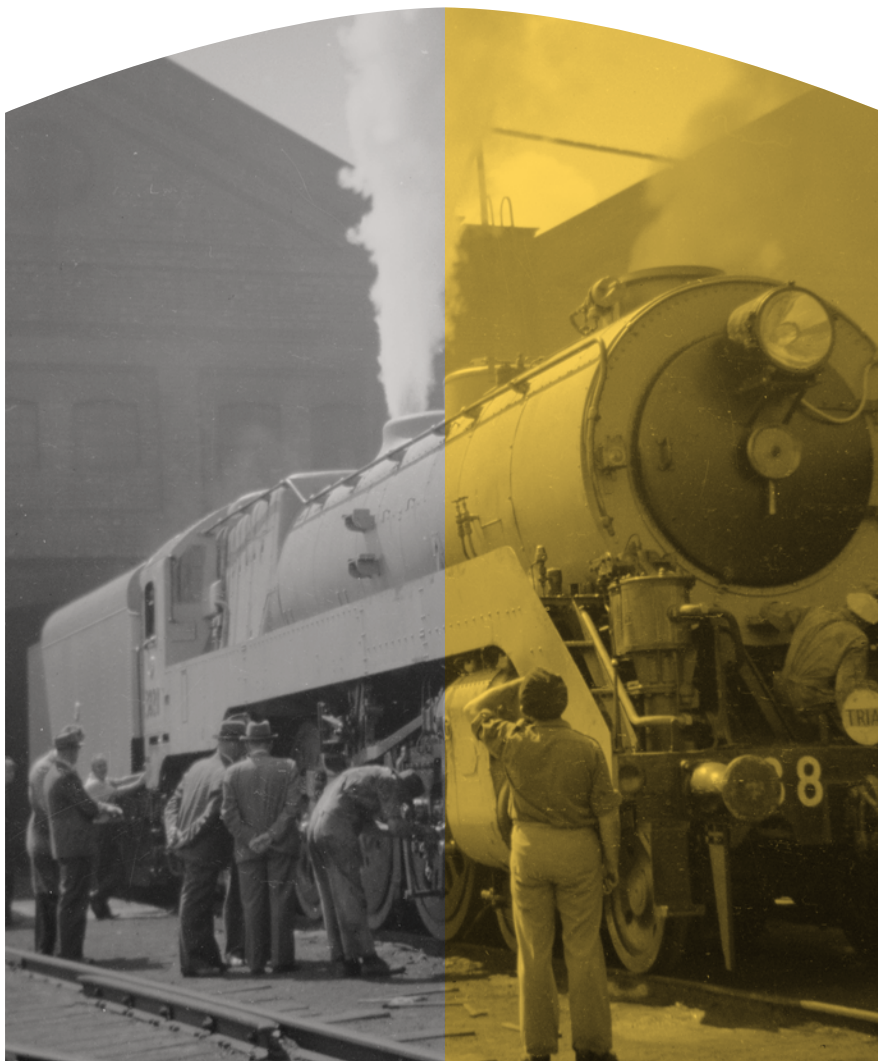


Large Erecting Shop Rezoning Proposal

Statement of Heritage Impact

Prepared for TAHE
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Executive Summary

Curio Projects Pty Ltd (Curio) has been commissioned by Transport Asset Holding Entity (TAHE) to prepare a Statement of Heritage Impact (SoHI) for the Large Erecting Shop (LES) Rezoning Proposal. The subject site sits within the Eveleigh Railway Workshops (ERW) curtilage.

The purpose of this report is to identify any potential heritage impact that the Rezoning Proposal may have on the values of the heritage item itself, as well as any impact that the proposal may have on other heritage items and conservation areas in the vicinity.

State-Led Rezoning Study Requirements

A request was made to the Department of Planning, Industry and Environment (DPIE) in February 2022 to request the NSW Minister for Planning and Public Spaces undertake a State-led rezoning to enable the adaptive reuse of the LES site. Specifically, a letter was sent to formally request that DPIE prepares *Study Requirements* to inform a future State Significant Precinct (SSP) Study for the LES site.

The following table provides a summary of the Study Requirements addressed by all heritage reports, including this SoHI, and where the relevant requirement is addressed.

Study Requirements	References
4. Heritage	
4.1 <i>Prepare an Integrated Aboriginal and Non-Aboriginal Cultural Heritage Study for the site that:</i>	Refer to the following reports: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Aboriginal Cultural Heritage Study & Statement of Impact: Large Erecting Shop – Rezoning Proposal</i>. Curio Projects, 2022. <i>Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal</i>.
<i>a) Undertakes Aboriginal Cultural Heritage Assessment (ACHA) for the site and surrounds including Aboriginal archaeology, culture, country, and intangible and social heritage, which:</i>	Refer to the following report: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Aboriginal Cultural Heritage Study & Statement of Impact: Large Erecting Shop – Rezoning Proposal</i>.
<ul style="list-style-type: none"> <i>Includes the results of consultation with relevant Aboriginal stakeholders and knowledge holders for the site and surrounds;</i> 	Refer to Chapter 3 (pages 29-31) of the above-mentioned report.
<ul style="list-style-type: none"> <i>Provides an overall Statement of Significance for Aboriginal values within and beyond the South Eveleigh precinct and recommendations to guide the protection, conservation and management of tangible and intangible Aboriginal values, in the context of the development</i> 	Refer to Chapters 7 (pages 72-77) and 10 (pages 95-97) of the above-mentioned report.
<ul style="list-style-type: none"> <i>Assesses the likely impact of the proposal on any identified Aboriginal cultural heritage;</i> 	Refer to Chapter 8 (pages 78-86) of the above-mentioned report.

Study Requirements	References
<ul style="list-style-type: none"> • Provides recommendations to guide the management of Aboriginal heritage significance, any items of significance and the likely impact on Aboriginal heritage as a result of the proposal; 	<p>Refer to Chapter 10 (pages 94-97) of the above-mentioned report.</p>
<p>b) Undertakes an assessment identifying all heritage items (state, local and potential) and conservation areas within and near the site, including built heritage, landscapes and archaeology, with detailed mapping of items and an assessment of why the items and Site(s) are of heritage significance;</p>	<p>Refer to Chapters 2 (pages 31-35) and 6 (pages 106-117) of the present report.</p> <p>Refer to Chapters 2 (pages 30-35) and 6 (pages 127-142) of the following report:</p> <ul style="list-style-type: none"> • Curio Projects, 2022. <i>Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal</i>.
<p>c) Provides recommendations to guide future development with specific consideration to the bulk height and scale of existing significant items within the South Eveleigh precinct, including its setting, context, streetscape and visual and physical character of the locality, broader Eveleigh Railway Workshops, surrounding conservation areas and heritage items. This should be integrated with the Urban Design Framework;</p>	<p>Refer to Chapter 9 (pages 174-177) of the present report.</p> <p>Refer to Chapter 8 (pages 153-158) of the following report:</p> <ul style="list-style-type: none"> • Curio Projects, 2022. <i>Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal</i>.
<p>d) Includes a Statement of Heritage Impact (SoHI) to assess the likely impact of the proposal on any identified non-Aboriginal cultural heritage, including to the broader State Heritage Register listed Eveleigh Railway Workshops, with recommendations for the management of the cultural and industrial heritage of the site and measures to avoid, minimise and mitigate heritage impacts;</p>	<p>Refer to the present report, in particular to Chapters 8 and 9 (pages 137-177).</p>
<p>e) Includes a comprehensive archaeological management plan and framework strategy to capture and collate all recent archaeological investigations and guide the management of potential archaeological resources.</p>	<p>Refer to Chapters 5 (pages 95-104) and Chapter 8 (pages 152-155) of the present report.</p> <p>Refer to Chapter 5 (pages 115-126) of the following report:</p> <ul style="list-style-type: none"> • Curio Projects, 2022. <i>Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal</i>.
<p>f) Informs and supports the preparation of the site planning framework.</p>	<p>Refer to Chapter 2 (pages 31-35) of the present report.</p> <p>Refer to Chapter 2 (pages 30-35) of the following report:</p> <ul style="list-style-type: none"> • Curio Projects, 2022. <i>Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal</i>. <p>Refer to Chapters 1 and 2 (pages 11-26) of the following report:</p> <ul style="list-style-type: none"> • Curio Projects, 2022. <i>Aboriginal Cultural Heritage Study & Statement of Impact: Large Erecting Shop – Rezoning Proposal</i>

Study Requirements	References
4.2 <i>Prepare a document that outlines an integrated approach to guide zoning and development proposals at North and South Eveleigh, including an updated Statement of Significance to inform the future opportunities and constraints for change that do not significantly diminish the heritage values of the SHR listed Eveleigh Railway Workshops.</i>	Refer to Chapter 8 (pages 156-168) of the present report. Also, refer to the following additional documents: <ul style="list-style-type: none"> • Otto Cserhalmi + Partners, 2022 (Curio Projects update). <i>ERW Overarching Conservation Management Plan</i>. • Curio Projects, 2022. <i>Overarching Opportunities & Constraints – ERW</i>.
4.3 <i>Prepare a Heritage Interpretation Strategy for the site that:</i>	Refer to the following report: <ul style="list-style-type: none"> • Curio Projects, 2022. <i>Stage 1 Heritage Interpretation Plan: Large Erecting Shop</i>.
<i>a) Recognises and celebrates Aboriginal connection to the site and addresses the full story of the place;</i>	Refer to Chapters 2 (pages 15, 21, and 33) and 4 (pages 37-38) of the following report: <ul style="list-style-type: none"> • Curio Projects, 2022. <i>Stage 1 Heritage Interpretation Plan: Large Erecting Shop</i>.
<i>b) Identifies key themes, social values, interpretive opportunities, measures and locations as an integral component of creating a unique and exciting destination as part of the broader State Heritage Register listed Eveleigh Railway Workshops item.</i>	Refer to Chapter 4 (pages 36-37) of the following report: <ul style="list-style-type: none"> • Curio Projects, 2022. <i>Stage 1 Heritage Interpretation Plan: Large Erecting Shop</i>.
<i>c) Provides the strategic direction for heritage interpretation having regard to the site's heritage significance (in particular the sites social, intangible, industrial and engineering values) and regard to the place's relationship with nearby heritage items, as an integral component of the development of detailed design;</i>	Refer to Chapters 2 (pages 15-33) and 4 (pages 36-37) of the following report: <ul style="list-style-type: none"> • Curio Projects, 2022. <i>Stage 1 Heritage Interpretation Plan: Large Erecting Shop</i>.
<i>d) Accounts for existing and planned (where possible) interpretive approaches as part of other projects within and in the vicinity of the site.</i>	Refer to Chapter 2 (pages 26-29) of the following report: <ul style="list-style-type: none"> • Curio Projects, 2022. <i>Stage 1 Heritage Interpretation Plan: Large Erecting Shop</i>.

Site Identification

The LES is an isolated building at the northwest of the South Eveleigh Precinct. The South Eveleigh Precinct is located approximately 200m to the southwest of Redfern Train Station and approximately 200m to the west of the future Sydney Metro Waterloo Metro Station. The South Eveleigh Precinct includes an overall area of approximately 13.2 hectares.

The LES site is currently legally described as being part of Lot 5, in Deposited Plan 1175706. This allotment also includes the North Eveleigh Precinct, the rail lines separating the North and South Eveleigh Precincts, and Redfern Railway Station. The LES building subdivision will form part of a future application.

The LES is owned by TAHE NSW and managed by Transport Heritage NSW. The LES ceased formal operation in 1985 and has been largely unoccupied since approximately 2017. It is currently being used as a maintenance facility for heritage locomotives but it requires significant capital investment to bring it up to the required standards for continued use and avoid further deterioration.

Project Overview

The NSW Government is committed to working with the local community to develop the biggest innovation district of its kind in Australia, being Tech Central. The South Eveleigh Precinct is a key neighbourhood within Tech Central, delivering workplaces and collaboration spaces that support the vision for a new tech and innovation ecosystem. The inclusion of the LES within the broader South Eveleigh Precinct has the potential to support further innovation, collaboration and jobs for the future.

Transport for NSW (TfNSW) is therefore seeking to adaptively use the LES for a mix of uses, including commercial office and retail premises. Such land uses are currently not permitted within the planning controls that apply to the LES building, which still reflects its former infrastructure function. As such, a State-led rezoning application is being proposed to the Department of Planning and Environment (DPE) to amend the planning controls that currently apply to the site under State Environmental Planning Policy (Precincts – Eastern Harbour City) 2021 (Precincts SEPP).

The proposed new planning controls are intended to facilitate the following:

- Alterations within the existing LES to convert the ground level into commercial office and retail premises,
- Creation of two new ‘internal’ storeys within the existing LES building envelope for use as commercial office premises,
- External upgrade and conservation work to the existing LES building to ensure it is fit for purpose and environmentally sustainable,
- Heritage interpretation and conservation work generally throughout the LES site,
- Services augmentation, and
- Publicly accessible space upgrades.

While external works are required, the proposal does not seek to significantly alter the existing building footprint of the LES. Further, it is proposed that the general form of the existing building and key architectural features of the existing building are retained in any future reuse of the building for commercial purposes, noting that the LES is part of the Eveleigh Railway Workshops complex included in the State Heritage Register.

Since 2015, Mirvac has successfully developed the South Eveleigh Precinct. With the completion of the Locomotive Workshop project, which also involved the adaptive use of industrial buildings listed on the State Heritage Register, it is now considered a logical time to adapt and integrate the LES into the broader technology precinct. TAHE is therefore preparing documentation to support the State-led rezoning application.

