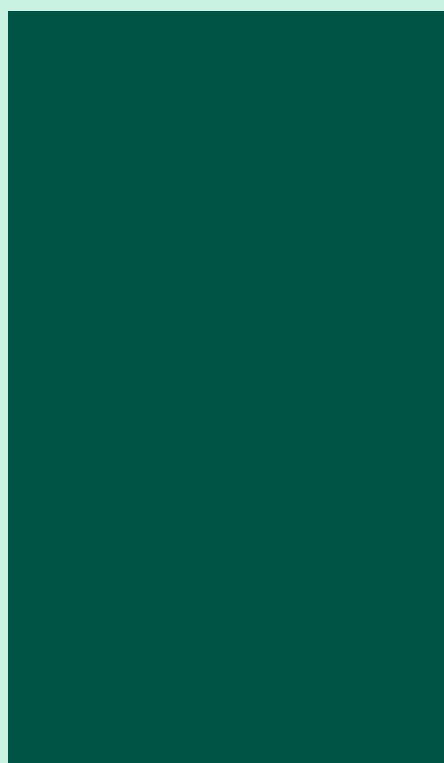
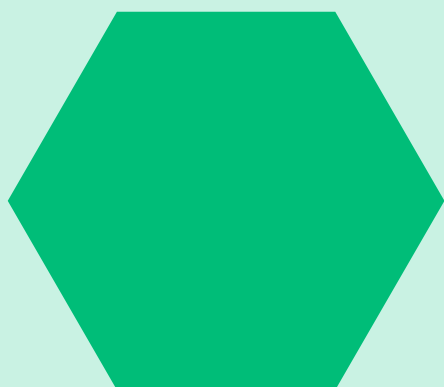


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# Homebush State-led Rezoning Urban Design Report

July 2024



# Acknowledgment of Country

*Cox acknowledges the Wangal people as the Traditional Custodians of the lands on which the project area lies. We pay respect to their Elders, past and present, and recognise ongoing connection to Country and culture.*



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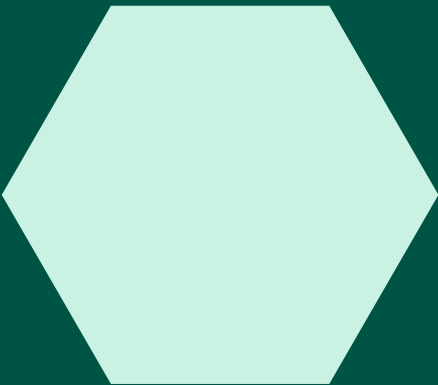
This report was prepared by COX Architecture on behalf of Department of Planning, Housing and Infrastructure.

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# Purpose of the Report



# Purpose of the Report

This Homebush Precinct TOD Urban Design Report (the Report) is a guiding document to explain the State-led rezoning process for the Homebush Precinct.

The purpose of the Report is to;

- Review, validate and clearly document the proposals and strategies within the various plans and policies that presently apply to the Homebush Precinct
- Prepare an Urban Design Report that is informed by Connecting with Country engagement processes and opportunities
- Inform the preparation of a Design Guide, Explanation of Intended Effects (EIE) and infrastructure listing for the Homebush Precinct; and
- Test that the planning controls proposed within the State-led rezoning can deliver the desired housing outcomes, whilst meeting or exceeding the achieving the amenity based performance controls within the Urban Design Report and the Design Guide.

The Report achieves this by;

- Identifying urban design controls parameters that will underpin the proposed development;
- Providing a site and context analysis that identifies opportunities to be considered;
- Demonstrating that potential future uses, built form and landscape can achieve high quality place outcomes;
- Proposing building heights, building envelopes, and draft development principles to be incorporated into future planning controls at a later stage; and
- Assess and test to reduce impacts amenity impacts, including the provision of new open spaces, preservation of heritage items and the delivery of new streets and through site links.

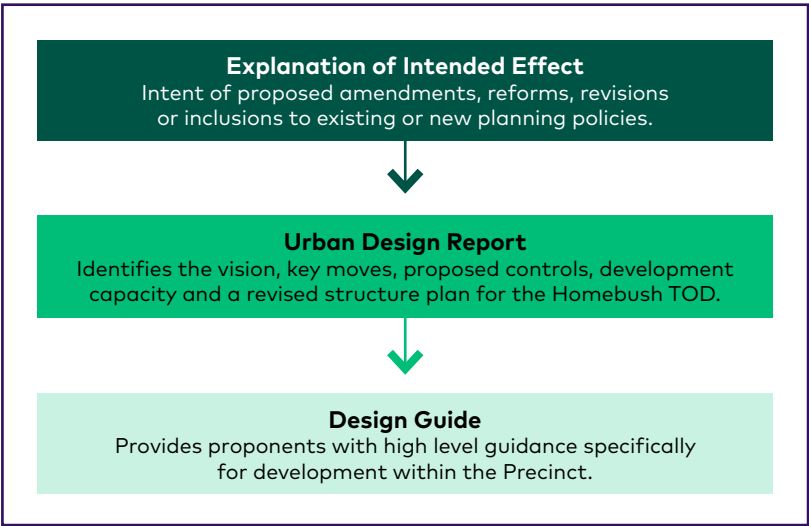
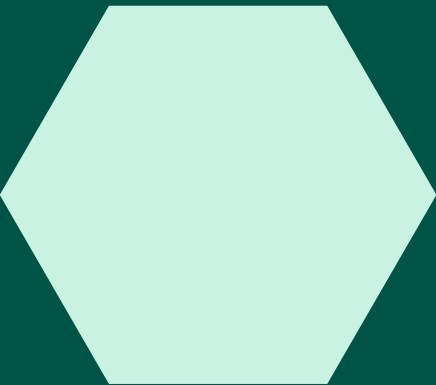


Figure 1: Relationship of Urban Design Report to the Homebush TOD documents

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## Starting with Country



The Homebush State-Led Rezoning Urban Design Report begins with the understanding of an Aboriginal worldview of 'Country' – it is important to think about how to approach implementing this learning in the design. The GANSW Designing with Country Discussion Paper recognises at the core, "Aboriginal people know that if we care for Country, it will care for us. For tens of thousands of years, they have managed, cultivated and cared for the landscape where our towns and cities were established and continue to grow"<sup>1</sup>

When planning on Country, it is important that proponents respect Country and the impacts decisions made have on Country, acknowledging the Homebush precinct and landscapes have changed significantly over generations.

It is important to acknowledge and consider how the Cultural landscape can be preserved.

To support our understanding of Country and what our project can do for Country, Cox Inall Ridgeway has supported the Urban Design with 'Outcomes for Country' – a guide from GANSW's Connecting with Country Framework, that take learnings from Country and support the implementation now and into the future.

<sup>1</sup> <https://www.aidr.org.au/media/7760/designing-with-country-discussion-paper.pdf>

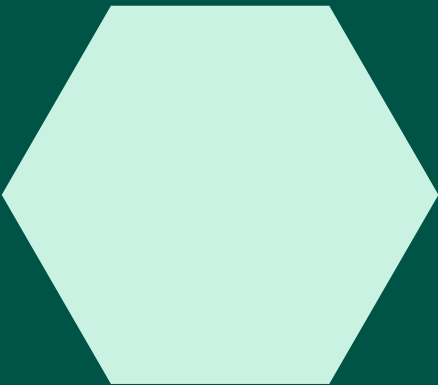
For further details on Connecting with Country framework, cultural landscape, flora, fauna and outcomes of Country, refer to Cox Inall Ridgeway *Connecting with Country Framework Homebush TOD*.

## Outcomes for Country

	Healthy Country	Healthy Community	Protecting Aboriginal Heritage	Cultural Competency	Better Places
Indicators of Success	<ul style="list-style-type: none"> <li>Open space planning needs to incorporate endemic flora and fauna outcomes, as well as opportunities for Aboriginal storytelling.</li> <li>Aboriginal housing targets need to be considered as part of ensuring First Nations people have places to live in the area.</li> <li>Economic opportunity for First Nations businesses and community services (designated spaces)</li> </ul>	<ul style="list-style-type: none"> <li>The design guide and future developments in the area must consider culturally safe and inclusive spaces that are inviting for First Nations people.</li> <li>A cultural space for the local community to come together to share stories and experiences in the area.</li> <li>Aboriginal employment, housing and socio-economic indicators could be considered as part of future developments on site.</li> </ul>	<ul style="list-style-type: none"> <li>Place naming, signage, wayfinding in local art and language</li> <li>Designated space to include art which tells a story of the area, creation and heritage.</li> <li>Aboriginal storytelling opportunities in open spaces within the Homebush Precinct. These can include Truth-Telling about historical traumas which occurred on the Country, re-introducing endemic flora species.</li> </ul>	<ul style="list-style-type: none"> <li>Engaging and creating a register of local knowledge holders who are willing and eager to educate which can be distributed out to the community e.g. schools, community centres etc.</li> <li>Mandating cultural competency training for designers and developers on site.</li> <li>A visual representation project which includes updating the technology and collaborating with culture.</li> </ul>	<ul style="list-style-type: none"> <li>Creating a serene environment protected from noise in open spaces within Homebush Precinct weaving in human and non-human elements</li> <li>A dedicated mural designed by local artists reflecting aspects of the local area connecting the younger generations and traditional owners through cultural activity</li> <li>An action plan with the recommendations of the younger generations who are the future of reconciliation in NSW.</li> </ul>

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## The Homebush Precinct



# Parramatta Road Corridor Urban Transformation Strategy

The Parramatta Road Corridor Urban Transformation Strategy (PRCUTS) is the NSW Government's 30-year plan setting out how the Parramatta Road Corridor will grow and bring new life to local communities living and working along the Corridor. The Parramatta Road Corridor traverses 20 kilometres from Granville in the west to Camperdown in the east. The corridor includes land adjoining Parramatta Road, which have been identified as Precincts.

The Homebush Precinct is located directly north-west of Strathfield Town Centre and Strathfield Train Station. As the largest among the eight Precincts along the corridor, it stretches from the Western Rail Line, extending northward along the Northern Rail Line into Concord West. The Precinct's borders are defined by Homebush Bay Drive, Mason and Bressington Parks, and Liberty Grove to the north and west. To the south, the boundary is marked by Parramatta Road and the Western Rail Line. Concord Road and Swan Avenue determine the eastern extent of the Precinct.

PRCUTS envisions the Homebush Precinct transforming into an active and varied hub, integrating higher-density housing with a variety of mixed uses. This transformation will be supported by a network of green links and open spaces, providing walking access to four train stations

The Parramatta Road Corridor Planning and Design Guidelines were established to manage land use changes and promote design excellence throughout the corridor as envisioned by the strategy.

The purpose of the guidelines is twofold: to outline priorities and principles ensuring future developments achieve high design quality and excellence, and to guide the evolving character of the corridor while preserving the distinctiveness of different areas. These guidelines provide recommendations for future Masterplans and Planning Proposals, including suggested land uses, building heights, and densities.

PRCUTS proposes realizing the vision through several key strategies:

- Leveraging the vibrancy and character of the Bakehouse Quarter.
- Establishing a high-quality open space network and enhancing the areas surrounding the train stations.
- Implementing tree planting initiatives and enhancing the environment along Parramatta Road.
- Ensuring the sustainability of shops and commercial establishments along Parramatta Road.
- Addressing issues related to on-street parking along Parramatta Road.
- Mitigating traffic congestion along Parramatta Road, including improving north-south connections.
- Increasing service frequency at Flemington, Homebush, Concord West, and North Strathfield Stations.
- Overcoming barriers such as the M4 Motorway and Concord Road.
- Managing challenges related to flooding, noise, and contamination.



Figure 2: PRCUTS Study area Source: Parramatta Road Implementation Tool Kit Planning and Design Guidelines

## Sydney Metro West

In 2021, the NSW Government granted approval for a concept plan and major civil construction works for Sydney Metro West, a new rail link set to operate between Westmead and the Sydney CBD, enhancing connectivity both east and west.

Expected to commence operations in 2032, Sydney Metro West will include a station at North Strathfield, adjacent to the existing North Strathfield train station, significantly improving public transport connectivity key employment and education precincts. With quick connections to the Sydney CBD and Parramatta in just 10 minutes, the Homebush area is poised to become one of the most accessible and interconnected precincts in Sydney.

Sydney Metro West will play a crucial role in serving the expanding Homebush Precinct, fostering stronger connections between the established and emerging industry and employment hubs and communities, both to the east and west of the city. Additionally, it offers a chance to expand the residential capacity of the Precinct, accommodating further population growth, coupled with local placemaking strategies aimed at rejuvenating public spaces.



Figure 3: Future North Strathfield Station Entrance. Source: Sydney Metro

## Transport Oriented Development Accelerated Precincts

On 7 December 2023, the NSW Government announced the TOD Program to create more well-located homes close to transport, jobs and services. As part of the TOD Program, the NSW Government identified eight Sydney transport hubs (tier one accelerated precincts) for State-led accelerated rezoning to deliver up to 47,800 new, well located, high and mid-rise homes over the next 15 years. Homebush has been included as one of the eight tier one accelerated precincts.

The TOD Program has stemmed from the National Housing Accord (the Accord) announced by the Commonwealth Government in October 2022 as part of the Federal Budget to address the supply and affordability of housing. The Accord includes an initial aspirational target to build one million new well-located homes over 5 years from mid-2024. NSW has been tasked to provide 377,000 new homes by 2029.

<https://www.planning.nsw.gov.au/sites/default/files/2023-12/transport-oriented-development-program.pdf>

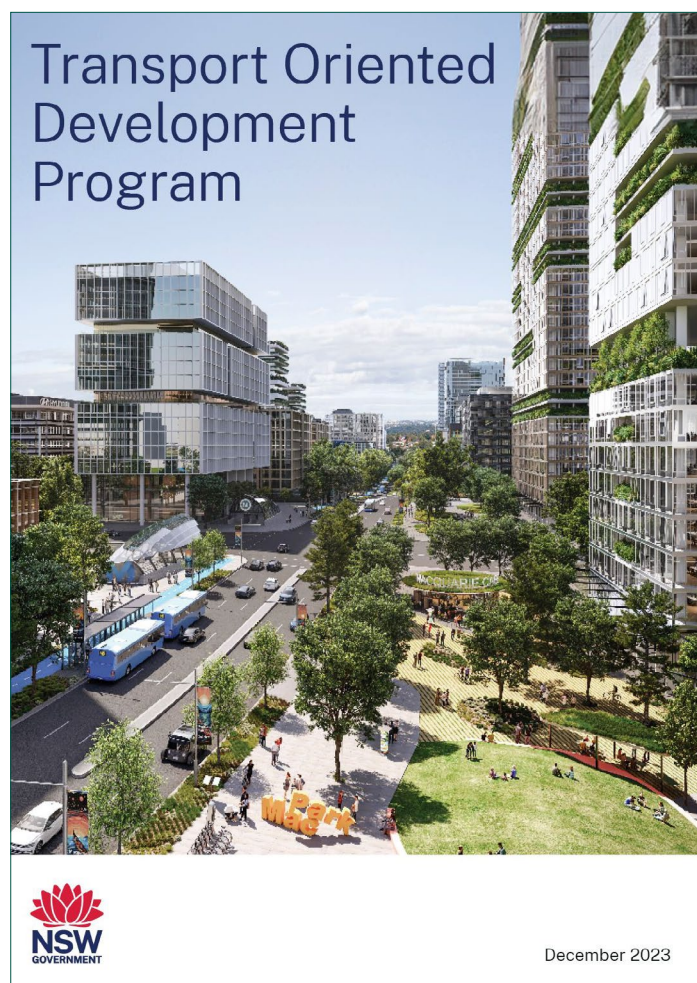


Figure 4: Transport Oriented Development Program. Source: Department of Planning, Housing and Infrastructure

## Regional Context

The Homebush Precinct is located approximately 12km west of the Sydney CBD. The Precinct straddles the boundaries of both Strathfield Council and City of Canada Bay Council. Homebush is located 20 minutes to Sydney CBD and 15 minutes to Parramatta CBD via existing train networks and services.

Homebush is also located within close proximity to Sydney Olympic Park, a major sporting and entertainment precinct. Sydney Olympic Park offers a range of facilities including stadia, arenas, parklands, and cultural venues, attracting visitors from across the city and beyond.

Homebush is connected via various transportation modes. Within the Precinct boundary are the stations of Homebush Station, North Strathfield Station, and Strathfield Station, and within close proximity of the Precinct boundary Concord West Station, Flemington Station and Sydney Olympic Park Station. Sydney Metro West will include a station at North Strathfield, adjacent to the existing North Strathfield train station. Major roadways such as Parramatta Road the A3 Ring Road and the M4 Motorway pass through Homebush, further enhancing its connectivity.

### The Greater Sydney Region Plan

The Greater Sydney Regional Plan outlines a 40-year vision for the development of Greater Sydney. Homebush is east of Sydney Olympic Park and south of Rhodes which are identified as strategic centres and these centres hold significant importance within the regions urban hierarchy, claiming to attract substantial private sector investment and growth.

### Eastern City District Plan

Located within the Eastern City District of the Greater Sydney Region Plan: A Metropolis of Three Cities, 2018, the corridor from Strathfield to Homebush and North Strathfield is identified as an Urban Renewal Area, positioned to contribute to the district's vision and housing targets.

The District Plan also identifies Powells Creek and Mason Park, Strathfield as areas to provide walking and cycling links, urban greening, stormwater treatment and a mix of open space uses that link Concord West, North Strathfield, Homebush and Strathfield to Parramatta Road, Bicentennial Park and the Parramatta River foreshore.

### Future Transport 2056

Future Transport Strategy 2022 outlines the transportation vision for NSW, including Homebush as a part of the corridor facilitating transportation services for the centre, including rapid buses, walking, and cycling.

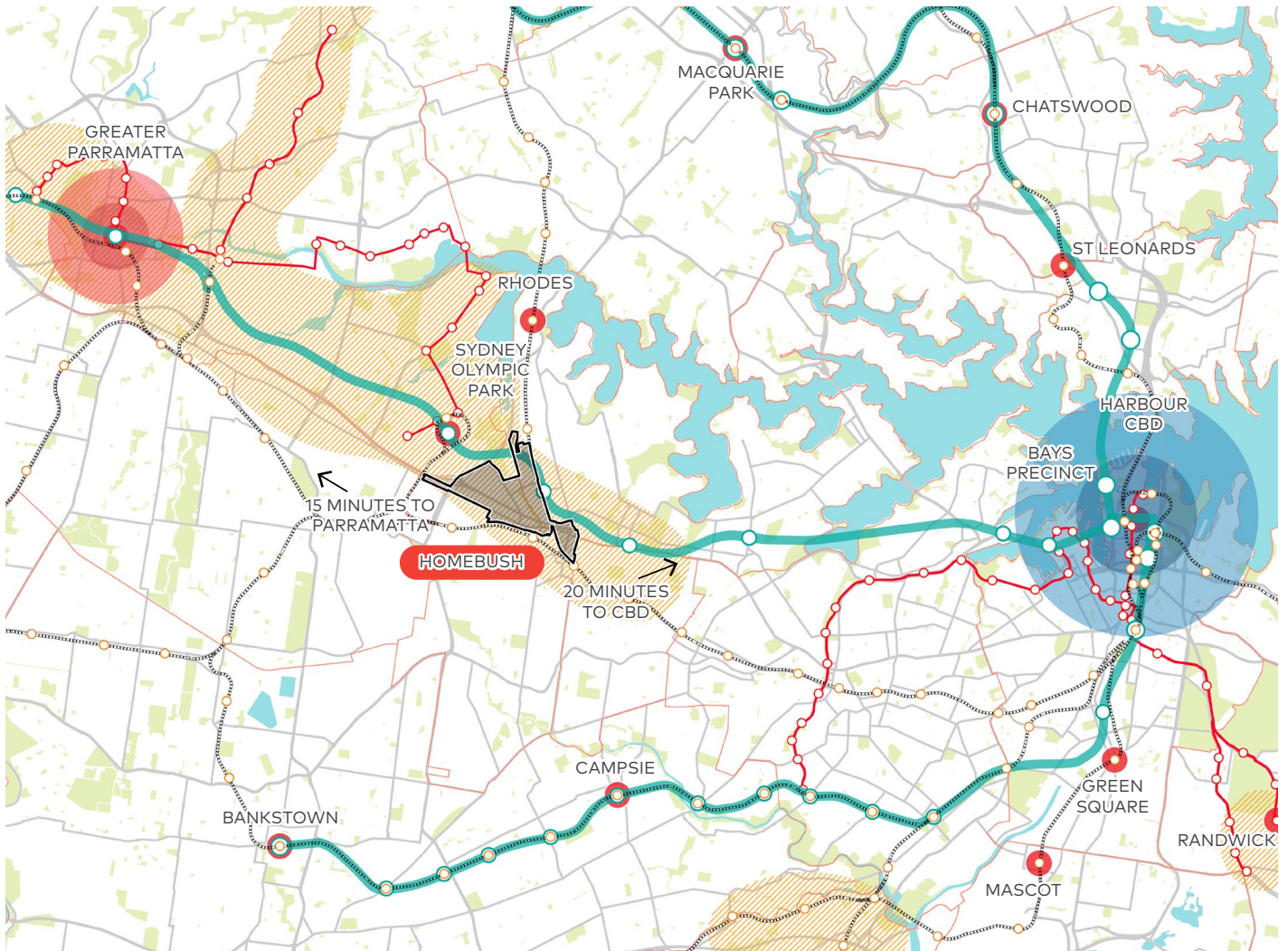
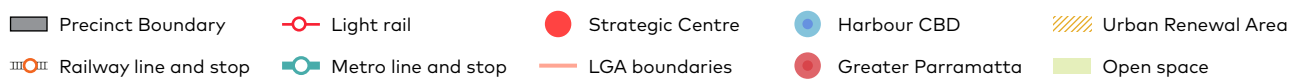


Figure 5: Strategic Context Diagram. Source: COX, GCC



## Homebush Planning Policy Context

There is a wide range of plans and policies that apply to the Homebush Precinct at both a State and Local Government level.

The following assessment is a small sample of the applicable plans and policies and relevant objectives have been captured in this Report for their relevance to urban renewal and urban design within the Precinct only.



### NSW Government - Better Placed

The Integrated Design Policy for the built environment in NSW offers a clear strategy to ensure quality design in architecture, public spaces, and environments, both for the present and future. Government Architect NSW (GANSW) has introduced "Better Placed," an integrated design policy that outlines seven key objectives: promoting designs that are contextually relevant, sustainable, inclusive, safe, efficient, value-adding, engaging, and attractive. These objectives establish expectations for good design in projects of all sizes, highlighting the public benefits of such design. The policy also defines well-designed built environments as healthy, responsive, integrated, equitable, and resilient. To complement Better Placed, GANSW has issued frameworks, advisory notes, and guidelines covering various design-related topics, such as country, heritage, movement, and place.



### NSW Government - Greener Places

GANSW has prepared Greener Places, an urban green infrastructure framework in NSW, supported by the draft Greener Places Design Guide and inspired by the Sydney Green Grid strategy. It is designed to improve the quality of urban life by strategically planning and managing green spaces, natural systems, and semi-natural systems. This framework promotes a healthier, more sustainable urban environment by enhancing community access to recreation, connectivity for walking and cycling, and urban resilience. It revolved around four key principles:

1. Integration with urban and grey infrastructure
2. Creating an interconnected open space network
3. Providing multiple ecosystem services
4. Involving stakeholders in development.



## Canada Bay Local Strategic Planning Statement

The City of Canada Bay Local Strategic Planning Statement (LSPS) is the core strategic planning document for the City of Canada Bay.

The LSPS identifies the Canada Bay portions of the Homebush Precinct as Urban Renewal Areas. The LSPS acknowledges that PRCUTS aims to renew Parramatta Road and adjacent communities through investment in homes, jobs, transport, open space and public amenity. It presents significant urban renewal opportunities for land within defined precincts.

The LSPS identifies North Strathfield in particular as an area of "low density housing on the western side of the main northern railway line falls within the Parramatta Road Corridor. This area however, is located within close proximity to North Strathfield train station and a new metro station."



## Canada Bay Local Housing Strategy

The objective of the Local Housing Strategy is to analyse the population, demographic and supply issues associated with the delivery and take up of housing in the LGA.

The Statement identifies actions relevant to the Homebush Precinct including;

- Develop detailed local area plans (LAPs) for local centres and surrounding local renewal areas, that are the basis for planning framework changes that promote medium density and infill development to diversify housing types
- Develop a staged program for the preparation of LAPs and planning control changes, with the program to include local centres with high accessibility including: Concord West, North Strathfield and Five Dock
- Ensure a diversity of housing forms, through development of guidelines for appropriate housing provision as well as ensuring that Council's DCP has controls that promote greater diversity of housing



Document ID: 100000  
Version: 1.0 (Public Draft) 2021-2022

## Canada Bay Our Future 2036

Our Future 2036 is a strategic plan developed by the City of Canada Bay in order to guide the council's growth towards an inclusive, sustainable, and thriving community. Following community consultation, the framework establishes several goals targeting social, environmental, economic and civic leadership issues, focused on preparing Canada Bay to sustainably support a growing population. Relevant goals to Homebush highlighted in this plan include:

- Create vibrant local village centres and community hubs
- Ensure the built environment respect neighbourhood character and responds deftly to evolving community needs
- Encourage active and accessible transport opportunities
- Manage local assets to ensure they continue to meet the City's needs and address climate adaptation issues.



## Strathfield Local Strategic Planning Statement

Strathfield 2040, is Council's Local Strategic Planning Statement (LSPS) which identifies opportunities for Council to outline local considerations so that they can become part of a coordinated response by Government in responding to growth and shaping the LGA's future.

The LSPS will guide the future character of our LGA through land use planning and collaboration between neighbouring councils, other levels of Government and key stakeholders.

The LSPS acknowledges that the Homebush Precinct is within the Parramatta Road Urban Transformation Corridor. This is further reinforced through documenting that "Urban renewal will be promoted along regional transport corridors and near strategic centres, such as northern Strathfield and Homebush with high residential amenity and access to transport and services."

Urban renewal within the Homebush precinct would also align with "the community has a preference to contain higher density development to the major transport corridors and protect the existing heritage and local character of low density residential across the LGA."



## Strathfield Local Housing Strategy

The Strathfield Local Housing Strategy sets out a plan for delivery of new housing in the Strathfield LGA for the next 20 years. This strategy has been developed to meet the requirements set out in the Greater Sydney Commission's Greater Sydney Region Plan - A Metropolis of Three Cities and the Eastern City District Plan.

The Strategy was to plan for up to approximately 13,500 additional dwellings in the 2016 to 2036 period to meet the needs of a growing population

Through exhibition of the draft Strategy, relevant community feedback was received that suggested "Mid-rise and high-rise apartments could be suitable for Homebush, Homebush West and Rail corridors".



## Strathfield 2035

The Strathfield 2035 Community Strategic Plan, prepared by Strathfield Council, provides long term direction for aligning the Council's resources with community priorities. It offers guidance to other stakeholders involved in planning and delivering services within Strathfield, and seeks to integrate environmental, social, economic and civic leadership considerations into its actions to help meet it's community's needs. Homebush's position in a key rail corridor is discussed in this plan, as well as other relevant goals such as:

- Sustainable growth supported by well-planned and accessible infrastructure and services
- Enticing, vibrant and safe centres blending services and social connectivity
- Quality, liveable and sustainable urban design and development.

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## The Precinct

Located north-west of Strathfield train station, the Precinct comprises an area of approximately 200 hectares which extends northward from Homebush station towards North Strathfield and Concord West train stations. It is traversed by Parramatta Road and the M4 Motorway, forming a significant east-west transportation corridor.

The Precinct is bordered by Homebush Bay Drive to the north, Western Rail Line to the south of the Precinct, and Main North rail line to the east. The site features notable natural landmarks such as Powells Creek, Mason Park, and wetlands, with adjacent public open spaces.



