

Coffs Harbour Jetty Foreshore Precinct Design Guidelines

DRAFT

Submitted to Property and Development NSW



'Gura Bulga'

Liz Belanjee Cameron

'Gura Bulga' – translates to Warm Green Country. Representing New South Wales.



'Dagura Buumarri'

Liz Belanjee Cameron

'Dagura Buumarri' – translates to Cold Brown Country. Representing Victoria.



'Gadalung Djarri'

Liz Belanjee Cameron

'Gadalung Djarri' – translates to Hot Red Country. Representing Queensland.

Ethos Urban acknowledges the Traditional Custodians of Country throughout Australia and recognises their continuing connection to land, waters and culture.

We pay our respects to their Elders past, present and emerging.

In supporting the Uluru Statement from the Heart, we walk with Aboriginal and Torres Strait Islander people in a movement of the Australian people for a better future.

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Glossary of Terms

Table 1 **Abbreviations**

Abbreviation	Definition
ACHAR	Aboriginal Cultural Heritage Assessment Report
AHIP	Aboriginal heritage impact permit
AS	Australian Standard
ATR	Archaeological Technical Report
B&C Act	Biodiversity and Conservation Act 2016
CHDCP	Coffs Harbour Development Control Plan 2015
CHLEP	Coffs Harbour Local Environmental Plan 2013
CLM Act	Crown Land Management Act 2016
Council	City of Coffs Harbour Council
DA	Development application
DCP	Development control plan
DPHI	NSW Department of Planning, Housing and Infrastructure
EP&A Act	Environmental Planning and Assessment Act 1979
EP&A Regulation	Environmental Planning and Assessment Regulation 2021
EPI	Environmental planning instrument
ESD	Ecologically sustainable development
FSR	Floor space ratio
GFA	Gross floor area
GPT	Gross pollutant trap
LEP	Local environmental plan
LGA	Coffs Harbour local government area
Minister	The Minister for Planning and Public Spaces
NABERS	National Australian Built Environment Rating System
PDNSW	Property and Development NSW
Planning Secretary	The Secretary of the Department of Planning, Housing and Infrastructure
PoM	Plan of management

Abbreviation	Definition
SDRP	State Design Review Panel
SEPP	State environmental planning policy
SSD	State significant development
SAPP	State Assessed Planning Proposal
SSDA	State significant development application
State	The state of NSW
TfNSW	Transport for NSW
WMP	Waste management plan

Table 2 **Key terms**

Key term	Definition
Active transport	Transport that is partly or wholly or partly human powered, such as walking or cycling.
Amenity	The extent to which a place, experience or service is pleasant, attractive or comfortable. Improved features, facilities or services may contribute to increased amenity.
Character	The combination of the attributes, characteristics and qualities of a place
Community	Particular groups of people in particular places who are both affected by, use and experience the outcomes and benefits of the Precinct.
Control	A numerical standard or defined parameter that is applied in a prescriptive manner.
Determination	The approval made in accordance with the <i>Environmental Planning and Assessment Act 1979</i> . In relation to the Jetty Precinct, a determination will be made by the Minister for Planning and Public Spaces.
Green grid	Links between parks, open spaces, bushland and walking and cycling paths.
Illustrative Masterplan	A non-statutory document that shows one way in which the Precinct may develop in the future in accordance with the proposed amendments to the planning framework.
Merit based assessment	An assessment of a matter that allows for reasonable flexibility to consider a range of possible solutions.
Mixed use	A building or area containing more than one type of use – has multiple functions.
NABERS	A national rating system that measures the environmental performance of Australian buildings and tenancies.
Objective	A statement of a desired outcome, generally expressed in a qualitative manner that enables merit-based assessment.
Placemaking	Reimagining and reinventing public spaces so they have long-term social, cultural, environmental and economic value for communities.
Planning instrument	Means any of the following: <ul style="list-style-type: none"> • strategic plan (comprising regional strategic plans and district strategic plans) and local strategic planning statements • environmental planning instrument (comprising State environmental planning policies and local environmental plans) • development control plan

Key term	Definition
Precinct	A designated geographical area with distinct boundaries determined by land formations, land use and other unique characteristics.
Principal development standards	Matters addressed in Part 4 of the Standard Instrument LEP
Property and Development NSW	Property and Development NSW is the central property agency for the NSW Government
Provisions	A broad term covering objectives and controls
Public spaces	Means areas that are publicly accessible where people can interact with each other and make social connections
Safer-By-Design	A series of principles that provide guidance on ensuring development reduces crime risk prepared by the NSW Police Force
Sub-precinct	A smaller defined area within the greater Jetty Foreshore Precinct with its own unique local character, opportunities and constraints
Vibrant streets / places	Places that have a high demand for movement as well as place with a need to balance different demands within available road space

1.0 Introduction

1.1 Name of the Design Guidelines

This document is the 'Coffs Harbour Jetty Foreshore Precinct Design Guidelines'.

1.2 Citation

This document may be referred to as the Design Guidelines.

1.3 Commencement

These Design Guidelines commenced on [Details for when the Design Guidelines come into effect].

1.4 Land to which these Design Guidelines applies

These Design Guidelines apply to the land shown in **Figure 1: Land application map**.



Figure 1 Land application map

1.5 Purpose and status of these Design Guidelines

The purpose of these Design Guidelines is to support the Coffs Harbour Local Environmental Plan 2013 (the CHLEP 2013) by providing more detailed design guidance. Where these Design Guidelines apply, any Development Control Plan ceases to apply. The provisions of these Design Guidelines are not statutory requirements.

1.6 Relationship to planning instruments and documents

These Design Guidelines form part of suite of planning provisions that apply to Coffs Harbour Jetty Foreshore Precinct (hereafter 'Precinct'). This includes Acts, Regulations and State Environmental Planning Policies. Relevant Acts and Regulations include:

- *Environmental Planning and Assessment Act 1979*
- *Heritage Act 1977*
- *National Parks and Wildlife Act 1974*
- *Crown Lands Management Act 2016*
- *Coastal Management Act 2016*
- *Rural Fires Act 1997*
- *Marine Estates Management Act 2014*
- *Environmental Planning and Assessment Regulation 2021*

The Design Guidelines are sub-ordinate to the CHLEP 2013 and state environmental planning policies. Where a provision of these Design Guidelines is inconsistent with a provision in the CHLEP 2013 or a state environmental planning policy, the CHLEP 2013 or state environmental planning policy prevails to the extent of the inconsistency.

Other documents and guidelines given effect by legislation that may be of particular relevance are:

- *Bushfire – Planning for Bushfire Protection 2019*
- *Residential Amenity – Apartment Design Guide*
- *Coastal Hazard – Coffs Harbour Coastal Zone Management Plan 2019*
- *Noise and Vibration – Development Near Rail Corridors and Busy Roads - Interim Guideline 2008*

To avoid duplication, controls relating to these matters have not been reproduced in these Design Guidelines.

1.6.1 Other referenced documents

In addition to those listed above, mention is made of the following guiding documents that should be read in conjunction with these Design Guidelines where referenced:

- City of Coffs Harbour Water Sensitive Urban Design Guideline (2018)
- Coffs Harbour City Council Public Realm Urban Design Guidelines (2020)
- Coffs Harbour Development Control Plan 2015
- Coffs Harbour Jetty Foreshore Connecting with Country Framework (draft document)
- Coffs Harbour Jetty Foreshore Precinct Public Domain and Landscape Manual (future document)
- National Light Pollution Guidelines for Wildlife (2023)
- NSW Coastal Design Guidelines (2023)
- Property & Development NSW Sustainability Framework (future document)
- Urban Design Framework prepared by SJB (2025)

1.7 How to use these Design Guidelines

1.7.1 Development assessment framework

Recognising the significance, complexity and long-term timeframe of redevelopment of the Coffs Harbour Jetty Foreshore Precinct, these Design Guidelines provide a performance-based planning framework for development assessment. Development applications (DAs), including state significant development applications (SSDAs), will be assessed on their individual merit having regard to the CHLEP 2013, these Design Guidelines, other matters listed in Section 4.15 of the *Environmental Planning and Assessment Act 1979* and any other adopted and relevant policies and documents that relate to development within the Precinct.

The Design Guidelines comprise a hierarchy of objectives and requirements. Each topic area is structured to provide the user with:

- **Objectives** that describe the desired outcome(s); and
- **Requirements** demonstrate how the objectives may be achieved, by addressing development, design and infrastructure necessities.

In some sections, where appropriate, a preamble has been provided. This preamble provides context only and is not assessable.

1.7.2 Preparing development applications

DAs should clearly demonstrate adherence to the objectives and requirements in these Design Guidelines. The guidance provides design responses for how the objectives could be achieved. The guidance does not represent the only way the objectives can be achieved. Where alternative solutions to the guidance are proposed it must be demonstrated how they achieve the objectives and align with the Shared Vision.

The objectives and requirements set out in these guidelines are twofold, as follows:

1. **General provisions** that apply to all development within the Precinct (refer to Chapter 3).
2. **Sub-precinct specific provisions** that describe the place-specific objectives and development requirements for each sub-precinct, aligned with the Precinct Shared Vision (refer to Chapter 4).

General provisions apply to all development within the Precinct, whereas sub-precinct controls are limited to the area shown in each respective section. In the event of any inconsistency between the general provisions and sub-precinct provisions of these guidelines, the sub-precinct requirements prevail. In the event that a development is undertaken outside of any identified sub-precinct, the general provisions are to be read in isolation.

1.7.3 Structure of these Design Guidelines

The Design Guidelines are structured into four (4) chapters as outlined in **Table 3**.

Table 3 Design Guidelines Chapters

Design Guidelines Chapter	Description
Chapter 1 Introduction	Identifies the purpose of the Design Guidelines, describes how to use the Design Guidelines and the identifies the land to which it applies.
Chapter 2 Shared Vision, Place Principles and Objectives	Defines the Shared Vision and Place Principles for the Precinct and provides a structure plan and objectives that apply to all development, landscaping, and infrastructure delivery within the Precinct.
Chapter 3 General Provisions	Outlines built form, transport and movement, public domain, landscaping and environmental management and sustainability objectives and requirements.
Chapter 4 Sub-precincts	Defines the sub-precinct outcomes, objectives and requirements.

2.0 Shared Vision, Place Principles and Objectives

2.1 Shared Vision

Coffs' family playground, a Precinct of parks and places, that connects community with Country. The community is and always has been at the heart of creating a thriving regional economy and destination for Coffs Harbour. During April & May 2021, PDNSW consulted with the community to develop a vision and place principles to guide the development of a draft Masterplan. As a result of feedback received during this Phase 1 consultation, the project's guiding vision and place principles were further developed to respond to community views, aspirations and sensitivities.

Development within the Precinct is required to demonstrate how it is consistent with the Shared Vision, Place Principles and Precinct objectives contained in these design guidelines. The Shared Vision established for the Precinct is shown in **Figure 2**.



Figure 2 Shared Vision for the Coffs Harbour Jetty Foreshore Precinct

2.2 Place Principles

The Place Principles were agreed with the community to guide the development of a Masterplan that responds to community needs for the Precinct. As with the Shared Vision, all development must demonstrate consistency with the principles shown in **Figure 3**.



Figure 3 Community-led Place Principles

2.3 Place Objectives

- Retaining and upgrading of the foreshore parkland area between Jordan Esplanade and the beach.
- Opening up of the fenced-off railway lands to provide opportunity for Precinct activation.
- Realigning and managing of carparking across the Precinct.
- Creating a more coherent and accessible connection to the Jetty Foreshore that connects the city with the harbour.
- Limiting any development to similar planning controls applied to areas immediately west of the rail line along Orlando Street and Harbour Drive.

2.4 Structure plan

The Structure Plan illustrated in **Figure 4**, defines the overall conceptual approach to the renewal of the Precinct. It identifies the primary approaches to movement, built form and open space. Development within the Precinct should be generally in accordance with the structure plan shown in **Figure 4**.

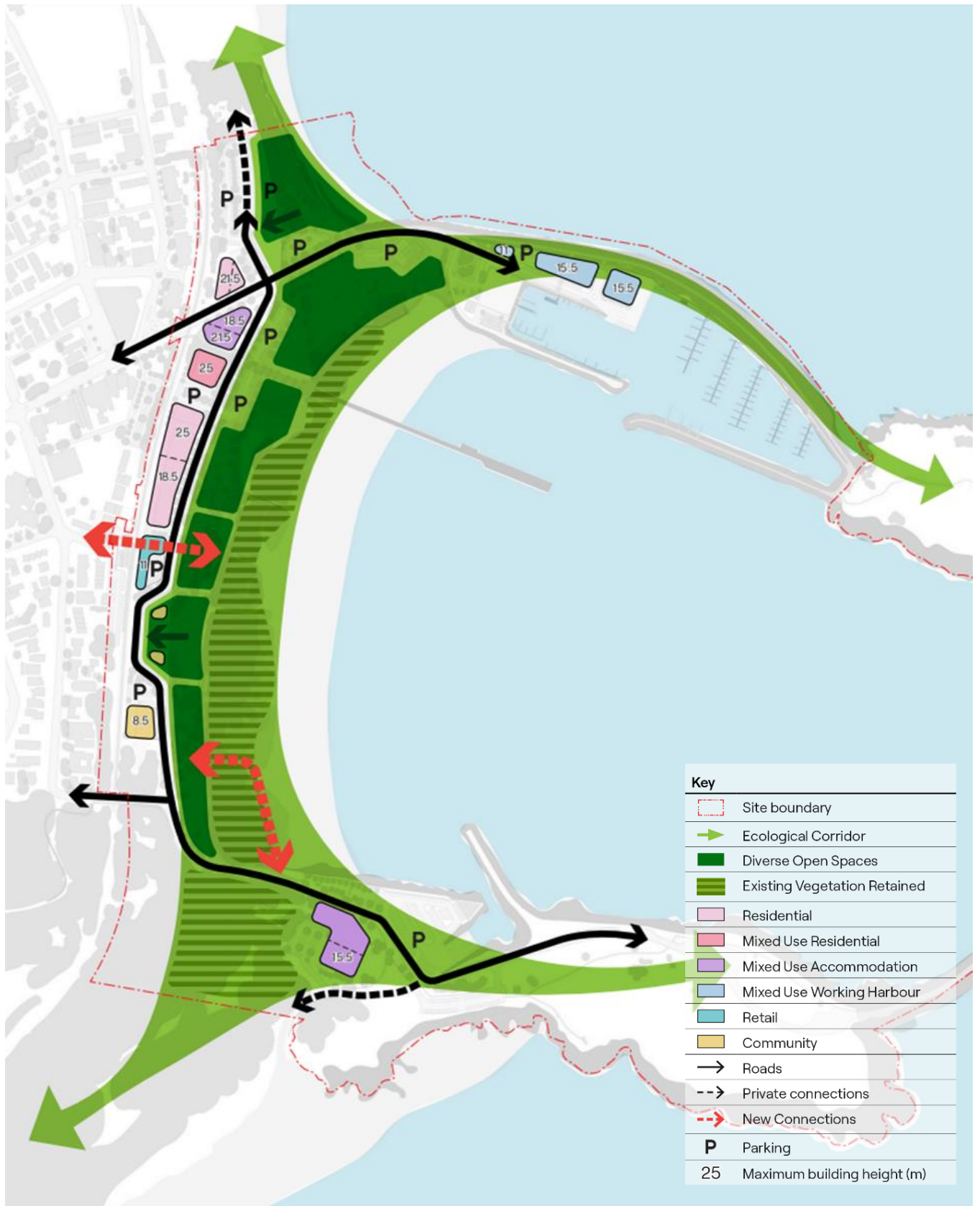


Figure 4 Precinct Structure Plan

2.5 Sub Precinct Plan

The Precinct has been split into several distinct sub-precincts that embody variable characteristics. These have been built upon the existing character areas in places, expanding others, and introducing new ones that will service the community in unique ways. Each area will serve a different function and experience while being unified under a consistent Precinct identity. **Figure 5** delineates the division of six sub-precincts within the Precinct.