Summary of Heritage Impact

Overall, the Rezoning Proposal has been well considered with respect to the heritage items, values, and overall significance of the LES site and the wider ERW.

The following table presents a preliminary assessment of the potential heritage impacts associated with the proposal on the heritage fabric of the LES. Detailed design will take place during a future stage of the planning and redevelopment process and will require an accompanying additional heritage impact assessment (subsequent and additional to this report), to specifically respond to and assess its impacts, once developed.

Proposed Alterations	Summary of Heritage Impact
Change of use	<p>Moderate impact.</p> <p>The LES has been underutilised and inaccessible to the public for the past 5 years, which could lead to future neglect and decay. The proposed adaptive reuse creates an opportunity for the building to remain relevant and meet contemporary and future users' needs. The proposed change to commercial use, although a shift from its former industrial self, will ensure the building is relevant, highly activated and occupied once again. The proposal offers the opportunity to return the LES to being a busy, industrious workplace that will be utilised and appreciated on a daily basis by workers and visitors, alike.</p> <p>In addition, the proposal is consistent with the South Eveleigh vision for a new tech ecosystem that will increase jobs and employment and strengthen NSW's position and global recognition in the innovation and technology sector. The proposal is also aligned with the SEPP strategy and guidelines for a Business Park as it will enable a mix of uses/functions in one place to meet the day-to-day demands of workers in the immediate area and provide significant employment opportunities to the community¹.</p> <p>With the LES redevelopment also proposing meaningful, engaging, and carefully considered heritage interpretation initiatives, as well as retention and adaptive re-use of the heritage building and moveable items, the project seeks to renew life in this area of the site while increasing the longevity of the building and ensuring the facilitation of greater understanding of the ERW complex, its heritage values and important stories for future site users and the local community.</p>
Internal reconfiguration of the LES, including the addition of two new levels within the building to accommodate commercial and retail tenancies.	<p>Moderate impact.</p> <p>The repurposing of the LES from a decommissioned, inaccessible and unsafe industrial space into a lively and busy technical workplace, is sympathetic to the original use of the space and commensurate with the historical use of the building albeit a shift in the "technical" work undertaken.</p> <p>The proposed internal reconfiguration has been intentionally concept designed to include lightweight, primarily reversible mezzanine-style floors suitably distanced from the magnificent external walls of the building. This lightweight approach and reversible nature anticipate that in the future, new site users may again reinvent the use of the space and require the complete removal of the proposed commercial layout and services.</p> <p>The development proposes improved access to the space and has been designed to allow views to the remarkable industrial architecture of the building. The new design will create an active work environment that will recover a degree of the high-density use of the space. The reintroduction of workers into the building is commensurate with the intangible heritage significance of the space as a busy and industrious</p>

¹ NSW Department of Planning and Environment, 2021. *Employment Lands: 2021 report on business parks*

Proposed Alterations	Summary of Heritage Impact
	<p>workplace, whilst retaining views and access to the tangible heritage significance of the building's industrial built fabric and architecture.</p> <p>The proposal includes the insertion of two full-height atria to interrupt the solidity of the floorplate and provide samples of the original scale, form, and clear views of retained elements (e.g., cast-iron columns, beams, roof trusses, etc.). In addition, the two eastern bays, where the communal space will be located, will be kept intact and free from any insertions, allowing users to experience the original openness, scale, height and bulkiness of the LES. The area also creates opportunities to display items of the moveable collection as well as other interpretation initiatives. The future design will encourage open-plan fit-outs to minimise the construction of unnecessary partition walls and avoid the interruption of the open nature of the space.</p> <p>Further, the look and feel of the proposed design (subject to future detail) will be contemporary and industrial to ensure a respectful and sympathetic interaction between modern and heritage fabrics. Also, the significant physical heritage fabric of the LES building will be required to undergo conservation works throughout the proposed redevelopment. To minimise the heritage impact, the proposal includes the insertion of two full-height atria to interrupt the solidity of the floorplate and provide samples of the original scale, form, and clear views of retained elements (e.g. cast-iron columns, beams, roof trusses, etc.).</p>
<p>Alteration to heritage fabric (subject to future detail):</p> <ul style="list-style-type: none"> ▪ In Ground Pits ▪ Arch Windows ▪ Arch Doors ▪ Overhead Gantry Cranes ▪ Roof / Skylights 	<p>Minor to nil impact</p> <p>The impact of the intended alterations has been preliminarily assessed only, as the Rezoning proposal does not include any detailed design. The impact of such alteration varies from minor to nil as per the summarised assessment below:</p> <ul style="list-style-type: none"> • The original in-ground pits are highly significant elements due to their importance during the locomotive overhaul process. The pits present the opportunity of being reinterpreted and could be converted into unique displays to showcase salvaged items or be kept as exemplars of the work conditions at the time, in particular the ones located in the communal space. • The arch windows will be retained, and a condition assessment report of the existing original panes will need to be prepared prior to any future SSDAs and DAs for the site, in order to ensure that the character of the original windows is retained as much as possible and to meet current standards (e.g., natural light). It is recommended that only damaged glazing be replaced with clear and transparent glass. The intact frosted panels should be retained. The contrast between the two glasses (modern glazing and original) will create a clear distinction between historical and contemporary fabrics, similar to the arched windows on the Locomotive Workshop building, having an overall positive visual and physical impact on the heritage values of the LES. • All arched doors on the eastern and western facades have been replaced, deteriorated or had temporary repair work to keep them operational. Therefore, the proposal's intention will not have an

Proposed Alterations	Summary of Heritage Impact
	<p>adverse visual or physical heritage impact on the modern doors as they are not part of the original fabric of the building. It is recommended that the only original door left (Road 6 – east end) is either kept in place in an open position or reused as part of the Heritage Interpretation Strategy for the site.</p> <ul style="list-style-type: none"> • Due to the proposed subdivision of the internal space into three levels, the 7 existing overhead gantry cranes will be relocated from their current positions to meet head clearance requirements and in order to maximise the continuity of floorspace. Although subject to future detail, it can be anticipated that the cranes will be relocated to the full height atria and to the communal area at the eastern end of the building to not only meet the head clearance requirements but also to place them within areas that retain original characteristics of the heritage building (e.g. scale, openness, height, etc.) as well as house communal activities (e.g. lobbies, events, circulation). Therefore, the cranes will remain visually accessible to the public on a daily basis, having a neutral impact on the heritage values of the building. • The existing skylights have been introduced in 1997 and do not form part of the original fabric of the LES, not having any physical impact on the heritage values of the site. In addition, the proposal will have a positive visual impact on the overall significance of the LES as it will restore the original longitudinal configuration of the skylights.
Extension of Locomotive Street	<p>Moderate impact.</p> <p>The extension of Locomotive Street will have a positive moderate impact on the heritage significance of the LES as it will improve the public realm conditions and attract users to explore the full extent of the building, enhancing the visual connectivity between the subject site and the remaining South Eveleigh Precinct, in particular the Locomotive Workshop. Likewise, the creation of a new gateway at the western end of the precinct will help to activate and define it as a significant portal to South Eveleigh.</p>
Construction of a car park zone along Locomotive Street	<p>Minor impact.</p> <p>The proposed western extension of Locomotive Street will enable the creation of a small car parking area on the northern side of the street. The car parking will attend to the future demand for parking arising from the redevelopment of the subject site and its chosen location is consistent with the existing parking zone on the southern side of Locomotive Street.</p> <p>The location of the car parking will be to the west of the eastern public domain seating/public art zone to not interrupt the pedestrian experience along the street. The car park will be to the east of the new annexe (loading dock) and will help to soften the visual impact of the new volume, being assessed as an overall appropriate solution with a minor impact on the heritage significance of the site.</p>

Proposed Alterations	Summary of Heritage Impact
Landscaping and public domain improvements	<p>Moderate impact.</p> <p>Although the Rezoning Proposal does not include specific details about landscaping design, it is anticipated that alterations to the public domain will be done in accordance with best practice heritage and design principles that will ensure the alterations are sympathetic and have an overall positive visual impact on the significance of the heritage site.</p> <p>The improvements will be located along Locomotive Street and between the LES and the Locomotive Workshops in order to create a seamless connection with the public domain improvements throughout the South Eveleigh Precinct as well as to attract users and visitors to the site to explore the extended heritage setting.</p>
Construction of two annexes at the western end of the site to accommodate the Loading Dock and the Plant & Equipment Room	<p>Moderate impact.</p> <p>The new volume of the proposed Loading Dock will have a moderate visual impact on the heritage fabric and significance of the site as it will be located adjacent to the southern facade of the building (western end).</p> <p>The location of this new annexe has carefully considered the significant view lines between the LES and the wider precinct, in particular the Locomotive Workshop, and therefore has been positioned at the western end of the site, to the west of the proposed car park and public domain area destined for seating, interpretation products and public art, ensuring no views to those areas are blocked from Locomotive Street.</p> <p>Further, the proposed annexe that will accommodate the Plant & Equipment Room will be fully detached from the LES building in order to minimise the visual impact on the legibility of the southern façade. In addition, part of the plant & equipment will be installed on the roof to ensure the annexe's size is as minimal as possible.</p> <p>The modern external walls of the two annexes create the opportunity for future interpretation solutions and sympathetic materiality will be developed to help mitigate their impact while proposing an innovative and engaging treatment to their façades.</p>
Installation of part of the plant and equipment on the roof	<p>Minor impact.</p> <p>The installation of plant and equipment on the roof will interact with modern fabric only (i.e. roof sheeting) and therefore will not have any impact on the physical original fabric of the LES.</p> <p>Visually, the plant and equipment will be predominantly concealed from any significant view lines, as the proposal includes a set of conditions to allow for the items to be installed on the roof, including conditioning to not be higher than the roof apex, not be visible from the ground floor or have difficult access for maintenance purposes. Therefore, the proposed works are considered an appropriate solution and will help to reduce the size of the annexe on the ground floor.</p>

Recommendations

The following overarching recommendations have been provided to guide any future redevelopment within the subject site, with specific consideration given to bulk, height, and scale of the development against the existing significant elements of the LES site and the wider ERW.

1. *Heritage Building and Fabric*

Recommendations and preliminary principles regarding the concept design elements of the Rezoning Proposal that will require further development and careful consideration through the detailed design phase in order to reduce or avoid the impact to heritage fabric and significance include the following:

- The proposed redevelopment should be recessive, deferential, and respectful to the original fabric, form, and industrial character of the LES.
- External additions/annexes to the subject site should be avoided where possible. If necessary (e.g. plant and equipment annexe), their design, final location and facade treatment should be developed in consultation with an experienced and qualified heritage specialist to minimise impacts where possible. The final design should ensure the significant view lines to the LES remain unobstructed and sympathetic materiality should be selected to ensure a respectful interaction between modern and heritage fabrics. Interpretive solutions to re-purpose and include the new additions/annexes as part of the interpretation strategy for the precinct should be explored as opportunities to mitigate their visual impact on the overall significance of the site.
- Treatment of the remnant rail tracks, in ground pits, overhead gantry cranes, and other original elements within the site
- Details and design of future adaptive re-use of the LES, including the proposal for management, conservation, and treatment of associated moveable heritage collection contained within—including additional surveys and structural assessment to inform future design constraints and opportunities with respect to form and condition.

2. *Future and Detailed Design*

Recommendations and preliminary principles regarding future detailed design for the LES include the following:

- Detailed design should be developed in close consultation with an appropriately experienced and qualified heritage consultant to provide heritage advice and input throughout the detailed design process, especially with respect to the key additions, changes, and modifications proposed to elements that have potential to have an adverse physical and/or visual impact to heritage items and significance, that will require minimisation and mitigation through careful, sensitive, and sympathetic design solutions.
- Adaptive reuse of the heritage building (specifics to be detailed through future detailed design phases) should be undertaken in a sympathetic, reversible, and sensitive manner that ensures the original use of the building continues to be understood and communicated to audiences through retention of as much of a building's original fabric and internal layout as possible, supported by complementary heritage interpretation initiatives where appropriate. Where modern additions must be incorporated into the significant heritage item, the design of these elements should adhere to the Burra charter principles of 'as much as necessary, as

little as possible', and should complement the original structure and form while also using contrasting but sympathetic materials to ensure that modern additions can be readily distinguished from the original form and fabric.

- Detailed design of the Rezoning Proposal elements – presumably to be undertaken through future stages of the planning and redevelopment process – will require the preparation of future heritage impact assessments (subsequent and additional to this SoHI) to specifically respond to and assess the impacts of the detailed design, once developed.
- New additional volume to the southern facade should consider the visual connectivity between the LES and Locomotive Workshop, as well as the significant view lines of the LES along Locomotive Street, Rowley Lane and when approaching the building from the South East Village Square, to ensure that the key physical and visual attributes of the heritage items are respected, prominent, and not obstructed.
- New elements should adopt appropriate materiality and colour palettes commensurate with the surrounding heritage fabric and context of the South Eveleigh Precinct.
- Investigate opportunities for reuse and recycling of materiality and fabric of little significance (e.g., rolling stock parts, memorabilia, etc.).