Figure 6: Precinct and notable landmarks

# Government Ownership

Parts of the Precinct are crown land or government owned land. This includes the transport corridor utilized by the M4 motorway and land occupied by Ausgrid. Additionally, some land along and to the north of Powells Creek, which contains open space and sports fields, falls under government ownership.



Figure 7: Government Land Ownership

- |                     |             |             |  |
|---------------------|-------------|-------------|--|
| ▬ Precinct Boundary | ■ Transport | ■ Treasury  | ■ Communities and Justice                |
| ■ Open space        | ■ Planning  | ■ Education | ■ Climate Change, Energy and Environment |

# Existing Access and Movement

Homebush is currently well serviced with rail connections at Homebush, Strathfield, North Strathfield and Concord West train stations, connecting the Precinct to the Western and Main North rail lines. Sydney Metro West is expected to open in 2032 with a new station and service located at North Strathfield.

Sydney's main east-west vehicular transport corridor, comprising of the M4 Motorway and Parramatta Road, intersect in Homebush. The M4 Motorway connects the western suburbs to the Sydney CBD and eastern areas. Parramatta Road, one of Sydney's oldest and busiest roads, runs from the Sydney CBD to Parramatta, passing through Homebush. Homebush Bay Drive and Centenary Drive form part of the A3 arterial road that connects

Mona Vale to Blakehurst and forms the northern boundary to the Precinct. While there are several rail stations in close proximity to the Homebush Precinct, there are currently only limited bus routes through the Precinct.

Currently there is limited local bus services within the Precinct and there are opportunities for services to be reviewed as the Precinct transforms, to support Sydney metro and to provide public transport on Parramatta Road.

Refer to Homebush TOD Rezoning Precinct Transport Statement by ARUP for further information on public transport.



Figure 8: Existing Access and Movement Network

Precinct Boundary
 T Train line and station
 M Metro line and station
 ● Bus route and stop
  Major Roads

# Existing Active Transport

The Precinct benefits from a well-established network of cycle routes, including pathways along the M4 Motorway, Pomeroy Street, and Bridge Road, forming part of the Bay to Bay Cycle and Walkway, a 23 kilometre track running from Settlers Park in Ryde to Botany Bay.

Additionally, George Street provides connections further north to Bicentennial Park and Sydney Olympic Park, which contains 35km of cycleways. Further enhancements are proposed under PRCUTS to expand cycling infrastructure.

Powells Creek is a valuable corridor for active transport, facilitating walking, jogging, cycling and access to open spaces and parks. The creek extends north to Mason Park and the wetlands.

These active transport routes promote connectivity between neighbourhoods, parks, and other amenities within and beyond the Precinct.

Refer to Homebush TOD Rezoning Precinct Transport Statement by ARUP for further information on active transport.

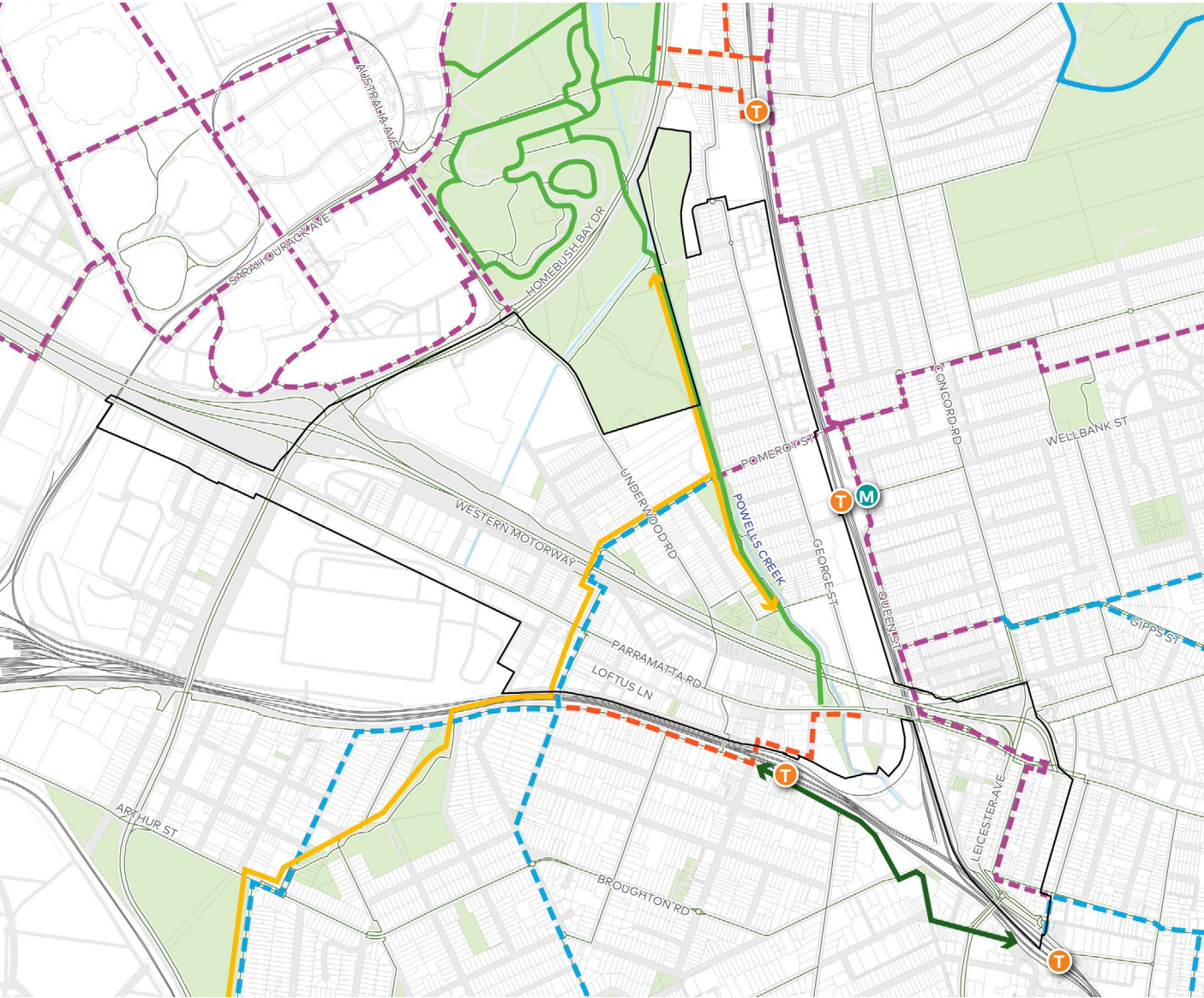


Figure 9: Existing and proposed active transport network

- Precinct Boundary

On-Road Cycle route (bike lane)

On-Road Cycle route (mixed traffic)
- Cycleways
- Bay to Bay Cycle and Walkway (Strathfield Council)
- Planned Cycle Route

Off-road Cycle Route

Proposed cycleway facilities delivered by others

Proposed cycle route

## Existing Character

Homebush has an existing character drawn from its industrial past and multiple road and rail transport connections. Today, the Precinct features a wide variety of neighbourhoods, commercial uses, destination retail and recreational amenities.

Housing within the Precinct has traditionally been associated with low-density residential areas. However, in recent years, the suburb has undergone significant urban redevelopment, resulting in a more diverse range of housing options.

The redevelopment has introduced a variety of housing types, including apartments, townhouse, and detached houses, catering to the evolving needs of residents. Along Parramatta Road, large parcels of land accommodate office/business park developments, wholesale and retail businesses, as well as light industrial operations. This mix of uses contributes to the vibrancy and economic activity of the area.



Figure 10: Northern approach to Strathfield Station from the Precinct. Source: COX



Figure 11: Powells Creek north-south active green connection. Source: COX



Figure 12: Existing Low Density West Side of George Street. Source: COX



Figure 13: Open Space and playground under the freeway. Source: COX



Figure 14: Closed off connections through Powells Creek. Source: COX



Figure 15: George Street – Low scale commercial/retail with Walk-Ups behind. Source: COX

The Precinct provides a balance of uses and serving local and regional needs for shops, restaurants and amenities, adjacent to Homebush, Strathfield and North Strathfield train stations. The Bakehouse Quarter, situated north of Parramatta Road, serves as a retail and food and beverage hub with a particular focus on George Street, whilst DFO is a regional retail destination.

Powells Creek runs parallel to George Street, offering a linear public open space and an active green connection. Some sections of this green space make use of otherwise underutilised areas beneath the elevated M4 motorway. Heritage elements are scattered throughout the Precinct, with the largest cluster near Strathfield Station comprised of number of dwellings. The Bakehouse Quarter and the Homebush Theatre are non-residential heritage items within the Precinct.



Figure 16: George Street – Low scale commercial/retail with Walk-Ups above. Source: COX



Figure 17: M4 Ventilation Shaft as viewed from Arnotts Reserve



Figure 18: Heritage item Homebush Theatre along Parramatta Road. Source: Google street view



Figure 19: View from Parramatta road at Western Motorway interchange. Source: Google street view



Figure 20: George Street running through the Bakehouse Quarter, in close proximity to restaurants. Source: COX



Figure 21: George Street with Arnotts factories in the Bakehouse Quarter. Source: COX



Figure 22: Car dealerships along Parramatta Road. Source: Google street view

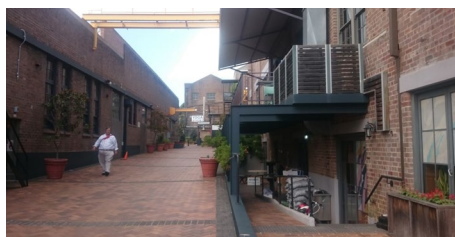


Figure 23: The Mews, activated by local businesses. Source: COX



Figure 24: "The Mews" former factories in the Bakehouse Quarter. Source: COX

# Natural Considerations

The Precinct is centred around an open space network that follows Powells Creek, extending all the way to the Parramatta River. The watercourse poses a risk of flooding, potentially affecting local streets and residences. The flood-prone area extends beyond the M4 Motorway, reaching down to the Strathfield triangle and south of Homebush Station. The WMAwater flood study which is part of the Homebush TOD technical reports, has identified a flood division line to limit development around Powells Creek, ensuring that new development minimizes flood risk and maintains the integrity of the floodplain. Please refer to their report for more information.

There are no areas of ecological significance within the Precinct boundaries, however Mason Park Wetlands, adjacent to Powells Creek, contains threatened ecological communities. Further to the north, where Powells Creek meets Parramatta River, high biodiversity areas are located.

Topography through the precinct is generally undulating, with ridgelines extending along the Main North rail line and from Loftus Lane through to Underwood Road, creating the depression through which Powells Creek flows.



Figure 25: Existing Natural Considerations

- |                          |             |                     |                                   |
|--------------------------|-------------|---------------------|-----------------------------------|
| Precinct Boundary        | 5m contours | Flood Prone Land    | Threatened Ecological Communities |
| Biodiversity Value Areas | Rivers      | Flood Division Line | Coastal Management Act Wetlands   |

# Built Considerations

The Precinct contains numerous built elements to consider with regard to urban renewal, including recent developments, strata properties, industrial zones, and educational institutions such as schools.

Recent developments in the Precinct are largely comprised of significant redevelopments, along Parramatta Road which transformed from light industrial and bulky goods uses to a mix of commercial and residential. Elsewhere in the Precinct recent development is largely comprised of alterations and additions to existing properties.

Strata-titled properties with over 10 lots have also been mapped as a consideration for urban renewal as they largely comprise existing multi-unit residential buildings.

Electricity supply uses such as Ausgrid occupy land to the south of Mason Park Wetlands and west of DFO.

There are two schools located within the Precinct, Our Lady of the Assumption Catholic Primary School and The McDonald College K-12 school. Just outside the precinct boundary are the Victoria Avenue Public School, Homebush Public School and Homebush Boys High.

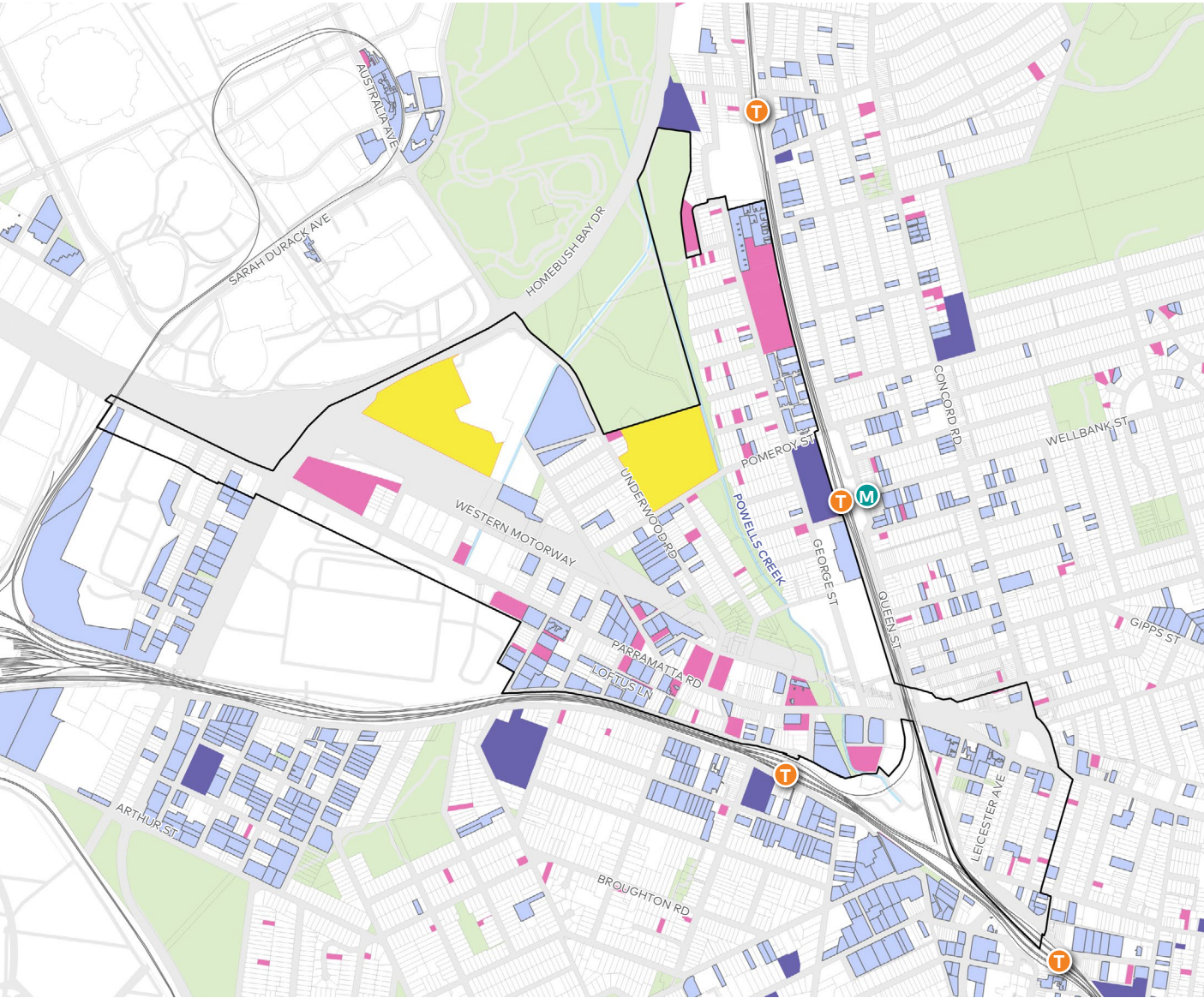


Figure 26: Existing Built Considerations

Legend: Precinct Boundary, Education, Electricity Supply, Strata Lots >10, Recent Developments

## Heritage

As an early established settlement, Homebush has several heritage listed areas within its boundaries. These include state and local heritage items, including a pumping station, theatre, inter-war commercial buildings, federation houses, and the Mason Park wetlands, which are listed on the Register of the National Estate and are crown land. Notable heritage features such as the Bakehouse Quarter and the iconic 'Arnotts' signage contribute to the distinct character and historical significance of the Precinct.

The following additional items have been identified by GML for potential heritage listing (refer to Homebush Stage-Led Rezoning Transport Orientated Development Precinct - Heritage

Significance Assessment Report, GML Heritage for more information).

- 7 Knight Street, Homebush
- 11 Knight Street, Homebush
- 41 Everton Road, Strathfield

A set of principles and controls have been prepared (refer to Homebush Stage-Led Rezoning Transport Orientated Development Precinct - Heritage Significance Assessment Report, GML Heritage for more information) that will relate to the urban design and built form element of the Precinct.

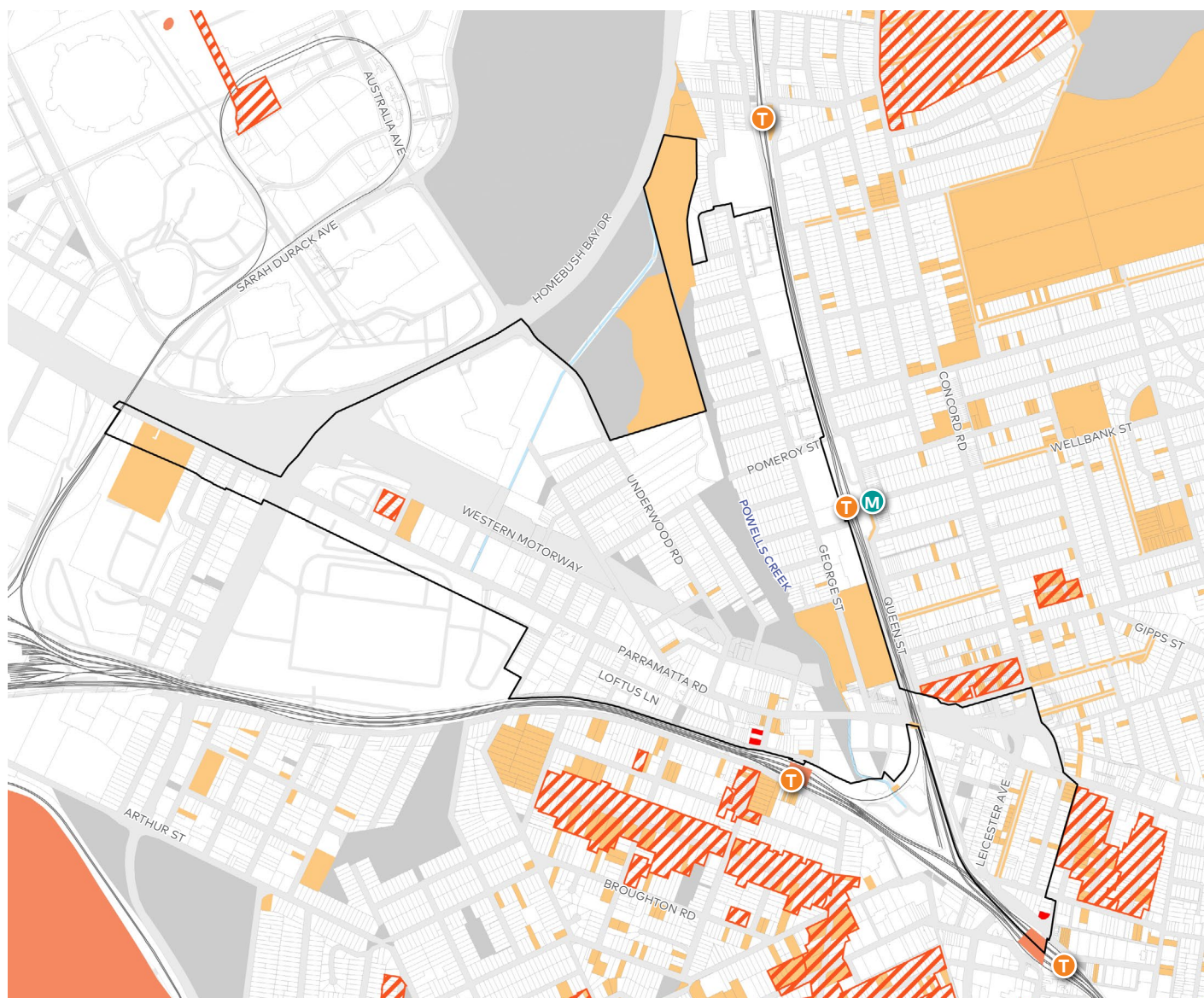


Figure 27: Heritage

Precinct Boundary
  State heritage
  Local heritage
  Heritage Conservation Area
  Additional heritage items identified

*Heritage principles (source: Heritage Significance Assessment Report, GML Heritage)*

- *Retain and protect the heritage significance of heritage items, HCAs, archaeological sites and places of Aboriginal cultural heritage significance when planning and delivering new development.*
- *Retain and conserve identified cultural heritage in accordance with statutory requirements and accepted industry standards.*
- *Recognise the embodied resources of existing building stock, and explore opportunities to maximise adaptive re-use of existing buildings in new development.*
- *Recognise and plan for climate adaptation, to ensure cultural heritage is conserved for current and future generations.*
- *Recognise, incorporate and enhance existing streetscape character and elements when designing and delivering new development.*
- *Foster an understanding of the heritage values and historical development of the Precinct through heritage interpretation opportunities in open space and along active transport routes.*
- *Encourage the retention and reinstatement of historical street and lot patterns.*
- *Consider the physical, visual and historical setting of heritage items when proposing new development including but not limited to the siting, setback and placement of heritage items within their lots.*
- *Explore innovative design responses to new development and around heritage items by fostering design excellence, place-specific heritage interpretation, and high-quality public domain initiatives.*
- *Consider the integration of heritage items into larger scale developments and/or amalgamated sites, where the setting and heritage values of the heritage item is retained and the spatial isolation of heritage items is avoided.*
- *When proposing higher density development adjacent to a heritage item, locate increased height and FSR to limit overshadowing, loss of amenity, and visual impacts.*
- *Retain historical ongoing retail and commercial uses of identified heritage items.*
- *Ensure the form, bulk, scale, materials and finishes of new development, adjacent to or incorporating heritage items, is designed to respond harmoniously to the height, scale, articulation, and architectural style of heritage items.*
- *Ensure the retention of significant views and historical visual relationships from and between heritage items, including views to heritage items from the public domain.*
- *Design high-quality public domain improvements that enhance the historic character of the area and provide public amenity.*
- *Ensure new development incorporates cultural heritage guidelines including the Connecting with Country Framework, Design Guide for Heritage and Better Placed, to support the conservation of heritage items within the Precinct.*



Figure 28: 1943 aerial



Figure 29: Strathfield Station 1855



Figure 30: Hotel Homebush

# Combined Constraints

The Precinct comprises of numerous sites that are wholly or somewhat constrained with regards to potential urban renewal.

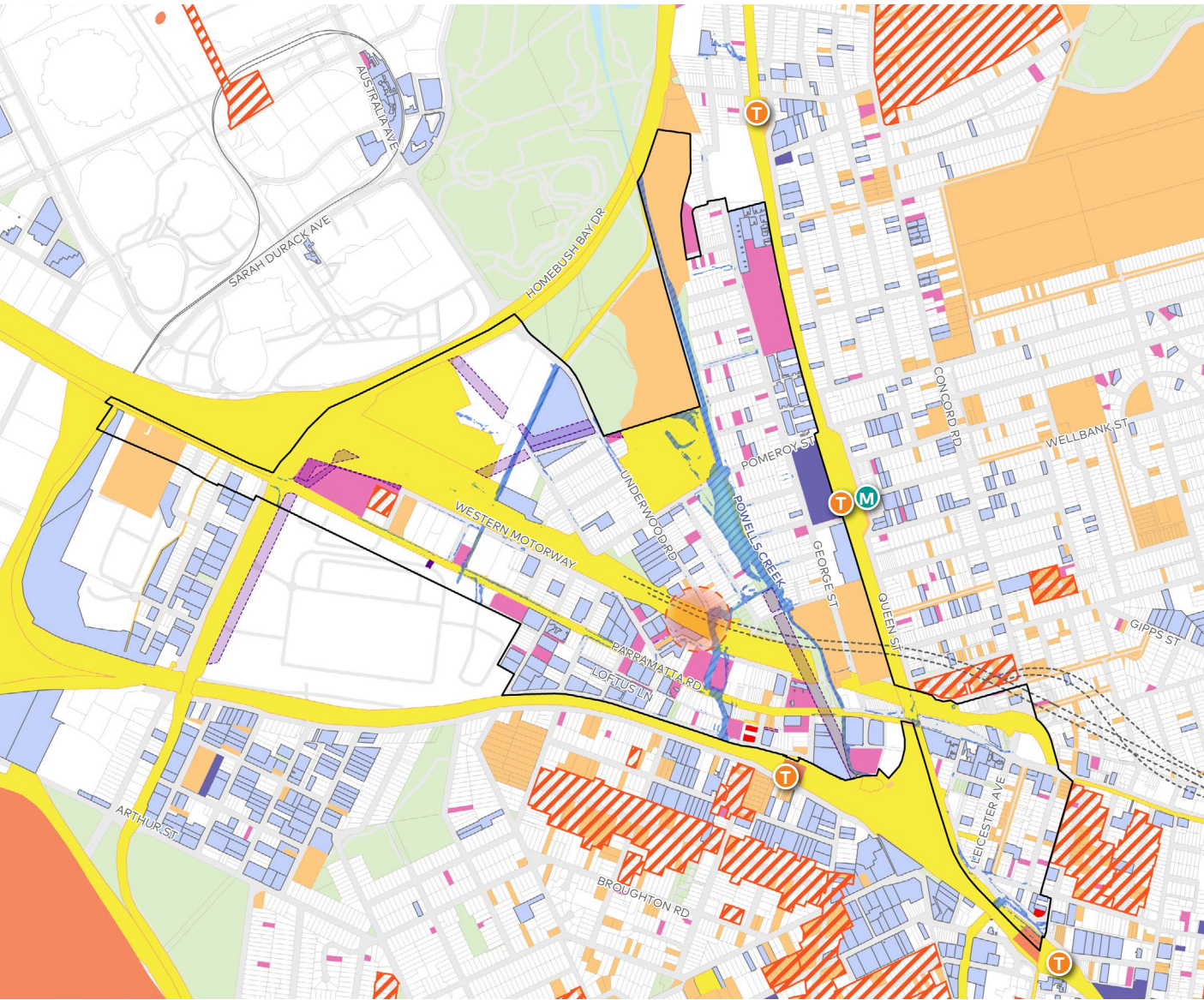


Figure 31: Constraints Map

- Precinct Boundary
- State heritage
- Local heritage
- SP2 - Education
- SP2 - Industrial Land
- Heritage Conservation Area
- Strata Lots >10
- Recent Developments
- Additional heritage items
- 60m buffer zone
- Easements
- Westconnex

## Opportunities

There are significant opportunities for the new and continued transformation of the Homebush Precinct into an active and diverse hub. These include, but are not limited to;

- Opportunities to expand the existing network of green spaces and the delivery of new open spaces and missing active transport links.
- New east-west connections through the Precinct and across Powells Creek
- New north-south connections through the Precinct and across Parramatta Road
- Improved pedestrian connectivity to existing and future stations and interchanges
- Retaining and celebrating heritage elements such as the Bakehouse Quarter and Homebush Theatre.
- New street connections to enhance permeability and traffic flow throughout the Precinct
- Increased residential densities to leverage the enhanced levels of accessibility and amenity afforded by the future Sydney Metro station and potential public open space and active transport upgrades

