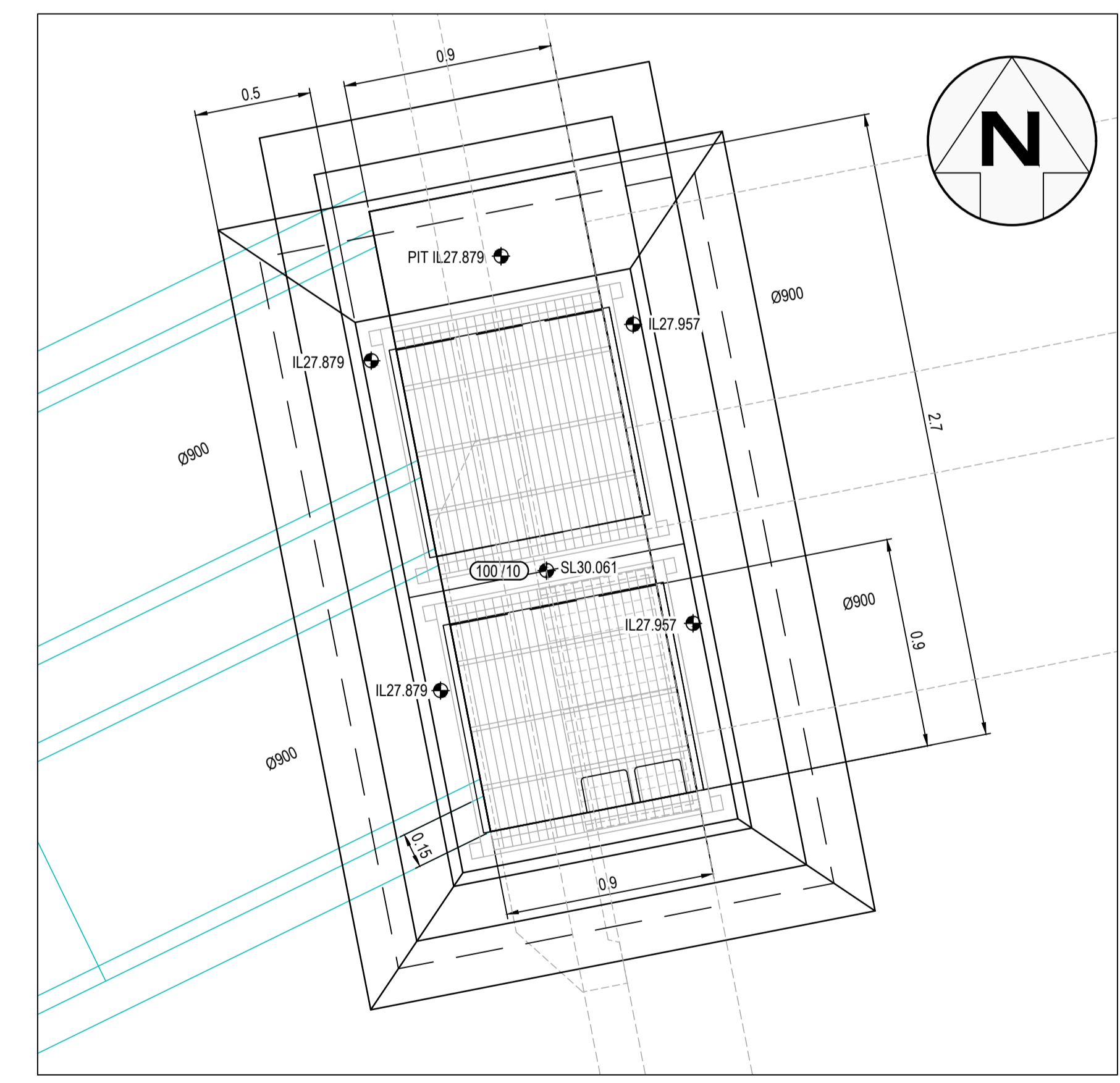
A1
 SHEET
 19
 OF
 21



HW 100/12
SCALE 1:20

DATUM R.L. 21				
W.A.E				
LEVEL				
LEVEL DIFF.				
SURFACE-INVERT	0.5	1.3	1.3	0.5
DESIGN				
SURFACE LEVEL	28	28.8	28.8	28
DESIGN INVERT				
LEVEL	27.5	27.5	27.5	27.5
EXISTING				
LEVEL	29.59	29.49	29.35	29.27
CHAINAGE	0	3.68	6.66	10.74