3. *Landscaping & Public Domain*

The detailed design of the landscaping and public domain should:

- Ensure a seamless connection between the LES and the South Eveleigh Precinct, proposing cohesive elements throughout Locomotive Street and in-between the LES and Locomotive Workshop to extend the overall look and feel (e.g. materiality, furniture, interpretation, etc.) to the subject site.
- Avoid over-landscaping, especially along the southern and eastern facades, as it would risk a reduction of the legibility of the significant fabric of the LES.
- Develop appropriate lighting solutions to improve the visibility of the façade during daytime and nighttime, highlighting the original and interpretive elements, and activating the public domain.
- Include inclusive, durable, safe, and inviting elements to help active the precinct by attracting visitors to the site. This includes the development of innovative and engaging interpretation and public art products that will help to retell significant stories related to the subject site and are consistent with the interpretation strategy developed for the South Eveleigh precinct.
- Any proposed car parking areas should not overtake the public domain area along Locomotive Street or interrupt the pedestrian route towards the LES building or any interpretive/public art element.

4. *Moveable Heritage*

The scope of the Rezoning Proposal does not include specific details about the proposed treatment, management, and incorporation of the significant moveable heritage collection associated with the LES. It will be required to be addressed in future detailed design phases and is also addressed as part of the Heritage Interpretation Plan (Curio, 2022).

Recommendations regarding the future management of the moveable heritage collection include:

- Transference and placement of rolling stock and heritage moveable items require careful consideration prior to being undertaken. An experienced and qualified heritage consultant commissioned by TAHE should manage the process, taking into consideration the strategies and guidelines provided by the Heritage Interpretation Plan (Curio, 2022) prepared for the site.
- Undertake a comprehensive moveable heritage survey and establish a current Moveable Collections Catalogue that records and catalogues all moveable heritage items across the LES, and updates relevant heritage listings with the details of this updated inventory. The inventory should form a sub-section of the broader South Eveleigh Moveable Heritage Catalogue being prepared by Curio and ICS and should be included as a sub-section of the Moveable Collections Management Plan for South Eveleigh (currently in progress).
- Include moveable heritage conservation and long-term storage into any detailed development planning for the site so that the immediate, medium and long-term costs associated with keeping and conserving this significant resource can be built into the project costs and ongoing asset maintenance costs.

5. *Historical Archaeology*

As the LES site is adjacent to South Eveleigh and both are elements of the same overarching site of the former ERW, a consistent approach will be applied to the management of archaeological resources. Therefore, it is proposed that a similar differentiation between ‘relics’ and ‘works’ (where relevant) be applied to the subject site (the context in which archaeological relics and works have been successfully managed at South Eveleigh previously), as while both relics and works may have the potential to be present, the way in which they are required to be managed may differ.

Historical archaeology within the LES study area should be managed in accordance with the archaeological potential and management framework outlined in Section 5 of this report, which should be further developed through a future Archaeological Research Design (ARD), specific to the impacts and final design of the precinct renewal development. It should also be managed in accordance with the archaeological provisions of the Draft South Eveleigh Precinct CMP.

Archaeological works carried out within the study area should be undertaken under a Section 60 Excavation Application under the NSW Heritage Act 1977, depending on the assessed significance of the potential archaeological deposit and location in question. The appropriate approvals pathway (i.e., a s60 Application, or a s60 Fast Track Application) would depend on the nature and extent of works proposed, and the corresponding impact to potential historical archaeological resources. This would be determined and confirmed during the preparation of the ARD.

Archaeological mitigation program developed through the ARD should be specific to the location and nature of ground penetrating works at the site, but may include:

- Targeted archaeological test excavation undertaken in the key areas identified as having moderate-high archaeological potential before construction works commence
- Unexpected finds protocol.
- Archaeological monitoring in areas with nil - low archaeological potential.

- Archival recording, surveying, photogrammetry, and 3D scanning should be performed to selected features for future heritage interpretation products if any archaeological features are uncovered during monitoring and testing.
- Significant archaeology uncovered will need to be incorporated into the heritage interpretation on site.

Landscaping may also be redesigned to avoid physical impacts on the archaeological resource by changing the nature of the intended planting, i.e., low impact shallow-rooted bushes rather than mature trees with the potential to create future damage through expansion of the root ball. Where it is not possible to avoid archaeology through redesign or use of previously disturbed areas, further archaeological assessment and investigation will be necessary.

Where possible, detailed design options and solutions should be investigated to reposition structural and built elements that have potential to impact archaeology, for example, minor relocation of support columns or options to span across areas identified as having high archaeological potential with two or more columns. A similar avoidance approach may also be employed with the installation of services, either avoiding areas of the archaeological resource and redesigning the route of the services or identifying existing service corridors, trenches etc and laying new services within areas that have previously been subject to excavation disturbance.

6. Heritage Interpretation

Heritage interpretation strategies as identified within the Stage 1 HIP will support the development of innovative interpretation elements within the LES site that will convey an inclusive account of the site's rich history, its function within the Eveleigh Railway Workshop as well as accounts from the local community, both past and present.

Heritage interpretation initiatives throughout the LES site should be engaging, authentic, relevant and appropriate to ensure that they not only continue to celebrate the significance of the subject site, but also the highly significant ongoing Aboriginal cultural heritage connection to the place.

Terminology & Abbreviations

The terms below used during the course of this report are defined as per the *Australia ICOMOS Charter for the Conservation of Places of Cultural Significance* (the Burra Charter) 2013, Article 1.1 to 1.17:

1. **Place** means a geographically defined area. It may include elements, objects, spaces, and views. Places may have tangible and intangible dimensions.
2. **Cultural Significance** means aesthetic, historic, scientific, social or spiritual value for past, present or future generations. *Cultural significance* is embodied in the *place* itself, its *fabric*, *setting*, *use*, *associations*, *meanings*, *records*, *related places* and *related objects*. Places may have a range of values for different individuals or groups.
3. **Fabric** means all the physical material of the place including elements, fixtures, contents and objects.
4. **Conservation** means all the processes of looking after a *place* so as to retain its *cultural significance*.
5. **Maintenance** means the continuous protective care of a place and its setting. Maintenance is to be distinguished from repair, which involves *restoration* or *reconstruction*.
6. **Preservation** means maintaining a *place* in its existing state and retarding deterioration.
7. **Restoration** means returning a *place* to a known earlier state by removing accretions or by reassembling existing elements without the introduction of new material.
8. **Reconstruction** means returning a place to a known earlier state and is distinguished from *restoration* by the introduction of new material.
9. **Adaptation** means changing a *place* to suit the existing *use* or a proposed use.
10. **Use** means the functions of a *place*, including the activities and traditional and customary practices that may occur at the place or are dependent on the place.
11. **Compatible use** means a *use* that respects the *cultural significance* of a place. Such use involves no, or minimal, impact on *cultural significance*.
12. **Setting** means the immediate and extended environment of a *place* that is part of or contributes to its *cultural significance* and distinctive character.
13. **Related place** means a *place* that contributes to the *cultural significance* of another place.
14. **Related object** means an object that contributes to the *cultural significance* of a *place* but is not at the place.
15. **Interpretation** means all the ways of presenting the *cultural significance* of a *place*.

Abbreviation	Explanation
ARD	Archaeological Research Design
ARHS	Australian Railway Historical Society
CMP	Conservation Management Plan
DPIE	Department of Planning, Industry and Environment
EP&A Act	Environmental Planning and Assessment Act 1979
EPBC Act	Environmental Protection and Biodiversity Act 1999
ELW	Eveleigh Locomotive Workshops
ERW	Eveleigh Railway Workshops
Heritage Act	Heritage Act 1977
HCA	Heritage Conservation Area
HIP	Heritage Interpretation Plan
ICOMOS	International Council on Monuments and Sites
LEP	Local Environmental Plan
LES	Large Erecting Shop
LGA	Local Government Area
NLA	National Library of Australia
RL	Reduced Level
RNE	Register of the National Estate
SCA	Sydney City Archives
SHI	State Heritage Inventory
SLNSW	State Library of New South Wales
SHR	State Heritage Register
SoHI	Statement of Heritage Impact
SSP	State Significant Precincts
SSP SEPP 2005	State Environmental Planning Policy (State Significant Precincts) 2005
Sydney LEP 2012	Sydney Local Environment Plan 2012
TAHE	Transport Asset Holding Entity
TEC	Telecommunications Equipment Centre
TfNSW	Transport for NSW
VIA	Visual Impact Assessment

1. Introduction

1. Introduction

1.1. The Purpose of this Report

Curio Projects Pty Ltd (Curio) has been commissioned by Transport Asset Holding Entity (TAHE) to prepare a Statement of Heritage Impact (SoHI) for the Large Erecting Shop (LES) Rezoning Proposal. The subject site sits within the Eveleigh Railway Workshops (ERW) curtilage.

The purpose of this report is to identify any potential heritage impact that the Rezoning Proposal may have on the values of the heritage item itself, as well as any impact that the proposal may have on other heritage items and conservation areas in the vicinity.

This SoHI has considered the impacts of the proposed development application in accordance with the relevant Heritage NSW guidelines and has been prepared in reference to the following project documents:

- Mirvac, 2022. Project Brief (LES) - Summarised high-level brief for Rezoning.
- FJMT Studio, 2022. *Large Erecting Shed, South Eveleigh / Rezoning Report*. Prepared for TAHE.
- Curio Projects, 2022. *Aboriginal Cultural Heritage Study & Statement of Impact: Large Erecting Shop – Rezoning Proposal*. Prepared for TAHE.
- Curio Projects, 2022. *Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal*. Prepared for TAHE.
- Curio Projects, 2022. *Stage 1 Heritage Interpretation Plan: Large Erecting Shop*. Prepared for TAHE.
- Curio Projects, 2018. *Heritage Risk Assessment - LES Redevelopment*. Prepared for Mirvac.

Additional heritage documents that provide the management principles, conservation policies, opportunities and constraints to the overall ERW site have been used to guide the preparation of this report. These include:

- Otto Cserhalmi + Partners, 2022 (Curio Projects update). *ERW Overarching Conservation Management Plan*, Prepared for UrbanGrowth NSW.
- Curio Projects, 2022. *Overarching Opportunities & Constraints – ERW*. Prepared for Transport for NSW.

The following documents have also been utilised for research purposes, however, they have been superseded by the above-mentioned reports:

- Simpson Dawbin Associates Architects and Heritage Consultants, 2003. *Large Erecting Shop: Conservation Management Plan*. Prepared for Rail Estate.
- Otto Cserhalmi + Partners, 2002. *Eveleigh Locomotive Workshops CMP*. Prepared for Sydney Harbour Foreshore Authority.

Further, the following reports related to the adjacent South Eveleigh Precinct (former Australian Technology Park) have also been consulted and utilised to guide the preparation of this report as the LES is proposed to be incorporated into the wider precinct and as agreed to with Heritage NSW in 2022:

- Curio Projects, 2022. *South Eveleigh Precinct - Conservation Management Plan (Draft in progress)*. Prepared for Mirvac.
- Godden Mackay Logan, 2013. *Australian Technology Park – Conservation Management Plan. Volume 1*.

1.2. State-Led Rezoning Study Requirements

A request was made to the Department of Planning, Industry and Environment (DPIE) in February 2022 to request the NSW Minister for Planning and Public Spaces undertake a State-led rezoning to enable the adaptive reuse of the LES site. Specifically, a letter was sent to formally request that DPIE prepares *Study Requirements* to inform a future State Significant Precinct (SSP) Study for the LES site.

Table 1.1 provides a summary of the Study Requirements addressed by all heritage reports, including this SoHI, and where the relevant requirement is addressed.

Table 1.1: Study Requirements – Heritage

Study Requirements	References
4. Heritage	
4.1 <i>Prepare an Integrated Aboriginal and Non-Aboriginal Cultural Heritage Study for the site that:</i>	Refer to the following reports: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Aboriginal Cultural Heritage Study & Statement of Impact: Large Erecting Shop – Rezoning Proposal</i>. Curio Projects, 2022. <i>Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal</i>.
<i>a) Undertakes Aboriginal Cultural Heritage Assessment (ACHA) for the site and surrounds including Aboriginal archaeology, culture, country, and intangible and social heritage, which:</i> <ul style="list-style-type: none"> <i>Includes the results of consultation with relevant Aboriginal stakeholders and knowledge holders for the site and surrounds;</i> <i>Provides an overall Statement of Significance for Aboriginal values within and beyond the South Eveleigh precinct and recommendations to guide the protection, conservation and management of tangible and intangible Aboriginal values, in the context of the development</i> <i>Assesses the likely impact of the proposal on any identified Aboriginal cultural heritage;</i> <i>Provides recommendations to guide the management of Aboriginal heritage significance, any items of significance and the likely impact on Aboriginal heritage as a result of the proposal;</i> 	Refer to the following report: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Aboriginal Cultural Heritage Study & Statement of Impact: Large Erecting Shop – Rezoning Proposal</i>. Refer to Chapter 3 (pages 29-31) of the above-mentioned report. Refer to Chapters 7 (pages 72-77) and 10 (pages 95-97) of the above-mentioned report. Refer to Chapter 8 (pages 78-86) of the above-mentioned report. Refer to Chapter 10 (pages 94-97) of the above-mentioned report.
<i>b) Undertakes an assessment identifying all heritage items (state, local and potential) and conservation areas within and near the site, including built heritage, landscapes and archaeology, with detailed mapping of items and an assessment of why the items and Site(s) are of heritage significance;</i>	Refer to Chapters 2 (pages 31-35) and 6 (pages 106-117) of the present report. Refer to Chapters 2 (pages 30-35) and 6 (pages 127-142) of the following report: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal</i>.
<i>c) Provides recommendations to guide future development with specific consideration to the bulk height and scale of existing significant items within the South Eveleigh</i>	Refer to Chapter 9 (pages 174-177) of the present report.