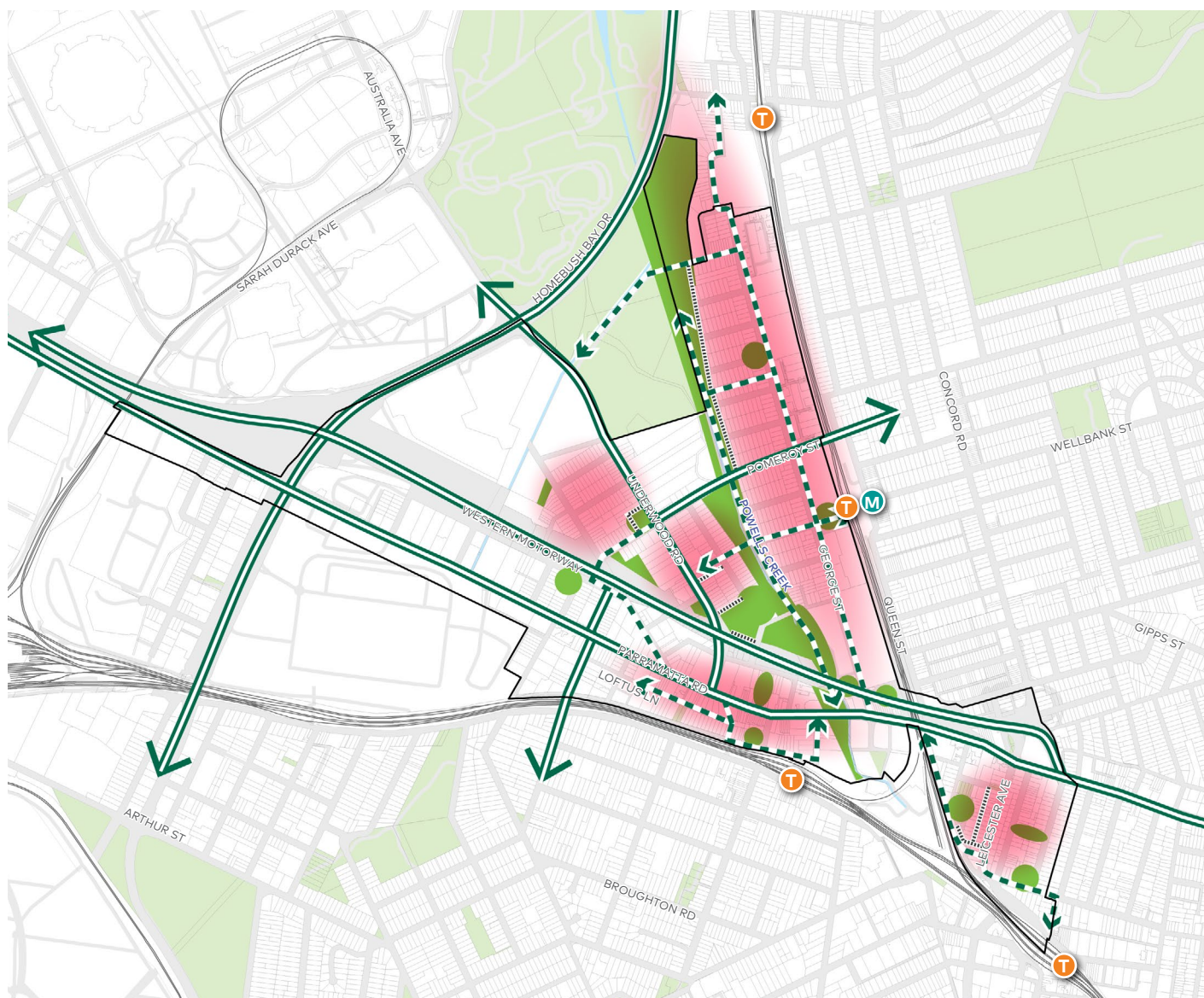


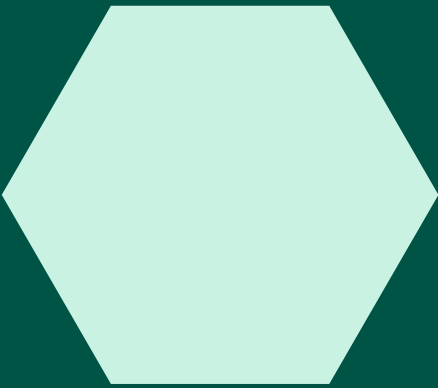
Figure 32: Opportunities Map

- |                                       |                 |                               |
|---------------------------------------|-----------------|-------------------------------|
| Precinct Boundary                     | Existing Links  | Increased Residential Density |
| Green Spaces (existing and potential) | Potential Links | Potential Vehicle Connection  |

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## Urban Design Principles and Key Moves

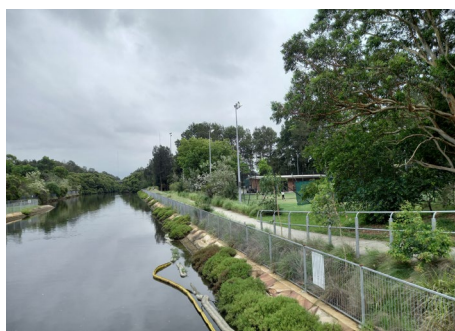


# Urban Design Principles



## 1 Identity and context

- Consideration of the diverse, existing and future local character of the Homebush Precinct
- Consideration of connections to surrounding distinct areas such as Sydney Olympic Park, Concord, Burwood and Flemington.



## 2 Sustainability and resilient

- Deliver a sustainable, green and resilient precinct
- Integrate the urban and natural environment into the Precinct and enhance the green and blue network



## 3 Connectivity

- Prioritise walking, cycling and public transport over private vehicles
- Enable a connected, direct and comfortable walking and cycling movement to and from key destinations and origins within the Precinct



## 4 Interface and activity

- Ensure that increases in resident, worker and visitor populations are matched with increases in areas and intensity of activity
- Locate open spaces, mixed uses, retail and food and beverage along priority walking, cycling and public transport corridors and the desire lines to these corridors



## 5 Responsive built form

- Ensure the design of built forms will maximise and encourage solar access to public and private open spaces and living areas and provide for appropriate interfaces to sensitive uses such as heritage items and schools



## 6 Place Making

- Reinforce existing public places and main streets
- Deliver new neighbourhood-scale centres that provide amenities and meet the daily needs of the local resident and worker communities

## Key Moves



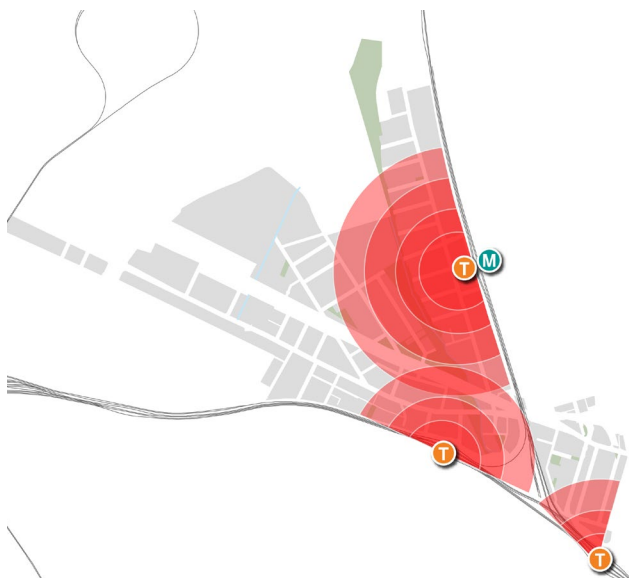
### Activation of Bakehouse Quarter

Activation of the streets and development of the Bakehouse Quarter into a hub for the Precinct



### New and Expanded Open Spaces

Expand and embellish existing open spaces, programme and activate open spaces for year-round use, plan and deliver new open spaces in key locations and connect missing active transport links.



### Density and Housing Choices

Delivering a significant amount of the housing targets for each LGA through high-quality urban renewal that provides a wide array of housing choices in close proximity to public transport nodes

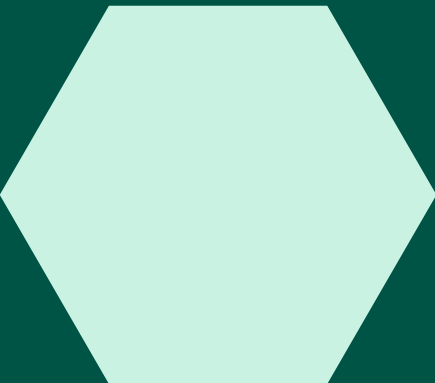


### Expansion of North Strathfield as a Centre

Leveraging the opportunity that Sydney Metro and a multi-modal interchange at North Strathfield affords, by planning for an expanded centre of mixed uses, retail, civic and food and beverage uses that straddles both sides of the rail corridor and extends west in to the Homebush Precinct. This would complement the existing hierarchy of centres at Strathfield and Homebush within the precinct.

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## Urban Design Framework



# Structure Plan

The Structure Plan for the Homebush Precinct envisages a vibrant hub with multiple transport modes, pedestrian-friendly links, passive and active green spaces, and abundant housing choices.

The Structure Plan seeks to enhance existing connections and provide new links from within the Precinct to the four existing train stations, and the under construction metro station, providing almost unparalleled public transport access to and from the Precinct.

Mixed uses are extended from Parramatta Road to Pomeroy Street on the eastern side of George Street to meet the retail,

food, beverage, social and cultural needs of a growing resident, worker and visitor population in the future.

Residential densities are highest within the areas of highest accessibility and amenity, around the existing stations and along Powells Creek.

Existing employment uses along Parramatta Road and west of Underwood Road are retained and reinforced allowing for intensification in existing employment generating uses.



Figure 33: Structure Plan

Precinct Boundary	Proposed Movement	Higher Density Residential	Higher Density Mixed Use	Special Activities
Metro Station	Existing Movement	Medium Density Residential	Mixed Use	Retail Markets
Train Station	Green Corridor	Productivity Support	Open space and recreation	

## Illustrative Master Plan

The Illustrative Master Plan represents just one permutation of how built form and urban development within the Homebush Precinct may be realised in line with the State-led Rezoning, the Design Guide and other applicable plans and policies.

The Illustrative Concept Plan represents the synthesis of the opportunities outlined in the supporting technical studies. It includes the following components:

### **A - Creation of a new plaza and open space to access the future North Strathfield Metro Station**

The potential to deliver a significant open space and direct pedestrian and cyclist connection to the North Strathfield Metro and rail station is a key element of the Master Plan. At present access is indirect and unsafe and the new open space is located and configured in a way that leverages desirable east-west pedestrian movements from the broader catchment and is of a scale that allows for a variety of programming within the open space.

### **B - Expansion of the North Strathfield Centre**

Leveraging the opportunity that Sydney Metro and a multi-modal interchange at North Strathfield affords, by planning for an expanded centre of mixed uses, retail, civic and food and beverage uses that straddles both sides of the rail corridor and extends west in to the Homebush Precinct.

### **C - Increased Activation of the Bakehouse Quarter**

The Plan seeks to activate the Bakehouse Quarter outside of just normal retail shopping and food and beverage uses by allowing for heritage-sensitive, appropriately-scaled, mixed use development within the Bakehouse Quarter.

### **D - Delivery of New Open Spaces**

The Plan seeks to deliver new open spaces in line with increased demand for active and passive recreational spaces in a variety of locations within the Precinct to ensure that all residents and workers are within short walking distance of open space.

### **E - Connect Missing Active Transport Links**

There is a wide network of active transport links within and surrounding the precinct however a number of critical links are missing. The plan proposes to make these connections so that active transport becomes an even more desirable way to move to, through and within the Homebush Precinct.

### **F - New Local Streets**

To reduce the demand on the Pomeroy and George Street intersection, a new local street has been proposed between Pomeroy Street and Conway Avenue. The new street also affords the opportunity to plan for parking on the park interface, to enable greater accessibility to the park, and passive surveillance of the park. Two new streets between Ismay Avenue and Underwood Road enhances permeability and the realignment of Allen Street to connect directly to Underwood Road facilitates the delivery of an expanded and contiguous open space between Underwood Road and Powells Creek. A new shareway wraps around the proposed open space at the corner of Underwood Road and Pomeroy Street and a new street frames the eastern side of proposed open space within Strathfield between Hilts Road and Cooper Street.

### **G - Retention of Heritage Items**

A number of potential heritage items are proposed to be retained by transferring development rights to adjoining lots and encouraging adaptive reuse for community uses and/or as part of new open spaces.

Figure 34: Illustrative master plan

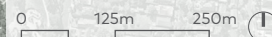






Figure 35: Homebush Precinct Artists Illustration looking north-west towards Sydney Olympic Park



# Public Domain and Open Space

The primary open space element within the Homebush Precinct is the Powells Creek corridor, which will be expanded and better connected through the proposal as a widened corridor along its length provides both a linear parkland and flood mitigation.

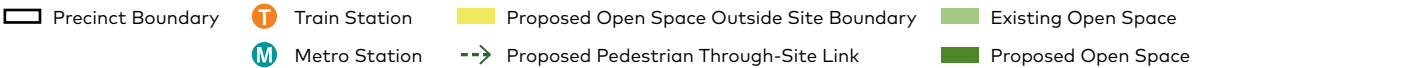
Existing open spaces are complemented by additional proposed open spaces along key connections such as Powells Creek, Parramatta Road, George Street, Underwood Road, and Pomeroy Street. A large contiguous open space is proposed between Underwood Road, Powells Creek and the M4 Motorway, by re-routing and closing existing streets and transferring development opportunities from flood prone land.

New parks are planned on north and south of Parramatta Road within the Strathfield and Homebush suburbs and a combined park and transport plaza are proposed on the western side of the North Strathfield interchange.

An extensive network of through site and mid-block links are proposed for primarily local movements along Loftus Lane, north of Parramatta Road between Underwood Road and Powell Street and between Leicester Ave and the new street between Hilts Road and Cooper Street.



Figure 36: Public domain and open space



# Solar Access to Open Space

Ensuring solar access to public open spaces is essential for maintaining a high level of amenity year-round for those visiting, working, and living in the Precinct. Key open spaces should receive a minimum of 2 hours of sunlight between 9am and 3pm on the winter solstice (21 June) across a significant portion of the area.

The diagram below documents the expected proportions of the existing and proposed open spaces that will receive the 2 hours of sunlight during the winter solstice. Areas that are below 50% are all located south of Parramatta Road and will require consideration as to their proposed programme and use, and how they can provide shaded areas in the summer to complement the areas with very high solar access.

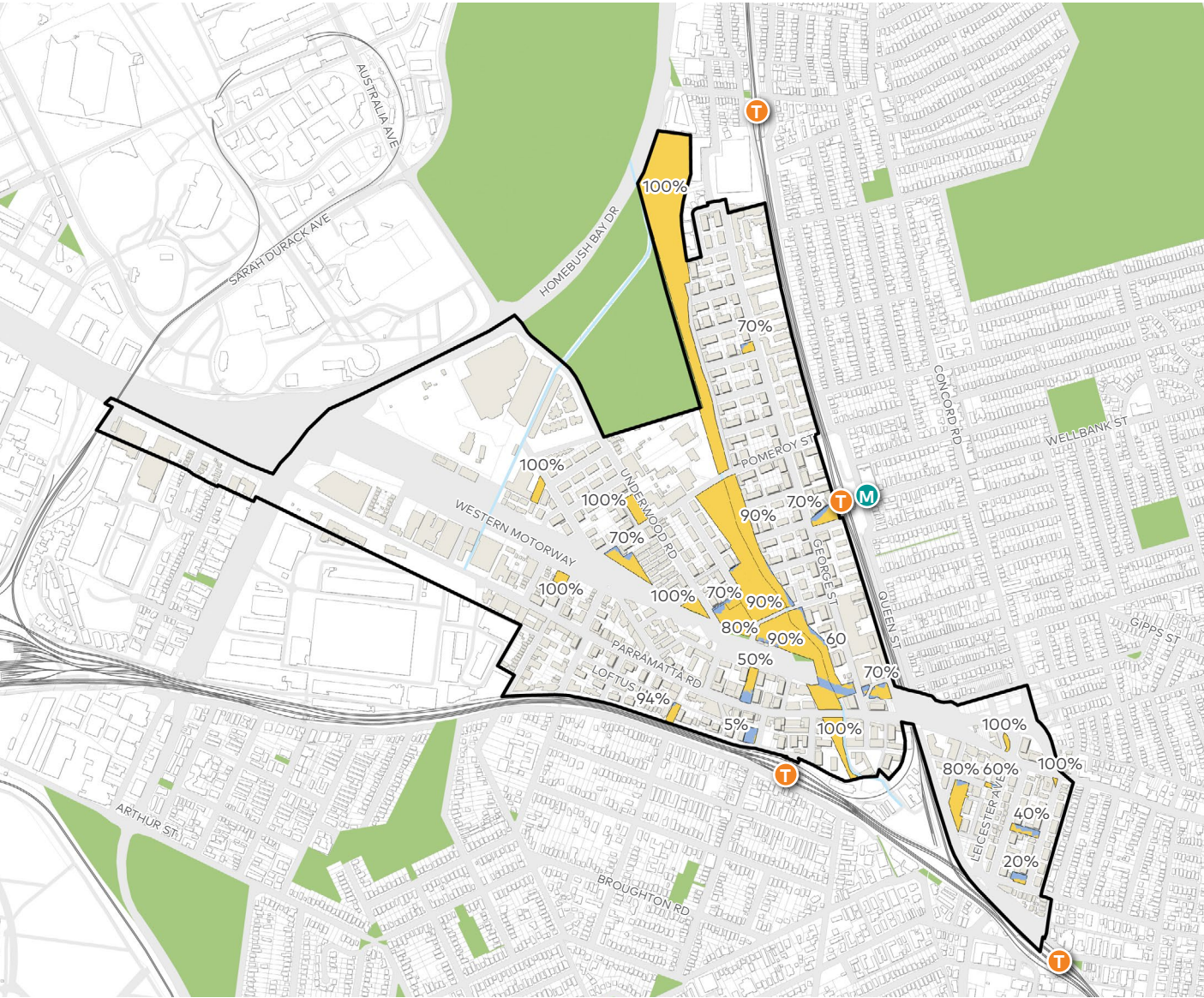


Figure 37: Solar access to open space

  Precinct Boundary
 M Metro Station
 T Train Station
   Open Space Solar Access >2 hours
   Open Space Solar Access <2 hours

## Access and Movement

### Public Transport

The opening of Sydney Metro West in 2032, with a station at North Strathfield, will provide a high-frequency, high-capacity rail link connecting to Westmead and the Sydney CBD, significantly enhancing public transport accessibility for the Precinct. North Strathfield will serve as a multi-modal interchange between rail, Metro and bus services.

Strathfield is one of the most accessible stations on the rail network and will continue to provide for rail access to both the Main North line, and Western Line.

Concord West will continue to provide access for the northern portions of the Precinct to the Main North line.

The existing Homebush train station, located to the south of the Precinct, will continue to support public transport trips south of Parramatta Road, with connections to Parramatta and the CBD in 30 minutes off-peak and 20 minutes during peak hours.

Currently there is limited local bus services within the Precinct and there are opportunities for services to be reviewed as the Precinct transforms, to support Sydney metro and to provide public transport on Parramatta Road.

Transport for NSW has a vision for Parramatta Road to “deliver a fully integrated transport network, to enhance public spaces along and adjacent to the corridor for people of all walks of life to connect, trade and visit”.

Refer to Homebush TOD Rezoning Precinct Transport Statement by ARUP for further information on public transport.

### Active Transport

The proposed active transport network will enhance opportunities for east-west connections, facilitating movement across Powells Creek to high-amenity areas such as North Strathfield Station and Metro, as well as the Bakehouse Quarter. North-south connections are well established through Powells Creek, extending to Parramatta Road.

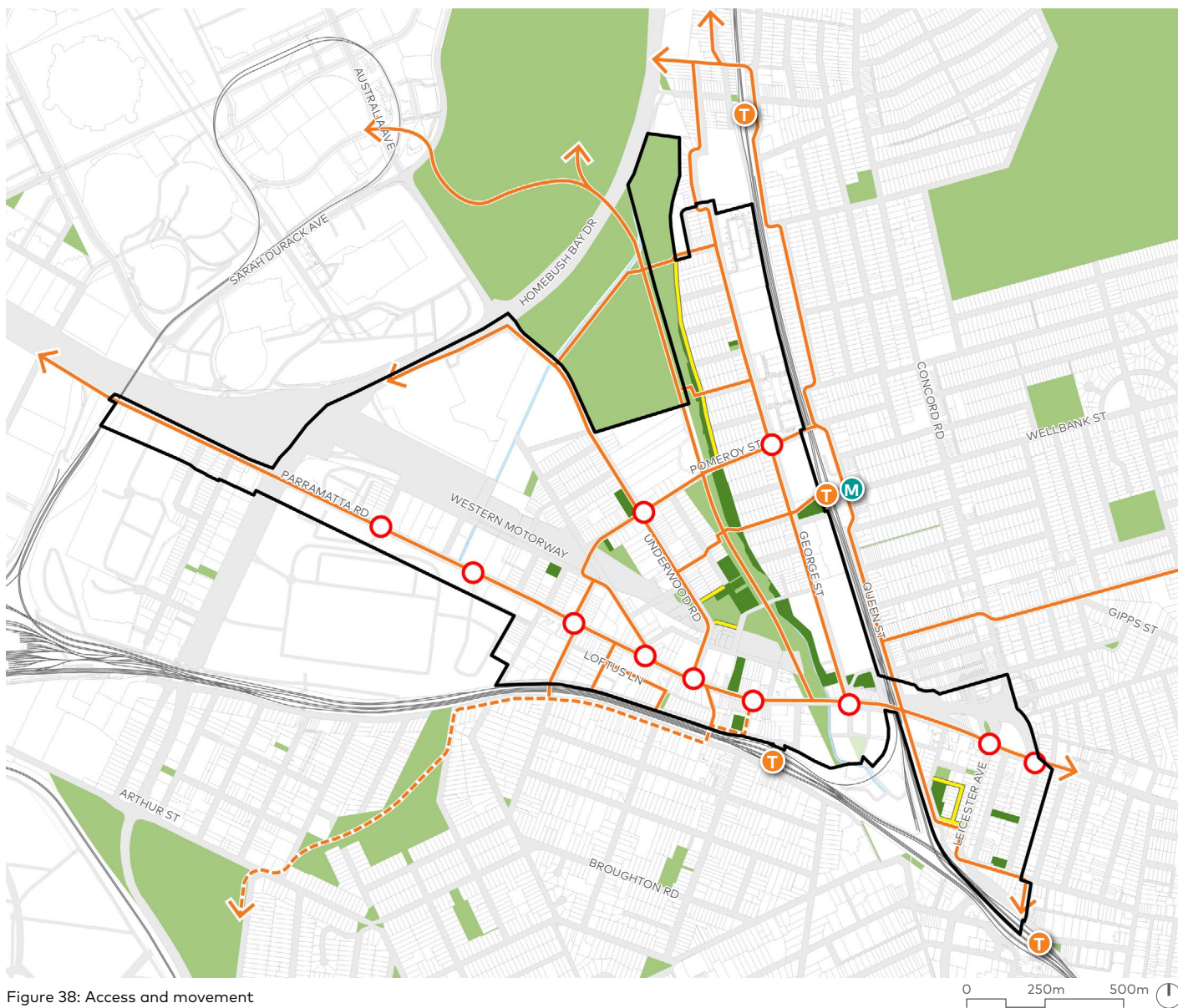
Key active transport has been prioritised in a rapid workshop, these include Pomeroy Street, Queen Street, Hamilton Street, Park Road and Subway Lane.

Refer to Homebush TOD Rezoning Precinct Transport Statement by ARUP for further information on active transport.

### Street Network

- To reduce the demand on the Pomeroy and George Street intersection, a new local street has been proposed between Pomeroy Street and Conway Avenue. The new street also affords the opportunity to plan for parking on the park interface, to enable greater accessibility to the park, and passive surveillance of the park.
- A new street between Ismay Avenue and Underwood Road enhances permeability through the provision of a mid-block one-way street.
- The realignment of Allen Street to connect directly to Underwood Road along the M4 interface, facilitates the delivery of an expanded and contiguous open space between Underwood Road and Powells Creek.
- A new street between the closed southern end of Ismay Ave and Underwood Road.
- A new shareway wraps around the proposed open space at the corner of Underwood Road and Pomeroy Street,
- The proposed realignment of Cooper Street allows for better traffic movement and an intersection with Leicester Avenue.
- A new north-south street between Hilts Road and the realigned Cooper Street allows for the servicing and access to development lots fronting Leicester Avenue without requiring vehicular access directly from Leicester Avenue.

Refer to Homebush TOD Rezoning Precinct Transport Statement by ARUP for further information on the proposed street network.



# Uses and Activity

The proposed mix of uses and activity within the Homebush Precinct promotes a vibrant residential and mixed-use area with an active core, extending retail, professional services, and food and beverage uses from the Bakehouse Quarter along George Street and Parramatta Road.

The North Strathfield metro station offers an opportunity to extend the existing main street of George Street from the Bakehouse Quarter precinct to the north, providing daily services and amenities for an increased residential population in close proximity to the new Metro station.

The mixed-use zones extend along Parramatta Road, increasing density around the Homebush train station and enhancing local amenity.

Higher density residential areas are complemented by a network of open spaces, providing connectivity to North Strathfield, Strathfield and Homebush train stations.

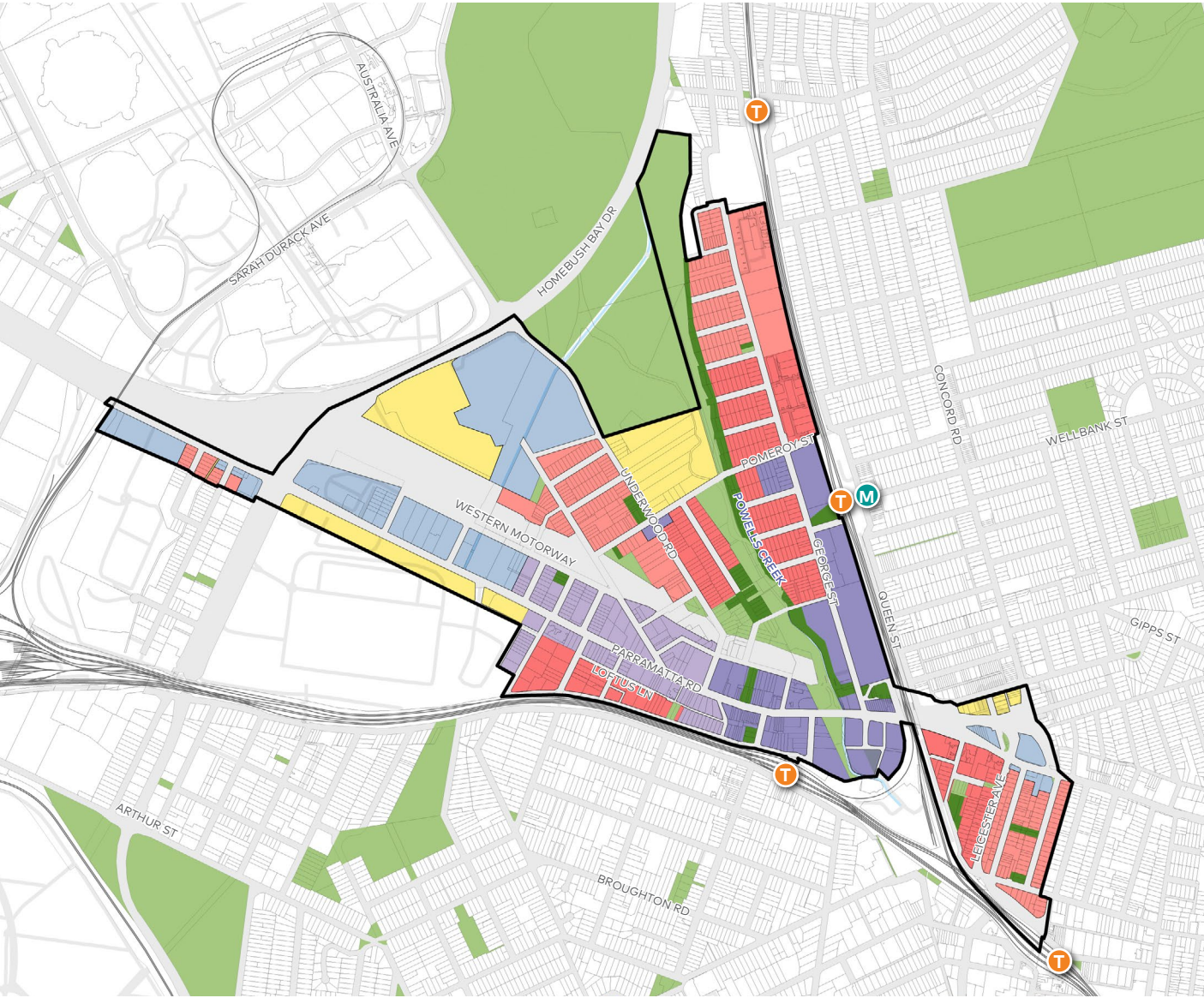


Figure 39: Proposed uses

Precinct Boundary	Existing Open Space	Special Activities + Markets	Higher Density Residential
Metro Station	Proposed Open Space and Road network	Higher Density Mixed Use	Medium Density Residential
Train Station	Productivity Support and General Industrial	Medium Density Mixed Use	

## Active Frontages

Active street frontages help to enliven the public domain, creating a vibrant, attractive and safe precinct. Featuring food and beverage tenancies, retail shopfronts, and small-scale commercial spaces, these frontages line the streetscape, promoting passive surveillance through direct visibility onto the street.