Figure 5 Sub-precincts plan

The sub-precincts are identified as:

1. Foreshore Parklands – with improved amenities, proposed new board walk and nature-based playground.
2. The Marina – An active marina revitalised to accommodate local marine based businesses that reflect their regional importance.
3. North Park – Functional open space with recreational courts and formalised parking.
4. Jetty Hub – A hub of residential and tourist accommodation supporting activation, tourism and regional attraction located adjacent to the current Jetty Walkway, with massing capped at 6 storeys stepping down in scale when closer to public areas.
5. Activity Hub and Village Green – An active village green that delivers increased public open space connected to the existing foreshore parklands and may include family-friendly food and beverage, community uses and club houses or facilities to support events. A local business activity zone connected to the rail station.
6. Corambirra Point – A new regional tourist destination on the site of the former Deep Sea Fishing Club site including publicly accessible cafes and restaurants, a function space, activity centre and tourist accommodation.

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3.0 General Provisions

3.1 Connecting with Country

Recognising the presence of sites of significance to the Gumbaynggirr people is crucial to promote reconciliation and foster a Continuing Connection to Country. By incorporating Aboriginal heritage into how we plan for cities and communities, we can create a more inclusive and culturally respectful environment that celebrates the rich Aboriginal history of the area.

3.1.1 Objectives

1. To ensure the revitalisation of the Precinct:
 - a. Honours and responds to Country;
 - b. Supports the health and wellbeing of Country;
 - c. Creates a welcoming and safe place for the local Gumbaynggirr community that strengthens Gumbaynggirr identity;
 - d. Values and respects Gumbaynggirr knowledge and stories;
 - e. Connects areas of cultural significance to the Gumbaynggirr community;
 - f. Provides ongoing benefits for Gumbaynggirr people through renewal of the Precinct ; and
 - g. Acknowledges the Gumbaynggirr Community's right to financially benefit from Indigenous Cultural Intellectual Property, and ensures knowledge and cultural narratives are valued and respected.

3.1.2 Requirements

1. The Connecting with Country Framework prepared for the Coffs Harbour Jetty Foreshore Precinct should be used as a reference and guide for all projects to be undertaken in the Precinct, and to shape the renewal of the Precinct through the processes of design, delivery and ongoing management.
2. Development applications are to include a Connecting with Country Strategy that demonstrates how the Precinct's Connecting with Country Framework has informed the design.

3.2 Design excellence

Design excellence provisions outlined under Clause 7.12 of the CHLEP 2013 seek to ensure that development exhibits design excellence that contributes to the natural, cultural, visual and built character values of Coffs Harbour. The below objectives and requirements support this Clause and seek to embed design excellence across the Precinct.

3.2.1 Objectives

1. To embed design excellence, including Designing with Country across all aspects and stages of the development of the Precinct.
2. To ensure development individually and collectively contributes to the architectural, public open space and overall urban design quality of the Precinct.
3. To achieve a high standard of design excellence that celebrates the Precinct's foreshore location and parkside setting.
4. To achieve a high standard of architectural, urban and landscape design excellence that enhances local character.
5. To encourage a diversity of architectural and landscape design character throughout the Precinct to provide for fine grain and place responsive development which enriches and enlivens the public domain.

3.2.2 Requirements

1. Development applications are accompanied by a design report that demonstrates that any relevant design excellence processes have been undertaken. It should describe:
 - a. the design review processes undertaken; and
 - b. how the design has responded to design advice.

3.3 Built form

3.3.1 Building design and materiality

3.3.1.1 Objectives

1. To ensure that the built form reflects the uniqueness of the location and provides site responsive outcomes through drawing inspiration from the water and surrounding landscape.
2. To optimise public views including views to the mountains, and views from residential areas.
3. Development responds to the natural topography and balances the amount of cut and fill required.

3.3.1.2 Requirements

1. Buildings are to respond to its sub-tropical location within Coffs Harbour and deliver a collective and coherent response to site and place, taking on a coastal form of architecture with warm colours and materials.
2. The layout and extent of built form should generally be located in accordance with **Figure 6**.
3. All buildings should incorporate active roof spaces, deep balconies, and façade articulation to break down the bulk of the building. Where appropriate, green roofs should be investigated.
4. Significant public views should be considered in the siting of development, including those regional views noted in **Figure 6**.
5. Development located nearby to the train station should give regard to the desire lines of passengers and acknowledge the station in the siting and orientation of built form.
6. Development should incorporate Safer-By-Design and accessibility principles into building design.

Façade articulation zone

7. A development may have an articulation zone that extends up to 1.5m forward of the minimum required setback.
8. The following building elements can be located in the articulation zone:
 - a. an entry feature, portico, or other architectural expression element.
 - b. a balcony, deck, patio or verandah.
 - c. a window box treatment.
 - d. a bay window or similar feature.
 - e. an awning or other feature over a window.
 - f. a sun shading feature.
 - g. an access ramp.
9. The maximum total area of all building elements in the articulation zone, other than a building element specified in subclause 8 (e), (f) or (g), must not comprise more than 25% of the area of the articulation zone.

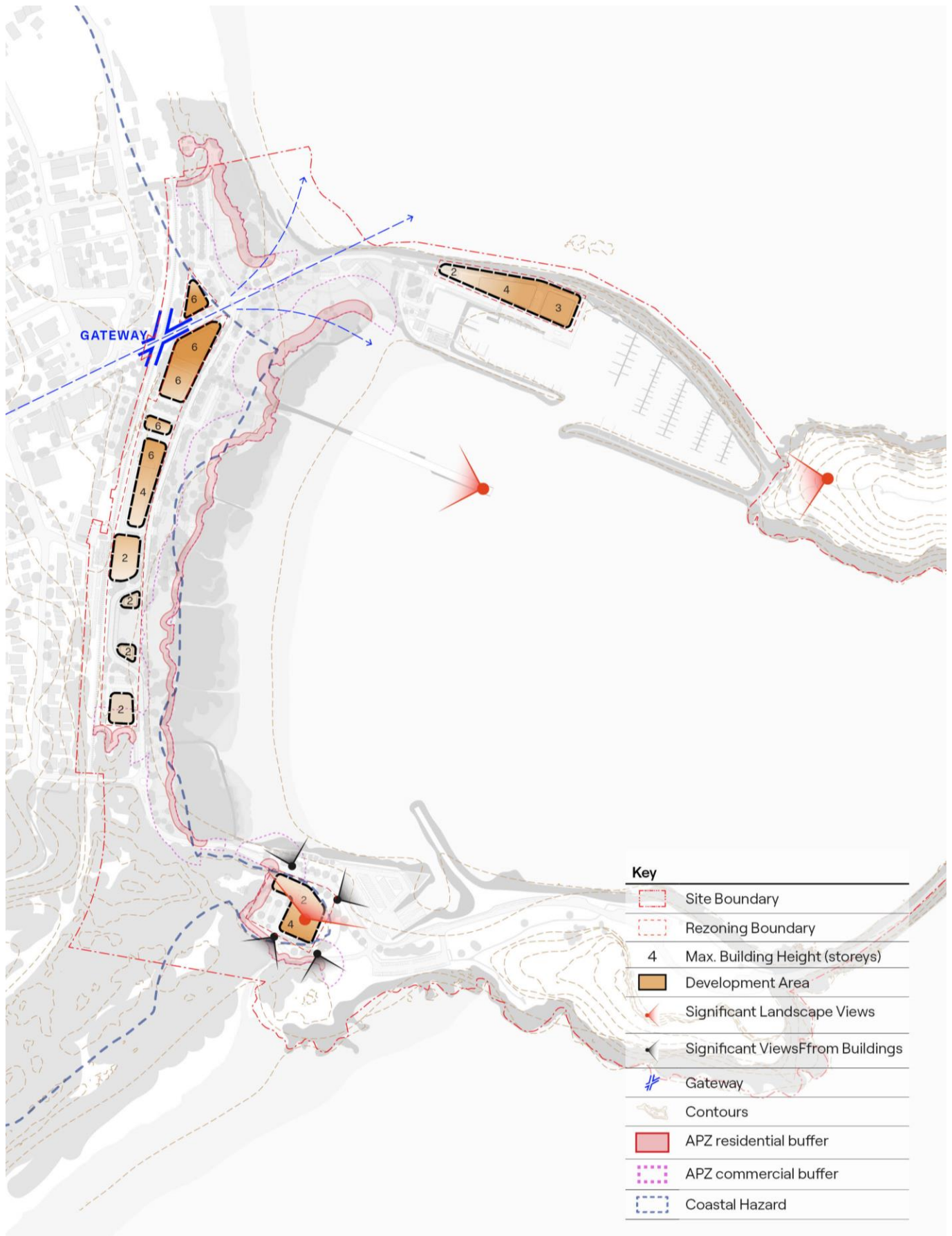


Figure 6 Built form structure plan

3.3.2 Inclusion of non-residential uses

3.3.2.1 Objectives

1. To deliver diversity across the Precinct with complementary active uses, including food and beverage establishments, outdoor dining, retail, cultural, entertainment and business premises that will enhance and activate public spaces.
2. To provide active uses in key areas of visibility, activity and higher pedestrian use.
3. To provide a variety of building typologies and tenancy opportunities.
4. To encourage clustering of non-residential uses to generate activity.

3.3.2.2 Requirements

1. Consider the inclusion of retail tenancies at ground level along key pedestrian movement corridors where identified in these Design Guidelines, catering to a diverse range of businesses including retail, cultural, entertainment and food and drink.
2. Ensure buildings have a street address and address the street directly.
3. Incorporate a variety of uses at street level, that contribute positively to economic and social vitality.
4. Design buildings to allow flexibility for future land uses.
5. Ensure outdoor seating areas are provided to promote activity without impact on key circulation requirements.
6. Ensure floor to floor levels are considered appropriately for proposed uses.

3.4 Transport, movement networks and parking

3.4.1 Active and public transport

3.4.1.1 Objectives

1. To provide a convenient, efficient and safe network of pedestrian and cycle paths to encourage active transport and activate passive open space.
2. To provide a legible and permeable movement network for pedestrians and cyclists along streets and paths to points of attraction within and adjoining any development.
3. To ensure pedestrian movement avoids sensitive ecologies.
4. To recognise the important role of the Railway Station for Coffs Harbour.
5. To ensure accessibility for all.

3.4.1.2 Requirements

1. The key pedestrian and cycle network is to be provided generally in accordance with **Figure 7**.
2. A section of Jordan Esplanade should reflect the character of a slow raised street in accordance with **Figure 7**. This section of street should be at grade with the footpath and public domain and should be finished in a manner distinct from that of the rest of Jordan Esplanade.
3. A continuous shared path is to be provided that extends throughout the Precinct to encourage cycling. The shared path shall be a minimum of 4m wide – enabling it to cater for both cyclist and pedestrian movement. It shall comprise of elevated boardwalk sections across areas of sensitive coastal vegetation and over water quality basins, and paved sections elsewhere in accordance with the Coffs Harbour City Council Public Realm Urban Design Guidelines (2020).
4. Dedicated on-road cycle lanes shall be provided along both sides of Jordan Esplanade.
5. Pathway widths are to be in accordance with **Table 4**.
6. The pedestrian and cycle network is to provide linkages between major activity areas and streets within and surrounding the Precinct.
7. Where practicable pedestrian paths and cycleways within open spaces should be located close to streets and carparks to take advantage of street lighting to allow casual surveillance by residents, visitors and motorists.
8. Subject to feasibility, a new pedestrian overpass across the railway is provided to improve connectivity between the Precinct and train station.
9. Pedestrian and cyclist access should be limited through culturally significant sites such as Happy Valley and Ferguson Cottage.
10. Development located nearby to the train station should give regard to the desire lines of passengers and acknowledge the station in the siting and orientation of built form.

Table 4 Minimum pathway widths

Path typology	Minimum width
Dedicated on-road cycle lane	1.5m
Shared path (excluding the path described at Section 3.4.1.2(3))	2.5m
Footpath	1.5m



Figure 7 Active transport network structure plan

3.4.2 Parking and vehicle access

3.4.2.1 Objectives

1. To provide co-ordinated, safe and managed vehicle access.
2. To enhance connectivity throughout the Precinct and connectivity between the Precinct and surrounding area.
3. To create a movement network that calms traffic speed, has clear visibility and open sightlines to promote safety for all users.
4. To ensure vehicle access across footpaths is safe for pedestrians and cyclists.
5. To ensure sufficient vehicle parking is provided and distributed across the Precinct.

3.4.2.2 Requirements

1. Landscaped buffers are to be provided to separate Happy Valley from the carparks located in North Park.
2. Shared zones and other traffic calming measures are to be implemented to slow vehicle down in high foot traffic areas to enable safe and efficient movement of pedestrians and cyclists.
3. Vehicle access and circulation is to be designed to:
 - a. cross footpaths at footpath level.
 - b. prevent vehicles from queuing or reversing across pedestrian crossings or footpaths.
4. Vehicle access and parking areas are to minimise impact on existing street trees and to maximise opportunities for new street tree plantings.
5. On-site carparking is provided at a rate in accordance with **Table 5**.
6. On-site car parking for visitors is:
 - a. grouped together in convenient locations relative to car parking area entrances, pedestrian lifts and access points.
 - b. separately marked and clearly sign-posted.
 - c. appropriately accessible.
7. Basement carparks should be restricted to the footprints of the building above.
8. Vehicle ramps should not be visible from the public domain and are located inside the building.
9. Car parking areas:
 - a. Are well lit, visible, and avoid hidden and enclosed areas to allow for casual surveillance.
 - b. Include mirrors or similar devices where hidden and enclosed areas such as staircases and lift lobbies cannot be avoided to allow for passive surveillance.
 - c. Are well ventilated and provide natural rather than mechanical ventilation where practicable.
 - d. Do not visually dominate when viewed from the public domain.
10. Car parking spaces are not located in areas used for the manoeuvring of service vehicles.
11. Where feasible, electric vehicle charging infrastructure should be delivered within carparking areas.

Table 5 *Parking rates*

Land Use	
Residential	
Less than 100m ²	1 space per dwelling
More than 100m ²	2 spaces per dwelling
Visitor spaces	1 space per 5 dwellings
Tourist accommodation	
Unit	1 space per unit
Employee	1 space per 2 employees
-	1 employee per 10 units
Commercial	
Function centre (as part tourist accommodation)	1 space per 13.2sqm
Food & beverage	1 space per 13.2sqm
Neighbourhood shops	1 space per 25sqm
Office and business	1 space per 40sqm
Other	
Community uses	1 space per 40sqm
Other uses	Subject to a parking study

3.5 Public domain and landscape

3.5.1 Public domain overview

The open space and public domain within the Precinct comprises several character areas that contribute to its diverse coastal landscape.

Street design aims to balance traffic capacity with amenity and character. Jordan Esplanade, the primary movement spine in the Precinct, will support pedestrians and cyclists through cycleways, wider pedestrian paths and shade trees as the primary spine of the Precinct, and accommodate vehicle access to the parklands and future development.

New pedestrian and shared paths will thread through the Precinct and improve connectivity to surrounding areas. New development on land adjoining public domain is to play an important role in supporting design quality, character, usability, amenity and active transport.

3.5.1.1 Objectives

1. To implement a Country-centred approach that supports a network of relationships through Country.
2. To provide high quality, integrated, permeable and multifunctional public spaces that cater for movement, recreation and social interaction and provide connectivity to the Harbour.
3. To achieve an adaptable public domain capable of accommodating a broad range of uses and events, experiences and activities.
4. To enhance existing vegetation communities within the Precinct.
5. To maximise public access to the open space via an integrated active transport network.
6. To ensure that new buildings are designed, located and orientated to help activate, and frame the public domain.
7. To incorporate Safer-By-Design and accessibility principles into public areas and building design to ensure access and inclusion for all ages, abilities, and cultural groups.
8. To enhance provision of shade, shelter, and comfort amenities for people within the Precinct to protect people from the heat and wind to maximise people's enjoyment.
9. To provide public open spaces which retain, conserve and interpret the heritage of the site.
10. To achieve consistency with the Coffs Harbour Jetty Foreshore Precinct Public Domain and Landscape Manual.

3.5.2 Public open space

The provision of high quality public open space is a key principle for the development of the Precinct. The Open Space Structure Plan at **Figure 8** indicates a variety of connected, high amenity, ecologically-focused spaces across the Precinct. The majority of the Precinct will be open space, designed for a wide range of uses and activities.