Study Requirements	References
<i>precinct, including its setting, context, streetscape and visual and physical character of the locality, broader Eveleigh Railway Workshops, surrounding conservation areas and heritage items. This should be integrated with the Urban Design Framework;</i>	Refer to Chapter 8 (pages 153-158) of the following report: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal</i>.
<i>d) Includes a Statement of Heritage Impact (SoHI) to assess the likely impact of the proposal on any identified non-Aboriginal cultural heritage, including to the broader State Heritage Register listed Eveleigh Railway Workshops, with recommendations for the management of the cultural and industrial heritage of the site and measures to avoid, minimise and mitigate heritage impacts;</i>	Refer to the present report, in particular to Chapters 8 and 9 (pages 137-177) .
<i>e) Includes a comprehensive archaeological management plan and framework strategy to capture and collate all recent archaeological investigations and guide the management of potential archaeological resources.</i>	Refer to Chapters 5 (pages 95-104) and Chapter 8 (pages 152-155) of the present report. Refer to Chapter 5 (pages 115-126) of the following report: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal</i>.
<i>f) Informs and supports the preparation of the site planning framework.</i>	Refer to Chapter 2 (pages 31-35) of the present report. Refer to Chapter 2 (pages 30-35) of the following report: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal</i>. Refer to Chapters 1 and 2 (pages 11-26) of the following report: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Aboriginal Cultural Heritage Study & Statement of Impact: Large Erecting Shop – Rezoning Proposal</i>
4.2 <i>Prepare a document that outlines an integrated approach to guide zoning and development proposals at North and South Eveleigh, including an updated Statement of Significance to inform the future opportunities and constraints for change that do not significantly diminish the heritage values of the SHR listed Eveleigh Railway Workshops.</i>	Refer to Chapter 8 (pages 156-168) of the present report. Also, refer to the following additional documents: <ul style="list-style-type: none"> Otto Cserhalmi + Partners, 2022 (Curio Projects update). <i>ERW Overarching Conservation Management Plan</i>. Curio Projects, 2022. <i>Overarching Opportunities & Constraints – ERW</i>.
4.3 <i>Prepare a Heritage Interpretation Strategy for the site that:</i>	Refer to the following report: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Stage 1 Heritage Interpretation Plan: Large Erecting Shop</i>.
<i>a) Recognises and celebrates Aboriginal connection to the site and addresses the full story of the place;</i>	Refer to Chapters 2 (pages 15, 21, and 33) and 4 (pages 37-38) of the following report: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Stage 1 Heritage Interpretation Plan: Large Erecting Shop</i>.

Study Requirements	References
<i>b) Identifies key themes, social values, interpretive opportunities, measures and locations as an integral component of creating a unique and exciting destination as part of the broader State Heritage Register listed Eveleigh Railway Workshops item.</i>	Refer to Chapter 4 (pages 36-37) of the following report: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Stage 1 Heritage Interpretation Plan: Large Erecting Shop.</i>
<i>c) Provides the strategic direction for heritage interpretation having regard to the site's heritage significance (in particular the sites social, intangible, industrial and engineering values) and regard to the place's relationship with nearby heritage items, as an integral component of the development of detailed design;</i>	Refer to Chapters 2 (pages 15-33) and 4 (pages 36-37) of the following report: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Stage 1 Heritage Interpretation Plan: Large Erecting Shop.</i>
<i>d) Accounts for existing and planned (where possible) interpretive approaches as part of other projects within and in the vicinity of the site.</i>	Refer to Chapter 2 (pages 26-29) of the following report: <ul style="list-style-type: none"> Curio Projects, 2022. <i>Stage 1 Heritage Interpretation Plan: Large Erecting Shop.</i>

1.3. Site Identification

The LES is an isolated building at the northwest of the South Eveleigh Precinct. The precinct is located approximately 200m to the southwest of Redfern Train Station and approximately 200m to the west of the future Sydney Metro Waterloo Metro Station. The South Eveleigh Precinct includes an overall area of approximately 13.2 hectares.

The LES site is currently legally described as being part of Lot 5, in Deposited Plan 1175706. This allotment also includes the North Eveleigh Precinct, the rail lines separating the North and South Eveleigh Precincts, and Redfern Railway Station. The LES building subdivision will form part of a future application.

The LES is owned by TAHE NSW and managed by Transport Heritage NSW. It is currently being used as a maintenance facility for heritage locomotives but it requires significant capital investment to bring it up to the required standards for continued use and avoid further deterioration.

A map of the precinct and relevant boundaries are illustrated in Figure 1.1 and Figure 1.2.

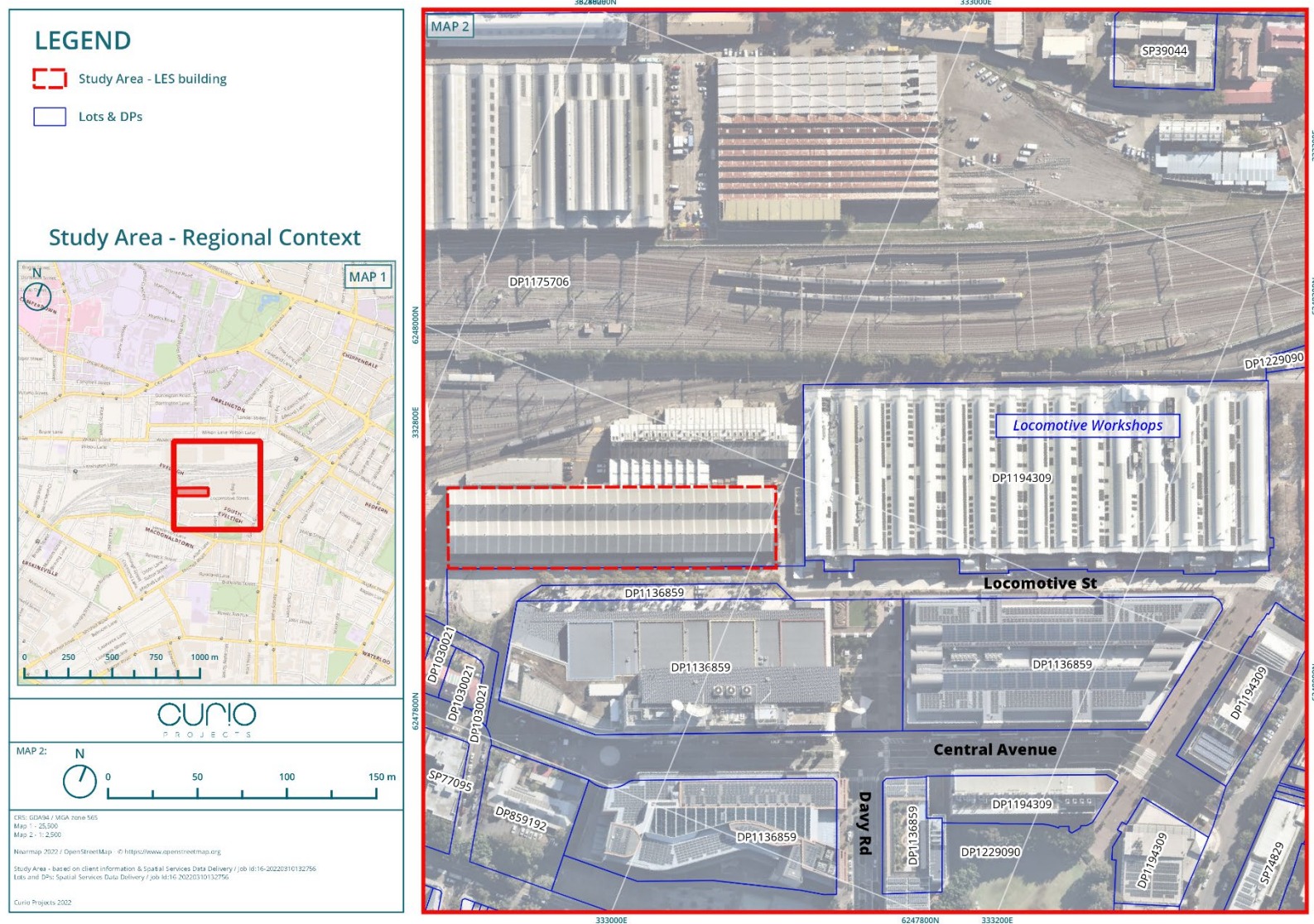


Figure 1.1: Regional Context. The subject site, the LES building, is highlighted in red. Source: Curio, 2022.

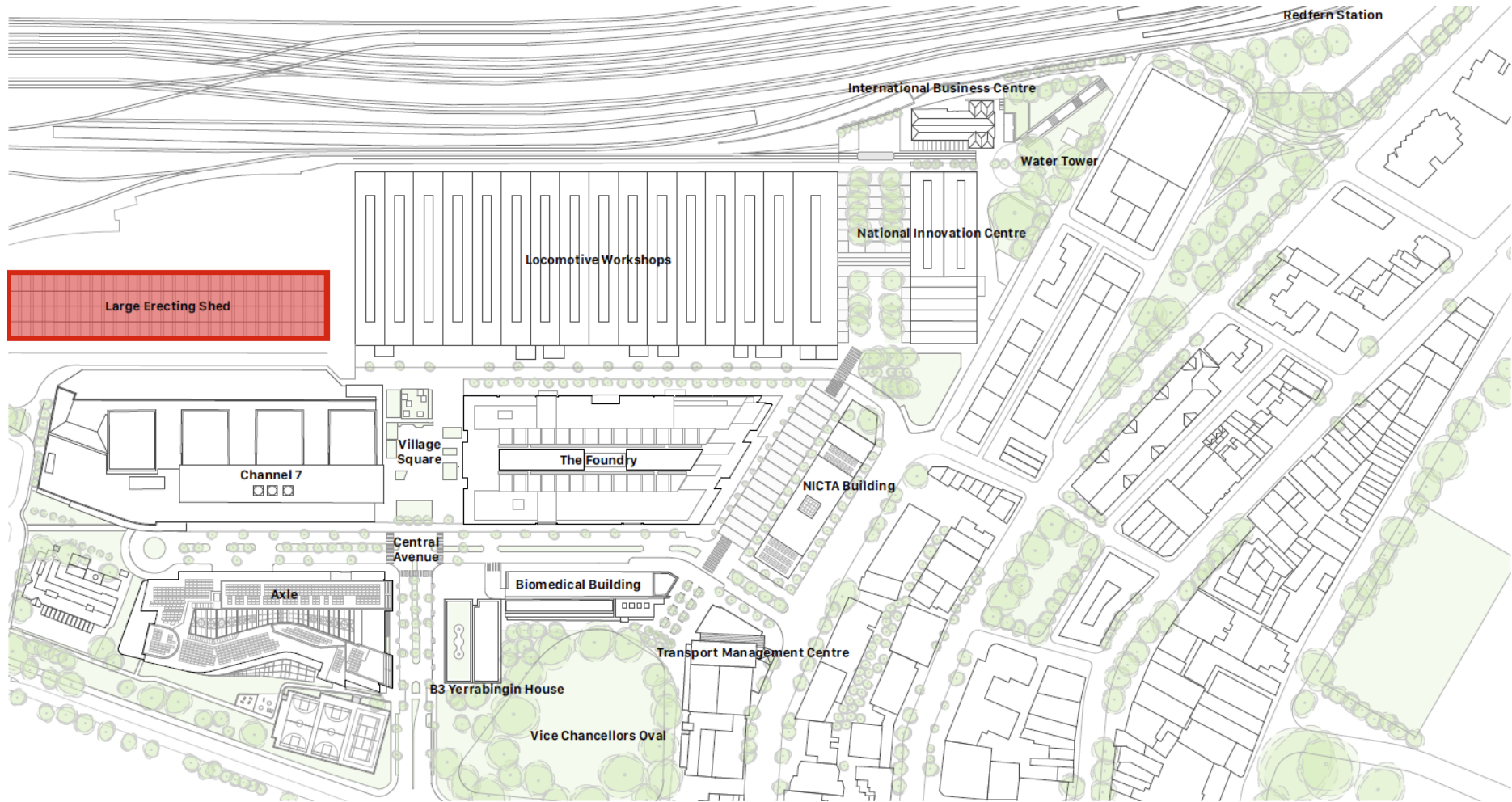


Figure 1.2: Subject site (in red) in context of South Eveleigh Precinct. Source: Curio Projects, 2022

1.4. Eveleigh Overarching CMP – Heritage Management Principles

Otto Cserhalmi + Partners (OCP) have written a 2017 Overarching Conservation Management Plan (CMP) for the entire Eveleigh heritage precinct. This CMP was recently updated by Curio Projects in 2021-2022 (commissioned by TfNSW and in consultation with OCP) as part of the Redfern North Eveleigh Renewal project SSP documentation.

As part of the CMP, OCP considered the heritage values of the overall Eveleigh site and created seven overarching heritage management principles with 32 accompanying policies to ensure that the state heritage values of the overall precinct are maintained as part of any future development.

For information, the OCP Heritage Management Principles are reproduced below², noting that they and the relevant policies are discussed more fully in Section 8.7 as part of the Assessment of Heritage Impact of the Rezoning Proposal for the LES building.