Active edges will be concentrated in mixed-use precincts with non-residential podiums and residential towers above, including along Parramatta Road, George Street in the Bakehouse Quarter up to the intersection with Pomeroy Street, and the expanded mixed-use area at the intersection of Pomeroy and Underwood Roads.

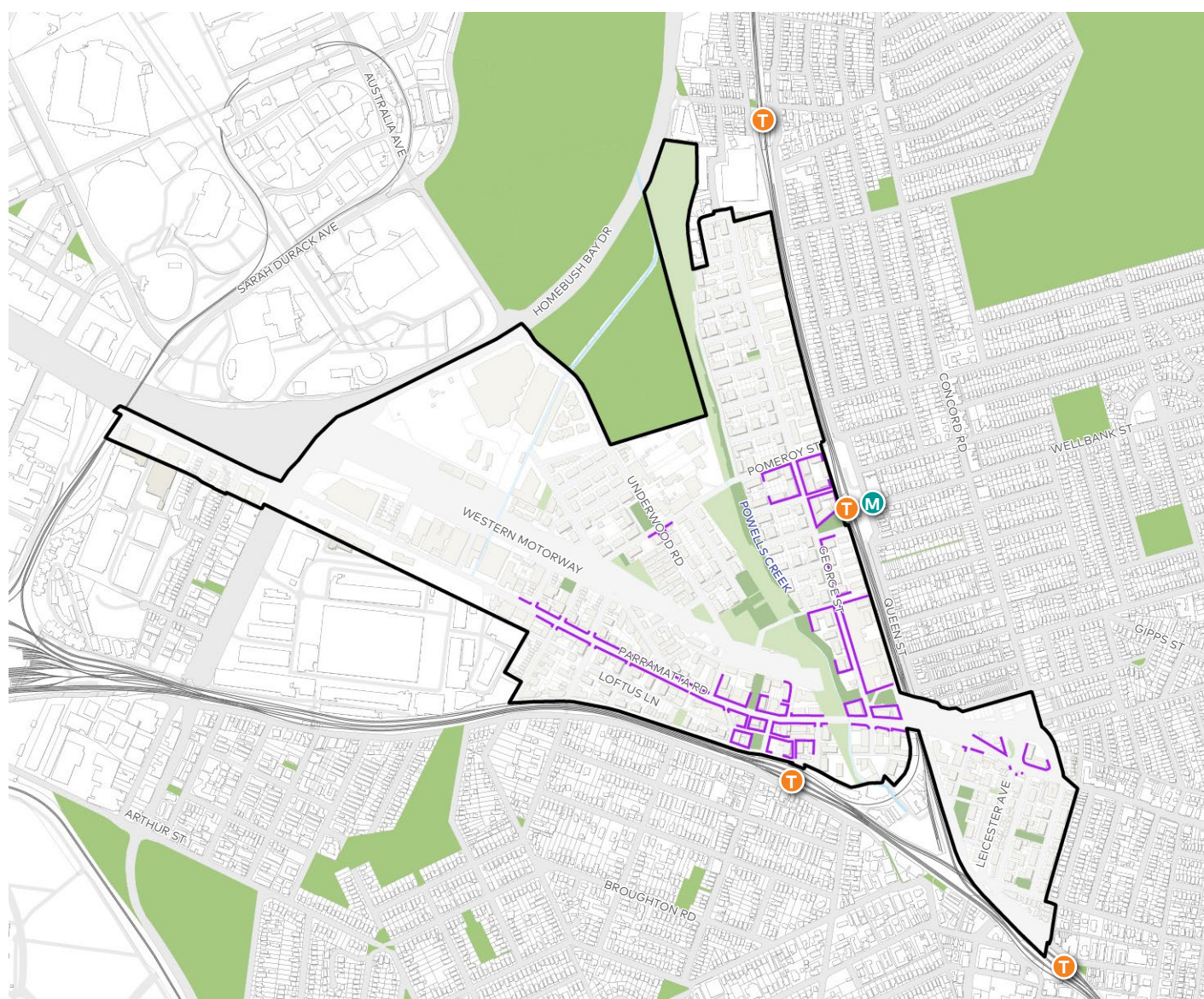


Figure 40: Active frontages

Precinct Boundary
 M Metro Station
 T Train Station
 — Existing and Proposed Active Frontages

# Primary Setbacks

Primary setbacks define the built form definition of the public and private domain at ground level, designed to enhance the urban environment, support community needs, and preserve the area’s unique character.

A 0 meter primary setback is proposed within areas where it is desirable to bring active, ground floor uses directly to the edge of the street and/or where development already exists with a 0 meter setback. This desired or existing interface is planned along primary roads such as Parramatta Road and George Street. Within the Bakehouse Quarter, secondary setback controls (applicable to built form above the street edge or podium) will be considered to preserve the heritage character while allowing for functional, commercial ground-floor spaces and mixed use development above.

Along George Street, north of the Bakehouse Quarter, the urban landscape is supported by 3 meter building setbacks to allow for landscaping, outdoor dining and/or footpath widening within the private domain to complement street trees along George Street.

6 meter setbacks are proposed for residential uses on the ground floor to provide sufficient area for private open space and landscaping between the public domain and ground floor dwellings. 6m setbacks are shown along Parramatta Road where existing or future development may allow the delivery of the desired 6m green setbacks envisaged as part of PRCUTS.

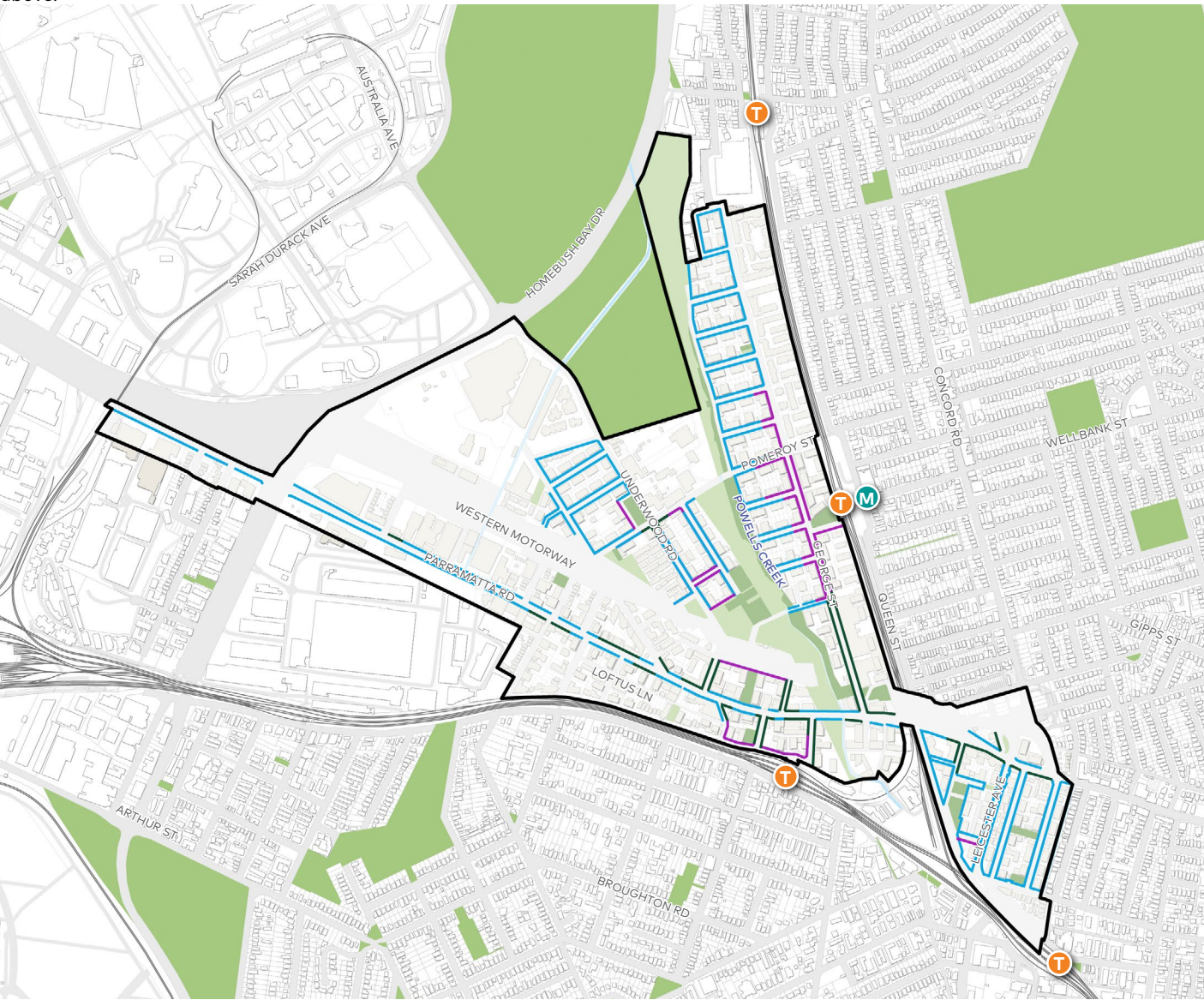


Figure 41: Setbacks

Precinct Boundary
  Metro Station
  Train Station
  0m Setback
  3m Setback
  6m Setback

# Street Wall and Podium Heights

The street wall and podium heights guideline are to ensure that the height of the street walls make a significant contribution to the experience of place and add uniformity of character on streetscapes and within the public domain. The street wall and podium heights also consider the viability of large-scale podiums and non-residential uses within the context of the geographic extent of the Homebush Precinct.

To achieve this, the Urban Design Report proposes that all new mixed use buildings are comprised of minimum 1 storey and maximum 2 storey and an 8-10m high street wall podium.

These podiums and street wall heights would primarily be built to the property boundary and define the streets and public domain interfaces of the mixed use buildings.

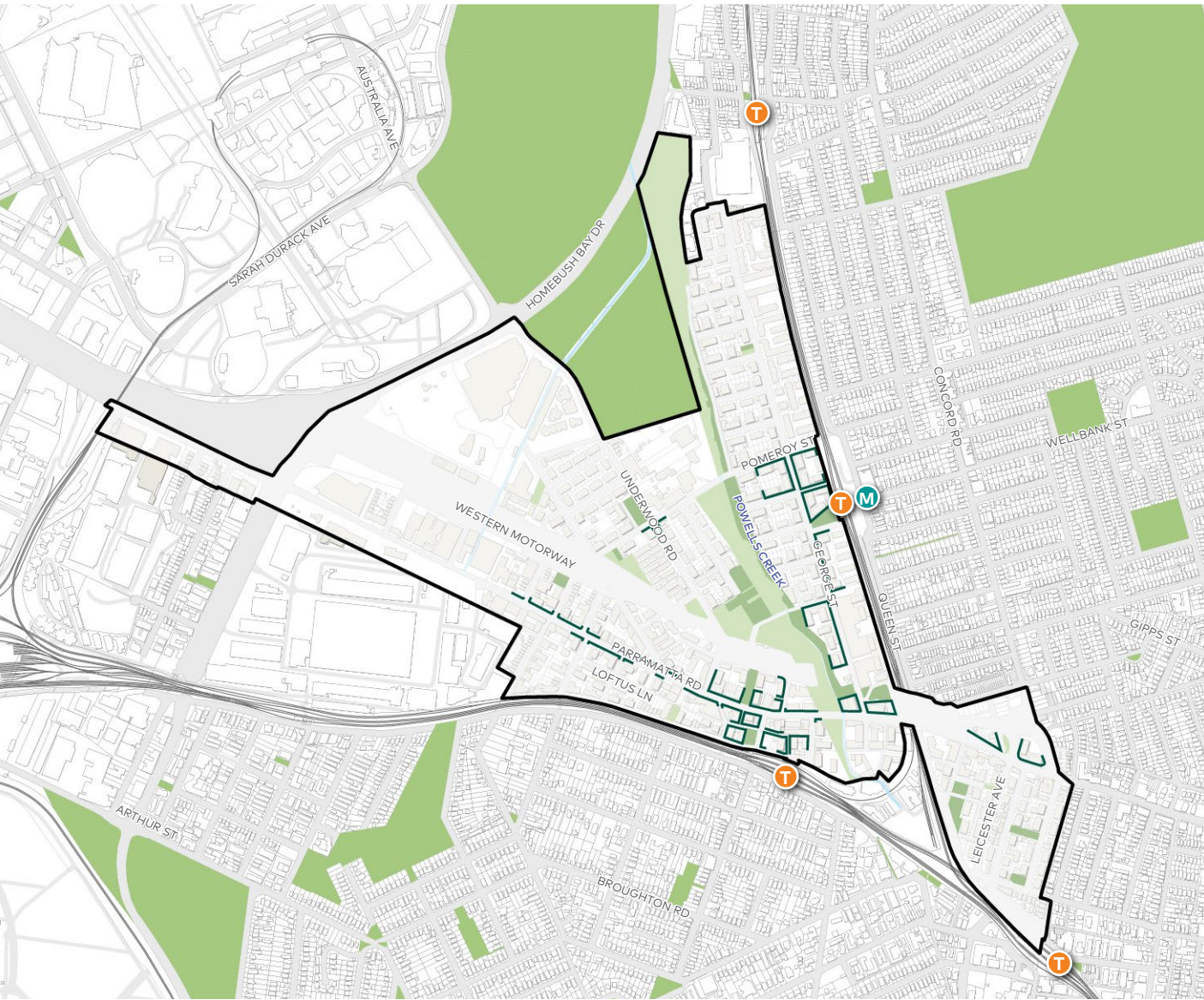


Figure 42: Street wall and podium heights

Precinct Boundary
 M Metro Station
 T Train Station
  1-2 Storey Mixed Use Street Wall and Podium

## Development Capacity

The Precinct's potential development capacity has been quantified to understand its potential future land uses, total dwellings, population and jobs. Land use capacities have been calculated assuming only unconstrained sites have the potential to be redeveloped in the future, or that their development rights are transferred to unconstrained sites.

### Constrained Sites

Sites identified with one (or more) of the following characteristics are assumed to be constrained and therefore may not present as redevelopment opportunities within the assumed timeframe of the masterplan:

- Strata titled lots containing >10 units - either commercial or residential
- Recent Development - significant recent development that is unlikely to present as an opportunity for redevelopment in the long term
- Heritage items\*
- Conservation zone
- Existing open space

*\*Some heritage sites are included as opportunity sites because of their ability to be adaptively reused, or accommodate sympathetic development outcomes, for instance, the Bakehouse Quarter.*

Sites where there are known development proposals within the Precinct but that do not have an approved DA are assumed to be unconstrained and are therefore an opportunity site for development uplift under the Plan.

Proposed open spaces and new streets anticipated to be delivered as part of a private development are assumed to adopt the adjoining lots FSR. Development sites that are also delivering open space can utilise the full FSR allocated to that site and redistribute that density to the developable parts of the site. These open space areas and heritage items are therefore retained within the capacity assessment as having some development potential.

In addition to this, the following information has also been identified for each parcel:

- Total site area of constrained sites to retain existing dwellings, population and jobs
- Heritage items
- Existing FSR

### Proposed Controls

A proposed FSR has been identified for each lot and block, responding to the overall accessibility and amenity within and adjoining the blocks now and in the future. The FSR controls recognise the importance of compatibility with existing character and consider the impact of future built form, constrained land uses, relationship to open space, and proximity to transport. It is assumed that the proposed FSR should not be less than the existing FSR, or the FSR identified under PRCUTS for that block for most areas. Bakehouse Quarter has been identified as an area with great opportunity with the limitation of FSR and height due to heritage constraints.

For each block a proportional split for mixed-uses into non-residential uses and residential uses has been assumed.

### Assumptions

The following assumptions have been used in the development of the masterplan.

GFA to dwelling and Job assumptions	
Average apartment size	90m <sup>2</sup> GFA
People per dwelling	2.5
Mixed-use retail: GFA m <sup>2</sup> per job	30
Mixed-use commercial: GFA m <sup>2</sup> per job	20
Mixed commercial/light industrial: GFA m <sup>2</sup> per job	50
Productivity Support: GFA m <sup>2</sup> per job	50

Mixed-Use (MU1) split			
FSR	Residential	Retail	Commercial
1.2:1	67.0%	33.0%	0.0%
2.2:1	85.0%	10.0%	5.0%
2.5:1	85.0%	10.0%	5.0%
3:1:1	90.0%	5.0%	5.0%
3.5:1	90.0%	5.0%	5.0%
3.6:1	95.0%	2.5%	2.5%
4:1	95.0%	2.5%	2.5%
5:1	95.0%	2.5%	2.5%
6:1	95.0%	2.5%	2.5%

Figure 43: Homebush assumptions tables

Homebush Precinct Future Employment Capacity	
Employment	Jobs
Existing jobs	6,200 (includes 370 retained jobs)
Additional Jobs	2,670
<b>TOTAL</b>	<b>8,870</b>

Homebush Precinct Future Residential Capacity	
Residential	Dwellings
Existing dwellings	6,800 (includes 5,700 retained dwellings)
Additional dwellings	16,100
<b>TOTAL</b>	<b>22,900</b>

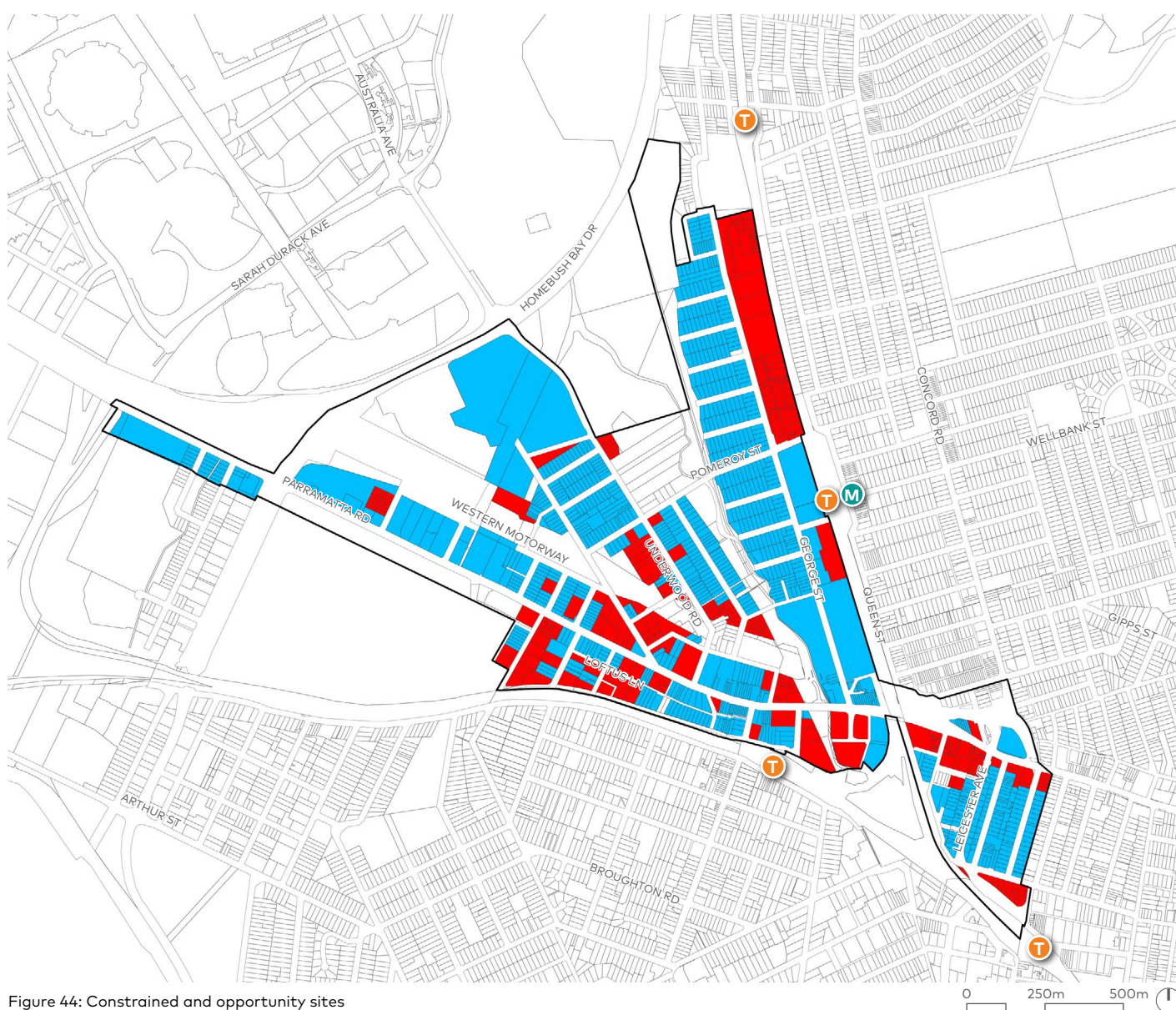


Figure 44: Constrained and opportunity sites

Precinct Boundary
 T Train Station
 M Metro Station
  Retained dwellings/jobs (constrained sites)
  Proposed dwelling/jobs (unconstrained sites)

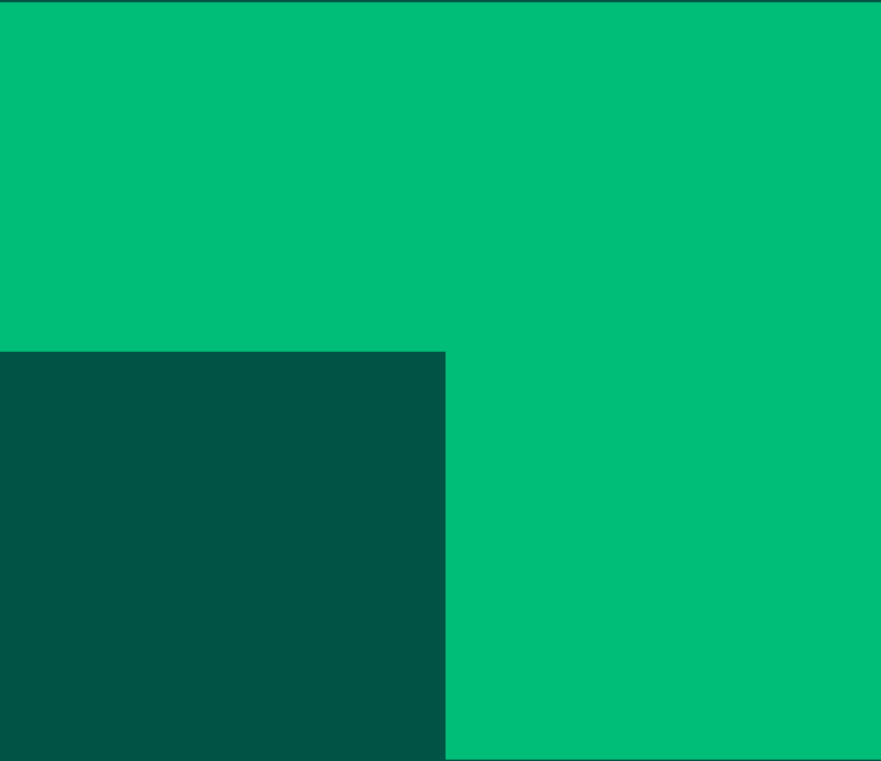
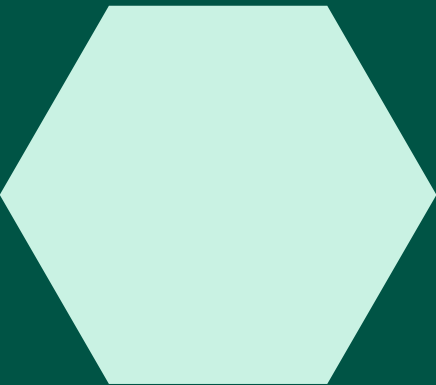


Figure 45: Homebush Precinct Artists Illustration looking north-east towards Concord



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## Implementation



# Land Use Zones

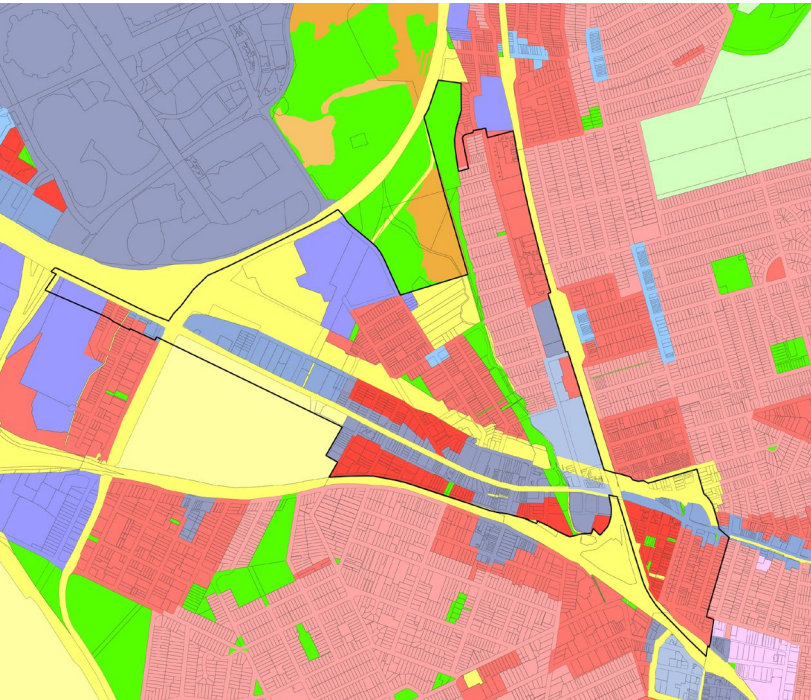


Figure 46: Existing Land use zoning

## Existing Land Zoning

Parramatta Road in the Homebush Precinct is zoned MU1 Mixed Use, R4 High-Density Residential, SP2 Infrastructure Educational Establishment, and E3 Productivity Support. This zoning supports the commercial and retail functions surrounding the railway station while also providing housing.

In the Bakehouse Precinct, the zoning includes E2 Commercial Centre. Moving north up George Street, the area transitions to R2 Low-Density Residential and R3 Medium-Density Residential. Underwood Road is predominantly zoned R3 Medium Density Residential, with the DFO site designated E4 General Industrial.

Additionally, RE1 Public Recreation extends along Powells Creek, enhancing the area’s green infrastructure and recreational spaces.