LONGITUDINAL SECTION - HEADWALL 100/12
SCALE 1:100 (H)
SCALE 1:100 (V)



PIT 100/10
SCALE 1:20

ISSUE 50% DESIGN

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REV	DATE	DESCRIPTION	CHECKED	APPROVED	DATE
1	16.02.22	ISSUED FOR 50% DESIGN REVIEW	DY	GE	16.02.22
0	02.11.21	ISSUED FOR DRP 50% REVIEW	DY	GE	02.11.21

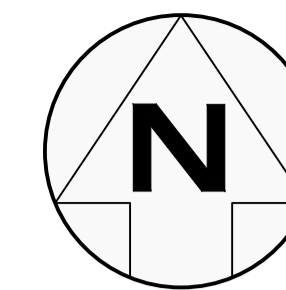
SCALE	DO NOT SCALE FROM DRAWING
SCALE 1:20 (A1)	SCALE 1:40 (A3)

DRAWINGS / DESIGN PREPARED BY		TITLE	NAME	DATE
DRAWINGS VERIFIED BY		SURVEYED	B.C.C.	
		DESIGNED	DY	
		DRAWN	LMCB	
		CHECKED		
		ACCEPTED	- BCC CPEng NER Certified	

CO-ORDINATE SYSTEM	HEIGHT DATUM
GDA 94 / MGA ZONE 56	AHD



PROJECT		N/W GROWTH CENTRE - ALEX AVENUE PRECINCT		A1
		BASIN E3.2 RAILWAY TERRACE SCHOFIELDS		
TITLE		PIT 100-10, HEADWALL 100-12 PLAN & ELEVATION		20
				OF
				21
CAD FILE	FILE No	PLAN No	REV	
E39-15V STRUCTURAL PITS.dwg	F16/646	E39-15V	1	



LANDSCAPE LEGEND

-  ANGOPHORA COSTATA
-  ANGOPHORA FLORIBUNDA
-  NATIVE MIX
REFER TO LANDSCAPE DETAILS
-  TURF
-  BIO MIX
REFER TO LANDSCAPE DETAILS



CODE	BOTANICAL NAME	COMMON NAME	POTSIZE	SPACING Plants per SqM	COVER %	No OF PLANTS
STREET TREES						
AC	ANGOPHORA COSTATA	SMOOTH-BARKED APPLE	75L	6m interval	N/A	48
RESERVE TREES						
AF	ANGOPHORA FLORIBUNDA	ROUGH-BARKED APPLE	75L	6m interval	N/A	65
NATIVE MIX						
				AREA		2995 sqm
Asbd	AUSTROSTIPA SCABRA	CORKSCREW GRASS	tube	8	15	3594
DCbd	DIANELLA CAERULEA	BLUE FLAX LILY	tube	8	25	5990
Ebbd	ERAGROSTIS BROWNI	PADDOCK LOVEGRASS	tube	8	10	2396
FNbd	FICINA NODOSA	KNOBBY CLUB RUSH	tube	8	15	3594
Lfbd	LACHNAGROSTIS FILIFORMIS	BLOWN GRASS	tube	8	10	2396
Lbbd	LACHNAGROSTIS BILLARDIEREI	COAST BLOWN-GRASS	tube	8	10	2396
Ausbd	AUSTROSTIPA STIPOIDES	PRICKLY SPEAR-GRASS	tube	8	15	3594
BIO MIX						
				AREA		1564 sqm
Cabw	CAREX APPRESSA	TALL SEDGE	tube	8	2	250
Jubw	JUNCUS USITATUS	COMMON RUSH	tube	8	10	1251
Fnbw	FICINA NODOSA	KNOBBY CLUB RUSH	tube	8	2	250
Jkbw	JUNCUS KRAUSSII	SEA RUSH	tube	8	10	1251
Gsbw	GAHNIA SIEBERIANA	CHAFFY SAW-SEDE	tube	8	5	626
Cebw	CYPERUS EXALTATUS	BUNCHY SEDGE	tube	8	5	626
Bpbw	BALOSKION / RESTIO PALLENS	DIDGERY STICKS	tube	8	2	250
Smbw	SCHOENOPLECTUS MUCRONATUS	BOG BULRUSH	tube	8	2	250
Svbw	SCHOENOPLECTUS VALIDUS	RIVER CLUB RUSH	tube	8	2	250
Bcbw	BOLBOSCHOENUS CALDWELLII	MARSH CLUB-RUSH	tube	8	10	1251
Mdbw	MELALEUCA DECORA	WHITE FEATHER HONEYMYRTLE	tube	4	10	626
Mibw	MELALEUCA LINARIFOLIA	SNOW IN SUMMER	tube	4	10	626
Msbw	MELALEUCA STYPHLOIDES	PRICKLY LEAVE PAPERBARK	tube	4	10	626
Lcbw	LEPTOSPERMUM CARDWELL	TEA TREE	tube	4	10	626
Lpbw	LEPTOSPERMUM PETERSONII	LEMON SCENTED TEA TREE	tube	4	10	626