- HMP 1. All future decisions should be based on an understanding of the heritage values of the place and with a view to retain the identified significant values.
- HMP 2. A coordinated approach to management of the individual precincts and areas within the ERW site should be undertaken by future owners to ensure that future decisions are based on consideration for the heritage significance of the whole ERW and its overall presentation.
- HMP 3. The ERW site is assessed as being of state significance and therefore any future owners and managers of the site and its individual precincts should manage the place in accordance with best-practice heritage guidelines.
- HMP 4. Proposals for change should not unduly affect the significant heritage values of the ERW site and its individual precincts and should facilitate understanding of the place and its heritage values.
- HMP 5. The history and significant values of the ERW site and its individual precincts should be interpreted, including within any future redevelopment and reuse of the site where existing elements are to be removed or modified. The place should be interpreted as a major railway workshop facility.
- HMP 6. Skilled conservation professionals should be engaged to advise on, document and/or implement conservation and upgrading work and future development proposals for heritage assets of the ERW site and its individual precincts.
- HMP 7. Planning for new development, adaptive reuse of heritage assets and heritage interpretation should include opportunities for community consultation.

1.5. Limitations and Constraints

The following report has been prepared using all readily available historical data and documentation available for the subject site and surroundings, including relevant archaeological reports and assessments. The report has been prepared in accordance with the best practice management guidelines issued by NSW Heritage and in accordance with Australia ICOMOS, *The Australian Charter for Places of Cultural Significance*, *The Burra Charter* 2013 guidelines.

² Otto Cserhalmi + Partners, 2022 (Curio update). *ERW Overarching Conservation Management Plan*. Prepared for UrbanGrowth NSW: 121-132.

The report considers heritage matters only and no other non-heritage matters associated with the proposed redevelopment of the subject site.

This report should be read in conjunction with the reports prepared by Curio in 2022: *Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal* and *Stage 1 Heritage Interpretation Plan – Large Erecting Shop*, which provides a full study of the non-Aboriginal history and heritage values of the subject site. In addition, *the Aboriginal Cultural Heritage Study & Statement of Impact: Large Erecting Shop – Rezoning Proposal* has been prepared by Curio regarding the Aboriginal Cultural heritage values of the site.

1.6. Authorship

This report has been prepared by Tatiana Barreto, Senior Architecture & Urban Design Specialist, with senior review and specialist input undertaken by Dr Jody Steele, Director of Curio Projects Tasmania, and Natalie Vinton, CEO of Curio Projects.

The non-Aboriginal archaeological assessment has been undertaken by Mikhaila Chaplin, Archaeologist and Heritage Consultant of Curio Projects.

GIS mapping has been undertaken by Andre Fleury, Historian and Archaeologist of Curio Projects.

Historical research has been undertaken by Mikhaila Chaplin and Sebastian Gerber-Hood, Interpretation & Archaeology Specialist of Curio Projects, in collaboration with the Australian Railway Historical Society (ARHS).

Physical Analysis has been undertaken by Tatiana Barreto, Sebastian Gerber-Hood and Ian Bainsbridge, Director of Operations, Buildability, and Fabrication of Curio Projects.

2. Statutory Context

2. Statutory Context

This section of the report discusses the local and State planning context for the LES building with respect to its non-Aboriginal heritage values.

In NSW, heritage items and known or potential archaeological resources (non-Aboriginal) are afforded statutory protection under two principal pieces of legislation:

- *Environmental Planning and Assessment Act 1979* (NSW) (EPA Act); and
- *Heritage Act 1977* (NSW) (Heritage Act).

The subject site is heritage-listed as part of the former ERW complex on the following statutory registers (Figure 2.2).

- State Heritage Register (SHR): ERW #01140;
- NSW State Agency Heritage Registers: Railcorp Heritage and Conservation Register (S170)
- Sydney Environmental Planning Policy (SEPP) (Major Development) 2005 Redfern Waterloo Authority Sites;
- Sydney Regional Environmental Plan (REP) No 26- City West Schedule 4.

The ERW and ERW Machinery are also listed on two non-statutory registers: the Register of National Estate and the National Trust of Australia Register.

2.1. Environmental Planning and Assessment Act (NSW) 1979

The NSW Department of Planning, Industry and Environment (DPIE) administers the EP&A Act, which provides the legislative context for environmental planning instruments made to legislate and guide the processes of development and land use. Local heritage items, including known archaeological items, identified Aboriginal Places and heritage conservation areas are protected through listings on Local Environmental Plans (LEPs), Regional Environmental Plans (REPs), and State Environmental Planning Policies (SEPPs). The EP&A Act also requires that potential historical archaeological resources are adequately assessed and considered as part of the development process, in accordance with the requirements of the Heritage Act (see relevant sections below for further on the Heritage Act).

2.1.1. State Environmental Planning Policy (Precincts – Eastern Harbour City) 2021

From 1 March 2022, the State Environmental Planning Policy (State Significant Precincts) 2005 has been replaced by State Environmental Planning Policy (Precincts – Eastern Harbour City) 2021 (SEPP 2021). The former ERW site is located within the Redfern-Waterloo Authority Sites State Significant Precinct under the SEPP 2021 (SEPP 2021, Appendix 3). Thus SEPP 2021 is the principle environmental planning instrument that applies to the entire former ERW site, including both the Redfern North Eveleigh Precinct as well as South Eveleigh. Part 2.2 and Appendix 3 (Redfern-Waterloo Authority Sites) of the SEPP sets out the zoning, land use and development controls that apply to the development of the site.

State Environmental Planning Policy (Planning Systems) 2021 (Planning Systems SEPP) identifies various types of development and particular sites upon which certain development is defined as State Significant Development (SSD). Schedule 2 of the Planning Systems SEPP lists specific sites where development has a capital investment value of more than \$10 million; works on those sites are state significant. Clause 2 of Schedule 2 'Redfern-Waterloo Sites' as a specific site. As the proposed adaptive reuse and conservation of the LES will have a capital investment value greater than \$10 million, the future development application to seek approval for the proposed development will be classified as SSD and will be submitted to the Department of Planning and Environment (DPE) for assessment.

Several built items of the former ERW are individually identified as heritage items under SEPP 2021, as listed below:

- Locomotive Workshop
- New Locomotive Workshop
- Works Manager's Office
- **Large Erecting Shop**
- Carriage Workshops
- Blacksmith's Shop
- Paint Shop
- Scientific Services Building No. 1
- Chief Mechanical Engineer's Office Building

The proposed amendments to the Precincts SEPP 2021 involve the inclusion of the LES site within the 'Business Zone—Business Park' zone, which applies to the majority of the South Eveleigh Precinct and notably is the existing zoning for the adjacent Locomotive Workshop.

Further, the State-led rezoning application proposes new development standards including a maximum gross floor area (GFA) control on the LES site of up to 15,000sqm. Other minor changes as required are being proposed to the Precincts SEPP to facilitate the reuse of the LES building for commercial office and retail purposes.

2.1.2. Sydney Local Environment Plan (LEP) 2012

The Sydney LEP 2012 provides local environmental planning provisions for land within the Sydney LGA. Clause 5.10 of the LEP sets out objectives and planning controls for the conservation of heritage in the City of Sydney Council area, including the conservation of built heritage and archaeological sites.

As ERW are subject to the overriding provisions of SEPP 2021, the land is excluded from the provisions of the Sydney LEP 2012.

2.2. Heritage Act (NSW) 1977

In NSW, heritage items are afforded statutory protection under the *NSW Heritage Act 1977* (the Heritage Act). Heritage places and items of importance to the people of New South Wales are listed on the NSW State Heritage Register (SHR). The Heritage Act defines a heritage item as a 'place, building, work, relic, moveable object or precinct'. The Heritage Act is responsible for the conservation and regulation of impacts to items of State heritage significance, with 'State Heritage Significance' defined as being of 'significance to the state in relation to the historical, scientific, cultural, social, archaeological, architectural, natural or aesthetic value of the item'.

Heritage NSW is the approval authority under the Heritage Act for works to gazetted State Heritage listed sites, with approval for certain works required under Section 57(1). Application for approval in accordance with Section 57(1) is undertaken by submission of a Section 60 Application or a Section 60 fast-track application to Heritage NSW. Section 60 applications are also required for excavation which could impact all archaeological deposits located within the curtilage of an SHR listing.

The subject site forms part and is located within the curtilage of the ERW SHR listing (#01140), the boundaries of which are outlined in Figure 2.1.

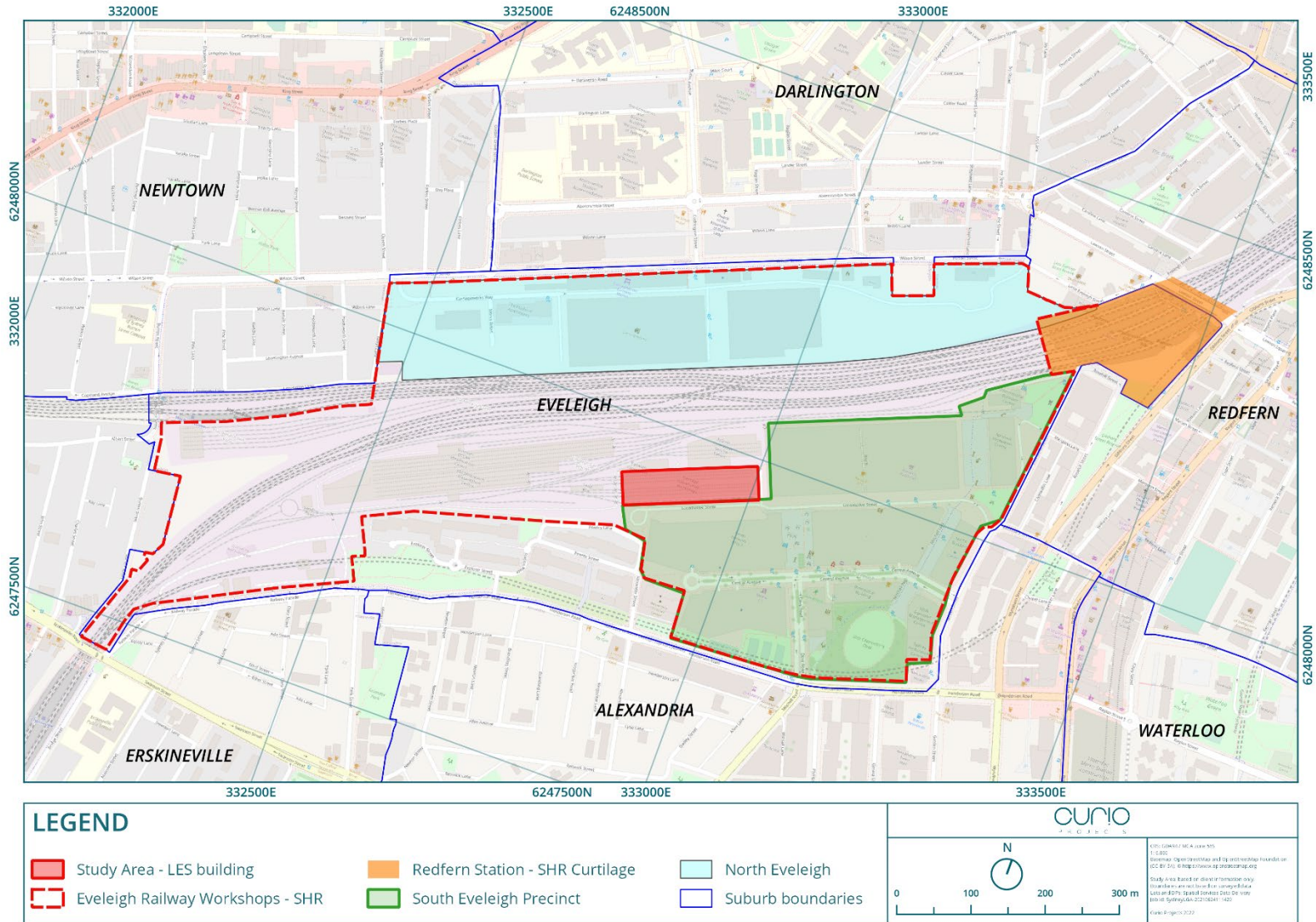


Figure 2.1: ERW SHR Curtilage. The subject site is highlighted in red. Source: Curio, 2022.

2.2.1. Section 170 Heritage and Conservation Register

Under Section 170 of the Heritage Act, government instrumentalities must keep a s170 Register which contains items under the control or ownership of the agency and which are or could be listed as heritage items (of State or Local significance).

The LES building is part of the whole ERW precinct, which is listed on the NSW Transport Asset Holding Entity (TAHE) (formerly known as the State Rail Authority) Section 170 Heritage & Conservation Register (2021) (managed by Sydney Trains/Transport for NSW on behalf of TAHE)³:

- Eveleigh Railway Workshops (SHI #4801102)

The RailCorp S170 Heritage & Conservation Register⁴ (2013) also includes a number of significant items of the rolling stock that have been noted to be located within the LES. They are as follows:

- FRN 2186 – Second-class Sitting / Buffet Car (SHI #4807101)
- Locomotive Diesel 4401 (SHI #4807242)
- Locomotive, Diesel Shunting 7344 (SHI #4807250)
- MFS 2028 – Second-class Sitting Car (SHI # 4807097)
- MFS 2096 – Second-class Sitting Car (SHI # 4807089)
- MFS 2121 – Second-class Sitting Car (SHI # 4807091)
- MFS 2145 – Second-class Sitting Car (SHI # 4807095)
- TAM 1883 – Main-line Sleeping Car (SHI # 4807079)

2.3. Heritage Items in the Vicinity

Table 2.1 provides a summary of the statutory heritage listings both included within as well as in the vicinity of the subject site as illustrated in Figure 2.2.