- |   |   |
|---|---|
| <ul style="list-style-type: none"> <li>Precinct Boundary</li> <li>C3 Environmental Management</li> <li>C2 Environmental Conservation</li> <li>R1 General residential</li> <li>R2 Low density residential</li> <li>R3 Medium density residential</li> <li>R4 High density residential</li> <li>SP1 Special activities</li> </ul> | <ul style="list-style-type: none"> <li>E1 Local centre</li> <li>E2 Commercial centre</li> <li>E3 Productivity support</li> <li>E4 General industrial</li> <li>MU1 Mixed use</li> <li>RE1 Public recreation</li> <li>RE2 Private recreational</li> <li>SP2 Infrastructure</li> </ul> |
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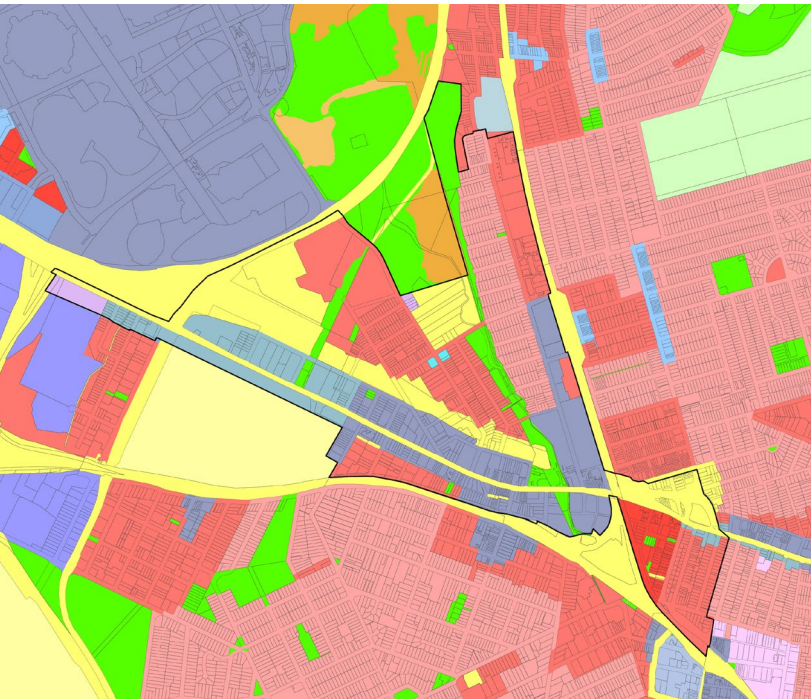


Figure 47: PRCUTS Land use zoning

## PRCUTS Land Zoning

Proposed changes under the PRCUTS include an expansion of the MU1 Mixed Use zoning from Parramatta Road, taking over the R4 High-Density Residential area. Despite this zoning change, the mixed-use designation will continue to deliver residential dwellings.

In the Bakehouse Precinct, the zoning has changed from E2 Commercial Centre to MU1 Mixed Use

- |   |   |
|---|---|
| <ul style="list-style-type: none"> <li>Precinct Boundary</li> <li>C3 Environmental Management</li> <li>C2 Environmental Conservation</li> <li>R1 General residential</li> <li>R2 Low density residential</li> <li>R3 Medium density residential</li> <li>R4 High density residential</li> <li>SP1 Special activities</li> </ul> | <ul style="list-style-type: none"> <li>E1 Local centre</li> <li>E2 Commercial centre</li> <li>E3 Productivity support</li> <li>E4 General industrial</li> <li>MU1 Mixed use</li> <li>RE1 Public recreation</li> <li>RE2 Private recreational</li> <li>SP2 Infrastructure</li> </ul> |
|---|---|

## State led rezoning Land Zoning

Proposed land zoning changes under the PRCUTS (Parramatta Road Corridor Urban Transformation Strategy) include rezoning from R2 Low-Density Residential and R3 Medium-Density Residential to R4 High-Density Residential along George Street and Underwood Road, increasing residential density while maintaining the existing MU1 Mixed Use zones along Parramatta Road, which continue to provide a mix of residential, commercial, and retail uses.

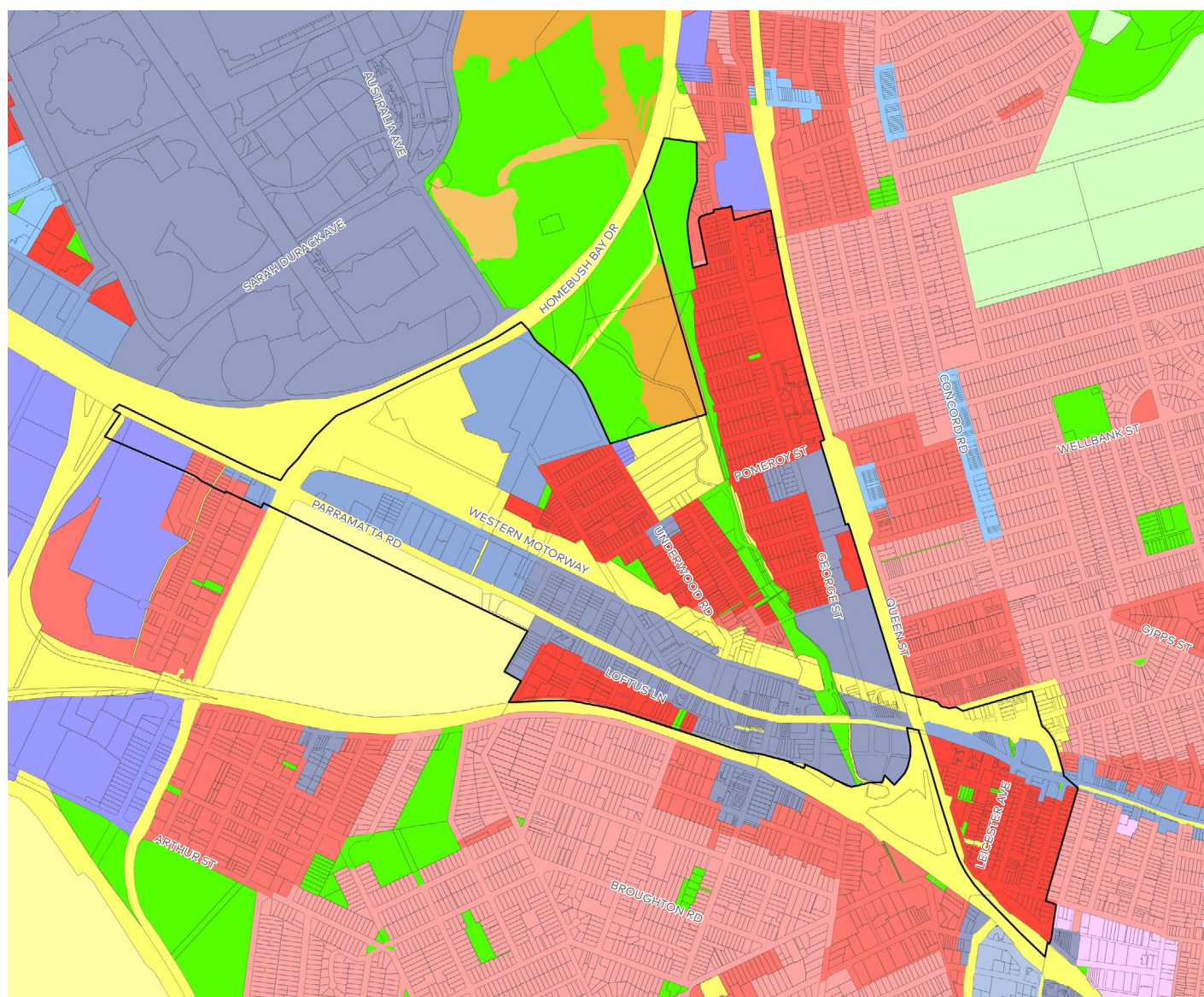


Figure 48: Proposed uses

Precinct Boundary	E1 Local centre	R1 General residential	MU1 Mixed use
C3 Environmental Management	E2 Commercial centre	R2 Low density residential	RE1 Public recreation
SP1 Special activities	E3 Productivity support	R3 Medium density residential	RE2 Private recreational
SP2 Infrastructure	E4 General industrial	R4 High density residential	C2 Environmental Conservation

# Floor Space Ratios

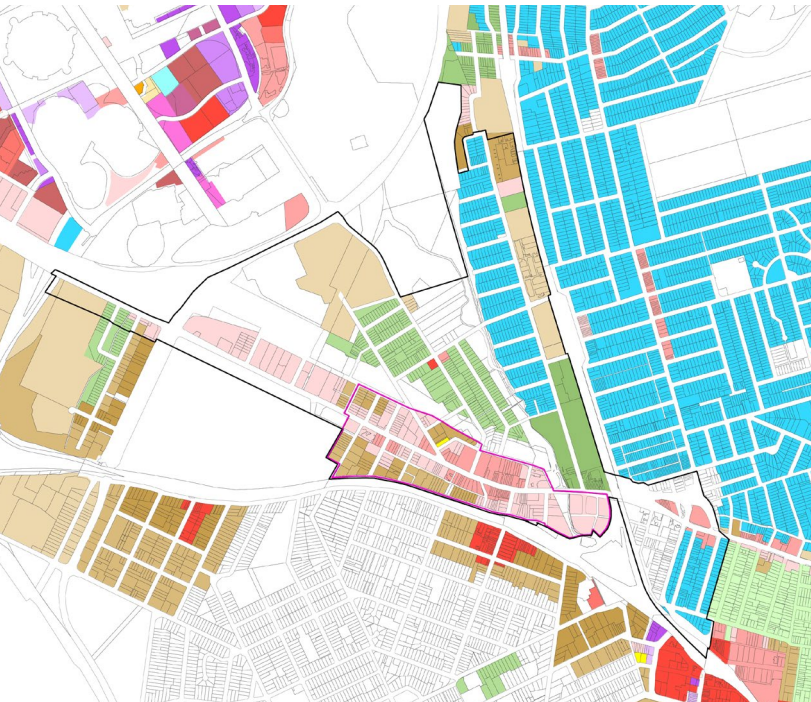


Figure 49: Existing floor space ratio

## Existing floor space ratio

The existing Floor Space Ratio (FSR) along Parramatta Road ranges from 1.2 to 2, reflecting a mix of commercial and residential development. In contrast, the Bakehouse Quarter has a lower FSR of 0.75, maintaining its heritage character. Residential areas with lower-scale housing have FSRs ranging from 0.65 to 1, supporting detached dwellings and preserving the suburban character.

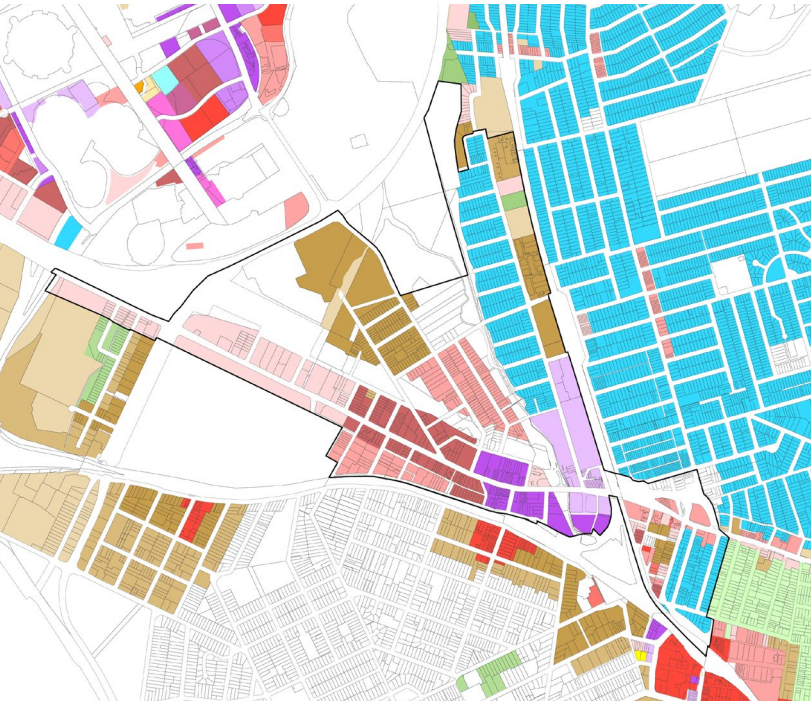
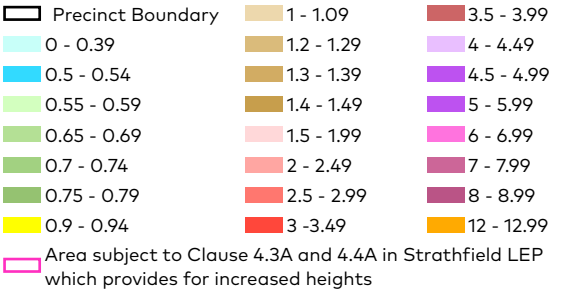
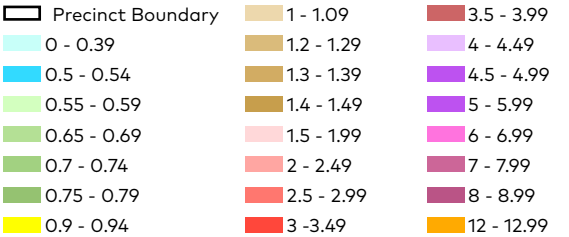


Figure 50: PRCUTS floor space ratio

## PRCUTS floor space ratio

PRCUTS introduces increased mixed-use density along Parramatta Road, ranging from 5:1 to 2.2:1 as the scale decreases down the corridor. The Bakehouse Precinct is also set to see an increase in density, with the FSR increased to 4:1, enhancing its development potential while respecting its heritage context.



# State led rezoning floor space ratio

Along Parramatta Road, the Floor Space Ratio (FSR) is maintained as per the PRCUTS guidelines, ranging from 5:1 to 2.2:1, with the scale decreasing down the corridor. Residential density has increased around Underwood Road and North George Street, supporting higher-density housing. This represents a change from an FSR of 0.5:1, typical of detached single dwellings, to an FSR of 1.5 to 3.5:1, accommodating apartment developments.

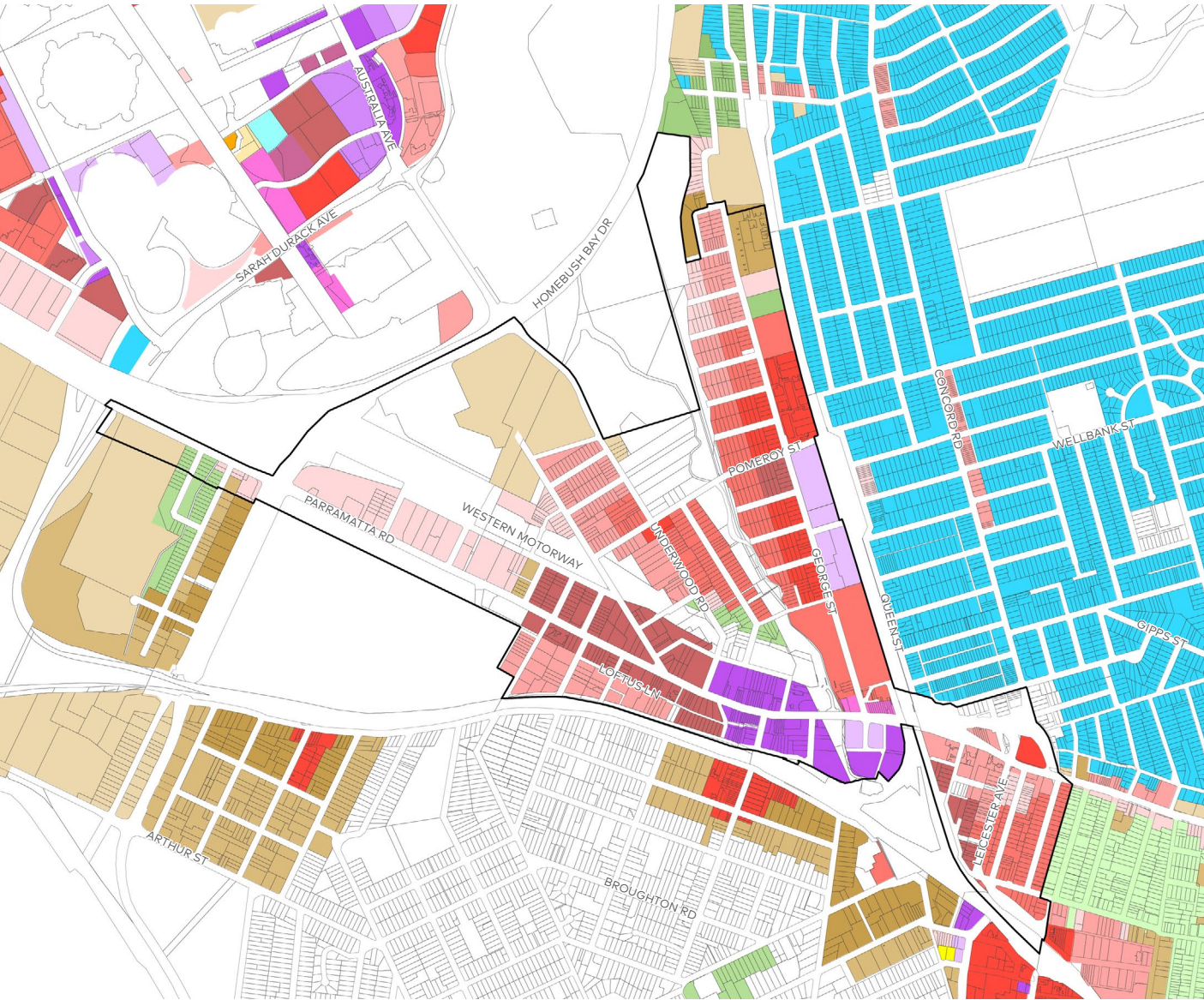


Figure 51: Proposed floor space ratio

<div></div> Precinct Boundary	<div></div> 0.65 - 0.69	<div></div> 1 - 1.09	<div></div> 1.5 - 1.99	<div></div> 3.5 - 3.99	<div></div> 6 - 6.99
<div></div> 0 - 0.39	<div></div> 0.7 - 0.74	<div></div> 1.2 - 1.29	<div></div> 2 - 2.49	<div></div> 4 - 4.49	<div></div> 7 - 7.99
<div></div> 0.5 - 0.54	<div></div> 0.75 - 0.79	<div></div> 1.3 - 1.39	<div></div> 2.5 - 2.99	<div></div> 4.5 - 4.99	<div></div> 8 - 8.99
<div></div> 0.55 - 0.59	<div></div> 0.9 - 0.94	<div></div> 1.4 - 1.49	<div></div> 3 - 3.49	<div></div> 5 - 5.99	<div></div> 12 - 12.99

# Height of Buildings



Figure 52: Existing height of building

## Existing height of buildings

The heights of buildings along Parramatta Road range from 11m to 22m for the majority, with areas near Homebush Station reaching up to 38m. The Bakehouse Quarter currently has a height limit of 27m. Lower-scale residential areas, such as Pomeroy Street, George Street north of Pomeroy, and Underwood Road, have a maximum height of 9.5m. The DFO site is limited to 12m.

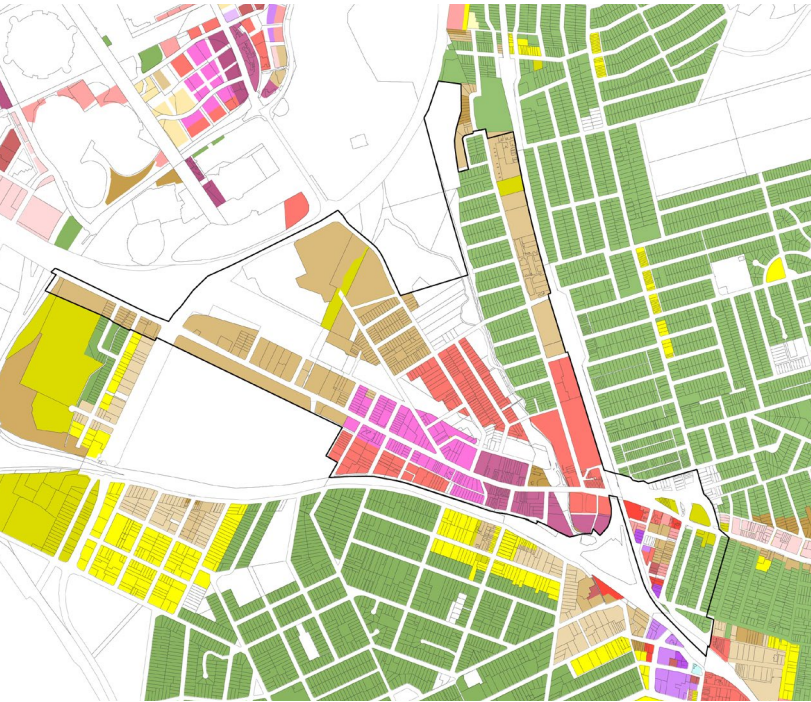
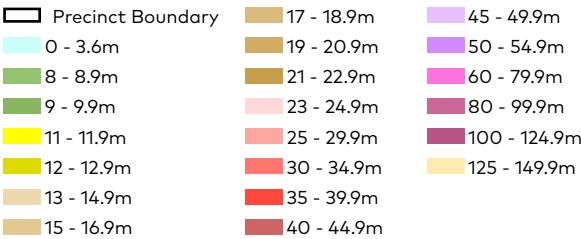


Figure 53: PRCUTS height of building

## PRCUTS height of buildings

Under PRCUTS, the Height of Buildings (HOB) along Parramatta Road has been increased, ranging from 35m to 80m, significantly raising the height limits from the existing conditions. The Bakehouse Quarter's height limit is set at 32m. Residential areas along George Street north of Pomeroy Street remain at 8.5m. Residential areas on Underwood Road have increased to 30m from 8.5m. The DFO site, under PRCUTS, has a height limit of 17m.



# State led rezoning height of buildings

Heights have been specifically adjusted under the state-led rezoning, responding to various factors such as open space, heritage, pedestrian walks, and access to transport. Along Parramatta Road, the height limits remain consistent with PRCUTS, ranging from 35m to 80m. The Strathfield Triangle has seen changes, with developable sites now topping at 80m.

George Street has several designations: the Bakehouse Precinct is now enabled to support residential buildings up to 40m, with setbacks designed to maintain the heritage character and keep

bulk development off the streetscape. Opposite North Strathfield Station and Metro, buildings range up to 50m at the junction of Pomeroy and George Streets. North of Pomeroy, residential heights along George Street step down from 45m to 25m. Heights along Powells Creek are capped at 35m, and Underwood Road is approximately 25m. The DFO site is maintained at 22m.

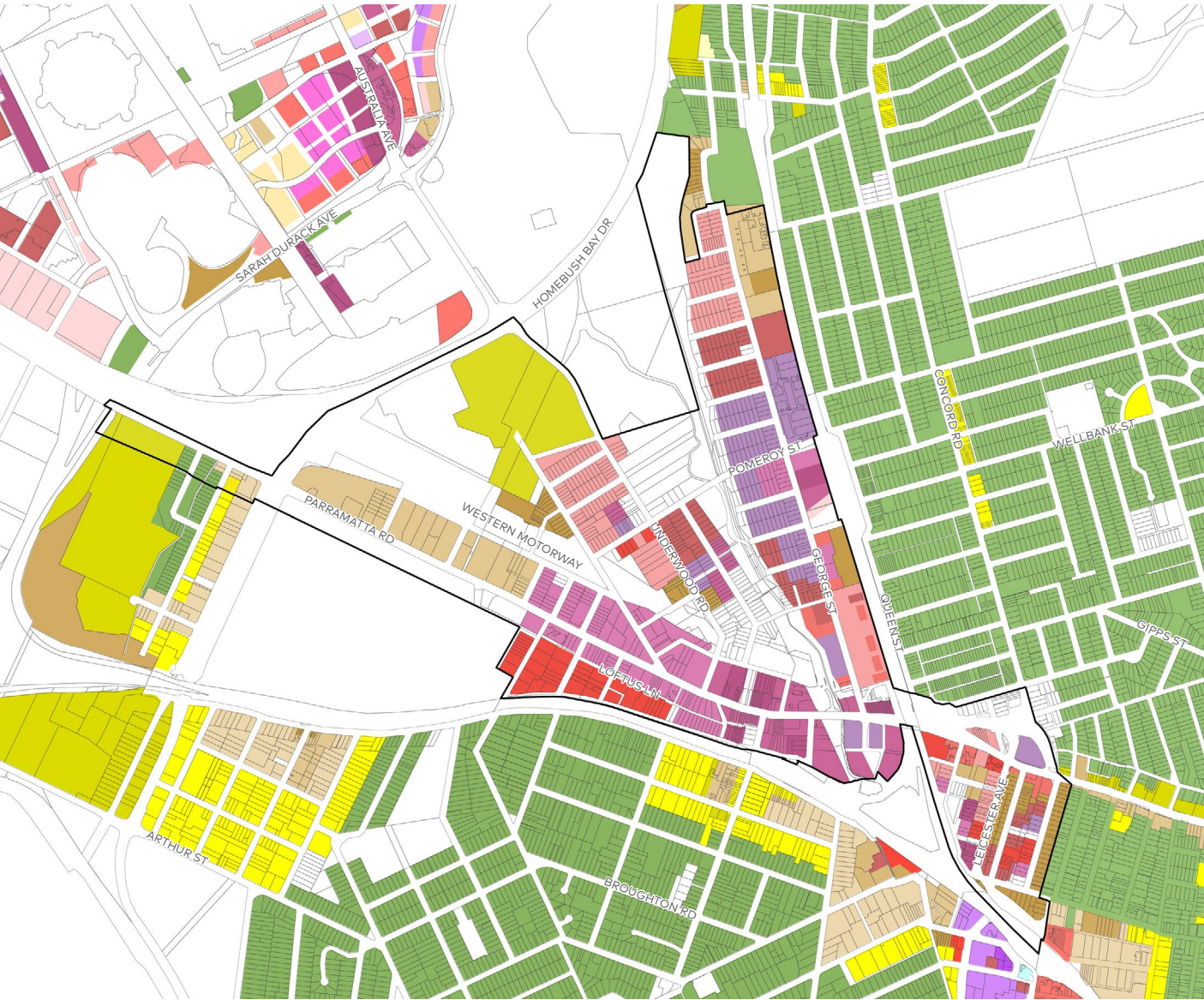


Figure 54: Proposed height of building

<div></div> <div>Precinct Boundary</div>	<div></div> <div>11 - 11.9m</div>	<div></div> <div>17 - 18.9m</div>	<div></div> <div>25 - 29.9m</div>	<div></div> <div>45 - 49.9m</div>	<div></div> <div>100 - 124.9m</div>
<div></div> <div>0 - 3.6m</div>	<div></div> <div>12 - 12.9m</div>	<div></div> <div>19 - 20.9m</div>	<div></div> <div>30 - 34.9m</div>	<div></div> <div>50 - 54.9m</div>	<div></div> <div>125 - 149.9m</div>
<div></div> <div>8 - 8.9m</div>	<div></div> <div>13 - 14.9m</div>	<div></div> <div>21 - 22.9m</div>	<div></div> <div>35 - 39.9m</div>	<div></div> <div>60 - 79.9m</div>	
<div></div> <div>9 - 9.9m</div>	<div></div> <div>15 - 16.9m</div>	<div></div> <div>23 - 24.9m</div>	<div></div> <div>40 - 44.9m</div>	<div></div> <div>80 - 99.9m</div>	

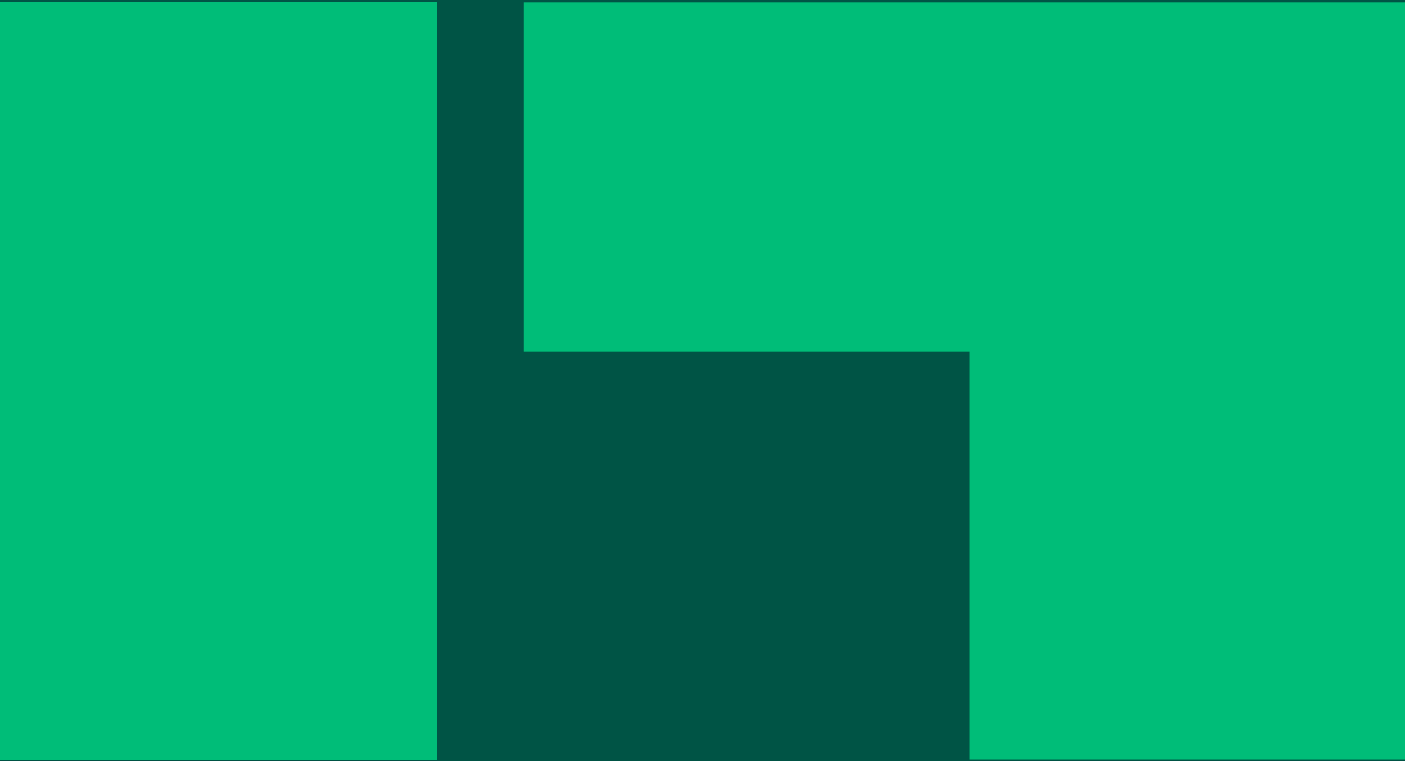
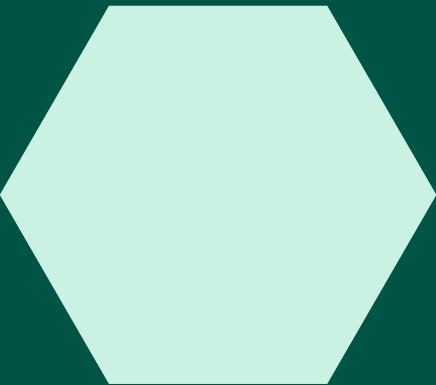


Figure 55: Homebush Precinct Artists Illustration looking south-east towards Strathfield



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## Proof of Concept



# Overview

The following Proof of Concept layouts represent just one of many permutations of what may be developed under the proposed permissible Land Uses and Floor Space Ratios and are not representative of any development outcomes that may be achieved through a development application and/or design excellence process.