ISSUE 50% DESIGN

THIS DRAWING MAY BE PREPARED IN COLOUR AND MAY BE INCOMPLETE IF COPIED

REV	DATE	DESCRIPTION	CHECKED	APPROVED	DATE
1	16.02.22	ISSUED FOR 50% DESIGN REVIEW	DY	GE	16.02.22
0	02.11.21	ISSUED FOR DRP 50% REVIEW	DY	GE	02.11.21

SCALE	DO NOT SCALE FROM DRAWING
0 2.5 5 7.5 10 12.5 15 17.5 20	SCALE 1:250 (A1) SCALE 1:500 (A3)

DRAWINGS / DESIGN PREPARED BY	TITLE	NAME	DATE
DRAWINGS VERIFIED BY	SURVEYED	B.C.C.	
	DESIGNED	DC	
	DRAWN	DC	
	CHECKED		
	ACCEPTED - BCC CP/Eng NER Certified		

CO-ORDINATE SYSTEM	HEIGHT DATUM
GDA 94 / MGA ZONE 56	AHD

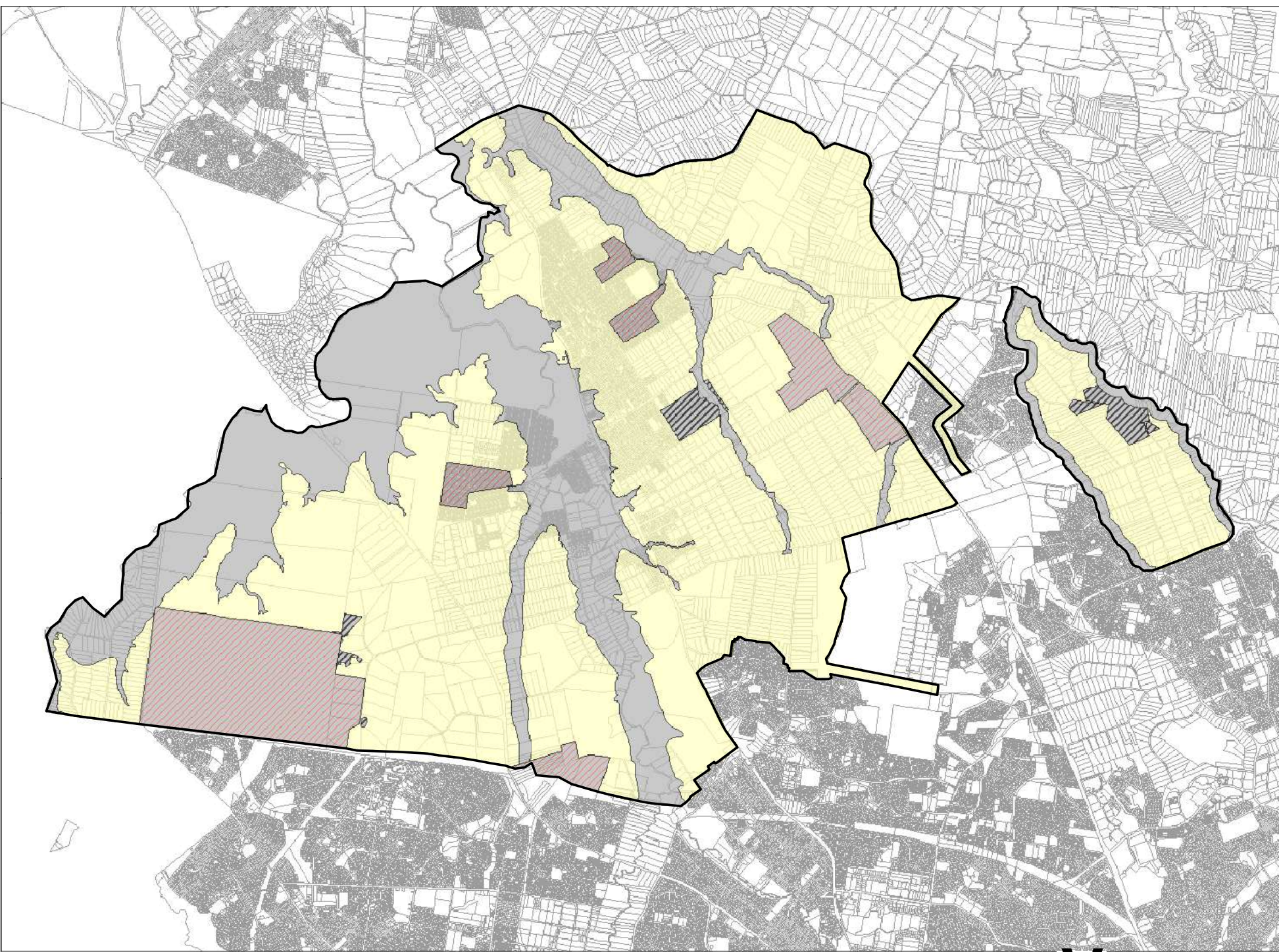


PROJECT NW GROWTH CENTRE - ALEX AVENUE PRECINCT BASIN E3.2 RAILWAY TERRACE SCHOFIELDS	A1 SHEET
TITLE LANDSCAPE PLAN	21 OF 21
CAD FILE E39-15V LANDSCAPE.dwg	REV 1
FILE No F16/646	
PLAN No E39-15V	

COUNCIL CHAMBERS: 62 FLUSHBOOME ROAD, BLACKTOWN, NSW 2148
ALL MAIL: GENERAL MANAGER, PO BOX 63, BLACKTOWN, NSW 2148
TELEPHONE: (02) 9839 6000 FAX: (02) 9831 1961 DX: 8117 BLACKTOWN

ATTACHMENT B

Biodiversity Certification Mapping



Legend

- NW Growth Centre Boundary
- Certified Area
- Non Certified Area
- Non Certified Area
Land subject to Condition 12 of the Biodiversity Certification Order.
- Non Certified Area
Subject to further investigation in accordance with Conditions 14, 17 and 18 of the Biodiversity Certification Order.

This is the Map referred to in the Biodiversity Certification Order.