3.5.2.1 Objectives

1. To consider connecting with Country values in the design of public open spaces, with consideration of how culturally important stories can be shared mindfully.
2. To establish a diverse range of types, uses and scales of public spaces throughout the Precinct that encourage social interaction across a broad range of user groups, user types and ages.
3. To achieve desirable public spaces with high levels of amenity addressing solar access, wind, safety, activity, circulation, seating and enclosure.
4. To deliver an environmentally and socially sensitive and responsive design that ensures the inherent qualities of the existing landscape are maintained and not negatively impacted.
5. To rehabilitate and enhance open green spaces.
6. To ensure green spaces are interconnected to enable safe movement.

3.5.2.2 Requirements

1. The Open Space Network within the Precinct is to be delivered generally in accordance with the Open Space Structure Plan in **Figure 8**. Alternative configurations and locations can be provided – subject to consistency with the objectives – and provision of an equivalent quantum of publicly accessible open space within the Precinct.
2. A minimum of 5,000sqm of new and upgraded publicly accessible open space is to be provided within or between the North Park, Jetty Hub and Activity Hub and Village Green sub-precincts.
3. Embellishment of existing and future parklands should be in accordance with **Table 6**.
4. Consideration should be given to the embedding of the following within public open spaces:
 - a. Aboriginal artwork, murals and storyboards etc honouring Aboriginal sites of significance, culture and history; and
 - b. Commemoration of key Aboriginal community identities and their contributions to the broader community.
5. Any boardwalk delivered on Jetty Beach must:
 - a. Be suspended over the dune system, to minimise damage to dune vegetation.
 - b. Connect appropriately with existing and proposed shared paths.
 - c. Include provision of an appropriate salt resistant 1.5m high fence access barrier installed along the eastern edge of the Tuckeroo-Paperbark Littoral Forest and dune vegetation communities running from the Community Centre to Corambirra Point, to limit access through the Littoral Forest and dune system to the beach.
6. Public open space should be generally in accordance with the Coffs Harbour Jetty Foreshore Precinct Public Domain and Landscape Manual.

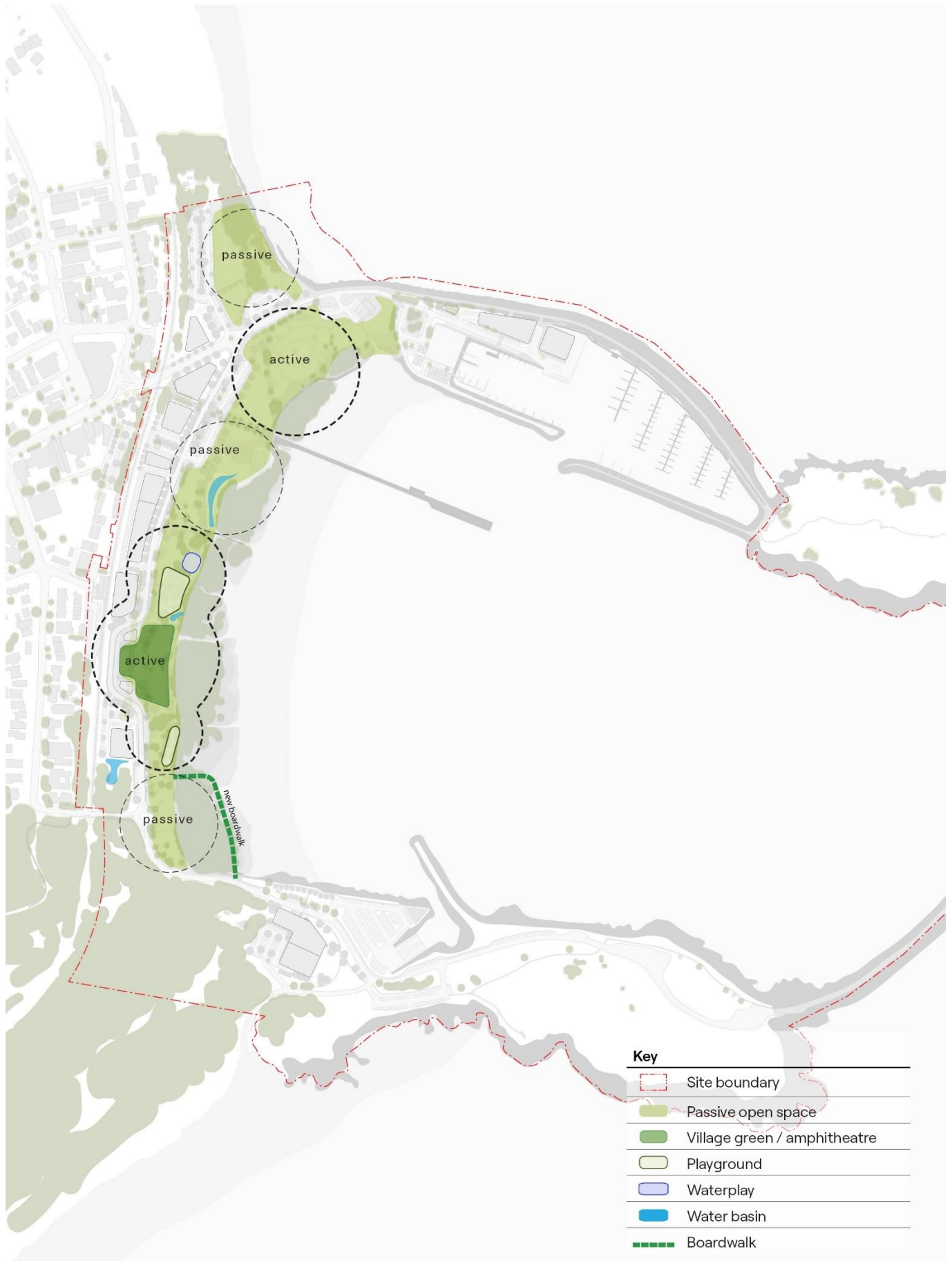


Figure 8 Open space structure plan

Table 6 **Publicly Accessible Open Space Characteristics**

Name and Type	Role and Function	Requirements	Guidelines
Village Green Amphitheatre <i>Outdoor Recreation</i>	A flexible outdoor recreation space that supports a range of everyday passive recreation activities and occasional outdoor events such as cinema and musical events, as well as cultural events.	Min. 2,000 sqm	<ul style="list-style-type: none"> Sited in a naturally occurring bowl-shaped grass space, with a gentle fall to the east, with minimal modification required to the existing landform and vegetation. Materiality of the amphitheatre is to be locally sourced rock or recycled material to celebrate Country.
Adventure Playground <i>Active outdoor recreation</i>	A large new adventure playground incorporating elements of nature play. The play space will be will contain a large variety of play elements/challenges suitable for a broad range of age groups.	Min. 1,500 sqm	<ul style="list-style-type: none"> Explore the opportunity to co-design the park with Gumbaynggir Knowledge Holders to incorporate themes of Country in the play space for children to engage with. Include a diverse range of play elements/challenges that are suitable and cater for a broad range of age groups. Ensure adequate shading is provided. Existing mature trees are to be retained around the perimeter of the new adventure play to provide adequate shading and nature play elements.
Waterplay	An all-ages water play space	Min. 250 sqm	<ul style="list-style-type: none"> Provide a dynamic environment where children can explore, experiment and engage with nature in cooperative play. The waterplay space is to be operated by a pump and controller room. Shading of the waterplay space is to be provided by manufactured shade structures and additional tree canopy, where appropriate.
North Park	New landscaped park to provide an extension to the Foreshore Parklands. Provides new connections to Happy Valley and includes outdoor courts, passive open space areas, picnic facilities and amenities.	Min 2,000sqm	<ul style="list-style-type: none"> Regenerating and enhancing landscape buffers around Happy Valley Provide multi-purpose sports fields that can function as overflow parking during events Additional amenities adjacent to existing open space.
Passive Open Space	New and upgraded passive areas are to complement the existing character and offering of the foreshore open space.	N/A	<ul style="list-style-type: none"> Passive areas are to be generously sized to support an array of passive recreational activities, including group gatherings, picnicking and general relaxation. Passive areas are to offer improved amenities. Existing canopies and BBQ facilities are to be retained where possible, and new facilities are to be built where needed.
Dune Vegetation & Littoral Rainforest	Vegetated areas to be conserved and protected	N/A	<ul style="list-style-type: none"> Ensure vegetation communities and their connections are protected and enhanced as part of the broader Littoral landscape. The vegetation community located in the North Park is to be preserved and enhanced to operate as a buffer protecting Happy Valley.

3.5.3 Solar access to public spaces

3.5.3.1 Objectives

1. To achieve appropriate levels of sunlight to public open space to enhance people's enjoyment and support appropriate vegetation growth.

3.5.3.2 Requirements

1. Development is to achieve direct sunlight to a minimum 50% of publicly accessible open space for a minimum of 4 hours between 9am and 3pm during midwinter.
2. Any requirement in this section prevails to the extent of any inconsistency with any solar access or overshadowing control contained within the NSW Coastal Design Guidelines referred to in **Section 3.6.6**.

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3.5.4 Biodiversity

3.5.4.1 Objectives

1. To increase biodiversity through planting predominantly local indigenous plant species including plant species valued in Gumbaynggirr culture.
2. To ensure that the health of Country and amenity of residents, workers and visitors is enhanced by high quality, biodiverse landscaping of all open spaces.
3. To promote environmental awareness and sustainability through connecting people with the landscape.
4. To select species which are able to thrive given the constraints of the location (bushfire, flooding and coastal conditions).
5. To avoid unacceptable adverse impacts on the operation of Coffs Harbour airport by trees and associated biodiversity.
6. To encourage green roofs and podiums in developments to improve amenity and aesthetic quality of the urban environment.

3.5.4.2 Requirements

1. Gumbaynggirr culture should be recognised and showcased in the natural landscape by promoting native practices and relationships with flora and fauna. This should include consideration of opportunities to incorporate indigenous cultural plants into landscaping design and associated educational signage, at key points of high community engagement.
2. All planting throughout the Precinct is to be in accordance with the listed species identified in the Coffs Harbour Jetty Foreshore Precinct Public Domain and Landscape Manual
3. The following requirements should be demonstrated:
 - a. Development applications are to identify the flora species to be planted during rehabilitation in areas where ground disturbance will occur, and native vegetation is to be reinstated, as well as part of the ecological habitat restoration within the dunal vegetation.
 - b. Development applications are to avoid, as far as is practicable, clearing within native vegetation and habitats, and ensure retained vegetation is protected from any inadvertent direct or indirect impacts.
 - c. Development applications will seek to relocate indigenous flora and fauna potential which may be impacted where possible.
 - d. Development applications are to identify areas where ecological restoration can be undertaken within degraded areas or areas where the Littoral Rainforest used to occur, at a planting ratio of 10 plants planted for each individual Littoral Rainforest plant species impacted (10:1 planting ratio).
4. Ecological restoration and habitat rehabilitation activities should be implemented, including nest boxes and biodiversity information signage, to ensure the protection, conservation, and improvement of ecological values over the long-term.
5. Where possible, consideration should be given to avoiding the planting of plant species that would grow to infringe on protected airspace or encourage bird species or nesting of birds that could potentially increase the risk of bird strikes by aircraft.

3.5.5 Tree canopy, street trees and planting

3.5.5.1 Objectives

1. To retain existing foreshore vegetation, particularly trees, wherever possible.
2. To create spaces in the private and public domain with ample soil volumes for trees to reach maturity.
3. To achieve connected canopies of street trees where viable.
4. To introduce native trees in open spaces on deep soil.
5. To increase biodiversity through planting predominantly local indigenous plant species.

3.5.5.2 Requirements

1. Greening and canopy cover:
 - a. Is in accordance with the relevant planting species identified in Coffs Harbour Jetty Foreshore Precinct Public Domain and Landscape Manual.
 - b. Considers sub-tropical climate conditions, coastal environment and microclimatic conditions (i.e. solar and wind), including for green roofs and walls.
2. Development should contribute to achieving a tree canopy target of 15% across the Precinct, excluding parkland areas, subject to compliance with asset protection zones.
3. Parkland embellishments and upgrades achieve a tree canopy target of 20%, subject to compliance with asset protection zone.
4. Development incorporates greening and canopy cover on rooftops, where practical.
5. Street trees are generally required for all streets. Street tree planting is to:
 - a. Be consistently used to distinguish between public and private spaces.
 - b. Minimise risk to utilities and services.
 - c. Be durable and suited to the street environment and, wherever appropriate, include indigenous species.
 - d. Maintain adequate lines of sight for vehicles and pedestrians, especially around driveways and street corners.
 - e. Provide appropriate shade.
6. A landscaping maintenance plan must be submitted with any development application that proposes landscape works to ensure successful establishment, structural integrity and ongoing health of plants, as well as managing weeds, pests and diseases, and monitoring the height of tall trees to ensure that they will not grow to heights that would infringe the protected airspace of Coffs Harbour Airport.

3.5.6 Street Design

3.5.6.1 Requirements

1. Street design is to be generally in accordance with the typical street cross-sections outlined in **Figure 9**, **Figure 10** and **Figure 11**.
2. Alternative street designs for local streets and accessways may be permitted on a case by case basis to accommodate local features if they preserve the functional objectives and requirements for the carriageway.
3. Street construction should be generally in accordance with the Coffs Harbour Jetty Foreshore Precinct Public Domain and Landscape Manual.

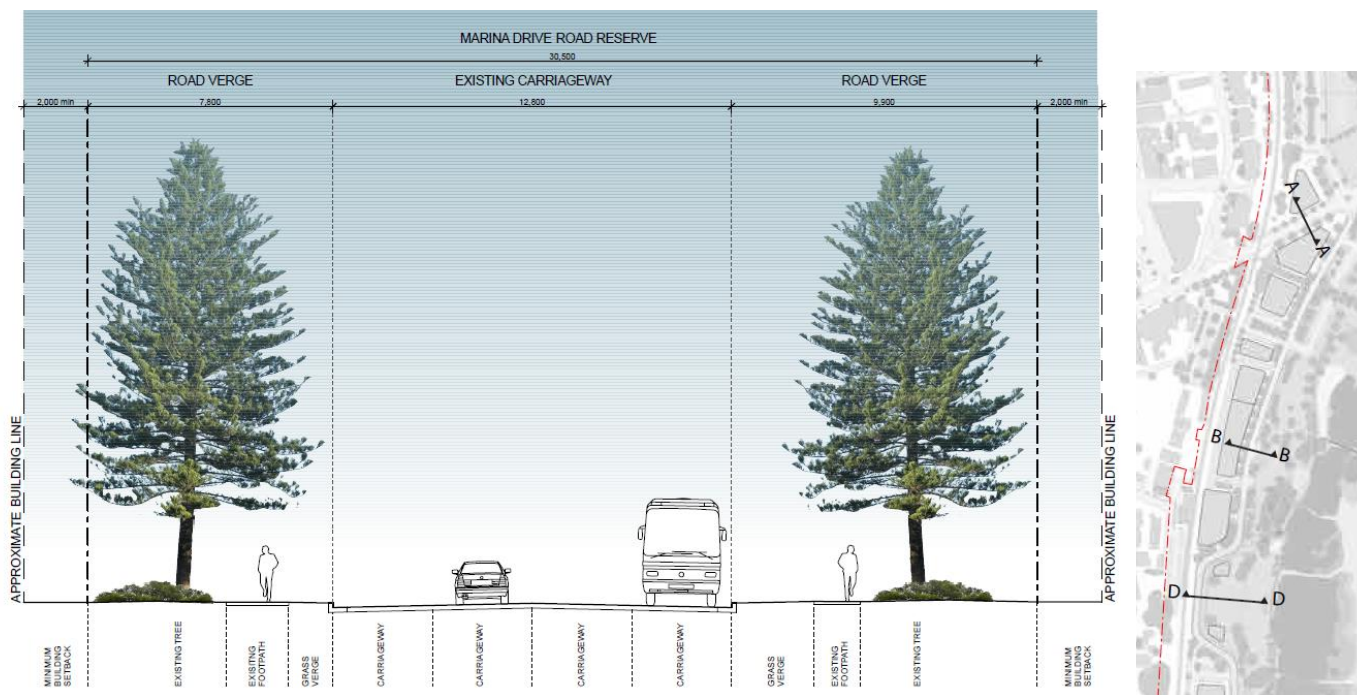


Figure 9 Typical street cross section (Marina Drive) (Section A)