The following section outlines this testing in alignment with:

- Homebush Precinct Design Guideline
- Apartment Design Guide (ADG) criteria and guidance

## Assumptions

The following tables outline key assumptions in relation to the Proof of Concept testing:

Assumptions	
Market apartment studio (minimum)	45m² NSA
Market Apartment 1 bed size (minimum)	50m² NSA
Market Apartment 2 bed size (minimum)	75m² NSA
Market Apartment 3 bed size (minimum)	99m² NSA
Average Apartment size	90 m² GFA
Non-residential GBA/GFA Efficiency	85%
Residential GBA/GFA Efficiency	75%
NSA to GFA Efficiency	80%

Floor heights	Min. floor to floor height
Ground floor	4.5
Commercial/retail	4.0
Residential	3.2

## Rothwell Avenue to Mena Street

### Proposed Planning Controls

#	Lot Size	HOB	No. Storeys	FSR	Non-Residential GFA	Residential GFA
1A	8,532m <sup>2</sup>	28m	8	2.2:1	-	18,770m <sup>2</sup>
1B	10,455m <sup>2</sup>	28m	8	2.2:1	-	23,001m <sup>2</sup>
1C	7,952m <sup>2</sup>	28m	8	2.2:1	-	17,494m <sup>2</sup>
2A	5,595m <sup>2</sup>	28m	8	1.8:1	-	10,071m <sup>2</sup>
2B	6,747m <sup>2</sup>	28m	8	1.8:1	-	12,144m <sup>2</sup>

### Assumptions

- Lot 2B to deliver new expanded open space east of Powells Creek (min 15m width)
- Lots 2A, 2B to deliver new street network along the proposed expanded open space connecting to Conway Avenue (12.5m width road reserve) with parking on one side
- East-west movement across Powells Creek via the existing bridge on Conway Avenue





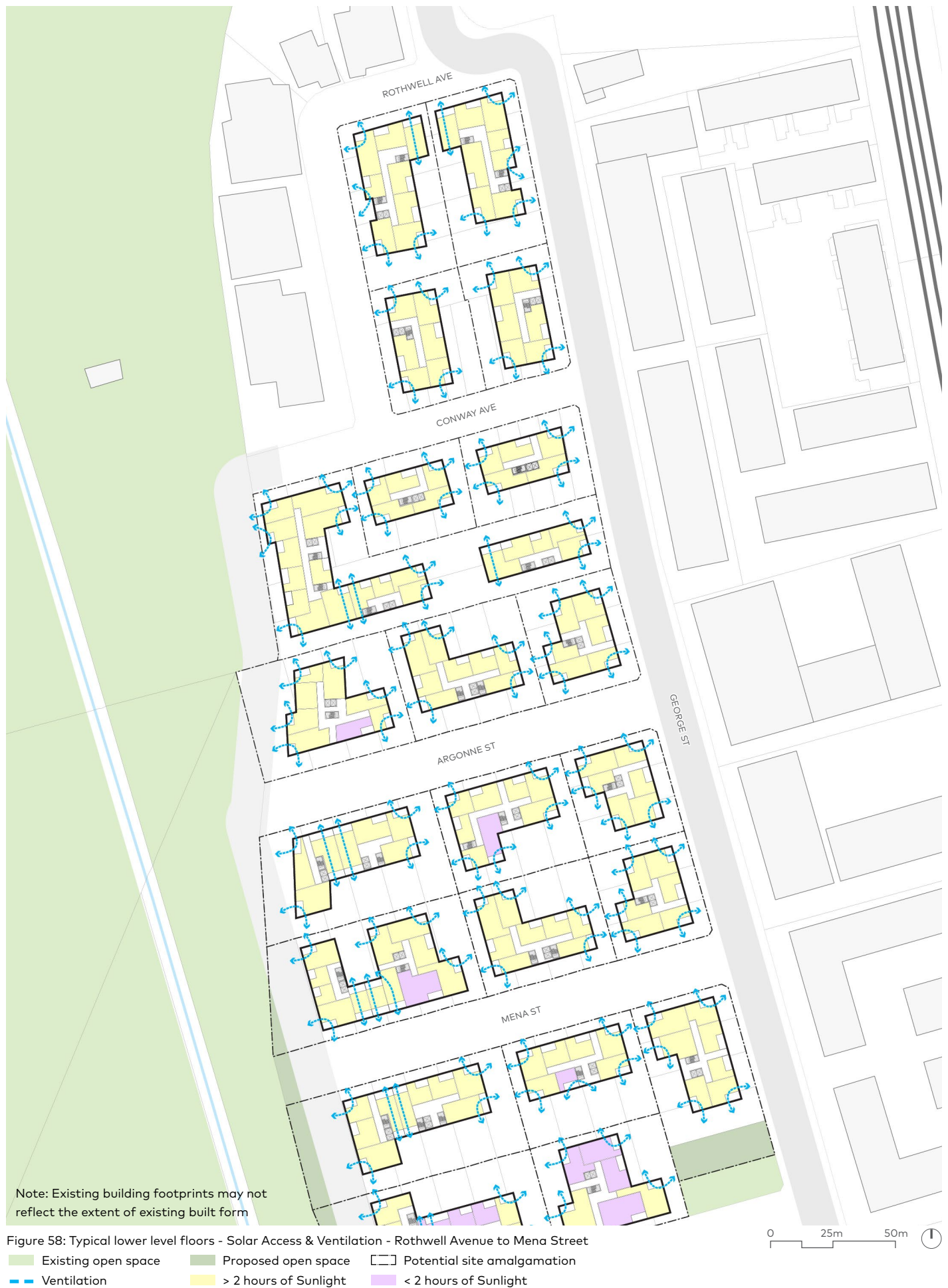
Figure 56: Rothwell Avenue to Mena Street

Existing open space    Proposed open space    Lot    # Lot number    Ⓢ Number of storeys    Existing building

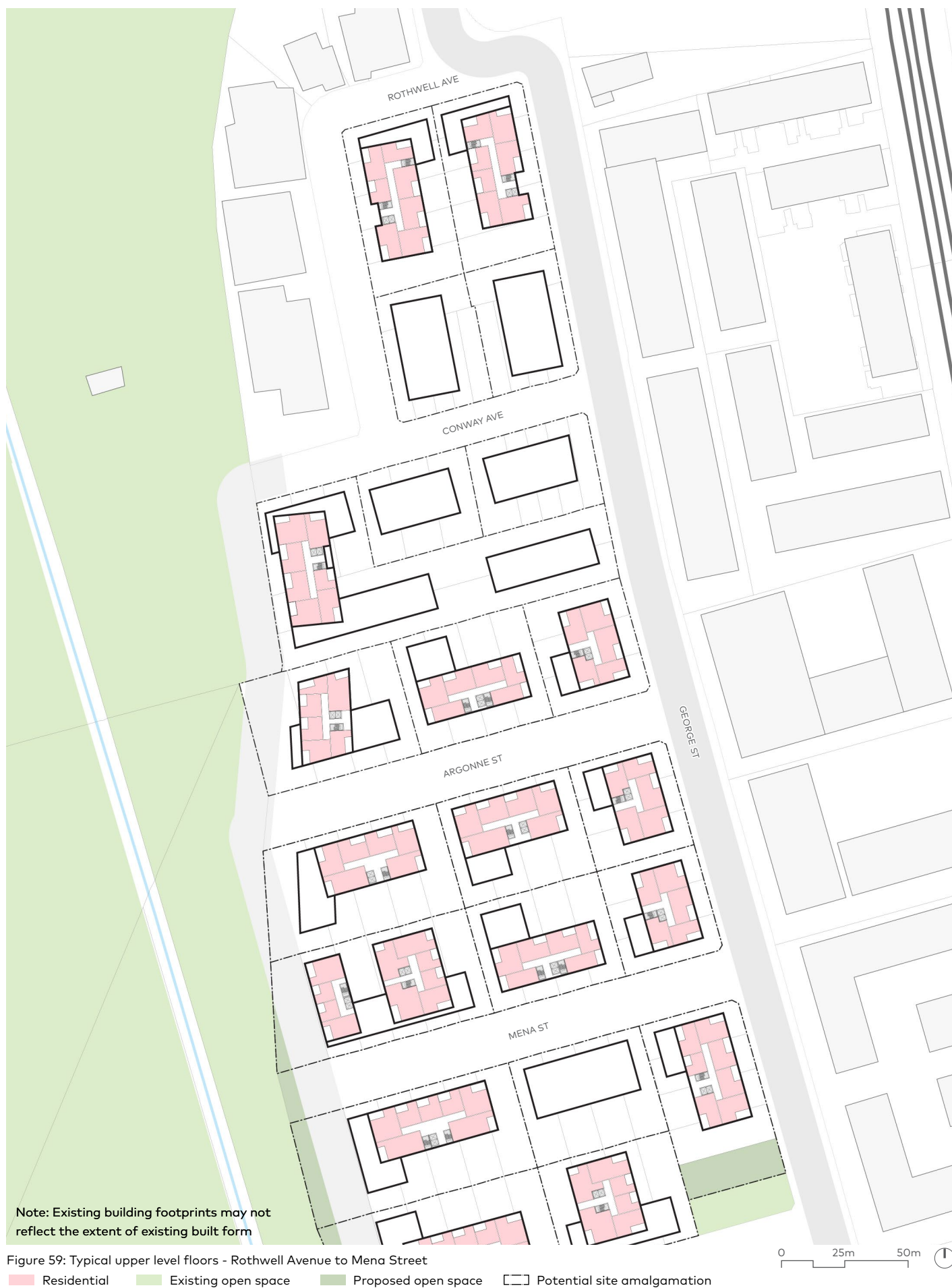
## Typical Lower level floors - Indicative layout



## Typical Lower level floors - Solar Access & Ventilation



## Typical upper level floors - Indicative layout



## Typical upper level floors - Solar Access & Ventilation



## Mena Street to Pomeroy Street

### Proposed Planning Controls

#	Lot Size	HOB	No. Storeys	FSR	Non-Residential GFA	Residential GFA
1A	7,391m <sup>2</sup>	42m	12	2.8:1		20,694m <sup>2</sup>
1B	8,160m <sup>2</sup>	42m	12	2.8:1		22,848m <sup>2</sup>
1C	7,678m <sup>2</sup>	52m	15	2.8:1		21,498m <sup>2</sup>
1D	9,270m <sup>2</sup>	52m	15	2.8:1		25,956m <sup>2</sup>
2A	7,507m <sup>2</sup>	48m	12	2.2:1		16,515m <sup>2</sup>
2B	7,472m <sup>2</sup>	42m	12	2.2:1		16,438m <sup>2</sup>
3A	8,160m <sup>2</sup>	52m	15	3.2:1		26,112m <sup>2</sup>
3B	8,538m <sup>2</sup>	52m	15	3.2:1		27,321m <sup>2</sup>

### Assumptions

- Lots 2A, 2B, 1C, 1D to deliver new expanded open space east of Powells Creek aligned with development footprint (min 15m width)
- Lots 2A, 2B, 1C, 1D to deliver new street network along the proposed expanded open space connecting to Pomeroy Street (12.5m width road reserve) with parking on one side
- East-west movement via the existing street network on Pomeroy Street
- Lot 1A to deliver expansion to the existing open space





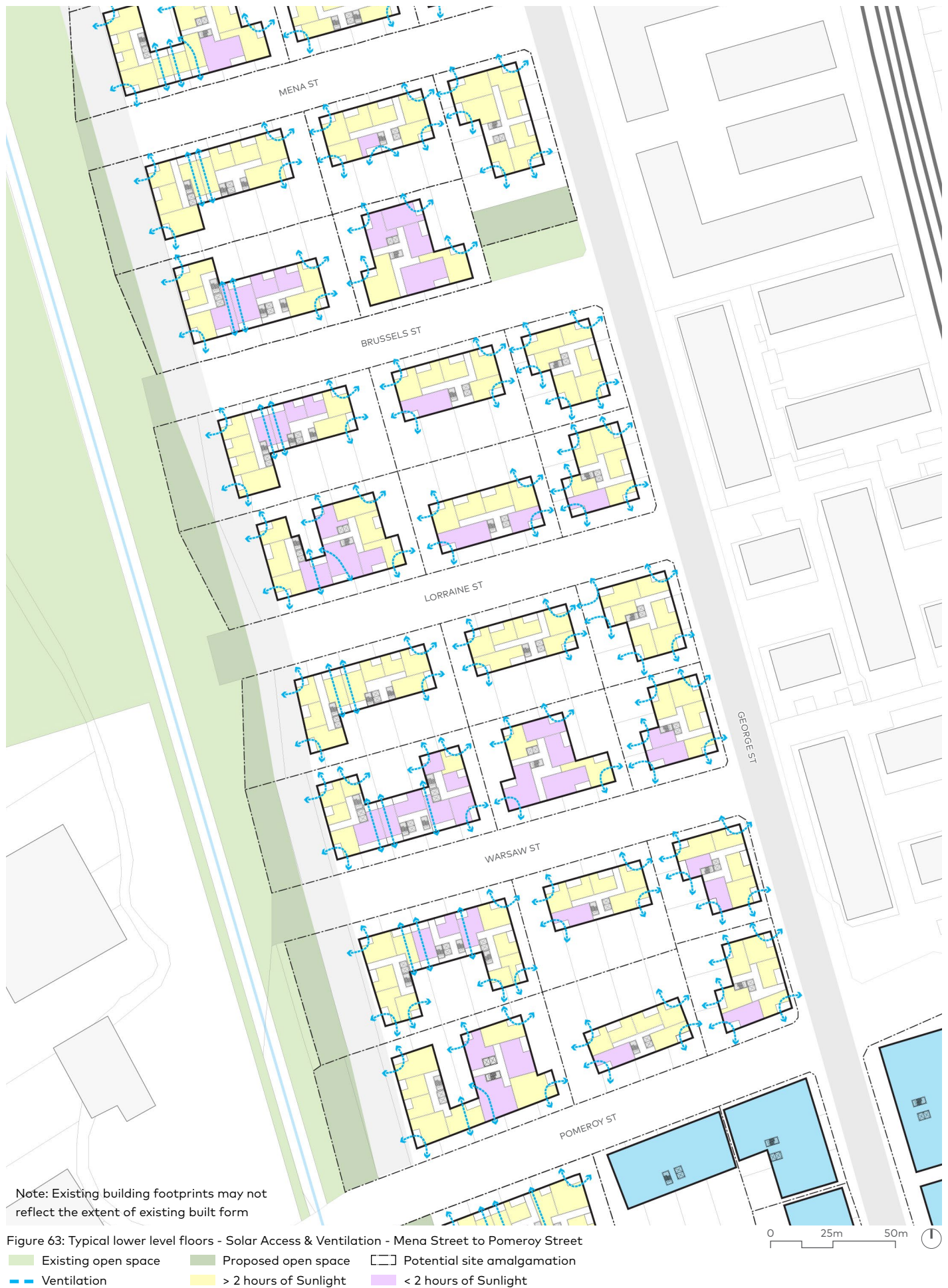
Figure 61: Mena Street to Pomeroy Street

Existing open space    Proposed open space    Lot    # Lot number    # Number of storeys    Existing building

## Typical lower level floors - Indicative layout



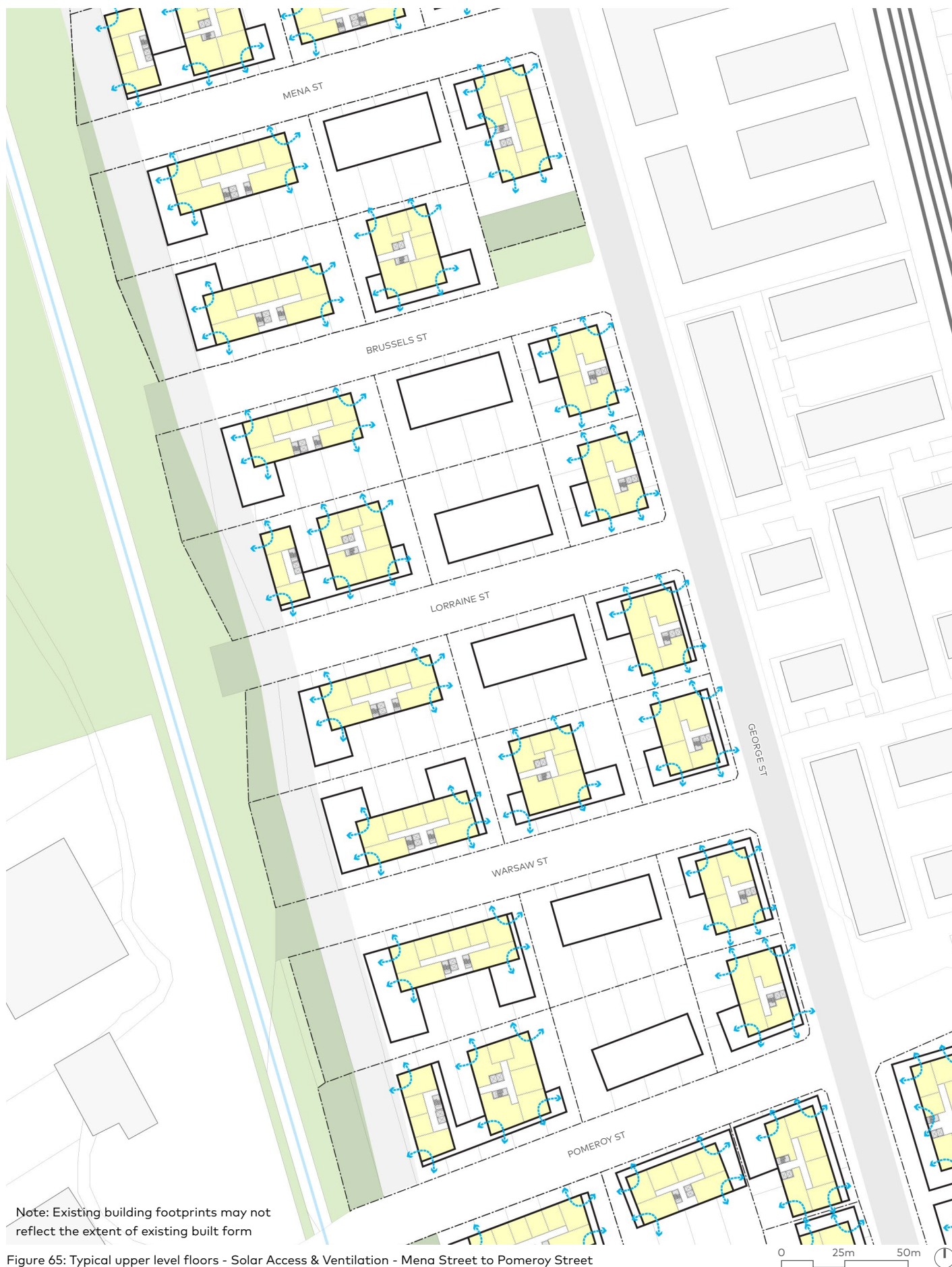
## Typical lower level floors - Solar Access & Ventilation



## Typical upper level floors - Indicative layout



## Typical upper level floors - Solar Access & Ventilation



## Pomeroy Street to Allen Street

### Proposed Planning Controls

#	Lot Size	HOB	No. Storeys	FSR	Non-Residential GFA	Residential GFA
1	9,388m <sup>2</sup>	62	18	3.5:1	3,285m <sup>2</sup>	29,572m <sup>2</sup>
2A	7,957m <sup>2</sup>	52	15	3.2:1		25,462m <sup>2</sup>
2B	7,956m <sup>2</sup>	52	15	3.2:1		25,459m <sup>2</sup>
2C	9,820m <sup>2</sup>	42-52	15	3.2:1		31,424m <sup>2</sup>
3A	8,897m <sup>2</sup>	52	15	2.8:1		24,911m <sup>2</sup>
3B	7,945m <sup>2</sup>	52	15	2.8:1		22,246m <sup>2</sup>
3C	6,118m <sup>2</sup>	42	12	2.8:1		17,130m <sup>2</sup>
3D	5,269m <sup>2</sup>	42	12	2.8:1		14,753m <sup>2</sup>
4	21,195m <sup>2</sup>	23-103	20-30	4:1	4,239m <sup>2</sup>	80,541m <sup>2</sup>
5	7,921m <sup>2</sup>	30-75	18-22	4:1	1,584m <sup>2</sup>	30,099m <sup>2</sup>

### Assumptions

- Lots 3A, 3B, 3C, 3D to deliver new expanded open space east of Powells Creek aligned with development footprint (min 15m width)
- Lot 4 to deliver open space on the south of the site, open space to be used for east-west movement via existing bridge on Hamilton Street to the North Strathfield station
- Lot 2C to consider the existing heritage building



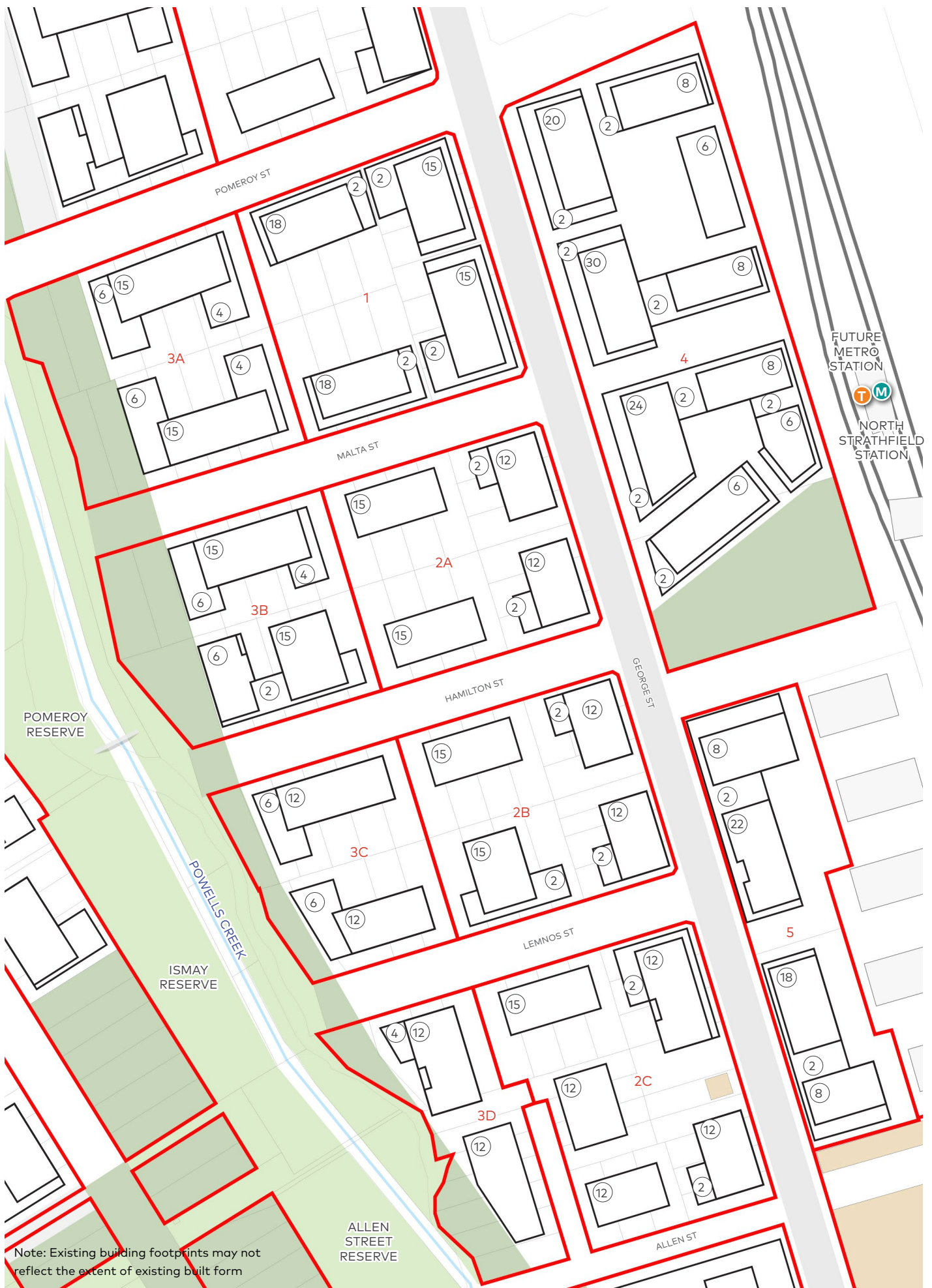


Figure 66: Pomeroy Street to Allen Street

Existing open space
  Proposed open space
  Lot
  # Lot number
  # Number of storeys