November 2007

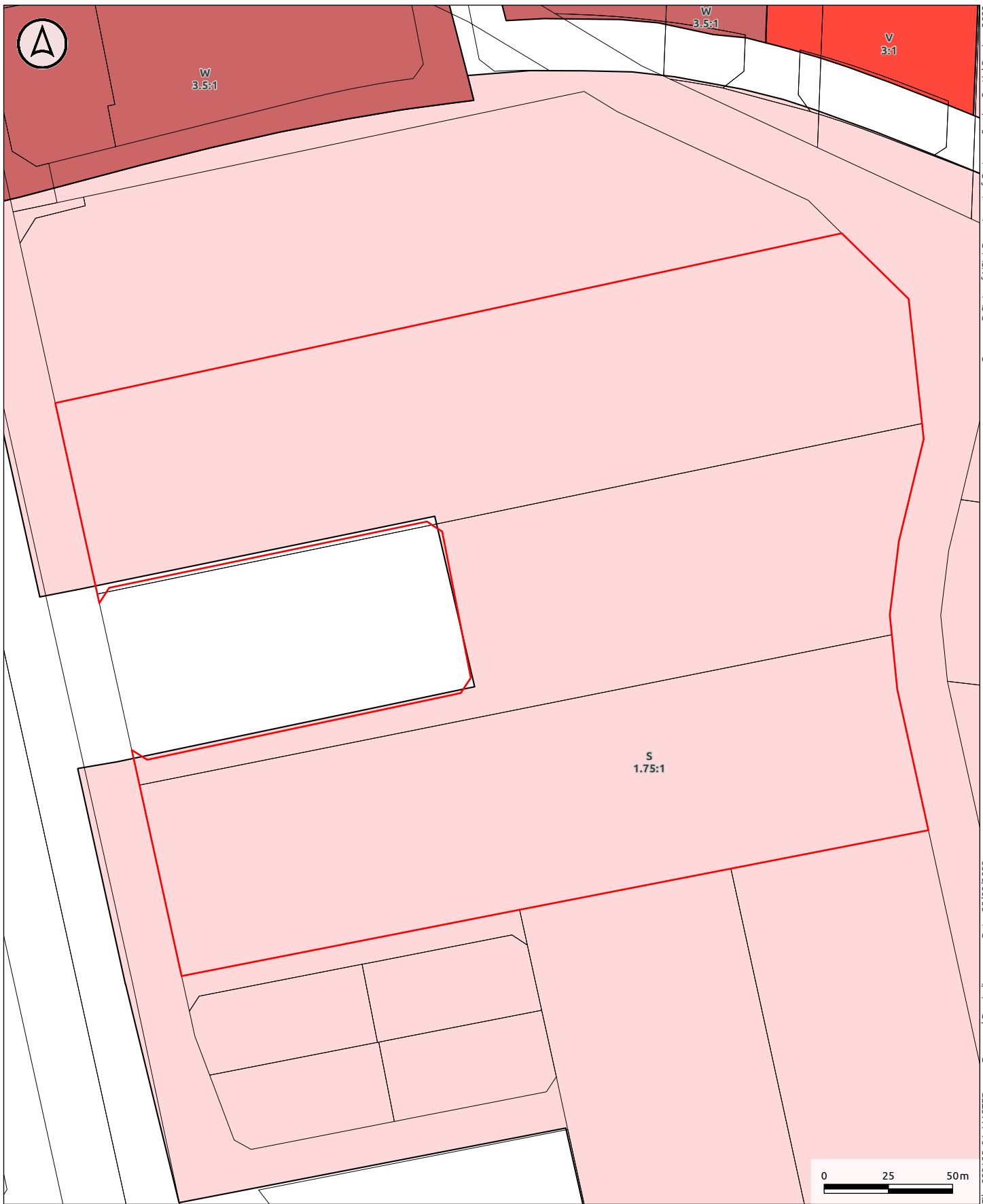
Disclaimer
The information contained on this map is, to the best of the Government's knowledge correct. However, no warranty or guarantee is provided by the Government and no liability is accepted for any loss or damage resulting from any person relying upon or using the information contained in the map.