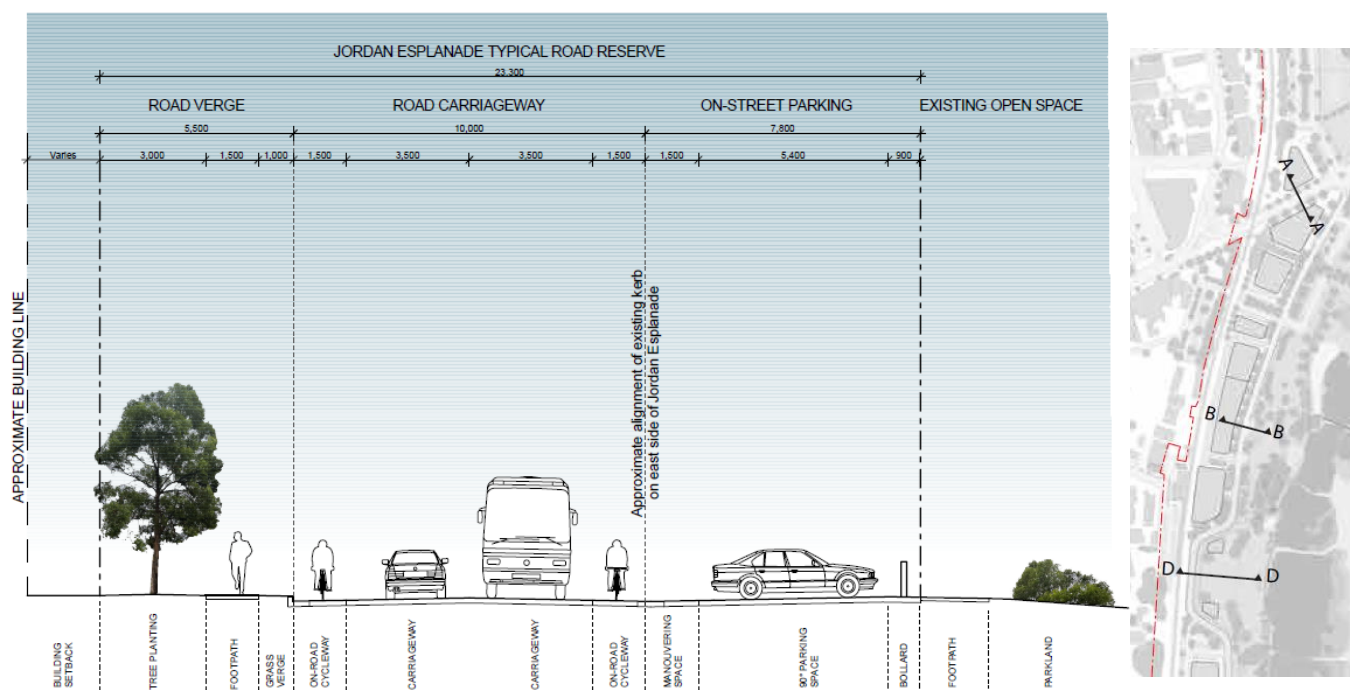


Figure 10 Typical street cross section (Jordan Esplanade) (Section B)

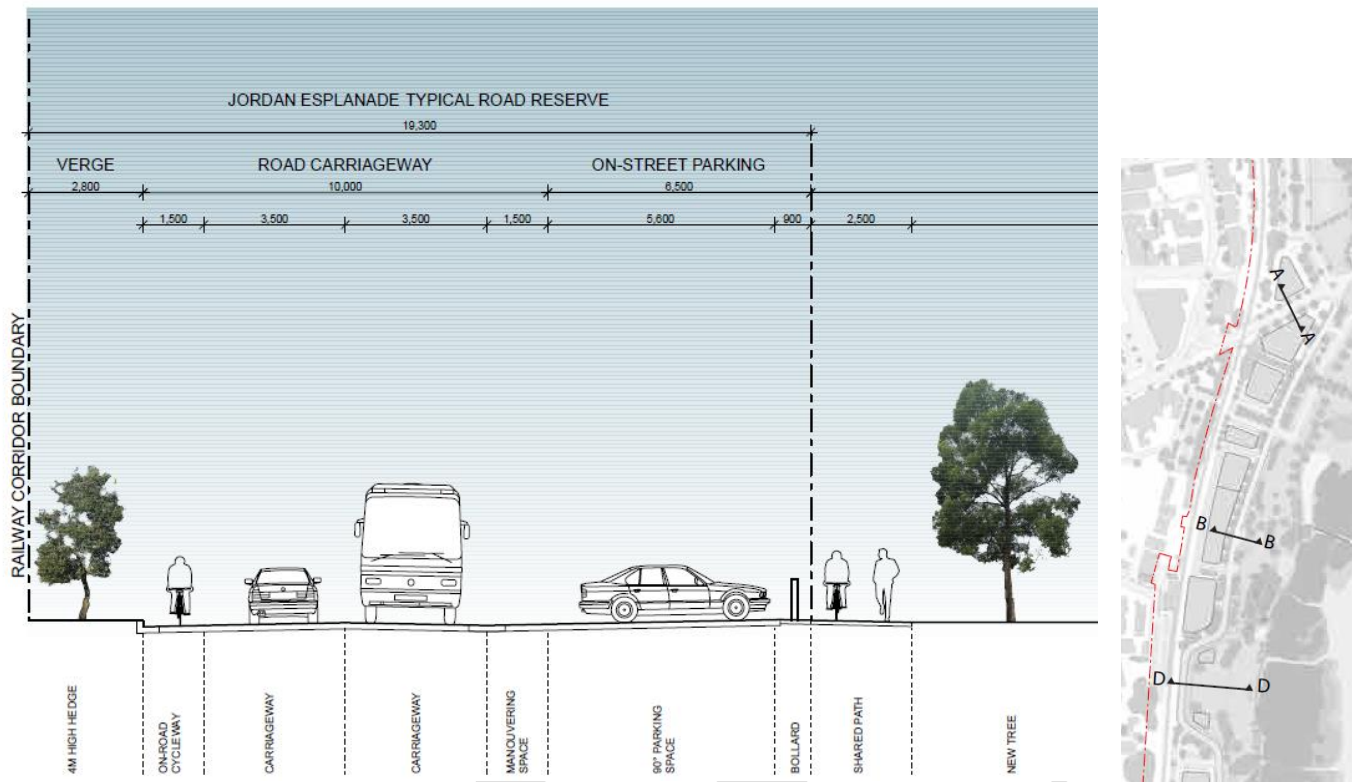


Figure 11 Typical street cross section adjoining new open space (Jordan Esplanade) (Section D)

3.6 Environmental management and sustainability

3.6.1 Sustainability and climate change

3.6.1.1 Objectives

1. To encourage a shift towards carbon neutral operation, electrification and net zero.
2. To ensure open space, public domain and infrastructure delivers cooler microclimates and mitigated urban heat island effect.
3. To deliver open space and public domain that clearly and visibly connect to the Precinct's natural context.
4. To protect and restore the ecological health of native flora and fauna within the Precinct.
5. To ensure that sustainability initiatives support visitor, owner and user experiences.

3.6.1.2 Requirements

1. Showcase renewable energy by setting the target for the public domain to be 100% powered by renewable energy
2. Incorporate appropriate sustainability measures and initiatives in buildings.
3. Encourage opportunities for on-site renewable energy generation, wherever possible.
4. Enhance the blue and green grid through delivery of enhanced stormwater infrastructure and tree canopy, as contemplated by these Design Guidelines.
5. Prioritise non-vehicular movement through the Precinct.
6. These requirements should be read in conjunction with the Property & Development NSW Sustainability Framework.

3.6.2 Heritage and archaeology

3.6.2.1 Objectives

1. To conserve, protect, enhance and appropriately interpret heritage values and significance and ensure appropriate interpretation.
2. Interpretation of the Aboriginal archaeology, history and cultural values of the Coffs Harbour Jetty Foreshore should be incorporated into publicly accessible areas of the Precinct to create a more visually interesting and culturally diverse public domain.
3. To facilitate the development of the Precinct in an integrated manner that relates to the surrounding context, with particular regard to the area's heritage qualities.
4. To ensure new development has regard to views and vistas relating to the location, siting and design of heritage buildings and cultural landscapes.
5. To ensure development is carried out to minimise disturbance of areas of high archaeology significance and follows protocols for managing artefacts.

3.6.2.2 Requirements

General

1. Development is to retain, conserve and enhance significant historical relationships, cultural landscapes, building curtilages and settings, views and vistas.
2. A Statement of Heritage Impact should be prepared for all development in close proximity to heritage items in the Precinct.

Aboriginal heritage

3. Archaeological investigation is required prior to any proposed ground disturbing works within future development stages taking place:
 - a. An Archaeological Technical Report (ATR) must be prepared by a qualified consultant in accordance with the *Code of Practice for Archaeological Investigation of Aboriginal Objects in New South Wales (DECCW 2010)*. The ATR should determine consultation requirements, test excavation requirements, and any requirements for an Aboriginal Cultural Heritage Assessment Report (ACHAR) and Aboriginal heritage impact permit (AHIP) application.
 - b. Archaeological test excavation under an AHIP will be required in any areas identified in future ATR and consultation processes as demonstrating potential for Aboriginal objects to occur beneath the ground surface. Alternative methodologies may be required to investigate deeper contexts not safely accessible by hand excavation.
 - c. Non-invasive methods of investigating potential burials, such as ground penetrating radar, should be considered.
4. Aboriginal objects must not be harmed without an AHIP issued by Heritage NSW authorising harm through carrying out specified activities.
5. The area's significance in terms of social/cultural, historical and aesthetic values must be acknowledged and consultation with Aboriginal communities should occur in association with development, specifically consultation with the Coffs Harbour and District Local Aboriginal Land Council and the Coffs Harbour Garlambirla Guyuu-Girrwa Coffs Harbour Local Aboriginal Elders.

Archaeology

6. Where archaeology is identified or suspected, an Archaeological Impact Assessment and an Archaeological Research Design (if required) be prepared by a qualified consultant to outline appropriate strategies to minimise and manage any potential impacts to archaeological remains.
7. Interpretation of archaeology should include a range of approaches and media and include retention of physical evidence, physical interpretation of sites or individual buildings, interpretive public art, memorials, archaeological displays, guided tours, signage, etc.

3.6.3 Drainage and Stormwater Management

3.6.3.1 Objectives

1. To protect waterways from erosion, pollution and sedimentation.
2. To maintain or enhance water quality in aquatic habitats, ensuring downstream flora and fauna are protected from stormwater impacts during and post construction.
3. To consider stormwater management in a cumulative and long-term context to sustain and enhance the flow of stormwater.
4. To integrate a high-quality water cycle management strategy throughout the Precinct to harmonise with the development.
5. To promote the sustained health of natural watercourses, ecosystems, and habitats in the Precinct and the wider Coffs Harbour region.

3.6.3.2 Requirements

1. An Integrated Water Management Plan for the Precinct is to be submitted with any comprehensive concept development application spanning the Precinct. It is to:
 - a. Be prepared in consultation with Coffs Harbour City Council and any other relevant drainage or water authority;
 - b. Outline the water-related servicing infrastructure required by the development (informed by the anticipated annual and ultimate increase in servicing demand) and evaluate opportunities to reduce water demand (such as recycling water provision);
 - c. Detail the proposed stormwater drainage design, including any on-site treatment, reuse and detention facilities, water quality management measures and nominated discharge points; and
 - d. Demonstrate compliance with the local council and other drainage or water authority requirements.
2. Integrate water sensitive urban design (WSUD) principles into the proposed stormwater management strategy throughout the Precinct.
3. Overland flow is to be directed to pits, open channels and stormwater quality basins.
4. Stormwater quality basins or bio-retention basins are to be located at natural low points of the site with the primary function of treating stormwater runoff and improving discharge quality.
5. Basins should be sized to treat 4EY (3-month ARI flow) runoff from proposed and upstream existing catchments.
6. Locations of basins for stormwater detention, stormwater treatment or sedimentation purposes should avoid areas that are known to be permanent or seasonal groundwater discharge areas.
7. Wherever practical and in areas known to be affected by high water tables and/or salinity of groundwater, retarding basins shall be designed to be water retentive and in accordance with AS/NZS 3500.1
8. A stormwater quality assessment is to be undertaken to demonstrate that the development will achieve suitable pollutant load reductions.
9. Ensure adequate provisions for controlling and discharging stormwater runoff from the site, preventing adverse impacts on stormwater drainage systems, waterways, the development itself, or adjacent properties.

3.6.4 Water sensitive urban design

3.6.4.1 Objectives

1. To protect waterways from erosion, pollution and sedimentation.
2. To improve the quality of water within the Precinct using water sensitive urban design (WSUD) methods.
3. To reduce overall discharge of stormwater from a site to less than pre-development levels where possible.
4. To ensure appropriate treatment and reuse of stormwater where possible.
5. To implement successful WSUD and stormwater quality improvements for private developments and the public domain at all scales.
6. To ensure that on-site stormwater management measures can be operated and maintained in accordance with design specifications.
7. To use simple landscape-based WSUD solutions wherever appropriate that achieve water management objectives without unusual or complicated maintenance demands.
8. To use WSUD to increase evapotranspiration, urban heat reduction, and to reduce uncontrolled runoff.

3.6.4.2 Requirements

1. WSUD measures and strategies are to be developed and implemented with reference to the City of Coffs Harbour Water Sensitive Urban Design Guideline (2018).
2. All developments should implement rainwater retention and a WSUD approach. The extent and types of this will be proportional to the scale of development.
3. WSUD principles are to be integrated into the development through the design of stormwater drainage, on-site detention, and landscaping, and in the orientation of the development rather than relying on 'end of pipe' treatment devices prior to discharge.
4. WSUD measures are to include:
 - a. Filtration devices such as cartridge systems within building footprints, small raingardens in road reserves or open spaces, and larger bioretention basins in open spaces.
 - b. Gross pollutant traps (GPTs) as end-of-pipe systems.
 - c. Potential harvesting of rainwater from roofs for potable or non-potable purposes within buildings.
 - d. Potential harvesting of stormwater to reuse for irrigation or other non-potable purposes.
 - e. WSUD measures are to be incorporated into streets where practicable.
 - f. Opportunities to reuse water in the landscape through WSUD are to be maximised.
 - g. On-site water capture and reuse are to be provided in all developments and used to water vegetation in parks and public spaces.
5. New development is to consider recycled water initiatives as part of integrated water management and, at a minimum, make provision for dual plumbing in all new development to allow the Precinct to connect in future centralized non-potable water systems, such as recycled water.

3.6.5 Waste management

3.6.5.1 Objectives

1. To minimise waste generation.
2. To set high standards and targets for waste diversion from landfill, commencing the transition towards a circular economy.
3. To establish an efficient waste management system that maximises resource recovery by:
 - a. Considering circular economy recovery and reuse principles.
 - b. Reducing construction and demolition waste sent to landfill.
 - c. Minimising waste generated in the operation of a development, redirecting it from landfill, and maximising resource recovery.
4. To ensure that each development has adequate space to manage waste and recycling.
5. To ensure that residential amenity is not impacted by waste systems and collection.

3.6.5.2 Requirements

1. Submit a Waste Management Plan (WMP) with all development applications involving demolition work, construction work, and/or waste generation. WMPs should adhere to Coffs Harbour Council's Waste Management Plan template.
2. These requirements should be read in conjunction with the Property & Development NSW Sustainability Framework.

3.6.6 Coastal hazard management

3.6.6.1 Objectives

1. To manage development in a coastal vulnerability area and protect the environmental assets of the coast.
2. To ensure that development is compatible with coastal risks.
3. To minimise the risk to life and property from coastal hazards associated with development within the coastal vulnerability area.