Existing building
  Heritage

## Typical lower level floors - Indicative layout



Note: Existing building footprints may not reflect the extent of existing built form

Figure 67: Lower level floors - Indicative layout - Pomeroy Street to Allen Street

Commercial/Retail Residential Existing open space Proposed open space Potential site amalgamation

## Typical lower level floors - Solar Access & Ventilation



Figure 68: Lower level floors - Solar Access & Ventilation - Pomeroy Street to Allen Street

■ Existing open space   
 ■ Proposed open space   
  Potential site amalgamation  
— Ventilation   
 > 2 hours of Sunlight   
 < 2 hours of Sunlight

## Typical upper level floors - Indicative layout



Figure 69: Typical upper level floors - Indicative layout - Pomeroy Street to Allen Street

■ Residential
 ■ Existing open space
 ■ Proposed open space
  Potential site amalgamation

## Typical upper level floors - Solar Access & Ventilation



Figure 70: Typical upper level floors - Solar Access & Ventilation - Pomeroy Street to Allen Street

- |  |  |  |
|--|--|--|
| <span style="display: inline-block; width: 15px; height: 10px; background-color: #d4edda; border: 1px solid #c3e6cb; margin-right: 5px;"></span> Existing open space | <span style="display: inline-block; width: 15px; height: 10px; background-color: #fff3cd; border: 1px solid #ffeeba; margin-right: 5px;"></span> Proposed open space | <span style="display: inline-block; width: 15px; height: 10px; border: 1px dashed black; margin-right: 5px;"></span> Potential site amalgamation |
| <span style="color: blue;">→</span> Ventilation  | <span style="display: inline-block; width: 15px; height: 10px; background-color: #fff3cd; margin-right: 5px;"></span> > 2 hours of Sunlight                          | <span style="display: inline-block; width: 15px; height: 10px; background-color: #f8d7da; margin-right: 5px;"></span> < 2 hours of Sunlight      |

# Bakehouse Quarter

## Proposed Planning Controls

#	Lot Size	HOB	No. Storeys	FSR	Non-Residential GFA	Residential GFA
1	33,903m <sup>2</sup>	27-44m	12	2.5:1	12,713m <sup>2</sup>	72,043m <sup>2</sup>
2	24,929m <sup>2</sup>	30-53m	15	2.5:1	9,348m <sup>2</sup>	52,974m <sup>2</sup>
3	2,389m <sup>2</sup>	82m	24	6:1	716m <sup>2</sup>	13,617m <sup>2</sup>
4	3,069m <sup>2</sup>	103m	30	6:1	920m <sup>2</sup>	17,493m <sup>2</sup>

## Assumptions

- 8-15 Storey limit within Bakehouse Quarter west
- 8-12 store limit within Bakehouse Quarter east

### Heritage principles

Refer to Homebush Stage-Led Rezoning Transport Orientated Development Precinct - Heritage Significance Assessment Report, GML Heritage for more information

- *Design new development around the retention of industrial built form, maintaining its visibility in the public domain, including roof forms, elevations, and building footprint.*
- *Development is required to be set back from boundary lot street frontages, reinforcing the predominance of heritage façades in height, setbacks and street alignment. Avoid additional building heights that are visible to George Street frontages.*

- *Visual impacts to heritage significance must be avoided or mitigated and impacts demonstrated through robust visual assessment.*
- *Design new parapets along the western side of George Street are to be lower at the street edge, to form a cohesive street edge with existing built form, and ameliorate potential heritage impacts from maximum heights proposed within the site.*
- *Locate new open space/through site links, to interpret historical locations of open space on Arnott's complex.*
- *New development should not compete with heritage fabric but be designed to complement and enhance heritage values.*
- *Future development is to be of high design and construction quality, and sympathetic to its key heritage qualities and context of the Bakehouse Quarter, in form, bulk, scale and material selection.*
- *Materials and colours of new development shall be responsive to existing heritage buildings and structures, and shall be designed to avoid visual dominance.*
- *Integrate and interpret the Bakehouse Quarter's industrial history into public domain design.*





## Typical lower level floors - Indicative layout



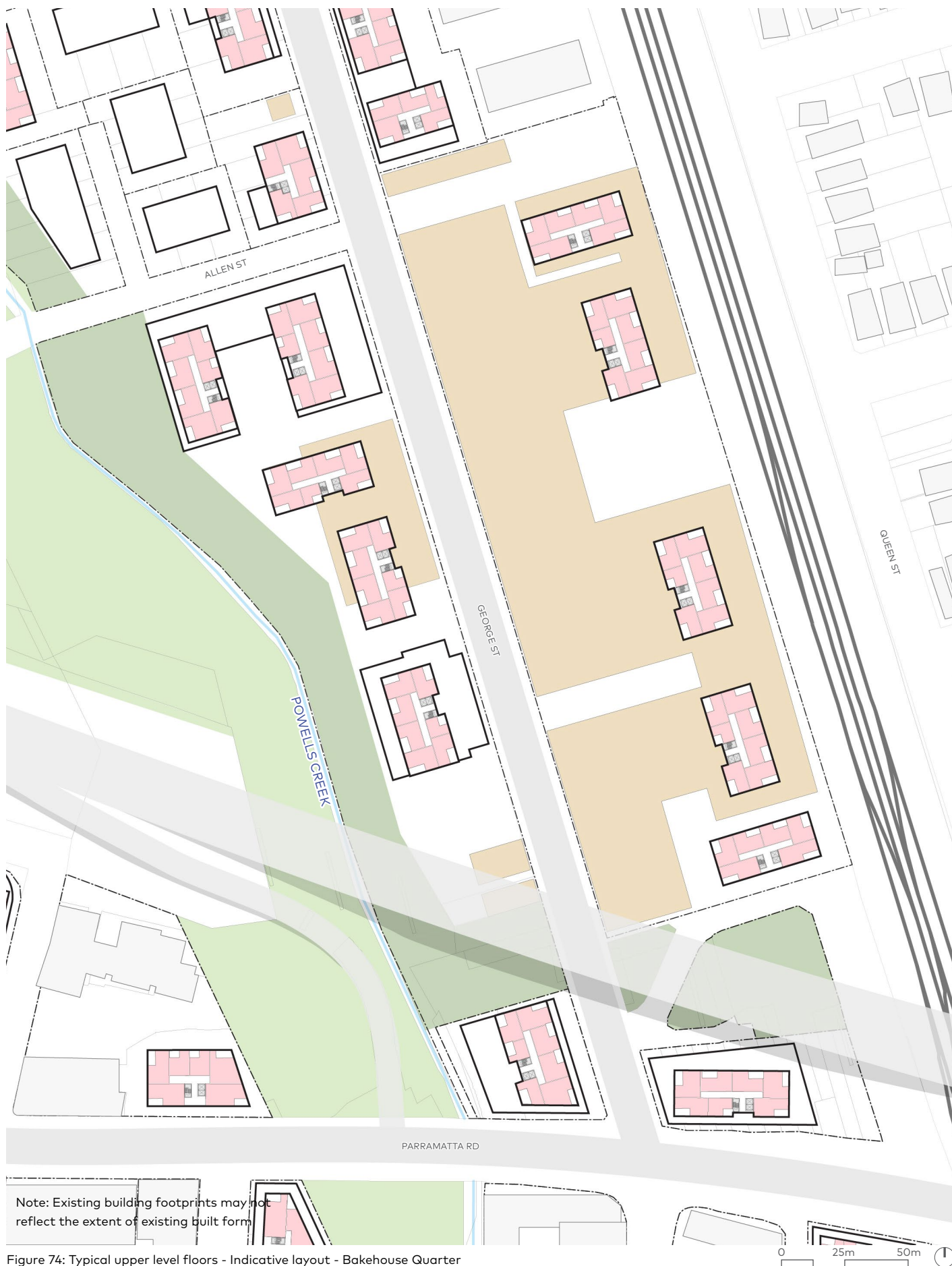
Figure 72: Typical lower level floors - Indicative layout - Bakehouse Quarter

■ Commercial/Retail   
 ■ Residential   
 ■ Existing open space   
 ■ Heritage   
 ■ Proposed open space   
  Potential site amalgamation

## Typical lower level floors - Solar Access & Ventilation



## Typical upper level floors - Indicative layout



## Typical upper level floors - Solar Access & Ventilation



## Underwood Road to Powells Creek

### Proposed Planning Controls

#	Lot Size	HOB	No. Storeys	FSR	Non-Residential GFA	Residential GFA
1A	17,333m <sup>2</sup>	42-81m	12-24	2.8:1		48,532m <sup>2</sup>
1B	24,339m <sup>2</sup>	42-81m	12-20	2.8:1		68,149m <sup>2</sup>
2	438m <sup>2</sup>	44m	12	3:1	131m <sup>2</sup>	1,182m <sup>2</sup>
3	2,421m <sup>2</sup>	21m	6	1.9:1		4,599m <sup>2</sup>
4	2,770m <sup>2</sup>	21m	6	2.2:1		6,094m <sup>2</sup>

### Assumptions

- Lots 1A, 1B to deliver new expanded open space west of Powells Creek
- Lot 1B to consider the existing heritage and church building
- Potential for re-alignment of Allen Street to Underwood Road





## Typical lower level floors - Indicative layout



Figure 77: Typical lower level floors - Indicative layout - Underwood Road to Powells Creek

Residential Existing open space Proposed open space Potential site amalgamation

0 25m 50m

## Typical lower level floors - Solar Access & Ventilation



Figure 78: Typical lower level floors - Solar Access & Ventilation - Underwood Road to Powells Creek

- |   |   |   |
|---|---|---|
| <span style="display:inline-block; width:15px; height:10px; background-color:lightgreen; border:1px solid black;"></span> Existing open space | <span style="display:inline-block; width:15px; height:10px; background-color:lightgreen; border:1px solid black;"></span> Proposed open space | <span style="display:inline-block; width:15px; height:10px; border:1px dashed black;"></span> Potential site amalgamation |
| <span style="color:blue;">—</span> Ventilation  | <span style="display:inline-block; width:15px; height:10px; background-color:yellow;"></span> > 2 hours of Sunlight                           | <span style="display:inline-block; width:15px; height:10px; background-color:purple;"></span> < 2 hours of Sunlight       |

0 25m 50m

## Typical upper level floors - Indicative layout



Figure 79: Typical upper level floors - Indicative layout - Underwood Road to Powells Creek

■ Residential   
 ■ Existing open space   
 ■ Proposed open space   
  Potential site amalgamation

## Typical upper level floors - Solar Access & Ventilation



Figure 80: Typical upper level floors - Solar Access & Ventilation - Underwood Road to Powells Creek

- Existing open space
- Proposed open space
- Potential site amalgamation
- Ventilation
- > 2 hours of Sunlight
- < 2 hours of Sunlight



## West of Underwood Road

### Proposed Planning Controls

#	Lot Size	HOB	No. Storeys	FSR	Non-Residential GFA	Residential GFA
1A	7,623m <sup>2</sup>	28m	8	2.2:1		16,770m <sup>2</sup>
1B	14,999m <sup>2</sup>	28m	8	2.2:1		32,997m <sup>2</sup>
1C	17,146m <sup>2</sup>	28m	8	2.2:1		37,721m <sup>2</sup>
1D	3,065m <sup>2</sup>	35m	10	2.2:1		6,743m <sup>2</sup>
2A	4,493m <sup>2</sup>	21m	6	1.9:1		8,536m <sup>2</sup>
2B	3,989m <sup>2</sup>	21m	6	1.9:1		7,579m <sup>2</sup>
3	9,651m <sup>2</sup>	52-81m	18-24	2.8:1		27,022m <sup>2</sup>
4	1,410m <sup>2</sup>	37m	10	3:1	423m <sup>2</sup>	3,807m <sup>2</sup>

### Assumptions

- Existing strata and church to remain in short to medium term
- Lot 3 to deliver open space and road around the proposed open space
- Lot 1D to deliver open space
- Lot 1C to consider the existing heritage and church building





Figure 81: West of Underwood Road

Existing open space    
  Proposed open space    
  Lot    
 # Lot number    
 Ⓢ Number of storeys  
 Existing building    
 Heritage

0 25m 50m

## Typical lower level floors - Indicative layout



Figure 82: Typical lower level floors - Indicative layout - West of Underwood Road

Commercial/Retail Residential Existing open space Proposed open space Potential site amalgamation

## Typical lower level floors - Solar Access & Ventilation



Figure 83: Typical lower level floors - Solar Access & Ventilation - West of Underwood Road

- |  |  |   |
|--|--|---|
| <span style="color: green;">■</span> Existing open space | <span style="color: lightgreen;">■</span> Proposed open space          | <span style="border: 1px dashed black; display: inline-block; width: 20px; height: 10px;"></span> Potential site amalgamation |
| <span style="color: blue;">--</span> Ventilation         | <span style="background-color: yellow;">■</span> > 2 hours of Sunlight | <span style="background-color: purple;">■</span> < 2 hours of Sunlight  |

0 25m 50m

Typical upper level floors - Indicative layout



Figure 84: Typical upper level floors - Indicative layout - West of Underwood Road

Residential Existing open space Proposed open space Potential site amalgamation

Typical upper level floors - Solar Access & Ventilation



Note: Existing building footprints may not reflect the extent of existing built form

Figure 85: Typical upper level floors - Solar Access & Ventilation - West of Underwood Road

Existing open space	Proposed open space	Potential site amalgamation
Ventilation	> 2 hours of Sunlight	< 2 hours of Sunlight

# Strathfield Triangle

## Proposed Planning Controls

#	Lot Size	HOB	No. Storeys	FSR	Non-Residential GFA	Residential GFA
1A	4,218m <sup>2</sup>	28-60m	8-18	3.6:1		15,184m <sup>2</sup>
1B	7,202m <sup>2</sup>	60-114m	18-34	3.6:1		25,927m <sup>2</sup>
1C	3,337m <sup>2</sup>	101m	30	3.6:1		12,013m <sup>2</sup>
2A	1,858m <sup>2</sup>	25m	8	2.2:1		4,087m <sup>2</sup>
2B	4,156m <sup>2</sup>	28m	8	2.2:1		9,143m <sup>2</sup>
3A	3,043m <sup>2</sup>	21m	8	1.9:1		5,781m <sup>2</sup>
3B	3,430m <sup>2</sup>	21m	6	1.9:1		6,517m <sup>2</sup>
4A	2,175m <sup>2</sup>	35m	10	2.8:1		6,090m <sup>2</sup>
4B	3,134m <sup>2</sup>	42m	12	2.8:1		8,775m <sup>2</sup>
4C	22,032m <sup>2</sup>	21m-60m	12-18	2.8:1		61,689m <sup>2</sup>
4D	10,323m <sup>2</sup>	21m	10-12	2.8:1		28,904m <sup>2</sup>
5	6,105m <sup>2</sup>	54m	16	3:1	1,831m <sup>2</sup>	16,483m <sup>2</sup>

## Assumptions

- Lot 1B, 4C to deliver new open space
- Lot 1B, 2B, 4A to accommodated for internal new streets
- Lot 3A, 4B, 4C, 4D to consider the existing heritage and church building

### Heritage principles

Refer to Homebush Stage-Led Rezoning Transport Orientated Development Precinct - Heritage Significance Assessment Report, GML Heritage for more information

- *Design future development on the eastern side of Swan Avenue with lower heights and FSR, to respond to the scale of the adjacent HCA on Mosely Street.*
- *East-west through-lot connections between Swan Avenue, Manson Road and Leicester Avenue should be located beside heritage items, to provide separation from new infill development.*

- *New open space should be located adjacent to heritage items, to provide separation from new infill development and contribute to retention of curtilage and setting.*
- *Encourage integration of isolated heritage items into the new amalgamating lots provided visual and spatial separation is ensured and the prominence of the items is emphasised.*
  - *Increased setbacks to new infill development structures should be used to maintain the prominence of isolated heritage items in this area.*
  - *Encourage the integration of these isolated heritage items as community spaces within an amalgamated lot.*
- *New infill development must include adequate space for additional canopy trees to complement street trees and maintain the amenity of Swan Avenue, Manson Road and Leicester Avenue.*



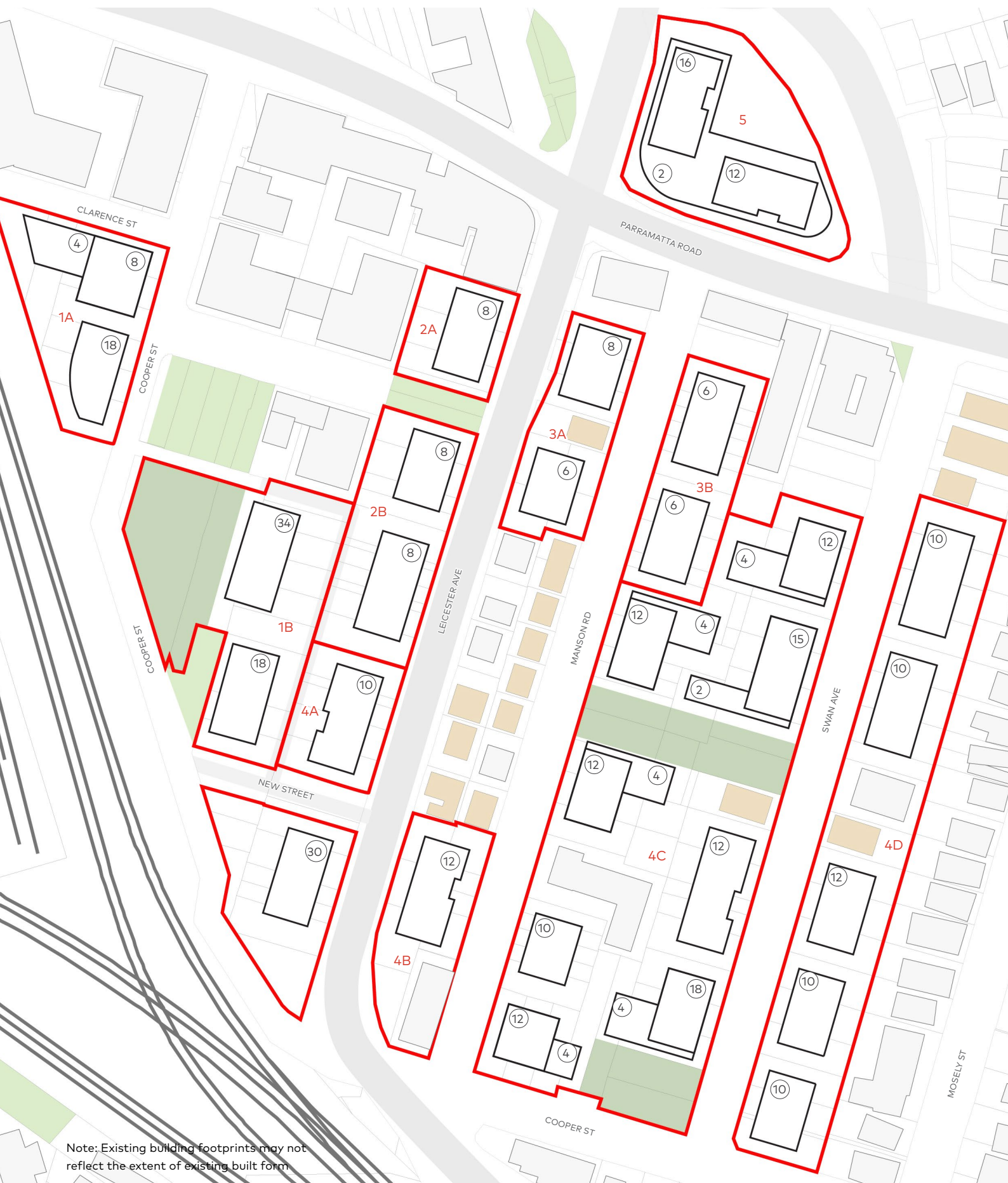


Figure 86: Strathfield Triangle

Existing open space
  Proposed open space
  Lot
  Heritage
 # Lot number
 Ⓢ Number of storeys

## Typical lower level floors - Indicative layout

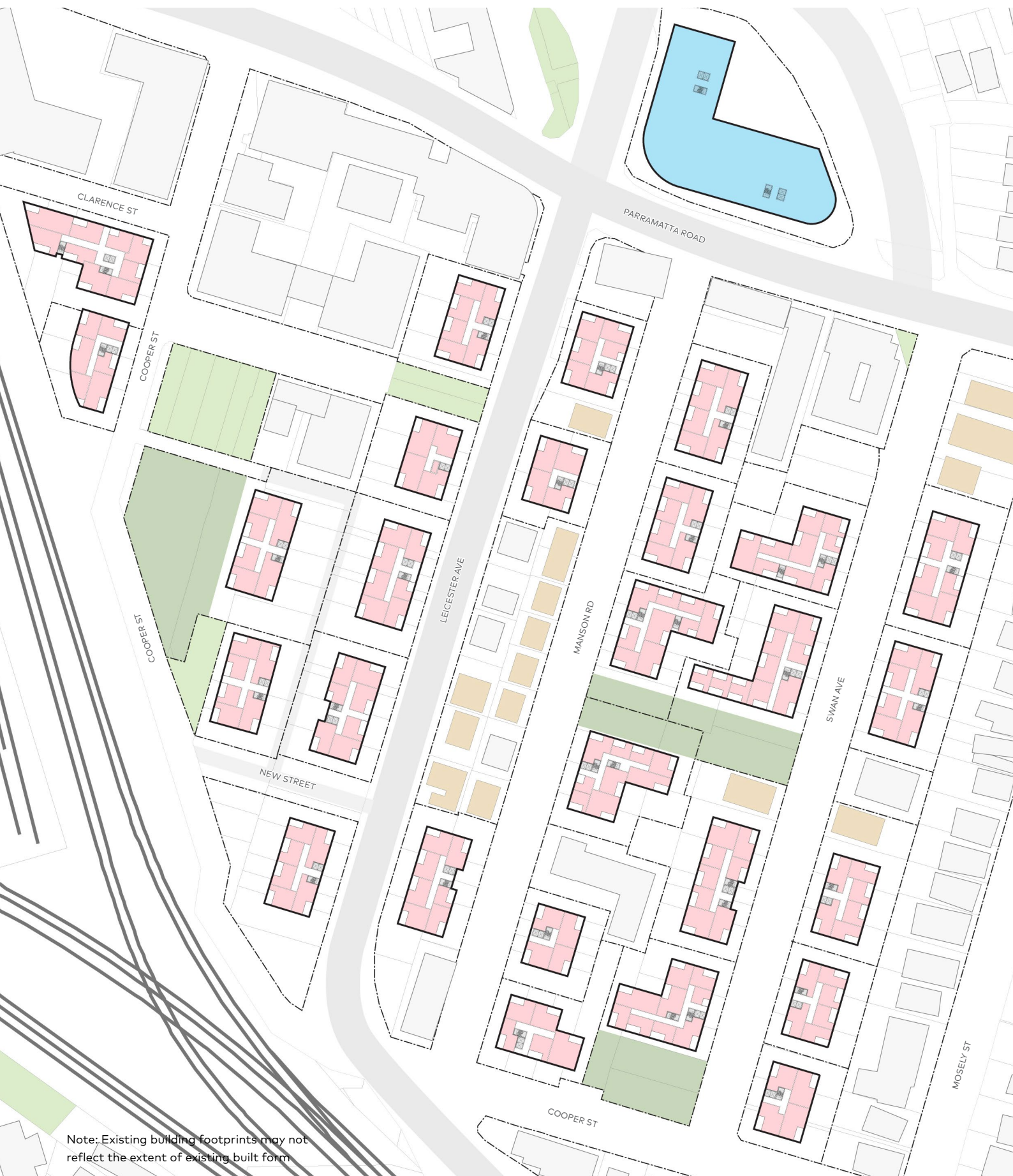


Figure 87: Typical lower level floors - Indicative layout - Strathfield Triangle

■ Commercial/Retail
 ■ Residential
 ■ Existing open space
 ■ Proposed open space
  Potential site amalgamation

## Typical lower level floors - Solar Access & Ventilation

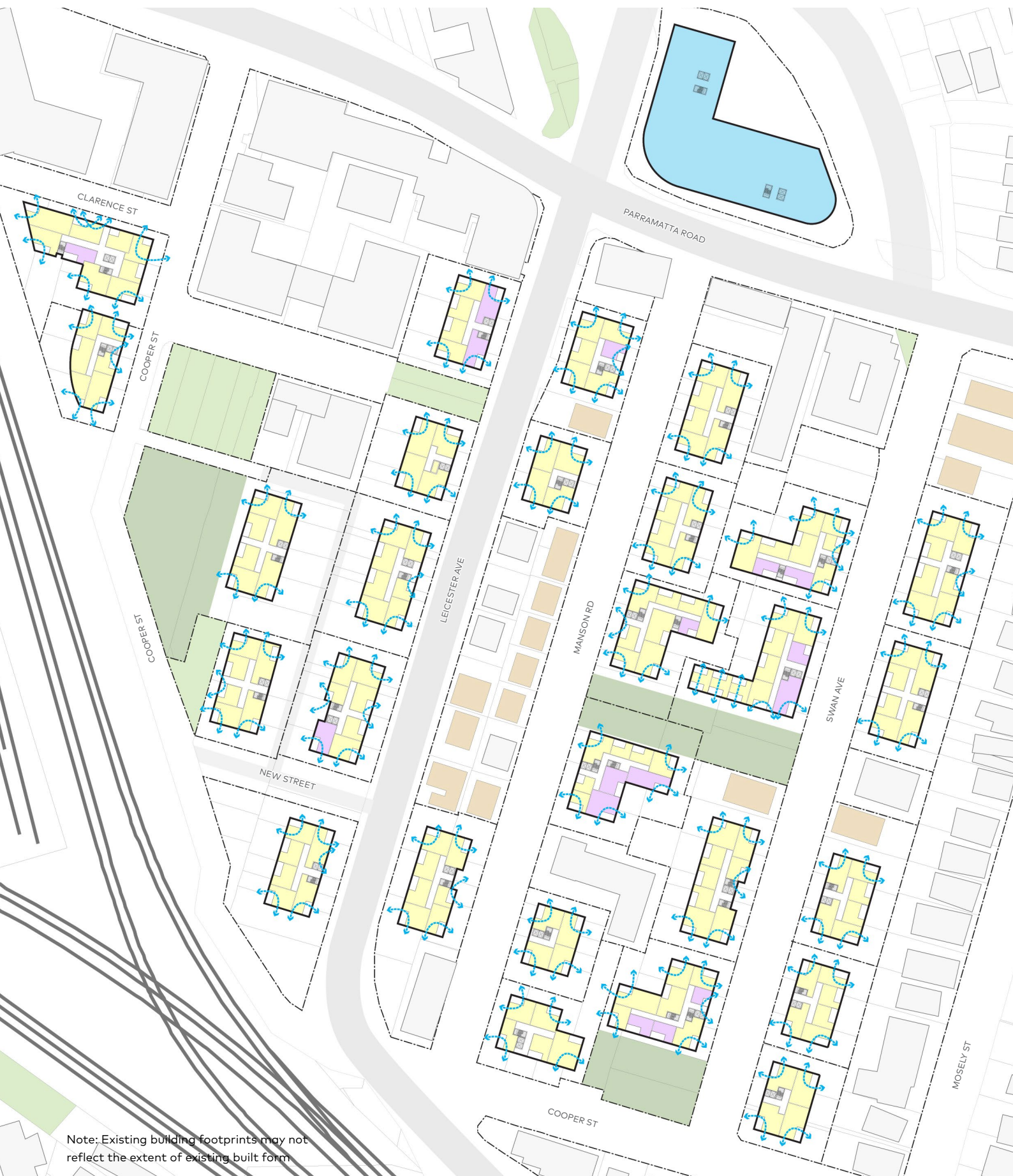


Figure 88: Typical lower level floors - Solar Access & Ventilation - Strathfield Triangle