North West Growth Centre - Biodiversity Certification



ATTACHMENT C

Existing and Proposed Mapping



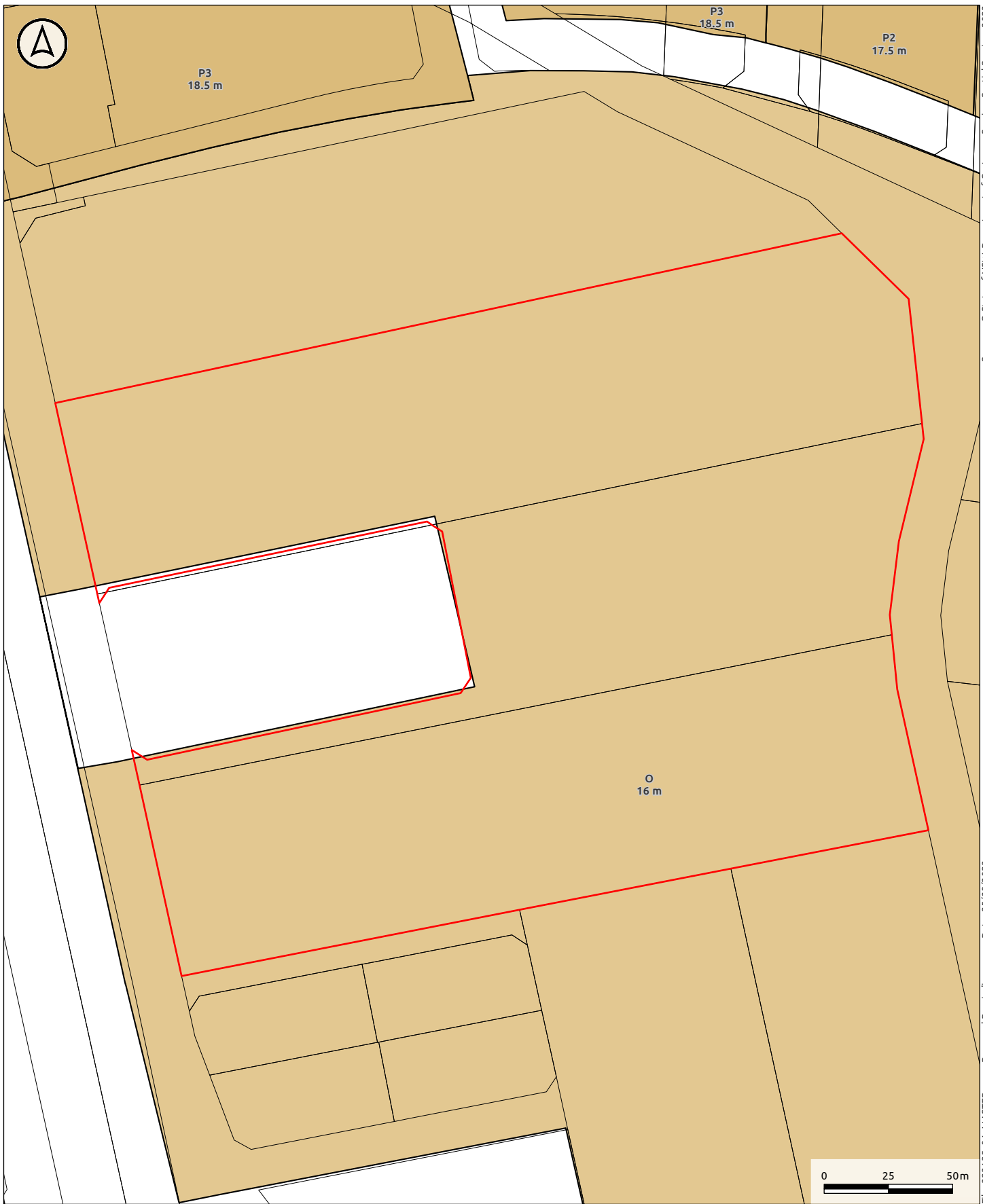
- Legend**
- Site
 - Cadastre
- Floor Space Ratio (FSR)**
- S (1.5 - 1.99)
 - V (3 - 3.49)
 - W (3.5 - 3.99)