3.6.6.2 Requirements

1. Development is to be generally in accordance with the NSW Coastal Design Guidelines prepared by the Department of Planning and Environment (2023), as it relates to coastal hazard management.
2. Development in the coastal vulnerability area is to be generally in accordance with Section E2 Coastal Vulnerability Area of the Coffs Harbour Development Control Plan 2015.

3.6.7 Light spill

3.6.7.1 Objectives

1. To minimise light spill to the sky.
2. To ensure that external lighting for buildings and outdoor facilities does not cause adverse impacts on aircraft approaching from and taking off to the north of Coffs Harbour Airport.
3. Adopt lighting design that reduces light pollution to minimise effects on wildlife.
4. To minimise light spill on sensitive habitats such as Muttonbird Island.
5. To achieve compliance with the *Marine Estates Management Act 2014* where necessary.

3.6.7.2 Requirements

1. All development should minimise light spill on sensitive habitats such as Muttonbird Island wherever possible.
2. Consideration should be given to the National Light Pollution Guidelines for Wildlife (2023), including migratory shorebirds.
3. For development subject to the *Marine Estates Management Act 2014*, the following guidance is provided to achieve compliance:
 - a. Lighting design should have regard to and be generally consistent with the general principles set out in AS4282-2019 Control of the Obtrusive Effect of Outdoor Lighting.
 - b. Outdoor lighting design should have regard to the 5 principles and guidelines for outdoor lighting, specifically:
 - i. Useful – Use light only if it is needed. All light should have a clear purpose. Consider how the use of light will impact the area, including wildlife and their habitats
 - ii. Targeted – Direct light so it falls only where it is needed. Use shielding and careful aiming to target the direction of the light beam so that it points downward and does not spill beyond where it is needed.
 - iii. Low level – Light should be no brighter than necessary. Use the lowest light level required. Be mindful of surface conditions, as some surfaces may reflect more light into the night sky than intended.
 - iv. Controlled – Use light only when it is needed. Use controls such as timers or motion detectors to ensure that light is available when it is needed, dimmed when possible, and turned off when not needed.
 - v. Warm-coloured – Use warmer colour lights where possible. Limit the amount of shorter wavelength (blue-violet) light to the least amount needed.
 - c. The lighting of vegetation at night, particularly up lighting, should be avoided.
4. In areas within the defined Lighting Control Zone for Coffs Harbour Airport (with provisions for the future runway extension) the external lighting must not exceed the defined candela limits for each zone.
5. Quantitative modelling to determine the extent of light spill should be undertaken as more detailed lighting design plans are generated, with mitigation measures to ensure compliance with AS 4282-2019.
6. Any requirement in this section ceases to apply insofar as it becomes inconsistent with the advice of the relevant Ministers or authorities subject to Sections 55 and 56 of the *Marine Estates Management Act 2014*.

4.0 Sub-precincts

The Precinct is broadly organised across six sub-precincts that will each have a distinct character and function. These are identified as:

1. Foreshore Parklands – with improved amenities, proposed new board walk and nature-based playground.
2. The Marina – An active marina revitalised to accommodate local marine based businesses that reflect their regional importance.
3. North Park – Functional open space with recreational courts and formalised parking.
4. Jetty Hub – A hub of residential and tourist accommodation supporting activation, tourism and regional attraction located adjacent to the current Jetty Walkway, with massing capped at 6 storeys stepping down in scale when closer to public areas.
5. Activity Hub and Village Green – An active village green that delivers increased public open space connected to the existing foreshore parklands and may include family friendly food and beverage, community uses and club houses or facilities to support events. A local business activity zone connected to the rail station.
6. Corambirra Point – A new regional tourist destination on the site of the former Deep Sea Fishing Club site including publicly accessible cafes and restaurants, a function space, activity centre and tourist accommodation.

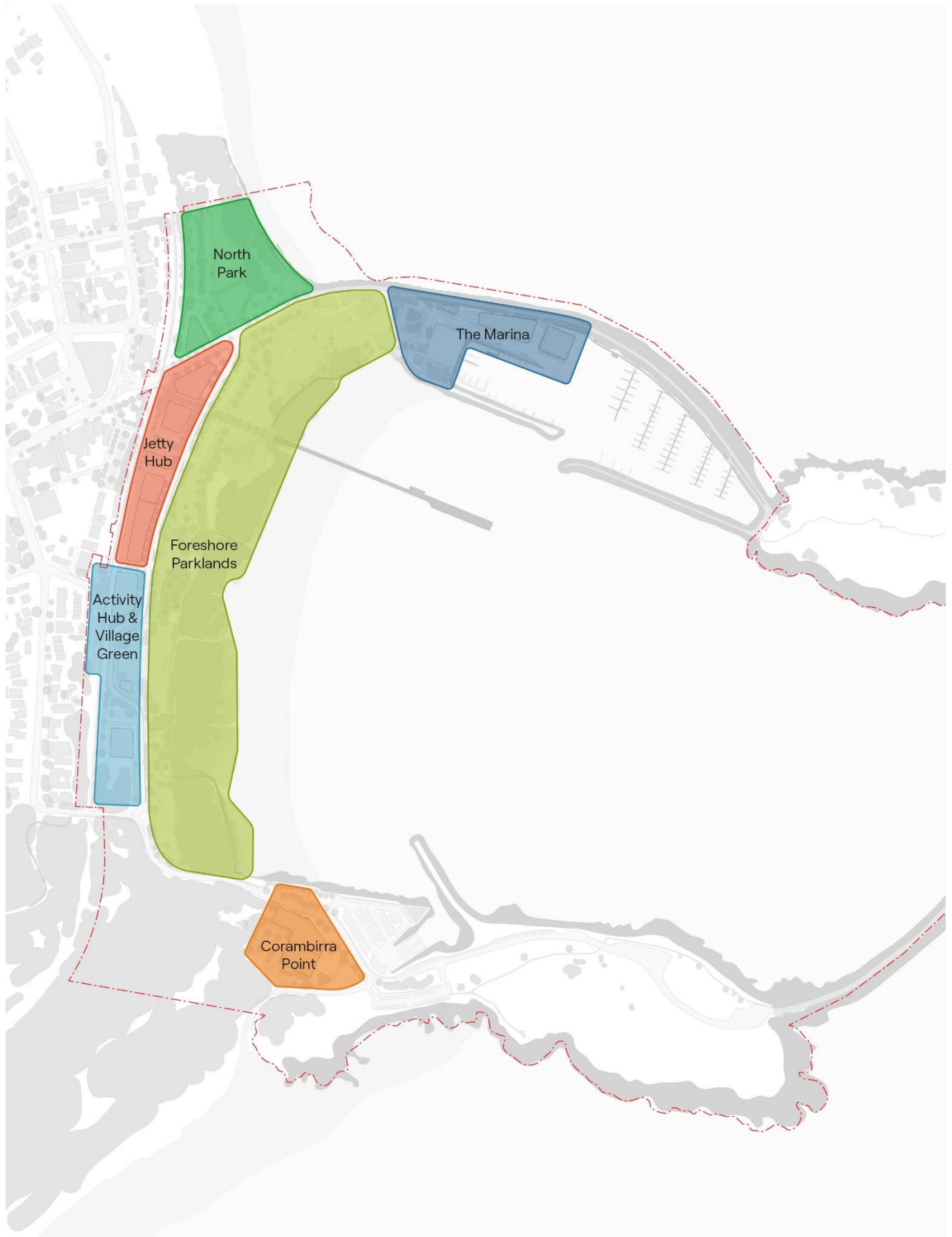


Figure 12 Sub-precincts plan

4.1 North Park

North Park marks the threshold into the Jetty Foreshore. This area will operate as an extension of the Foreshore Parklands by converting the existing informal parking into a consolidated formal parking area to the west and increasing passive open space to the east. The new passive spaces, that also include recreational sports courts, may also be utilised for informal parking during events such as the Sunday Markets when additional parking is required. Happy Valley to the north will be uninterrupted, with new access provided from the formal parking area and enhancement of the landscaping to provide a buffer, ensuring privacy.

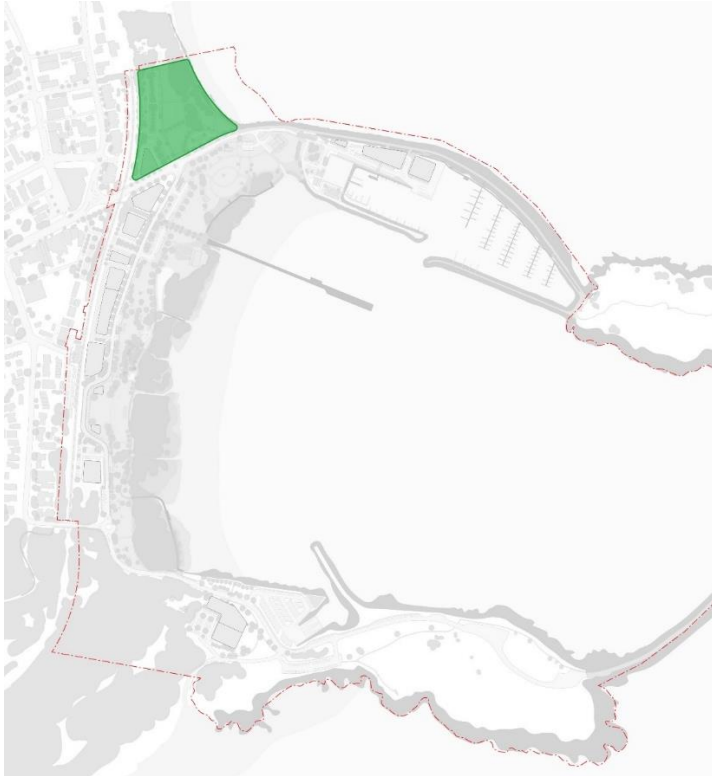


Figure 13 North Park reference plan

4.1.1 Objectives

1. To regenerate and enhance landscape buffers around Happy Valley connecting the Precinct with its surrounding landscaping with tree planting schemes.
2. To establish green areas that can double as overflow parking in place of the existing informal parking areas.
3. To provide additional parkland amenities adjacent to existing open space.
4. To facilitate new residential development along the eastern side of the railway that is not within coastal vulnerability areas.
5. To provide sufficient space for view lines and deep soil planting along Harbour Drive creating a gateway to the Precinct.

4.1.2 Requirements

1. Building form should be generally consistent with the heights, setbacks and separation distances shown in **Figure 14** and **Figure 15**.
2. No basement carpark entry is to be permitted from Marina Drive.

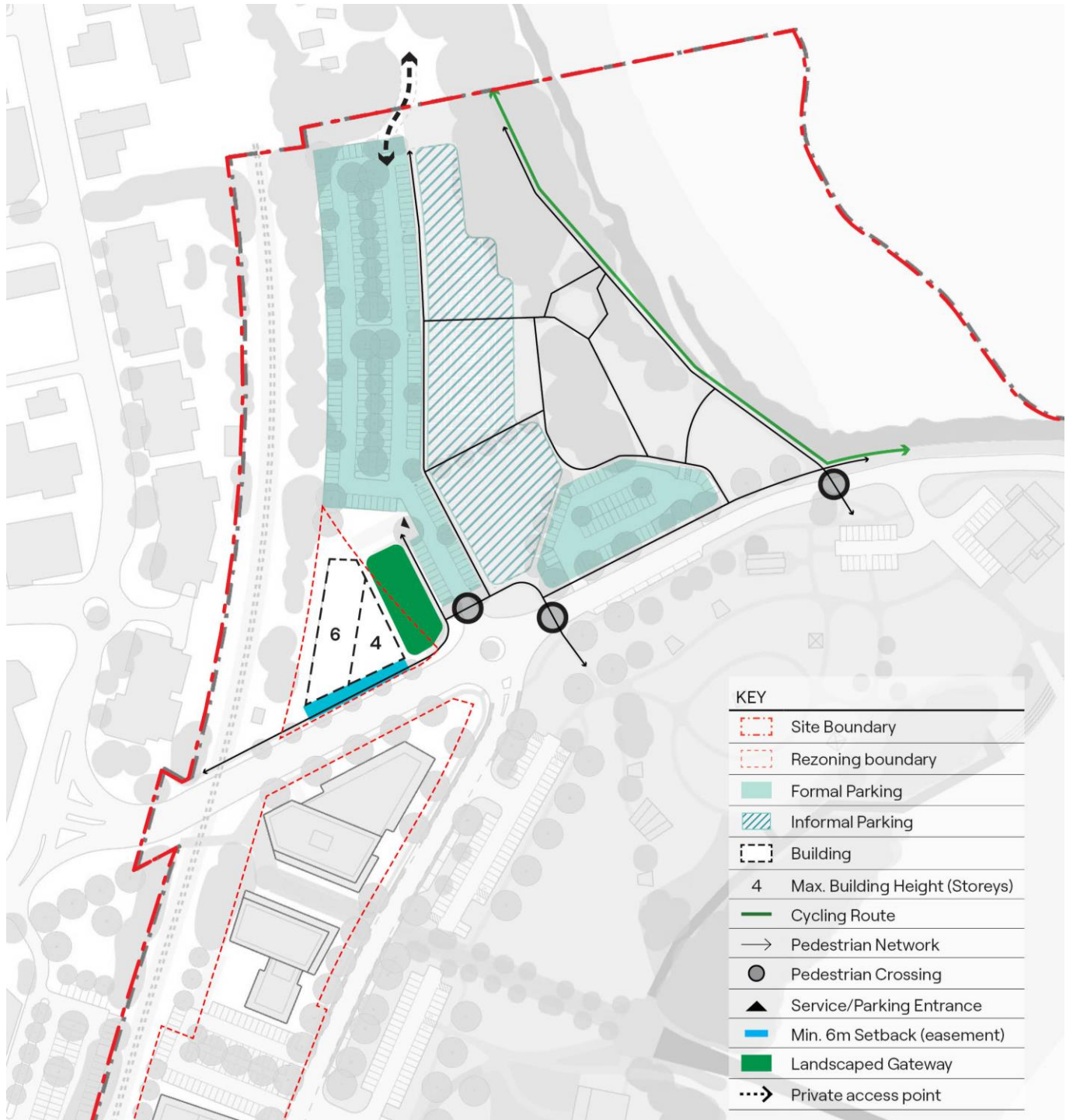
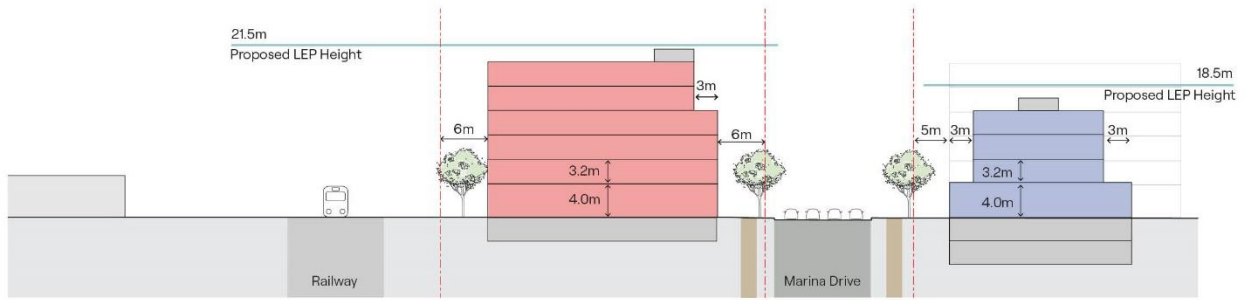
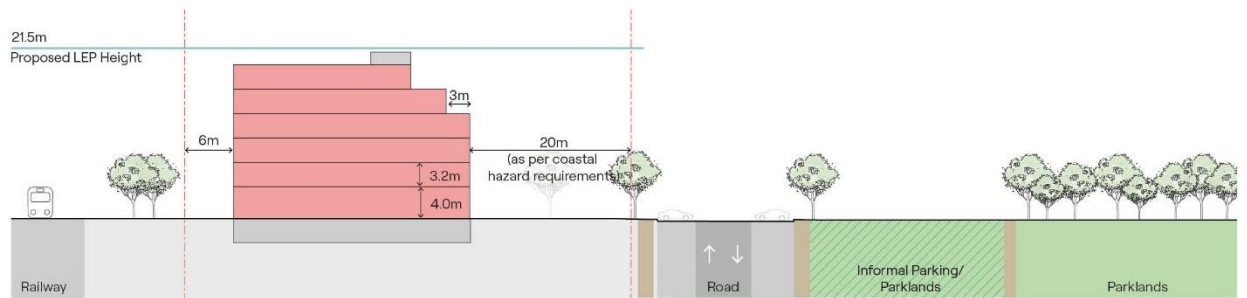


Figure 14 North Park sub-precinct plan



Section a



Section b

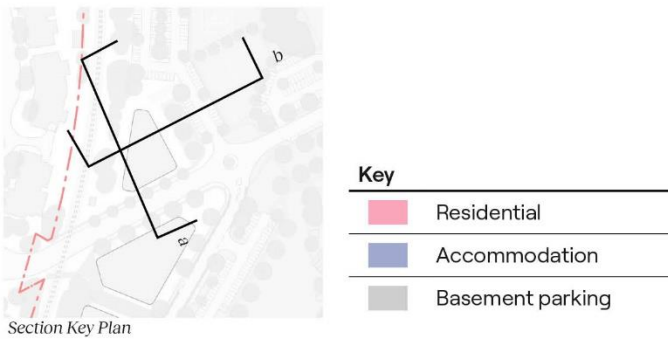


Figure 15 North Park indicative sections – A & B

4.2 Jetty Hub

The Jetty Hub marks the 'Active Heart' of the Jetty Foreshore area providing a key pedestrian connection from the Coffs Jetty Strip whilst also acting as the built form gateway when entering the Precinct along Marina Drive. These connections are fronted by mixed-use developments that incorporate active uses such as cafes and restaurants at ground floor facing the through-site link. Further south, a new parking area helps to distribute parking along the Foreshore, ensuring direct access to parking from any of the passive spaces in the Parklands. Residential buildings south of this provide dwellings in close proximity to the Parklands and amenity, ensuring long-term activation of public spaces and uses.