Table 2.1: Summary of heritage listings in the vicinity of the subject site

Item No.	Heritage Register	Item Name	Address
01140	SHR	ERW	Great Southern and Western Railway
01139	SHR	Eveleigh Chief Mechanical Engineer's Office and Moveable Relics	Great Southern and Western Railway
01234	SHR	Redfern Railway Station group	Great Southern and Western Railway
I2223	SLEP 2012	Alexandria Hotel including interiors	35 Henderson Road
C1	SLEP 2012	Alexandria Park Heritage Conservation Area	Alexandria
C3	SLEP 2012	Kingsclear Road Heritage Conservation Area	Alexandria/Erskineville

³ TAHE s170 Register, 2021. <https://www.transport.nsw.gov.au/projects/community-engagement/sydney-trains-community/heritage-and-conservation-register>. Accessed May 2022

⁴ Transport RailCorp NSW, 2017. Section 170 Heritage & Conservation Register - Movable Heritage

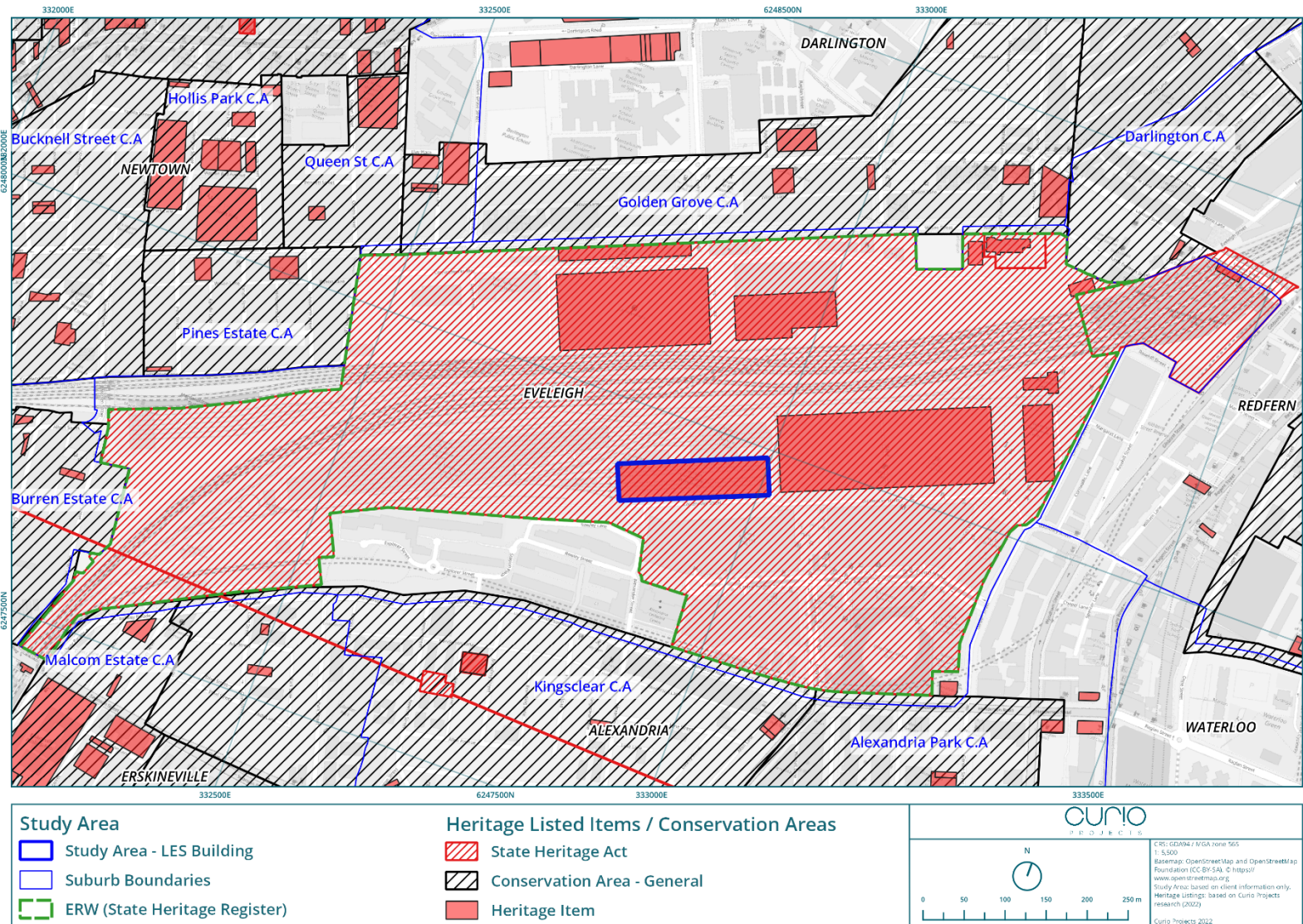


Figure 2.2: Heritage Items and Conservation Areas in the vicinity of the subject site. Source: Curio Projects, 2022.

3. Historical Summary

3. Historical Summary

This chapter provides a brief summary of the historical phases of use and development activity at the subject site in order to provide historical context for the assessment of heritage impact in this report. For a full historical overview, reference should be made to the Non-Aboriginal Heritage Study⁵ and Aboriginal Cultural Heritage Study⁶ prepared by Curio (2022) for the site.

In general, the history of the subject site can be summarised into four main phases of occupation and use:

Phase 1	Pre-European Environment/Aboriginal Occupation (c20,000BP–1794)
Phase 2	Early Land Grants, Chisholm Estate (1835–1880)
Phase 3	Establishment and development of Eveleigh Railway Workshops (1880–1989)
Phase 4	Recent History/History of the LES and the wider South Eveleigh Precinct (1989–present)

3.1. Aboriginal Ethnohistory

The coastal Dharug or Eora people occupied the central and eastern Sydney area prior to European arrival. The term ‘Eora’, while used frequently to describe the inhabitants of the Sydney area does not appear to have a basis in Aboriginal ethnography. Rather, the term is a colonial interpretation used to collectively identify Aboriginal people without specific reference to clan or country. As such, the use of the term Eora is not suitable when describing a specific location or Country.

Much of the evidence of traditional Aboriginal lifestyle and economy was disturbed in the early years of European settlement and much of our information on the local people is based on ethnohistorical sources.

The Gadigal people living in the area would have pursued a mixed food economy in the region, utilising and relying upon the abundant natural resources of the area including marine resources from Sydney harbour, and surrounding waters, hunting terrestrial mammals, as well as collecting and processing local plants. The area included a range of natural environments and resources available. Accessing food was acquired through fishing, shellfish collection, hunting and gathering of small plants and animals. The area surrounding the subject site included swamps, bays and estuarine mud flats which likely provided reliable resources of fish, shellfish, and crustacean groups. Besides a range of plant and animal foods within the subject site, the landscape offered a variety of medicinal plants and raw materials used for shelters, the manufacture of tools, weapons and ceremonial purposes such as body decoration⁷.

Swamps and waterlines were located surrounding the subject site, for example, east of the site was a large swamp that is located where Redfern Park is today, and Blackwattle Swamp was located northwest of the subject site. Shea’s Creek and Blackwattle Creek are both connected to these waterways. A traditional Aboriginal pathway ran in the area between Eveleigh and Central Station which was on higher ground with watercourses located in low-lying areas.

⁵ Curio Projects, 2022. *Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal*.

⁶ Curio Projects, 2022. *Aboriginal Cultural Heritage Study & Statement of Impact: Large Erecting Shop – Rezoning Proposal*.

⁷ Godden Mackay Logan, 2013. *Australian Technology Park: Conservation Management Plan*, Vol 1.

3.2. Early Contact Period

Within the first few years of colonisation, historical records describe numerous accounts of contact between settlers and Aboriginal inhabitants, many of which highlight the cultural disturbance and conflict between them. As settlers began to fish, clear land and shoot games, Aboriginal society and spiritual life were greatly affected.

Shortly following the arrival of the First Fleet in January 1788, Governor Phillip reported to Lord Sydney in England an estimate of at least 1500 Aboriginal people living along the coastal region between Broken Bay and Botany Bay⁸ at that time. The arrival of the First Fleet devastated the lives and activities of Aboriginal people of the Sydney Harbour area, restricting access to areas traditionally used for hunting and gathering, shelter and ceremonial purposes while introducing devastating diseases. Within the first three years of European settlement, an estimated fifty to ninety percent of Aboriginal people in the Sydney area died from smallpox. Most of the clans around Port Jackson were completely wiped out⁹.

Serious documentation of local Aboriginal culture and history by early amateur and professional anthropologists was not properly undertaken until around the 1890s. At this point, many surviving Aboriginal people from local and surrounding groups were living in fringe camps, on properties (owned by non-Aboriginal people), missions, and reserves.

There were many constraints and barriers during that time that adversely impacted the cultural continuance. Despite these constraints and barriers, levels of traditional knowledge and practices have been carried on in Redfern and its surrounding areas.

3.3. Early Land Grants

The land that makes up the ERW was originally part of a land grant given to John Davis in 1794. The grant was soon cancelled, however, and the land itself ultimately became part of a 62-acre grant given to James Chisholm in 1835. Chisholm, who had arrived in NSW in 1790 as a soldier in the NSW Corps, would build himself a modest home in the northeast corner of the estate in 1820-1830. This house would be known as 'Calder House' and was occupied by Chisholm's widow and family for 18 years after his death in 1837. In 1855, the Chisholm estate would be separated into two parts by the construction of the western rail line running from Sydney to Parramatta and was eventually selected to be the site for the Rail Workshops in 1875¹⁰. Surrounding the Chisholm estate were two other land grants to the north and east. These included a 52-acre grant given to ex-convict William Hutchinson and a 95-acre grant given to free settler William Chippendale, both in 1819. Both plots of land would later be subdivided into farmyards and estates, and then later into the familiar Redfern residential blocks in the 1850s¹¹.

⁸ Attenbrow, V. 2010. *Sydney's Aboriginal Past: Investigating the archaeological and historical records*, 2nd ed. University of New South Wales Press Ltd, Sydney.

⁹ Ibid

¹⁰ Otto Cserhalmi + Partners 2002a, *Eveleigh Carriage Workshops: Conservation Management Plan- Volume 1*. Prepared for State Rail Authority of NSW.

¹¹ OCP Architects, 2022 (Curio update). *ERW- Overarching Conservation Management Plan*. Prepared for UrbanGrowth NSW.

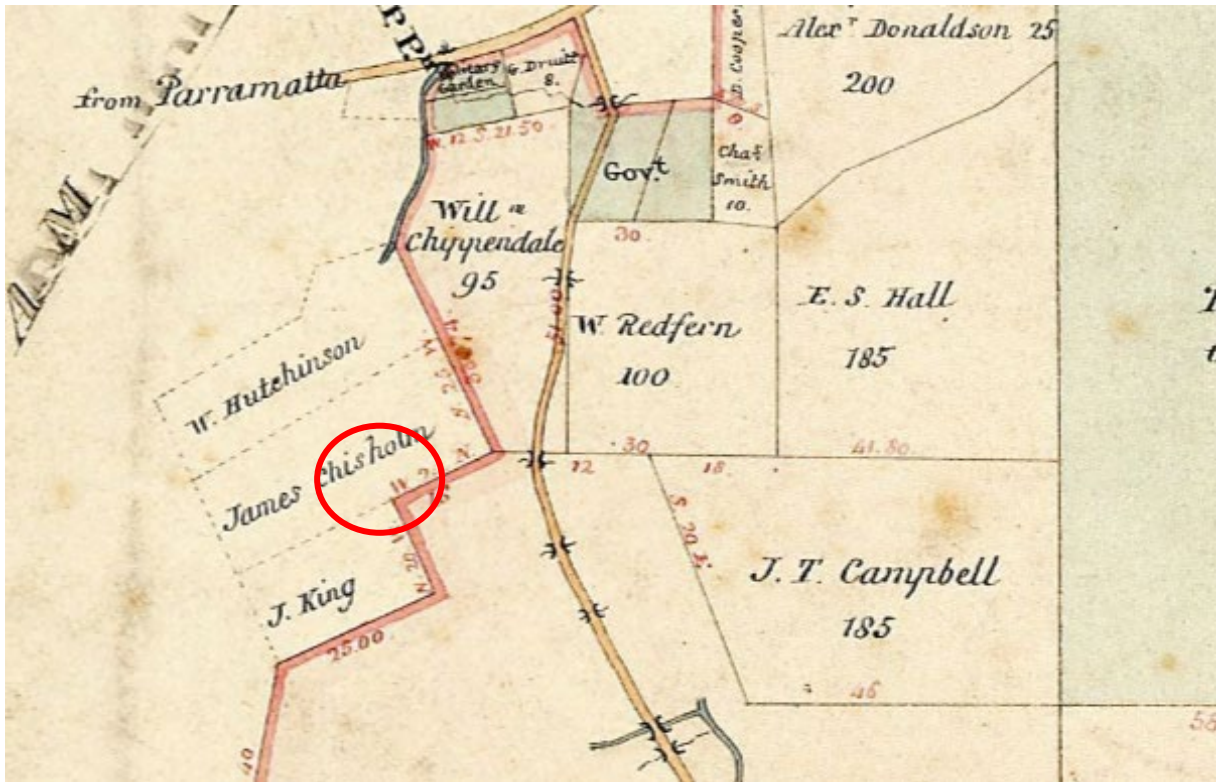


Figure 3.1: Undated map of Parish of Alexandria, early land grants. The general area of the LES is indicated in red. Source: Historical Lands Record Viewer County of Cumberland - Parish of Alexandria displaying Chisolm Estate.