EXISTING FLOOR SPACE RATIO

PROVINCIAL INVESTMENTS (NSW) PTY LTD

Planning Proposal





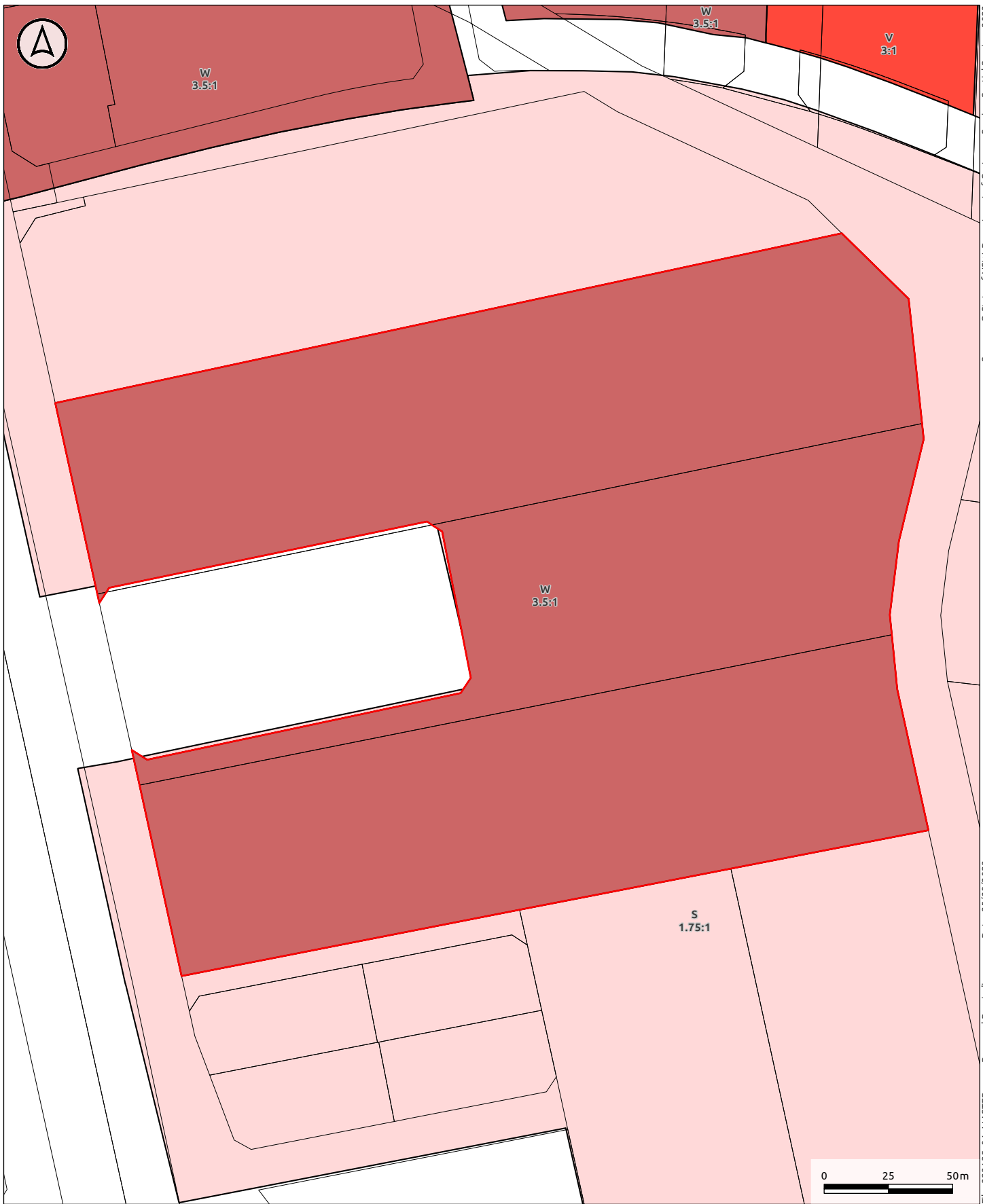
File: 323135_01_MASTER.aprx Prepared By: zindia.narver Date: 25/08/2023
 Sources: © State of NSW, Department of Customer Service, Spatial Services 2023
 © State of NSW, Department of Planning and Environment 2023

- Legend**
- Site
 - Cadastre
- Height of Building (HOB)**
- J (9-9.9 m)
 - O (15-16.9 m)
 - P2 (17-18.9 m)
 - P3 (17-18.9 m)