Figure 16 Jetty Hub reference plan

4.2.1 Objectives

1. To transform current low use and inaccessible land to create activity and amenity.
2. To create a gateway in form use and activity to provide coherent identity for the Precinct.
3. To develop a diversity of use typologies including residential and tourist accommodation to cater to multiple uses.
4. To provide clear and direct active transport connectivity between the Precinct, parklands, and Harbour Drive, reinforcing alignment of the Jetty walkway.
5. To increase activation along Jordan Esplanade with new cafes and restaurants.
6. To use built form to buffer noise between the train line and the parklands, reducing impacts from noise.
7. To provide for setbacks along Jordan Esplanade to provide space for active frontages and pedestrian movement to complement the proposed retail typologies.
8. To provide for residential setbacks allowing for greater landscaping and privacy.

4.2.2 Requirements

1. Building form should be generally consistent with the heights, setbacks and separation distances shown in **Figure 17** and **Figure 18**.
2. Building length should not exceed 24m.
3. A landscaped gateway plaza at the north-east corner of the sub-precinct should be provided in accordance with **Figure 17**.
4. Active frontages in key locations should employ different architectural character to any residential and accommodation uses above to ensure clear legibility of these elements as publicly accessible.
5. Residential buildings are to be separated as per Apartment Design Guide (ADG) minimum building separation requirements.

6. Ground floor terraces should be raised to provide visual privacy.
7. Carpark entrances should aim to minimise impact on the public domain to avoid conflict.
8. Semi-private access roads should be provided along the rail-line to service residential buildings.
9. No basement or private car park access is to be provided from Jordan Esplanade to limit impacts on pedestrian connectivity.
10. Any residential parking to be provided in an undercroft space must be screened towards Jordan Esplanade and public spaces, ie. with planting or landscaped terracing.

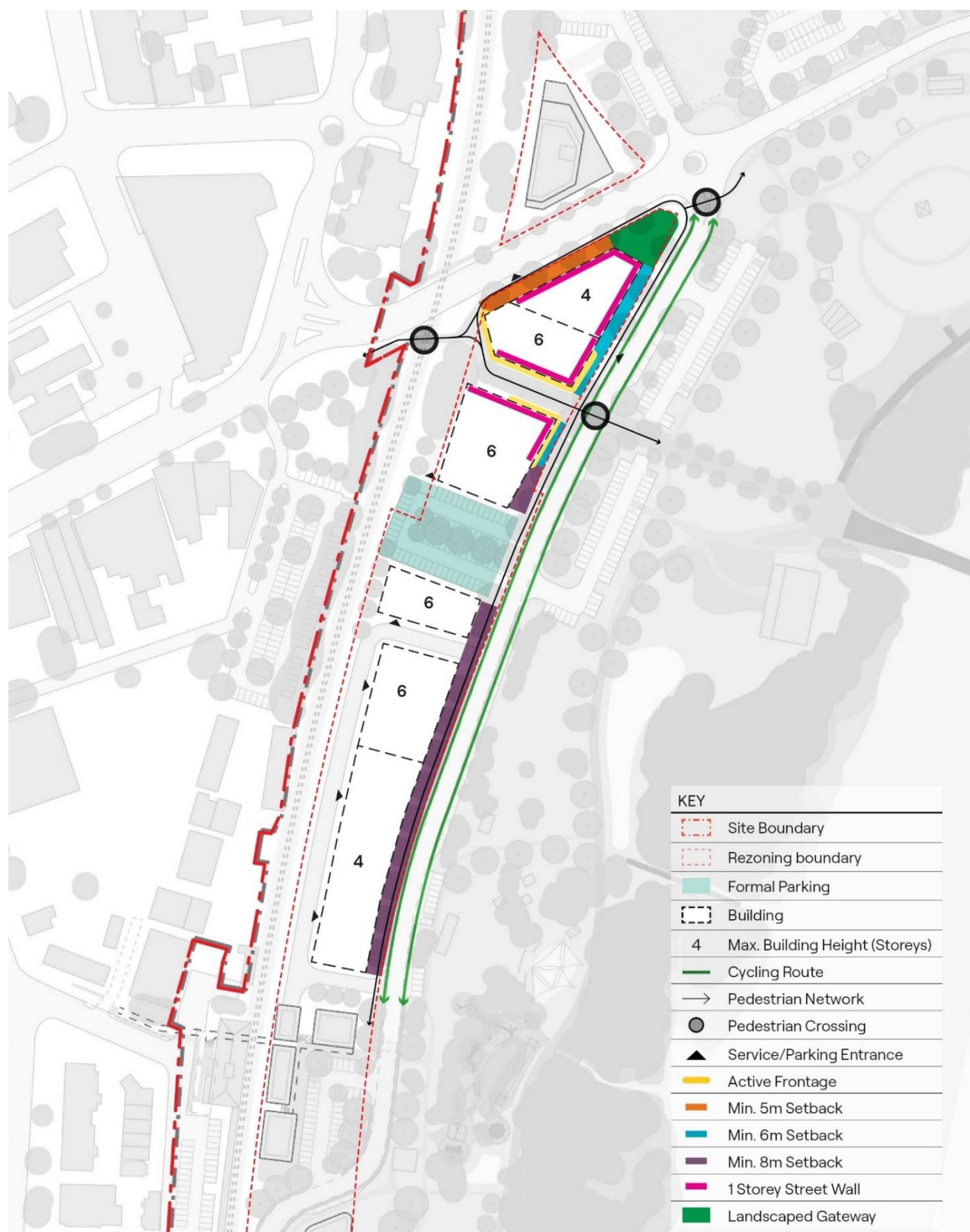
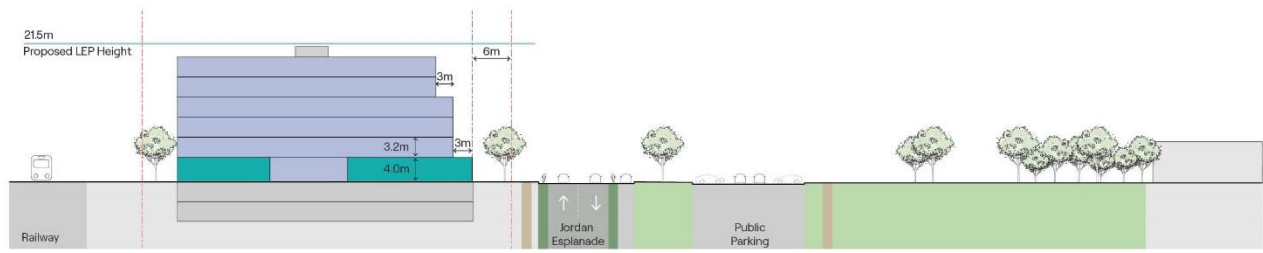
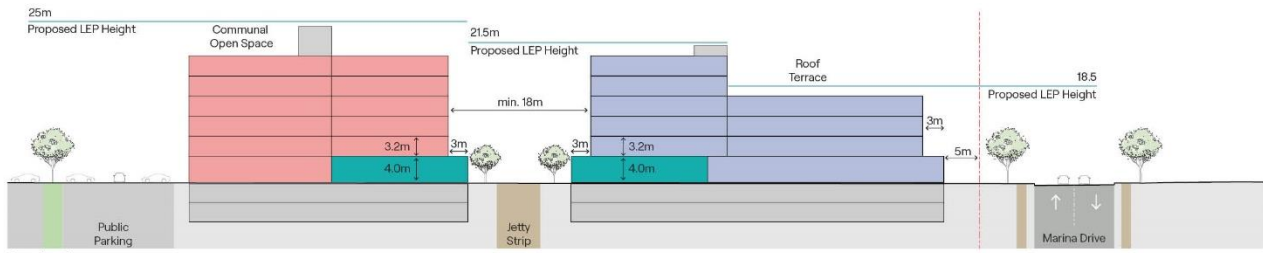


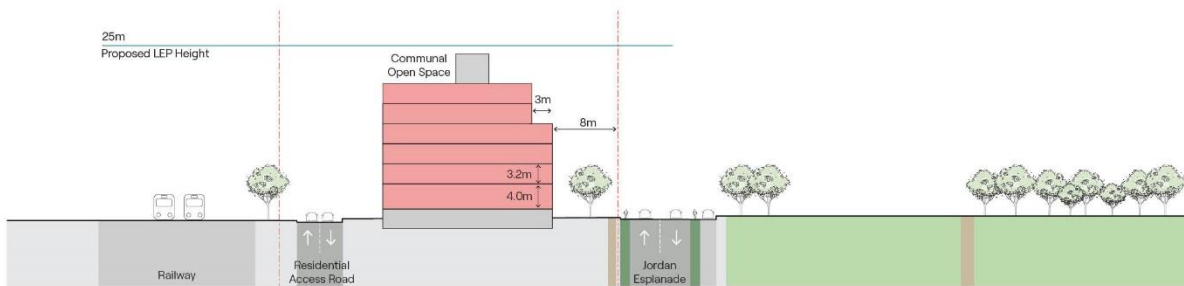
Figure 17 Jetty Hub sub-precinct plan



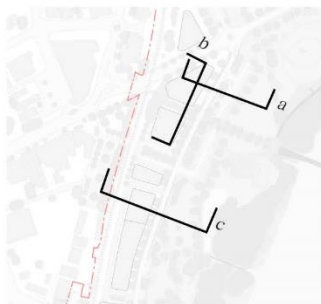
Section a



Section b



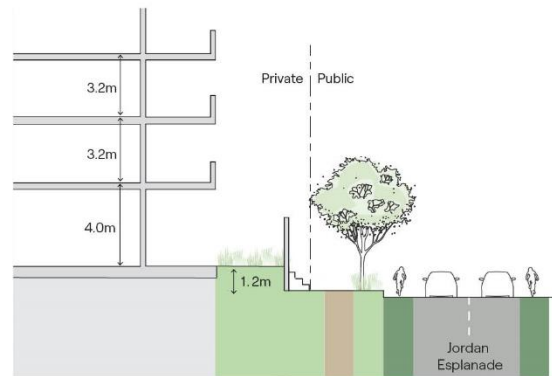
Section c



Section Key Plan

Key

- Residential
- Retail
- Accommodation
- Basement parking



Typical Residential/Street Interface

Figure 18 Jetty Hub indicative sections

4.3 Activity Hub and Village Green

Serving as the parkland core, this sub-precinct will act as a vital pivot point and will provide an additional east-west pedestrian link via a pedestrian bridge over the rail corridor. The introduction of new retail will create opportunities for local businesses and attract more visitors. Additionally, the abundance of open green spaces, complemented by pavilions for food and beverage, will foster community engagement, provide gathering spaces, and hosting events. The new boardwalk will enhance connectivity to Jetty Beach, offering improved access and enhancing the overall attractiveness and liveability of the area.

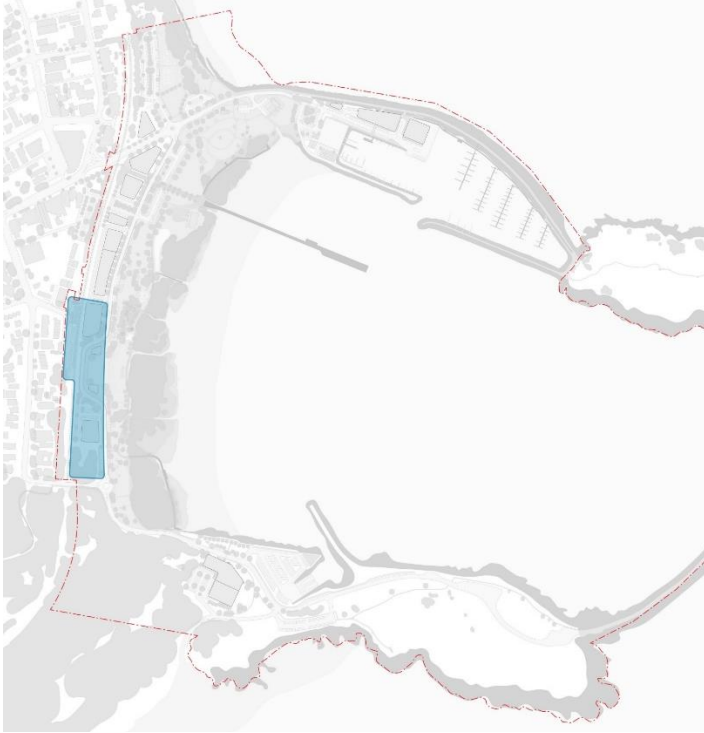


Figure 19 Activity Hub and Village Green Reference Plan

4.3.1 Objectives

1. To introduce new green spaces.
2. To retain and enhance existing landscaping and play areas through the Precinct.
3. To provide additional car parking near the train station.
4. To provide pedestrian connections and cycleways throughout the Precinct.
5. To provide direct pedestrian connection across the railway corridor.
6. To deliver new multi-use public amphitheatre and potential for new public pavilions to promote social gatherings and events.
7. To provide additional parkland amenities adjacent to existing open space.
8. To provide additional opportunity for local businesses to grow.
9. To provide character-appropriate retail fronts, pavilions and open green spaces that are nestled within the context.
10. To consider opportunities to recognise Gumbaynggirr culture and knowledge through promoting native practices and relationships with flora and fauna.
11. To promote environmental awareness and sustainability through connecting people with the landscape.
12. To provide for setbacks that support an active pedestrian environment to complement the proposed retail and community uses.

4.3.2 Requirements

1. Building form should be generally consistent with the layout, heights, setbacks and separation distances shown in **Figure 20** and **Figure 21**.
2. Active frontages are to be provided in accordance with **Figure 20**.
3. Public parking should be provided along Jordan Esplanade.
4. Pavilions within a parkland setting are to be open to the landscape, connecting indoor spaces to the green amphitheatre.

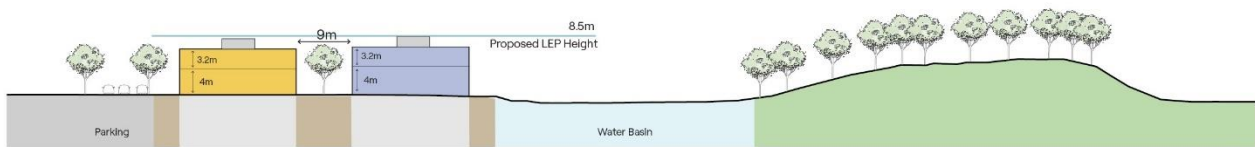
5. Any pedestrian overpass between the Precinct and Coffs Harbour Station must originate in a public plaza within the Precinct.
6. Any pedestrian overpass must be publicly accessible, safe and well-illuminated 24-hours a day, subject to appropriate residential amenity outcomes and allow for both pedestrian and bicycle access.