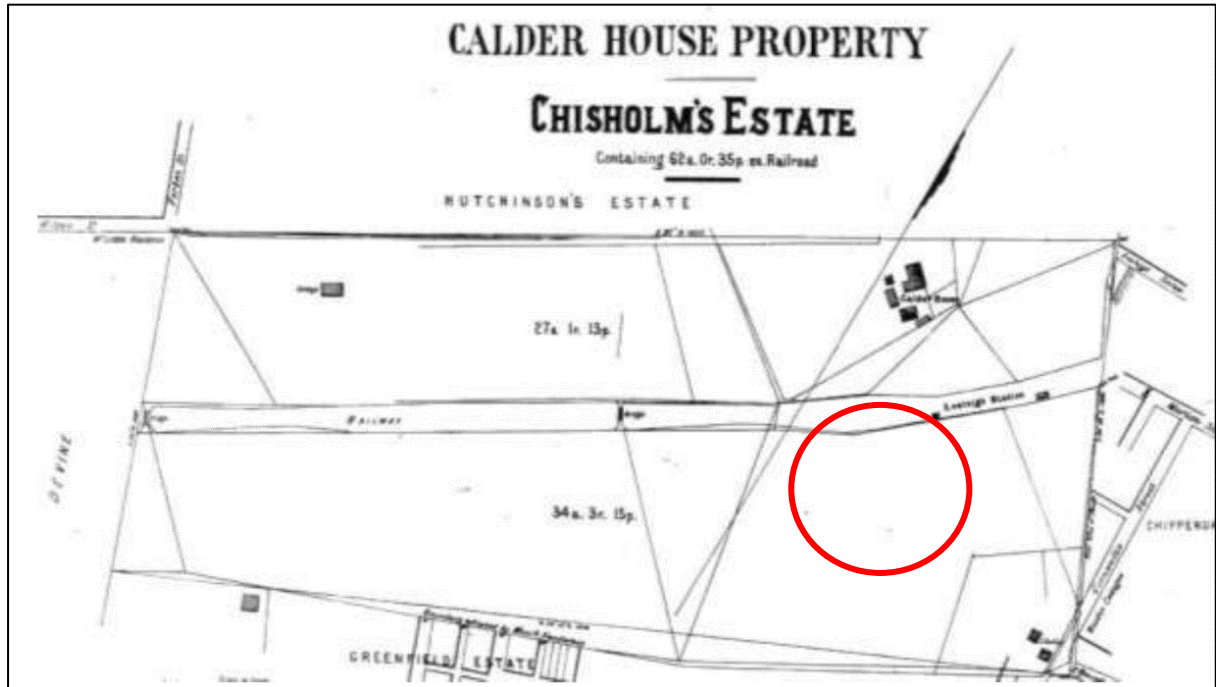


Figure 3.2: Chisholm Estate. The general location of the LES subject site is circled in red. Source: Australian Railway Historical Society, Eveleigh 1.

3.4. Eveleigh Railway Workshops

After the opening of the Sydney to Parramatta railway line in 1855, the growth of railway infrastructure and locomotive usage skyrocketed. With this growth came an increasing demand for a place to construct and maintain the state's ever-growing fleet of rail locomotives to replace the now inadequate Sydney Terminal yards near the inner city¹². Planning for this new railway workshop commenced in 1875 and would involve the resumption of the Chisholm estate, land preparation, and finally the construction of the Eveleigh workshops in 1885. Finishing in 1887, the new Eveleigh Railway Workshops consisted of the Locomotive Workshop on the southern side of the railway line, with the Carriage Workshops taking the space on the northern side. The ERW would become among the most important drivers in the expansion and development of the NSW Railway line, and therefore the growth of the entire state of NSW.

Making use of then-state-of-the-art iron and steel technology, the southern half of the ERW, the Eveleigh Locomotive Workshops (ELW), consisted of four main buildings, as well as several ancillary operations were also undertaken in other buildings around the site¹³. These four major buildings were the Locomotive Workshop itself, the Engine Running Sheds, the New Locomotive Shop, and the Large Erecting Shop¹⁴. The largest and most prominent of these was the Locomotive Workshop, which consisted of 16 bays of equal size, with internal hollow-cast iron columns and wrought iron trusses, all topped with corrugated iron roofing. In the building's initial form, bays 1-4 and bays 5-15 were two separate structures, separated by a laneway, but the two were united by a new bay known as Bay 4a.

¹² Ibid.

¹³ OCP Architects 2022 (Curio update), p.28-29

¹⁴ Simpson Dawbin Architects, 2003. *Large Erecting Shop Conservation Management Plan*, p. 52

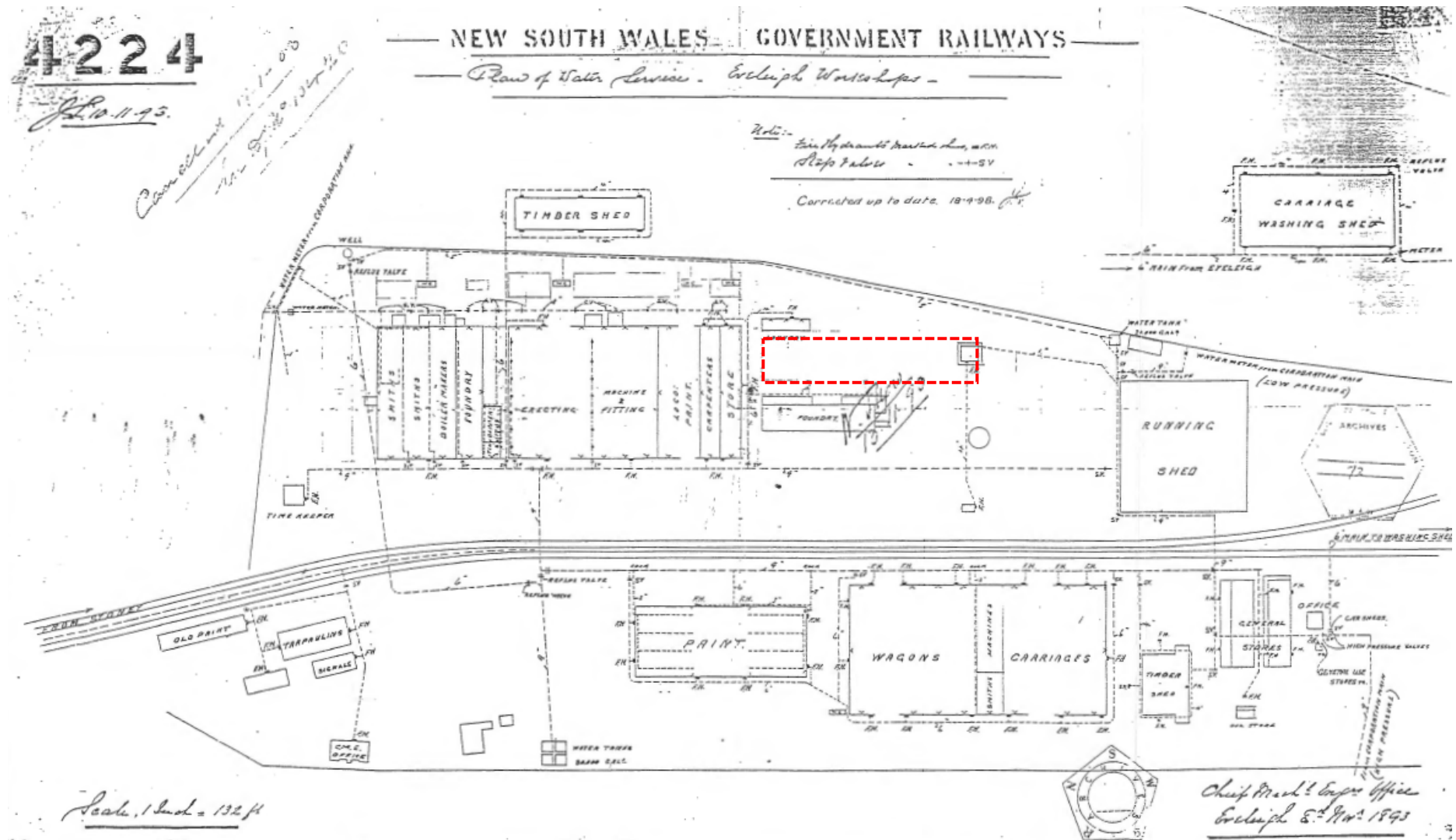


Figure 3.3: 1893 Site Plan of the Eveleigh Railway Workshops prior to the construction of the LES. The location of the subject site is marked in red. Source: State Rail Authority Archives, NSW State Archives, 4224-72.

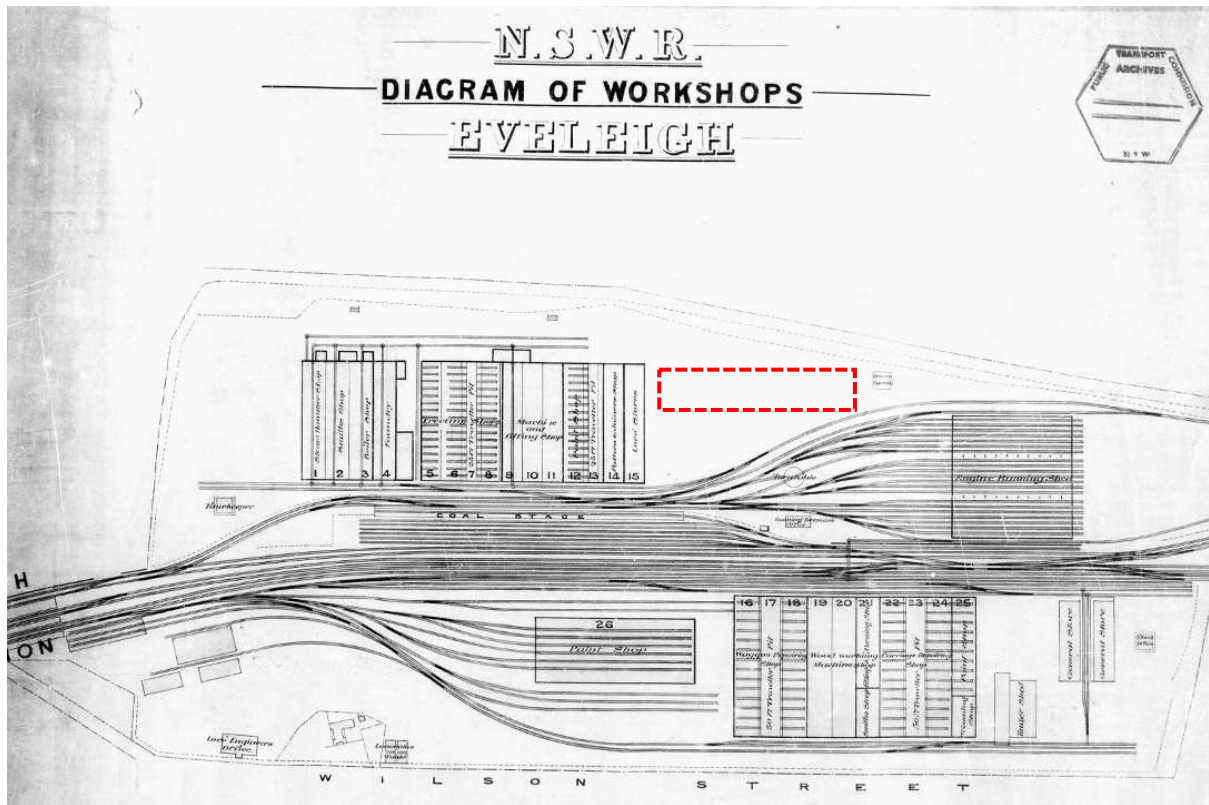


Figure 3.4: NSW Railways Diagram of Eveleigh Workshops, 1887, prior to the construction of the LES. Plan oriented to the south. The subject site location is marked in red. Source: NSW State Records (R560 1 11 7)

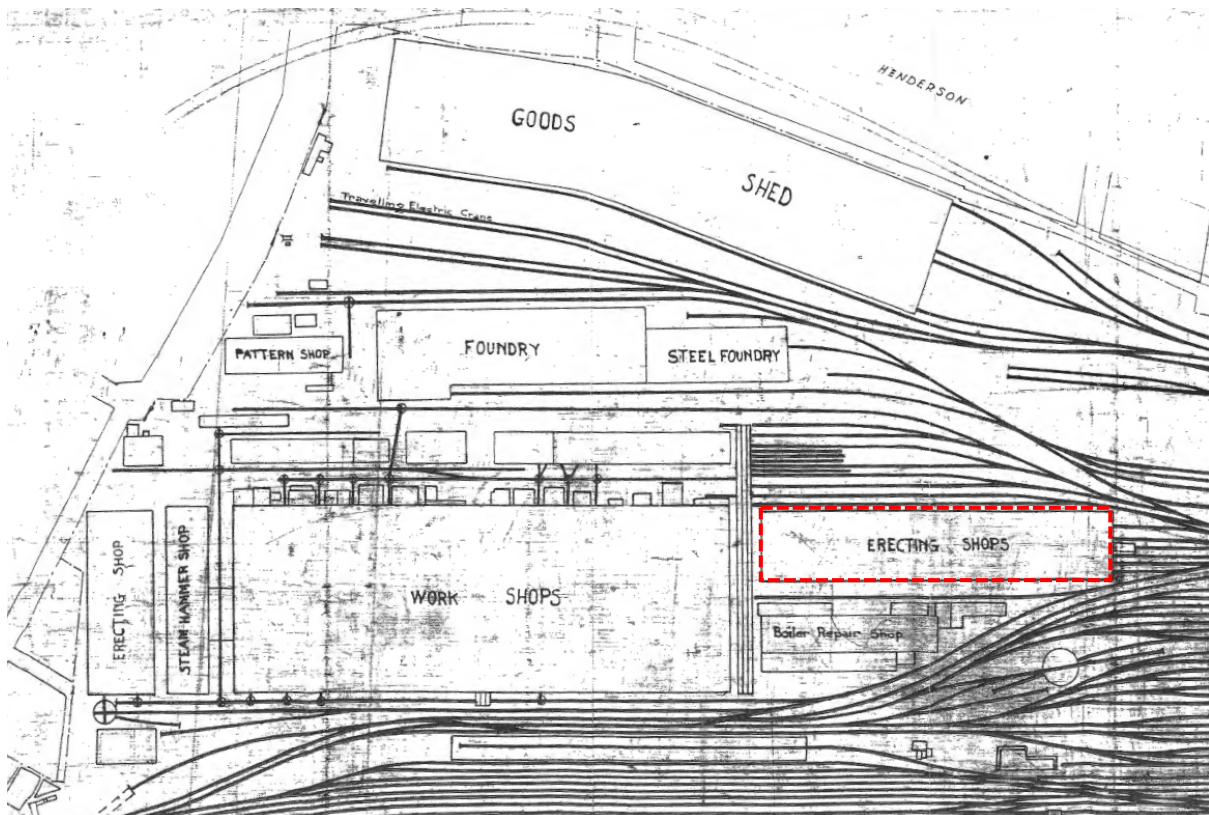


Figure 3.5: c1926 Extract from Site Plan from the Eveleigh Locomotive Workshops (ELW). The subject site (red) is identified as "Erecting Shops". Source: NSW State Library, FL8942147.