- Existing open space
- Proposed open space
- Potential site amalgamation
- Ventilation
- > 2 hours of Sunlight
- < 2 hours of Sunlight

## Typical upper level floors - Indicative layout

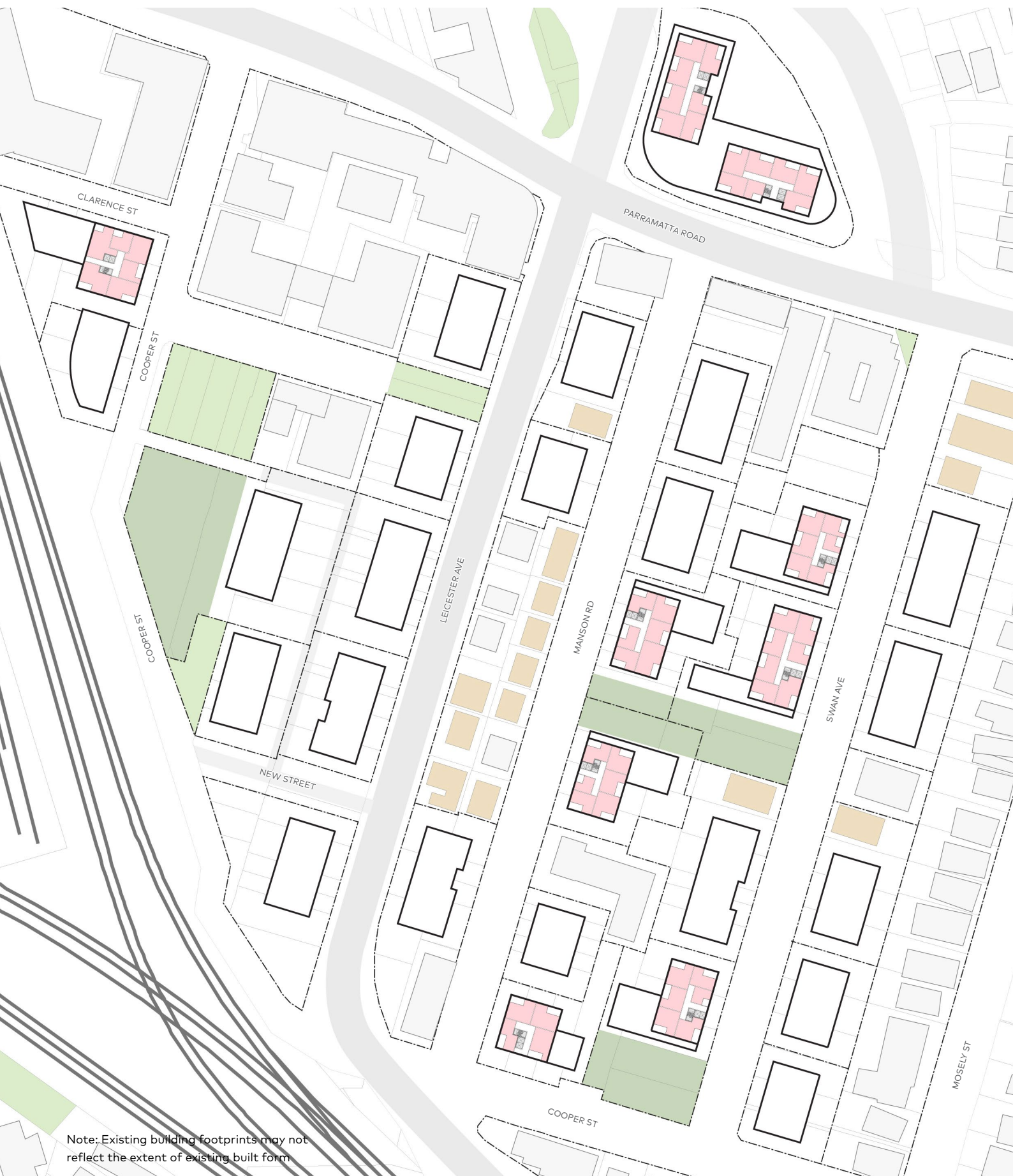


Figure 89: Typical upper level floors - Indicative layout - Strathfield Triangle

■ Residential
 ■ Existing open space
 ■ Proposed open space
  Potential site amalgamation

## Typical upper level floors - Solar Access & Ventilation

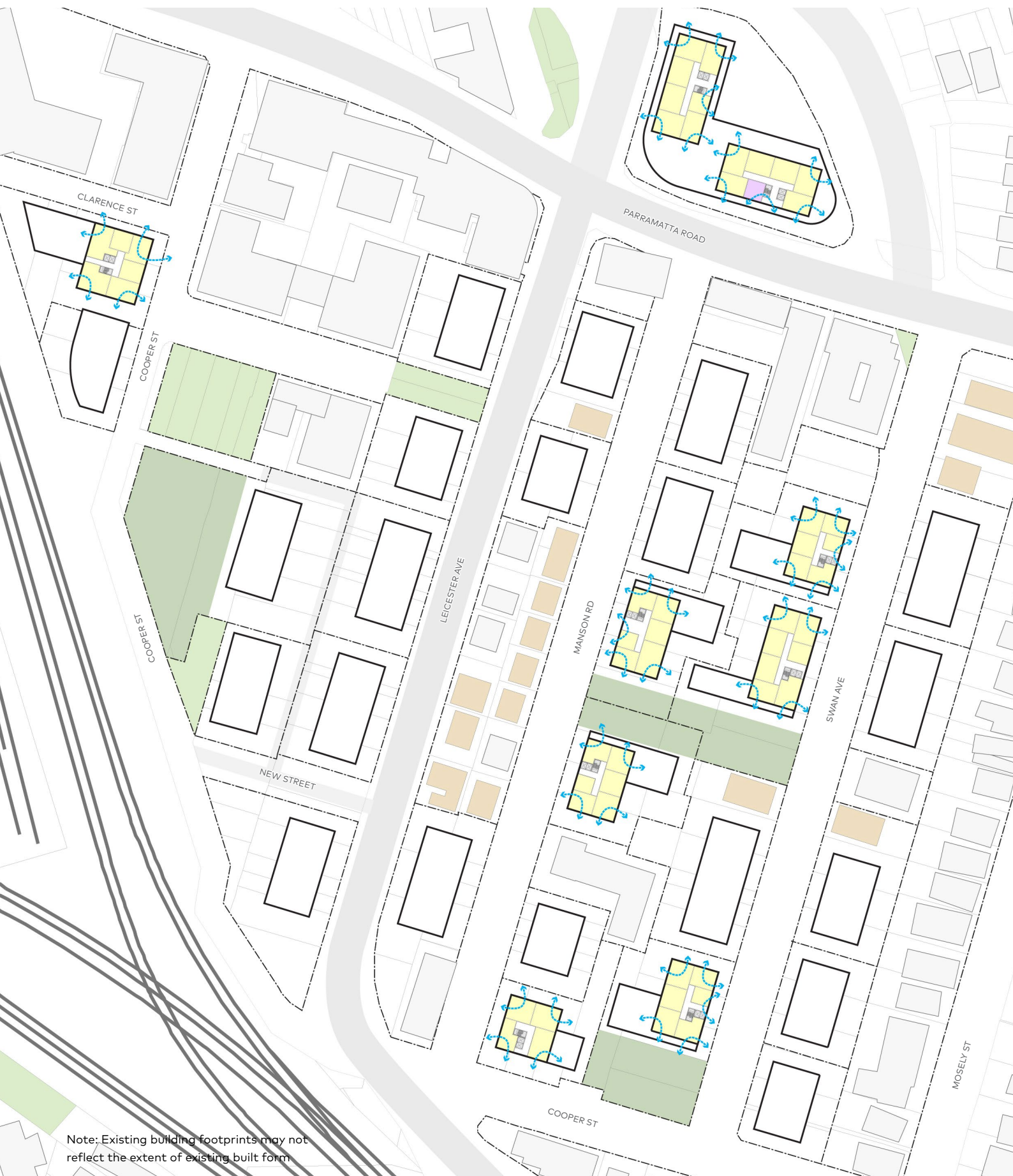


Figure 90: Typical upper level floors - Solar Access & Ventilation - Strathfield Triangle

- Existing open space
- Proposed open space
- Potential site amalgamation
- Ventilation
- > 2 hours of Sunlight
- < 2 hours of Sunlight

## Homebush (South of M4)

### Proposed Planning Controls

#	Lot Size	HOB	No. Storeys	FSR	Non-Residential GFA	Residential GFA
1	23,587m <sup>2</sup>	86-103m	18-30	5:1	5,896m <sup>2</sup>	112,038m <sup>2</sup>
2	6,257m <sup>2</sup>	86m	24	5:1	1,564m <sup>2</sup>	29,720m <sup>2</sup>
3	3,041m <sup>2</sup>	86m	24	5:1	760m <sup>2</sup>	14,444m <sup>2</sup>
4	4,861m <sup>2</sup>	103m	30	5:1	1,215m <sup>2</sup>	23,089m <sup>2</sup>
5	9,117m <sup>2</sup>	86-103m	24-30	5:1	2,279m <sup>2</sup>	43,305m <sup>2</sup>
6	11,607m <sup>2</sup>	86m	24	5:1	2,901m <sup>2</sup>	55,133m <sup>2</sup>
7	9,081m <sup>2</sup>	86m	24	5:1	2,270m <sup>2</sup>	43,134m <sup>2</sup>

### Assumptions

- Lot 1, 4 to deliver new open space
- Lot 1, 3, 4, 5 to consider existing and potential heritage items

#### Heritage principles

Refer to Homebush Stage-Led Rezoning Transport Orientated Development Precinct - Heritage Significance Assessment Report, GML Heritage for more information

- *Create a special character precinct on Knight Street and Parramatta Road, incorporating existing and potential heritage items, to protect the heritage character of this area.*
- *Encourage a mix of uses that supports residential, retail, cultural and commercial activities.*
- *Prioritise non-residential uses around the commercial heritage buildings at the intersection of Knight Street and Parramatta Road, to ensure the historical uses of the heritage items are retained.*

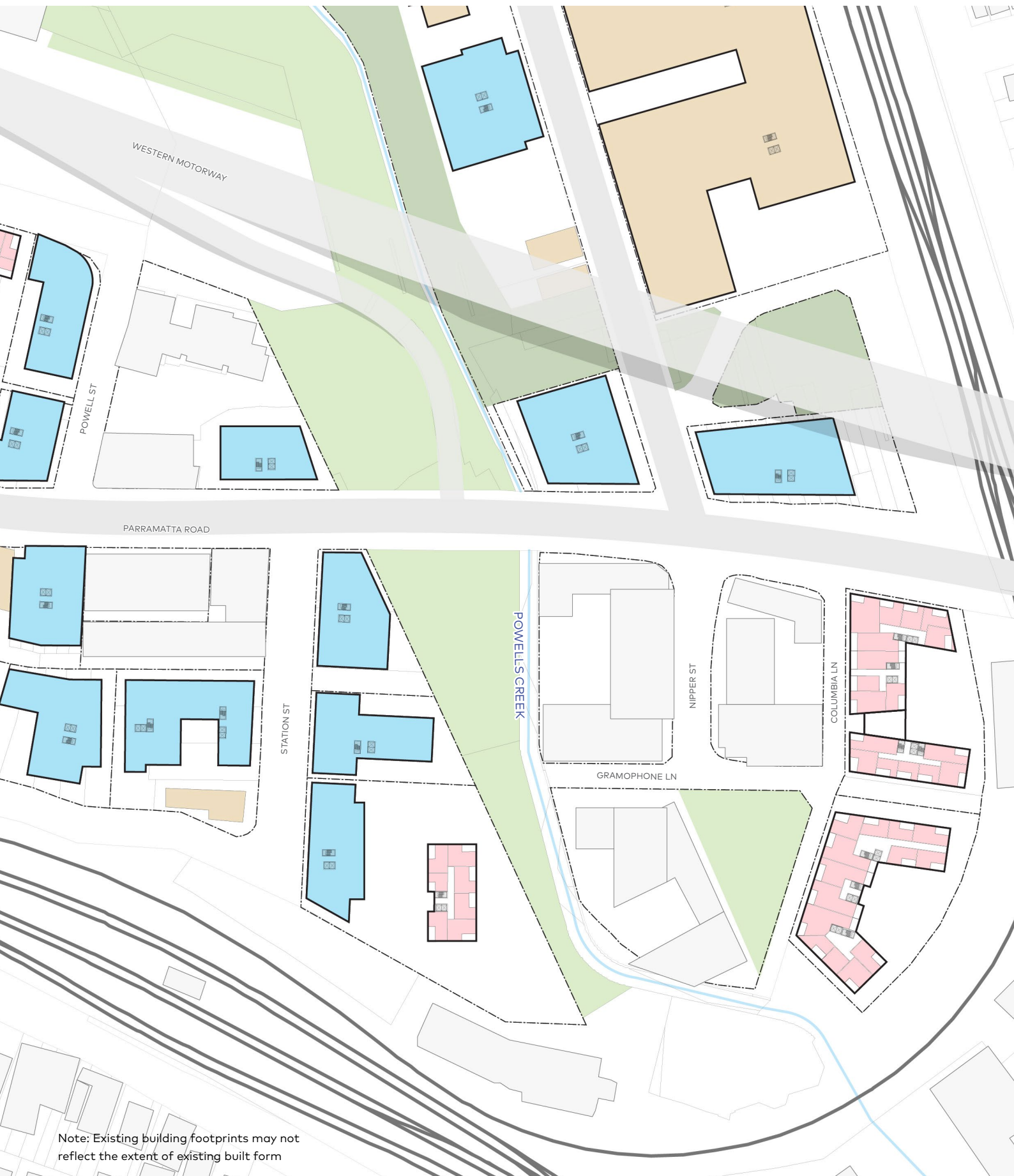




Figure 91: Homebush (South of M4)

Existing open space
  Proposed open space
  Lot
  Heritage
 # Lot number
 Ⓢ Number of storeys





Note: Existing building footprints may not reflect the extent of existing built form

Figure 92: Typical lower level floors - Indicative layout - Homebush (South of M4)

- Commercial/Retail
- Residential
- Existing open space
- Proposed open space
- Potential site amalgamation
- Heritage



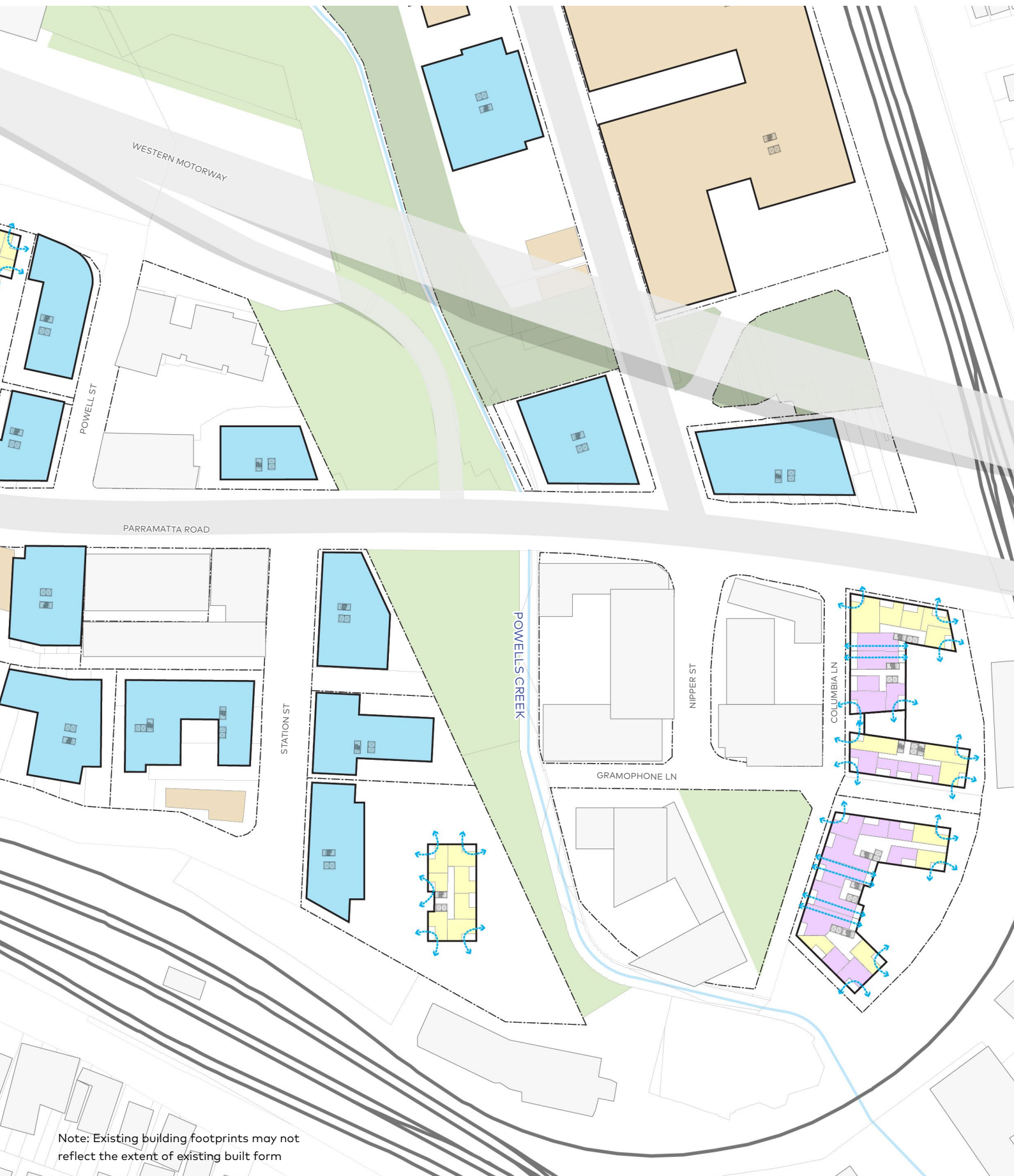
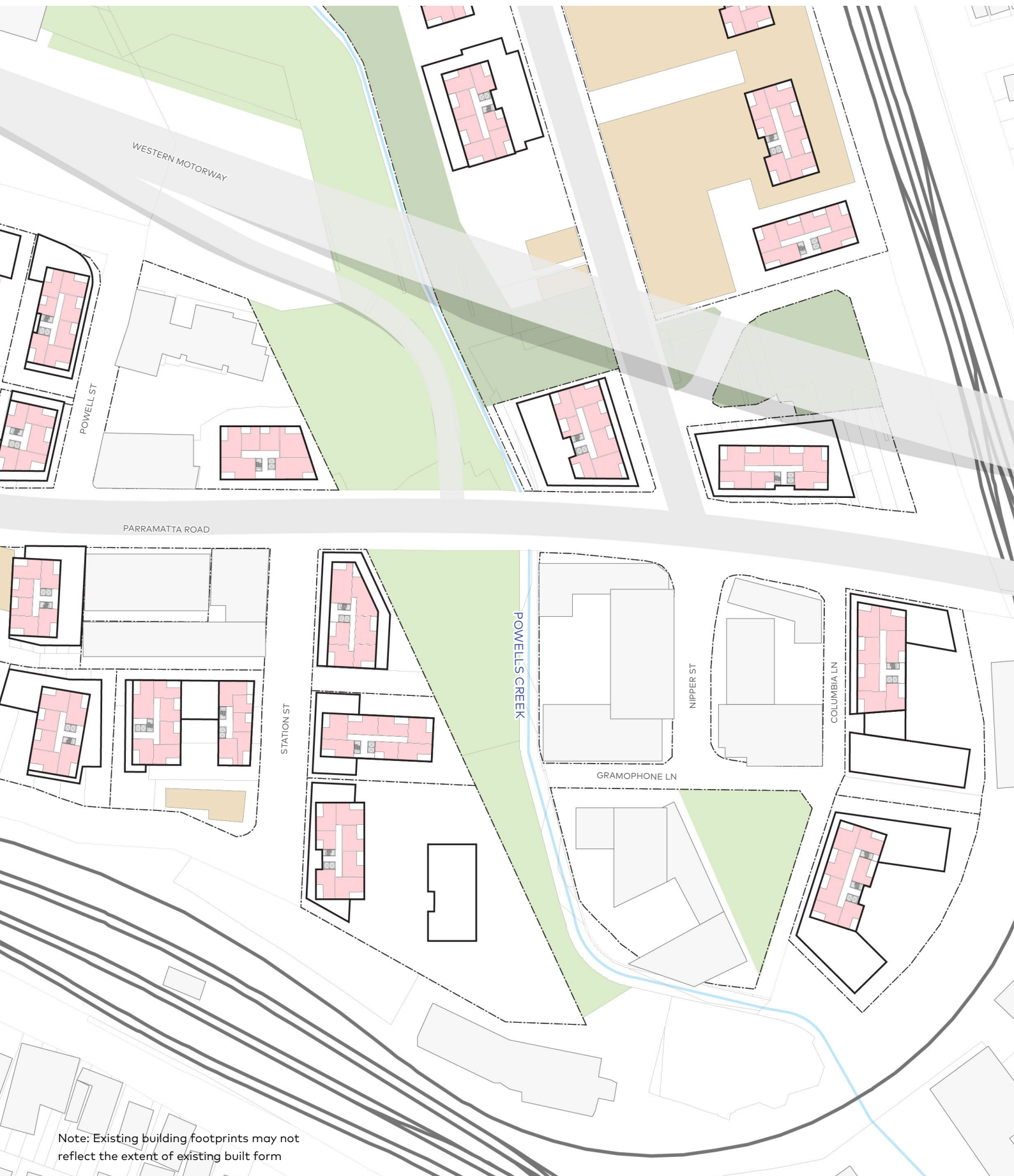


Figure 93: Typical lower level floors - Solar Access & Ventilation - Homebush (South of M4)

- |  |   |   |   |
|--|---|---|---|
| <span style="color: green;">■</span> Existing open space | <span style="color: green;">■</span> Proposed open space    | <span style="border: 1px dashed black; display: inline-block; width: 20px; height: 10px;"></span> Potential site amalgamation | <span style="color: brown;">■</span> Heritage |
| <span style="color: blue;">→</span> Ventilation          | <span style="color: yellow;">■</span> > 2 hours of Sunlight | <span style="color: purple;">■</span> < 2 hours of Sunlight   |   |





Note: Existing building footprints may not reflect the extent of existing built form

Figure 94: Typical upper level floors - Indicative layout - Homebush (South of M4)

- Residential
  Existing open space
  Proposed open space
  Potential site amalgamation
- Heritage



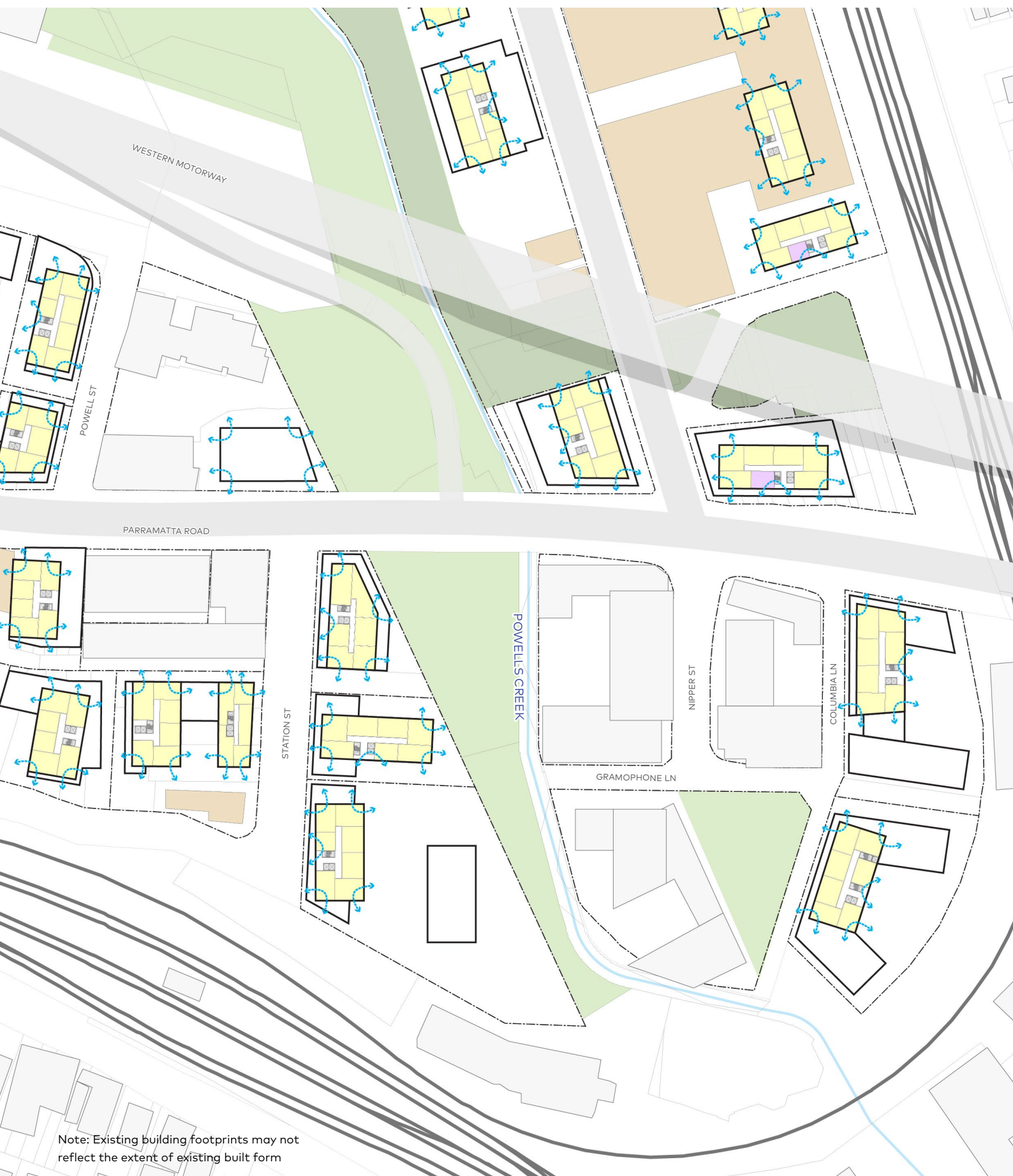


Figure 95: Typical upper level floors - Solar Access & Ventilation - Homebush (South of M4)

- |   |   |   |  |
|---|---|---|--|
| <span style="display: inline-block; width: 15px; height: 10px; background-color: #90EE90; border: 1px solid black;"></span> Existing open space | <span style="display: inline-block; width: 15px; height: 10px; background-color: #90EE90; border: 1px solid black;"></span> Proposed open space | <span style="display: inline-block; width: 15px; height: 10px; border: 1px dashed black;"></span> Potential site amalgamation | <span style="display: inline-block; width: 15px; height: 10px; background-color: #D2B48C; border: 1px solid black;"></span> Heritage |
| <span style="color: blue;">—</span> Ventilation   | <span style="display: inline-block; width: 15px; height: 10px; background-color: yellow;"></span> > 2 hours of Sunlight                         | <span style="display: inline-block; width: 15px; height: 10px; background-color: #FFB6C1;"></span> < 2 hours of Sunlight      |  |