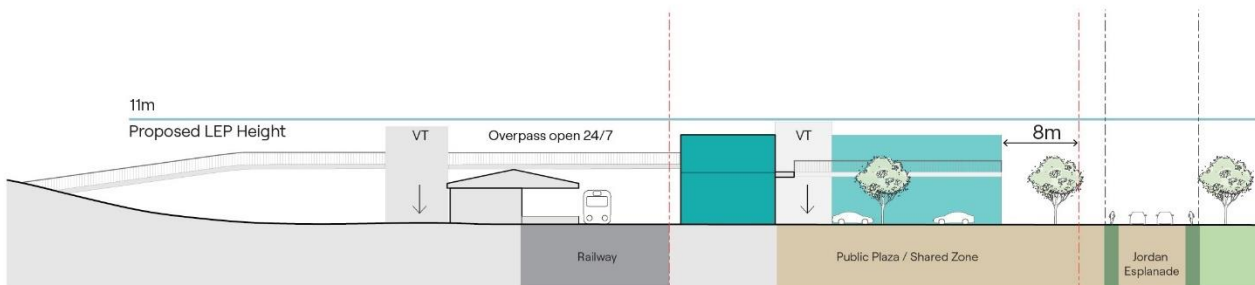
Figure 20 Activity Hub and Village Green sub-precinct plan



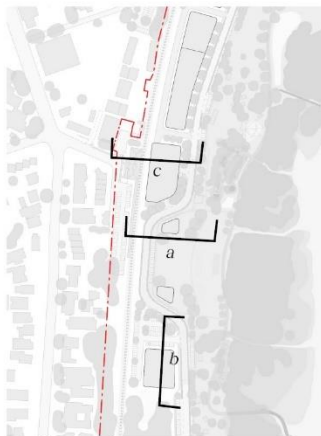
Section a



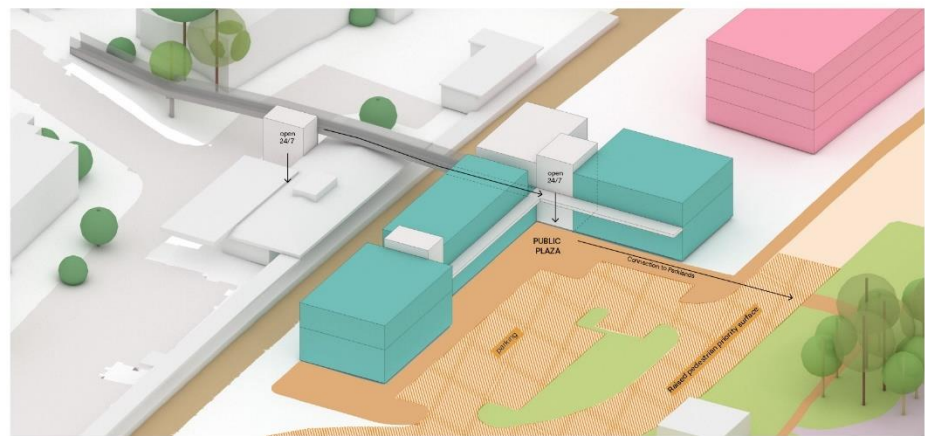
Section b



Section c



Section Key Plan



Bridge overpass detail

Figure 21 Activity Hub and Village Green indicative sections and overpass detail

4.4 Corambirra Point

Corambirra Point is to become a regional tourist destination integrated into an existing hill with captivating ocean views. This sub-precinct celebrates the preservation of sacred bushland and artifacts, honouring the area's significance to its Aboriginal custodians. The proposed tourist accommodation not only aims to boost economic activity but also offers visitors an immersive experience, allowing them to delve deeper into the area's rich history. This development strikes a balance between tourism and cultural preservation, enhancing the allure and educational value of the Precinct.

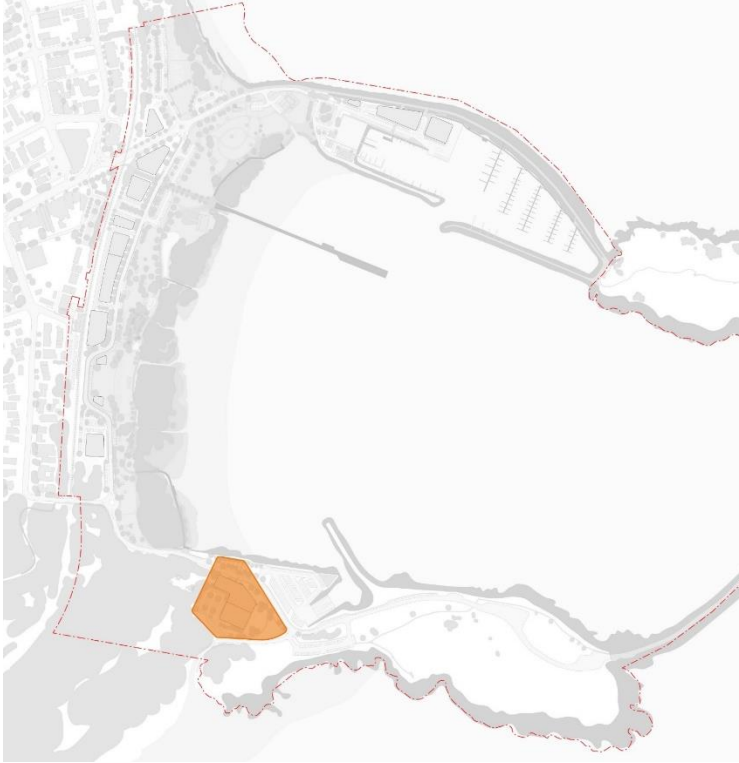


Figure 22 Corambirra Point reference plan

4.4.1 Objectives

1. To deliver a new tourist attraction for the region that is publicly accessible, includes function spaces and activities integrated into the landscape while maximising harbour and ocean views.
2. To regenerate and enhance landscape buffers to Ferguson's cottage.
3. To provide opportunities for traditional owners and local knowledge holders to be involved with the site.
4. To provide appropriate car parking with extensive tree canopy, subject to bushfire constraints.
5. To enhance pedestrian connections and cycleways in and around the site.
6. To provide opportunities for local business through new tourist accommodation, tourism activities and food and beverage spaces.
7. To provide character-appropriate built form that responds to its context and consider views to and from the site.
8. To provide for activated uses that invites pedestrians from Jordan Esplanade to enjoy views back towards the Harbour.

4.4.2 Requirements

1. Respect Ferguson's Cottage with minimal overlooking or disruption to accessibility and views.
2. The layout, siting and height in storeys of built form should be generally consistent with **Figure 23** and **Figure 24**.
3. A minimum 15m setback from Ferguson's Cottage must be provided.
4. Development must respond appropriately to bushfire constraints.
5. Building form is to respond to the surrounding topography and view lines.
6. Ensure design is context-appropriate, is unique to this Country and draws inspiration from surrounding water, landscape, local vernacular and coastal setting.

7. Active frontages should be provided in accordance with **Figure 23** with retail, food and beverage, and community spaces on the lower level of development.
8. Any carpark entrance should minimise impact on the public domain to avoid conflict.
9. Preserve and respect areas of heritage and cultural significance.

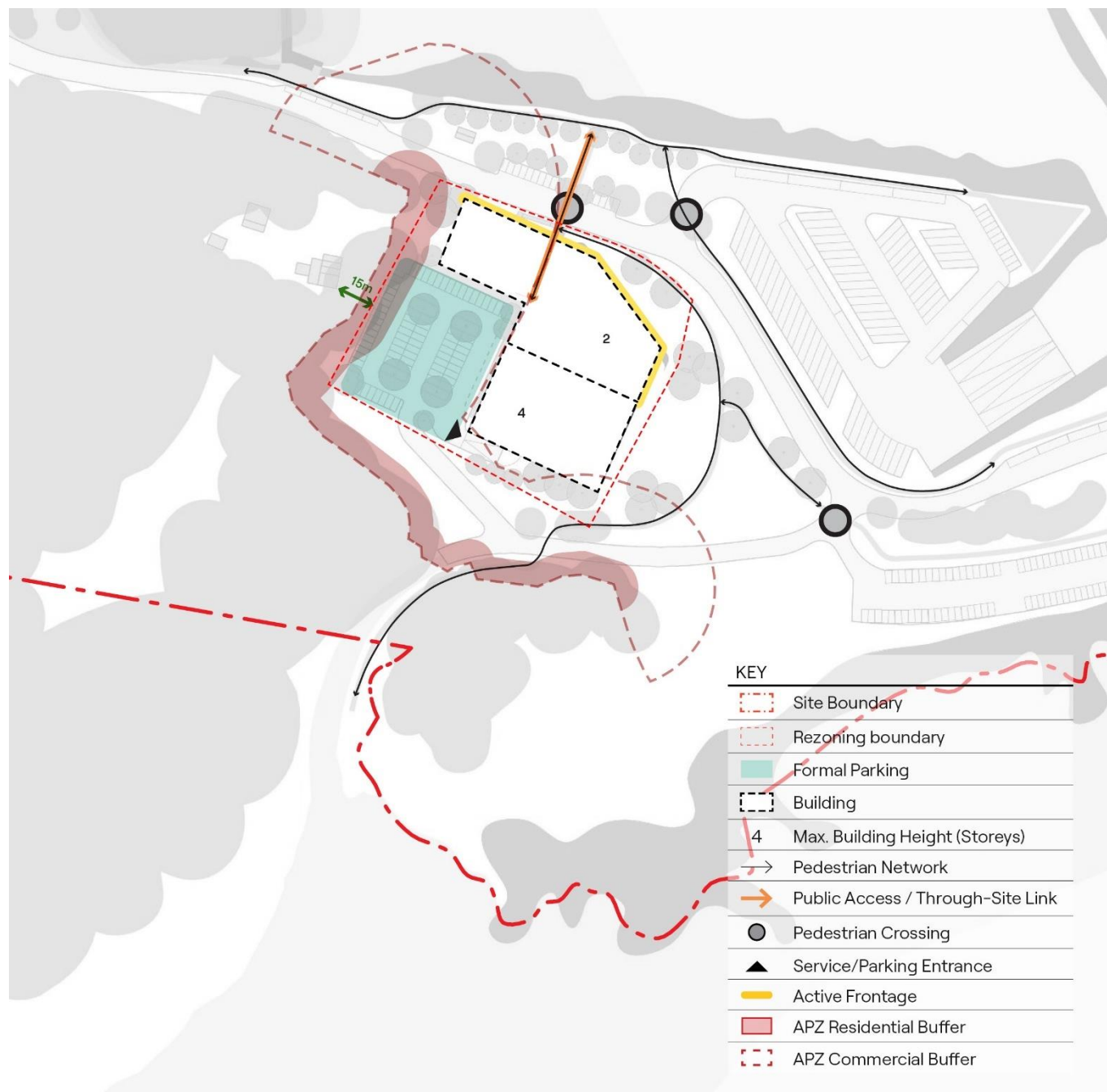
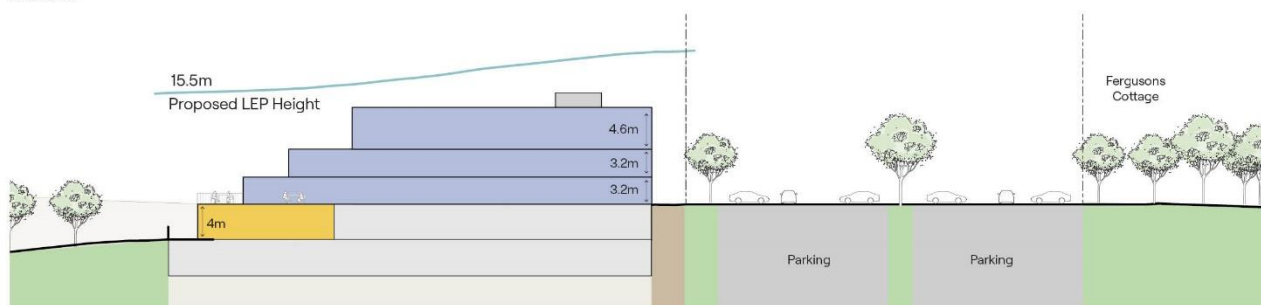
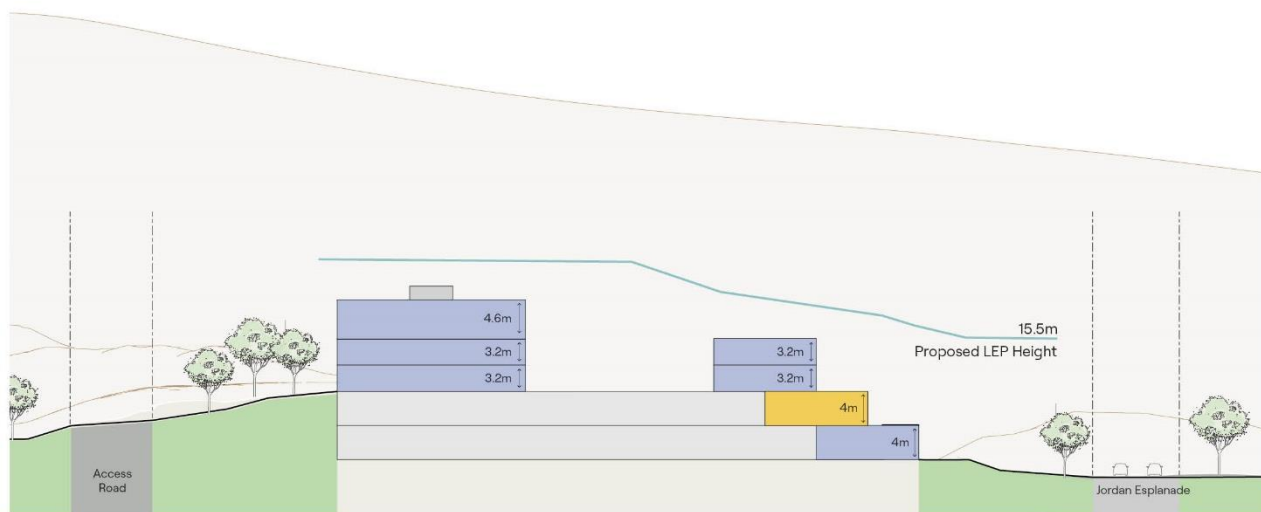


Figure 23 Corambirra Point sub-precinct plan



Key	
	Accommodation
	Community
	Retail
	Basement parking

Figure 24 Corambirra Point indicative sections

4.5 The Marina

The Marina is a dynamic and bustling sub-precinct that is anchored by its status as a working harbour. The Marina presents an alluring fusion of functionality and leisure. It is a thriving commercial and culinary hub, offering a newly improved Fish Co-op, commercial spaces and various dining options. With its blend of working waterfront activities, convenient on-grade parking, and improved access points, the Marina sets the stage for a captivating destination within the Coffs Harbour Jetty Foreshore.