EXISTING BUILDING HEIGHT

PROVINCIAL INVESTMENTS (NSW) PTY LTD
Planning Proposal





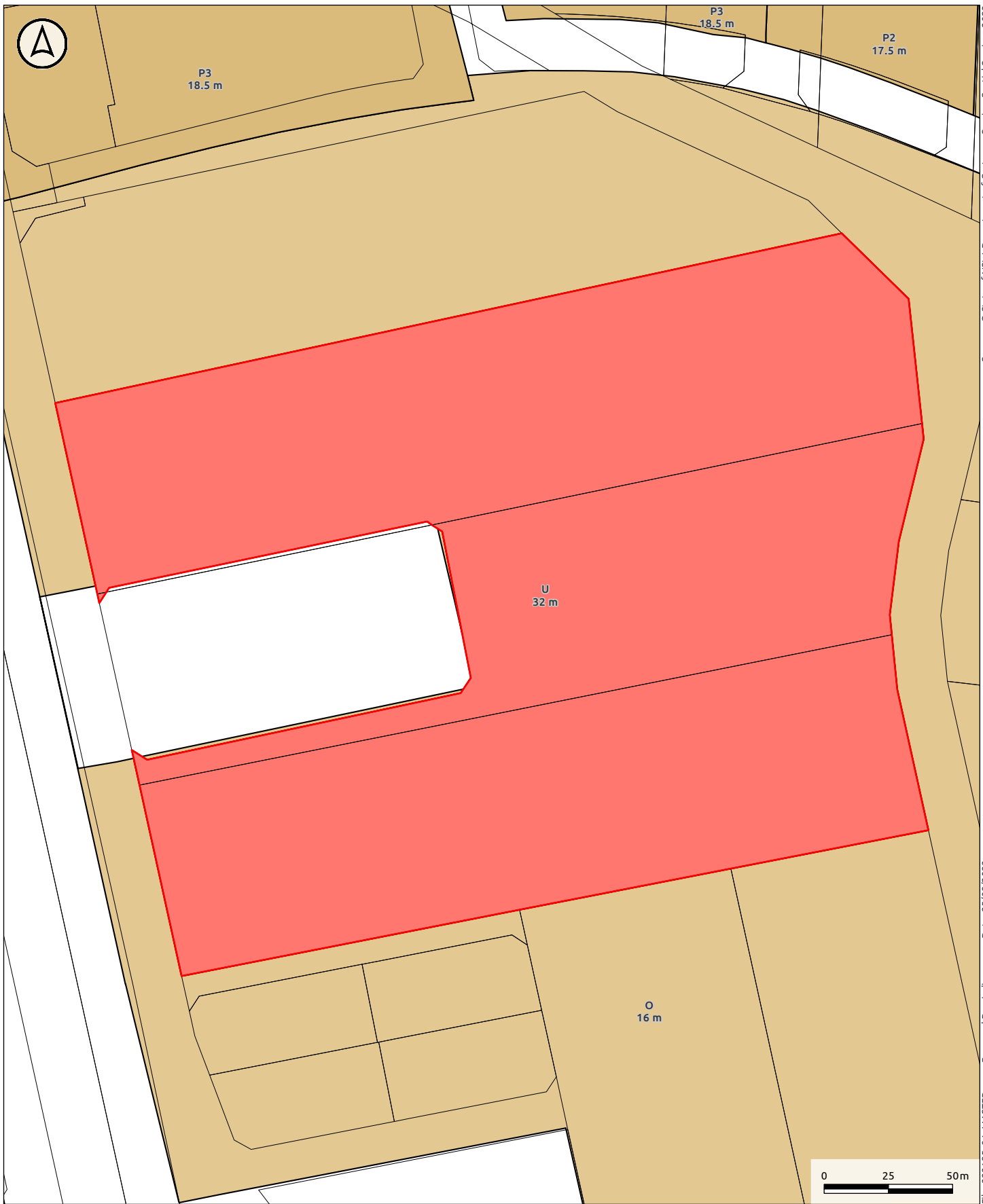
Sources: © State of NSW, Department of Customer Service, Spatial Services 2023
© State of NSW, Department of Planning and Environment 2023
File: 323135_01_MASTER.aprx Prepared By: zindia.narver Date: 25/08/2023

- Legend**
- Site
 - Cadastre
- Floor Space Ratio (FSR)**
- S (1.5 - 1.99)
 - V (3 - 3.49)
 - W (3.5 - 3.99)

PROPOSED FLOOR SPACE RATIO

PROVINCIAL INVESTMENTS (NSW) PTY LTD
Planning Proposal





File: 323135_01_MASTER.aprx Prepared By: zindia.narver Date: 25/08/2023
 Sources: © State of NSW, Department of Customer Service, Spatial Services 2023
 © State of NSW, Department of Planning and Environment 2023

Legend

	Site
	Cadastre
Height of Building (HOB)	
	J (9-9.9 m)
	O (15-16.9 m)
	P2 (17-18.9 m)
	P3 (17-18.9 m)
	U (30 - 34.9 m)

PROPOSED BUILDING HEIGHT

PROVINCIAL INVESTMENTS (NSW) PTY LTD
 Planning Proposal