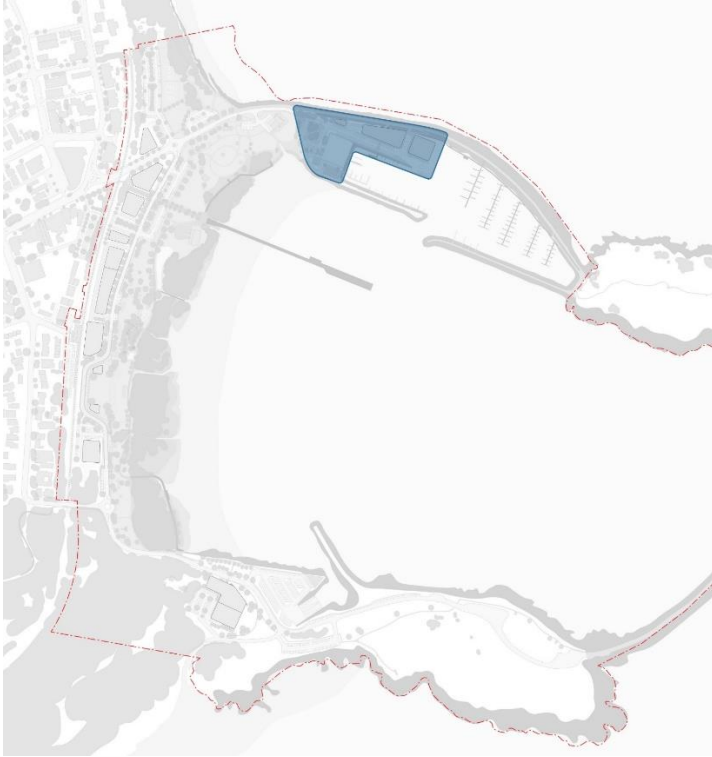


Figure 25 The Marina reference plan

4.5.1 Objectives

1. To maintain and enhance the existing working marina
2. To preserve sacred artefacts and sites including the site of the Lobster Story and Moon Rock.
3. To ensure any new development demonstrates connectivity to surrounding topography levels.
4. To provide additional car parking within building envelopes.
5. To enhance pedestrian connections and cycleways.
6. To facilitate the delivery of new cafes, commercial spaces and fish co-op upgrades.
7. To provide additional opportunities for local businesses through new commercial and retail spaces.
8. To provide amenity for both the local community and tourists.

4.5.2 Requirements

1. Building form should be consistent with the heights and setbacks shown in **Figure 26** and **Figure 27**.
2. Active frontages should be provided in accordance with **Figure 26**.
3. Buildings should be sited and designed to respond to the surrounding topography.
4. Development must not unreasonably inhibit or restrict the operations of the Marina.
5. Carpark entrances should minimise impact on the public domain.
6. Ensure development delivers a publicly accessible pedestrian connection that provides direct access into the Marina from the Coffs Harbour Marina Walkway.

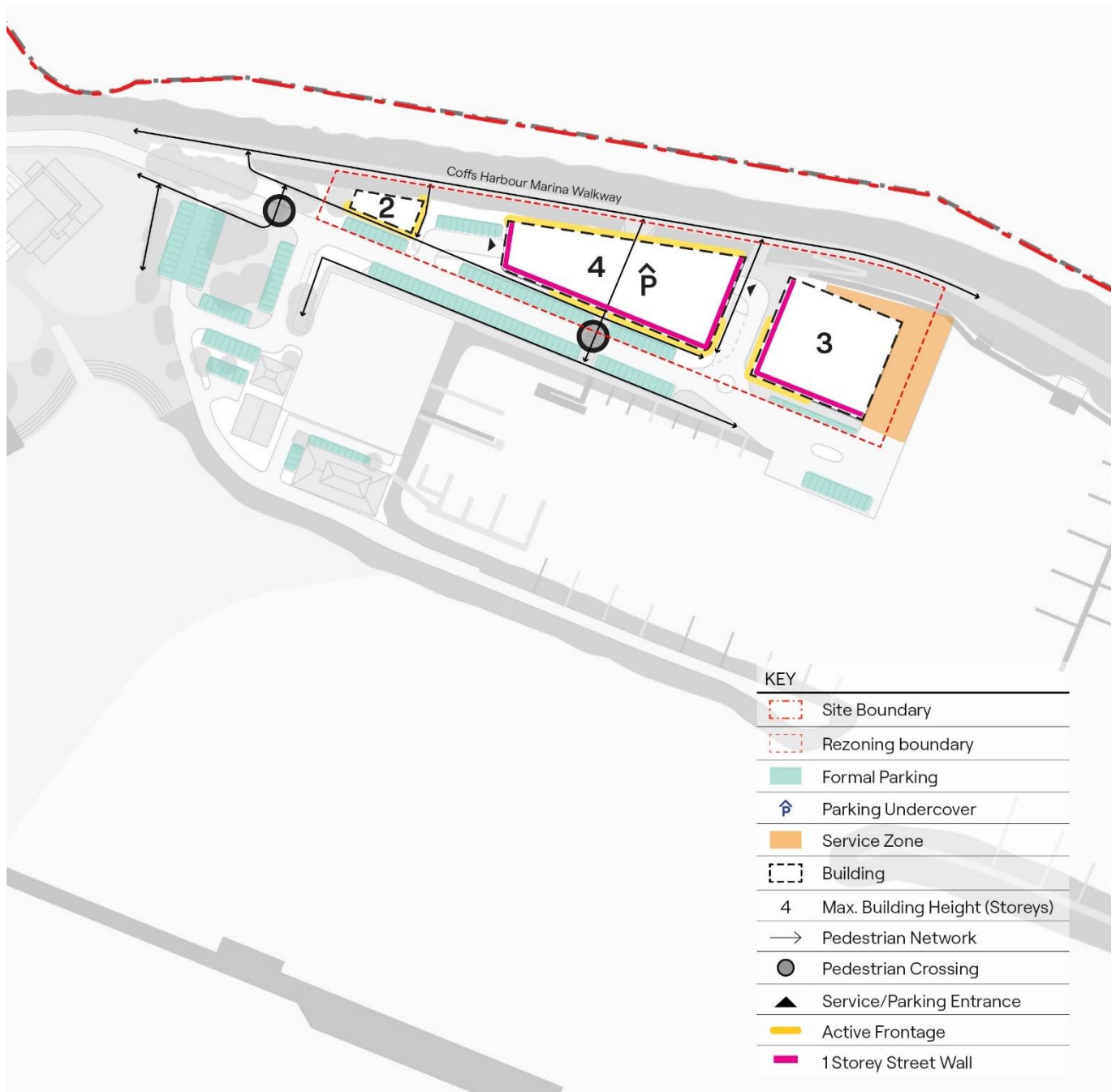


Figure 26 The Marina sub-precinct plan

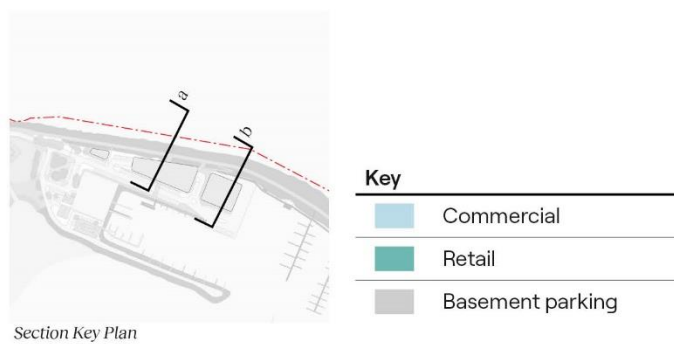
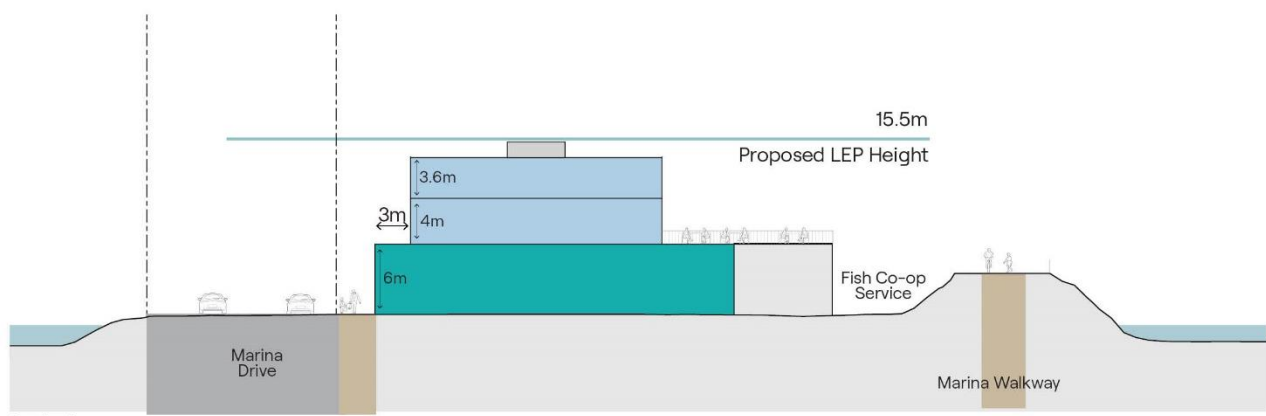
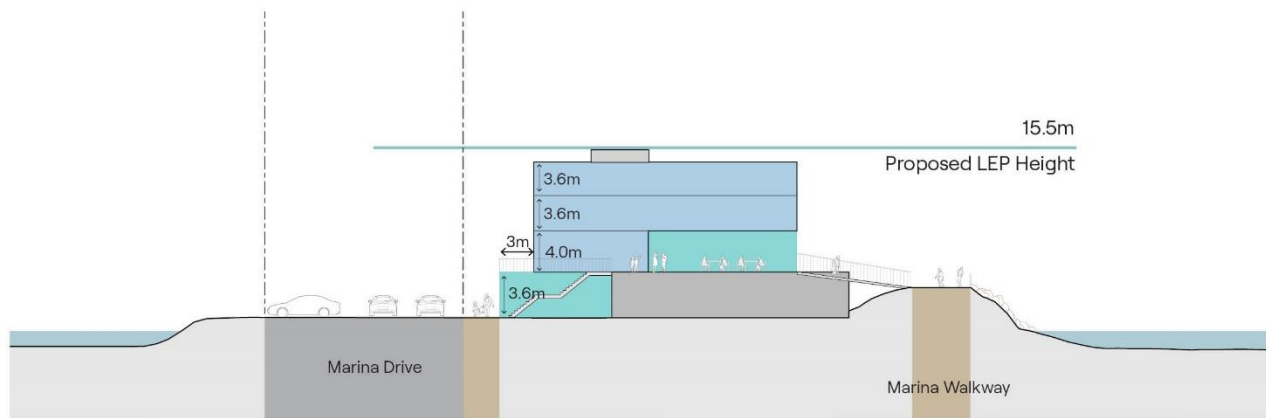


Figure 27 The Marina indicative sections

4.6 Foreshore Parklands

Amplifying the essence of Coffs Harbour's charm, the Foreshore Parklands showcases abundant green spaces and public amenities. It serves as the heart of the Foreshore, seamlessly blending with the local seascape character and emphasising preservation of the ecologies and coastal connectivity. The Masterplan promotes increased walkability and accessibility. A newly constructed boardwalk creates a direct link to the beach, complemented by the addition of playgrounds, waterplay areas, a vibrant green amphitheatre, and upgraded public amenities.



Figure 28 Foreshore Parklands reference plan

4.6.1 Objectives

1. To introduce new green space and an iconic regional playground.
2. To retain and enhance existing landscaping and play areas through the sub-precinct.
3. To protect existing native vegetation and introduce more planting through the sub-precinct.
4. To clean water as it moves throughout the landscape before it enters the ocean or other bodies of water.
5. To preserve and improve existing stormwater basins.
6. To protect and enhance wildlife habitats through regeneration and protection of native flora.
7. To create plaques commemorating key Aboriginal communities identities and their contributions to the broader community.
8. To limit access across dune vegetation to the beach.
9. To enhance pedestrian connections and cycleways.
10. To deliver new playgrounds, vibrant green spaces and waterplay areas, to provide amenity for both the local communities and tourists.
11. To deliver additional parkland amenities within existing open space.

4.6.2 Requirements

General

1. Minimise impacts of built form on existing vegetation areas.
2. Maximise retention of existing landscape plantings.
3. Excepting those contemplated in the structure plan outlined in **Section 2.4**, no new access paths through the dune vegetation to the beach are to be constructed.
4. Ensure all new planting comprises of indigenous species that are appropriate for the area (refer to Coffs Harbour Jetty Foreshore Precinct Public Domain and Landscape Manual).

5. Ensure dedicated stormwater basins through the parklands that allow for stormwater to be filtered before flowing into the harbour.
6. Ensure public domain elements respond to surrounding landscape by implementing softer edges.

Amphitheatre

7. Cultural events and practices accommodated at the amphitheatre should not unreasonably disturb or disrupt the amenity of surrounding residential uses.

Boardwalk

8. Any boardwalk must minimise impacts to the littoral rainforest and dune vegetation.
9. Any boardwalk must limit access points to the beach that disrupt existing vegetation areas.

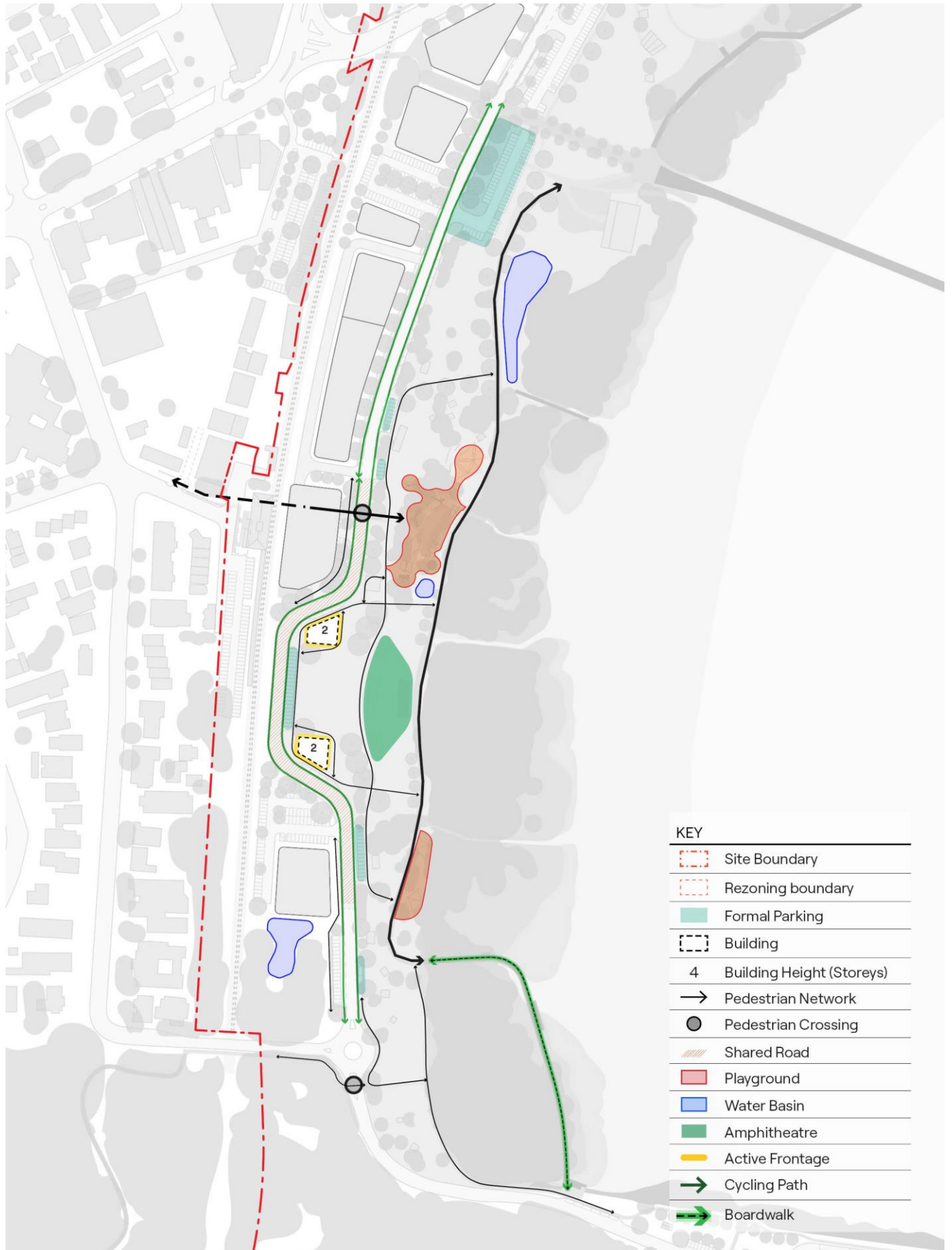


Figure 29 Foreshore Parklands sub-precinct plan