3.5. The Large Erecting Shop

The Locomotive Workshop included an in-house erecting shop, taking up bays 5-8¹⁵. Originating in English railway traditions of the mid-19th century, the term 'erecting' refers in this context to both the construction of a steam locomotive from its various component parts, as well as the overhaul of a locomotive via dismantling, repair, or reconstruction and then subsequent reassembly¹⁶. While this original erecting shop could hold 24 engines and 12 tenders for construction and repair, it soon became apparent that even this was not enough. To answer this demand, a new erecting shop was built in 1899 to supplement and eventually replace the increasingly inadequate working space of the original building's shop. Initial plans for this building name it simply the 'Erecting Shop', and documents from 1900 named it '*New Erecting Shop*' to differentiate it from the old shop within the main building¹⁷. When the *New Locomotive Workshop* (now National Innovation Centre) was built to the west of the main workshop, to avoid confusion, the erecting shop was subsequently referred to as the '*Large Erecting Shop*' (LES) or '*The Large*' colloquially¹⁸.

Built on an enormous concrete foundation, the LES consisted of brick walls with sandstone windowsills and was divided into two huge parallel galleries each separated by a row of iron pillars, separating it into 20 bays. With an effective floor space of 45,900 sq. ft., LES had the capacity for 32 engines simultaneously. Each of these galleries had three rail lines built into the floor for the locomotives to travel on, with pits for accessing the underside of the engines built into the ground between the outer two lines of each trio of rails.

In 1919, a new foundry building was constructed to the southeast of the LES, replacing a similar building to the north. The older, northern building was converted to a repair station for boilers, smoke boxes, and ash pans coming from the Locomotive Workshop, but soon moved to predominantly repair boilers coming from the adjacent LES¹⁹. In 1937, the 'old' Erecting Shop located in the Locomotive Workshop building was finally closed for good and was officially replaced by a new erecting shop located in the Chullora Locomotive Works, which would gradually take over the more modern aspects of locomotive construction and repair, namely the newer and more complicated C36s and 057 steam engines²⁰. These steam engines gradually became less and less common on NSW railways and, by the late 1960s, the LES was changing to the repair and overhaul of diesel engines instead. By 1970, work on steam engines was no longer undertaken at all at the LES²¹ and, by 1981, a final decision was made to relocate all state locomotive work to the workshop at Chullora and the LES was leased to the heritage steam train operator company 3801 Limited, in 1985²². 3801 Limited would retain the LES as a location for the storage and maintenance of heritage diesel engines and eventually heritage electric trains including the famous 3801 steam locomotive, along with space allocated for storage use by the Powerhouse Museum of locomotives 3265 and 3830. 3801 Ltd undertook steam train tours with the 3801 engine, including the 'Cockatoo Run', until 2007 when the train was removed from the LES and taken to the NSW Rail Museum in Thirlmere. 3801 Ltd continued to operate a variety of railway tours in and out of the LES until 2017 when the group

¹⁵ NSW Railways, F Fewtrell, internally written 'History of Eveleigh Workshops' for Chief Mechanical Engineer, 1955, NSW Rail Mechanical Branch File No. 55/1 0322-39, p 1

¹⁶ Simpson Dawbin Architects, Large Erecting Shop HIP, 2003, p. 10

¹⁷ NSW Railways, Plan Nos. 38 -1/10 inclusive; NSW Railways, Railway Budget, July 21, 1900, p 240.

¹⁸ Fewtrell, op cit, p 2; NSW Railways, data-card formerly held by Archives section, copy held by Godden, Mackay, Logan, Redfern; NSW Railways, Mechanical Branch Shop Order, 28/6/1899; NSW Railways, Mechanical Branch, Locomotive Accountants data -card for Large Erecting Shop, No. 2/1 of 1899

¹⁹ Simpson Dawbin Architects, 2003. *Large Erecting Shop Conservation Management Plan*, p. 14

²⁰ Locomotive Accountants card; for Loco erecting Shop, No. 1/1 ; Fewtrell, op cit, p 2.

²¹ Simpson Dawbin Architects, 2003. *Large Erecting Shop Conservation Management Plan*, p. 15

²² Ibid

terminated its use of the Large Erecting Shop²³. The LES subsequently passed into the hands of Transport for NSW (TfNSW)²⁴.

The LES soon became one of the most important parts of the ELW, acting as both the birthplace of the finished steam engines and as a reoccurring location in their lives. Work in the LES was known to be extremely dirty, noisy, and dangerous, but necessary, with fitters and turners, in particular speaking of 'their time in the 'large' as one of the key parts of their basic training²⁵. The Illustrated Sydney News puts it eloquently, describing the LES as among the most important places in a locomotive's life:

"This is the hospital of the steam engine, which makes its start in life from here, returns from time to time for repairs during its career, and comes back finally, when worn out, for dismantlement—the locomotive's death." (Illustrated Sydney News, 18 June 1891)²⁶

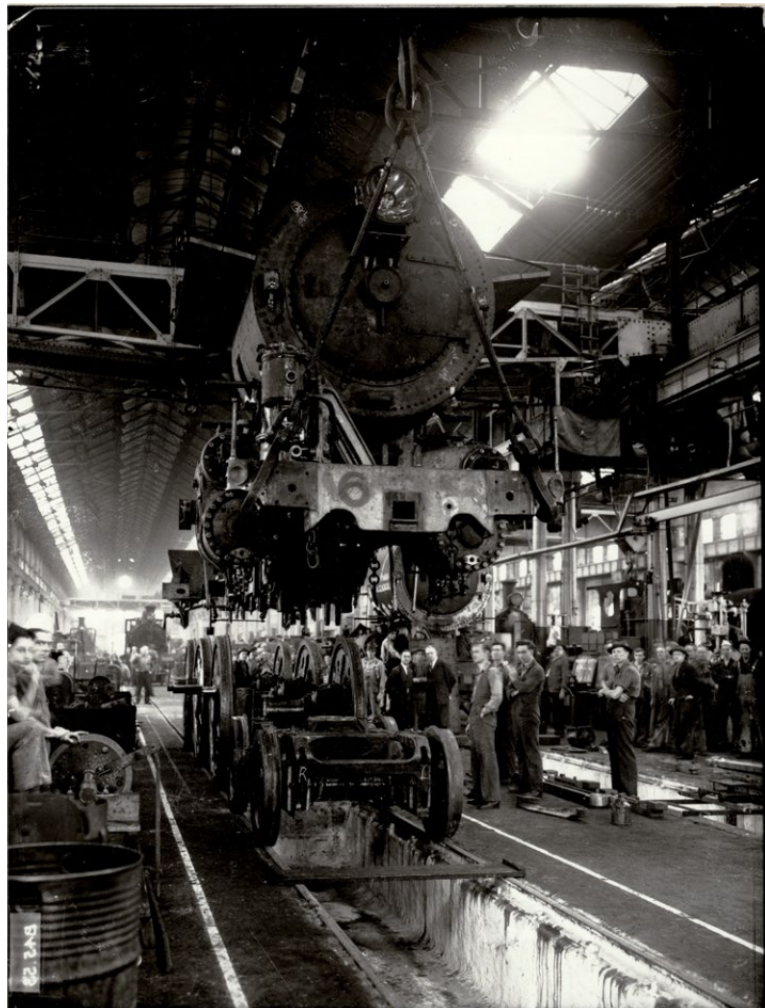


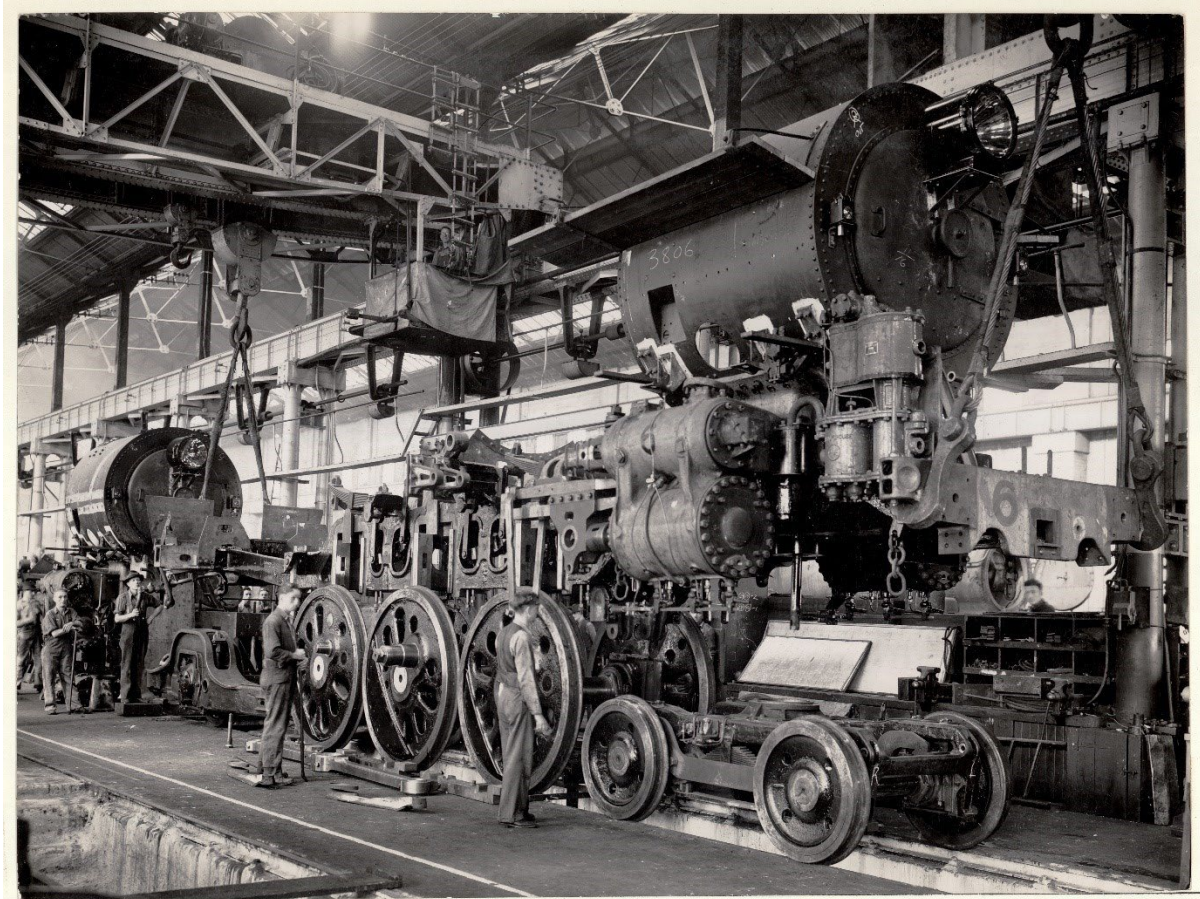
Figure 3.6: 1945 photograph of a steam locomotive engine being lifted off its wheelset and bogie by a crane in the LES. Note the access pit in between the tracks for working on the underside of the engine. Source: State Rail Authority Archives, NSW State Archives, B4553, NID 449/3.

²³ Ben Graham, '3801 Limited cancels heritage train service after group locked out of Eveleigh rail workshops', *The Daily Telegraph*, March 9, 2017.

²⁴ Ben Langford 2017, 'The Large Erecting Shed has been locked and 3801 Ltd's tours ex Eveleigh suspended' *Redwatch*. Accessed 12th May 2022 at: <http://www.redwatch.org.au/RWA/statesignificant/southeveleigh/large/170213les>

²⁵ Simpson Dawbin Architects, 2003. *Large Erecting Shop Conservation Management Plan*, p. 16-17

²⁶ 'The N.S.W Railway Workshops at Eveleigh: A State Enterprise' *Illustrated Sydney News*, Sat 18 July, 1891, p.12



N.S.W. Railway Prints

Figure 3.2: 1945 photograph of a steam locomotive engine being stripped down to its frame during construction or maintenance in the LES in 1945. Source: State Rail Authority Archives, NSW State Archives, B4554-21864-449/47.

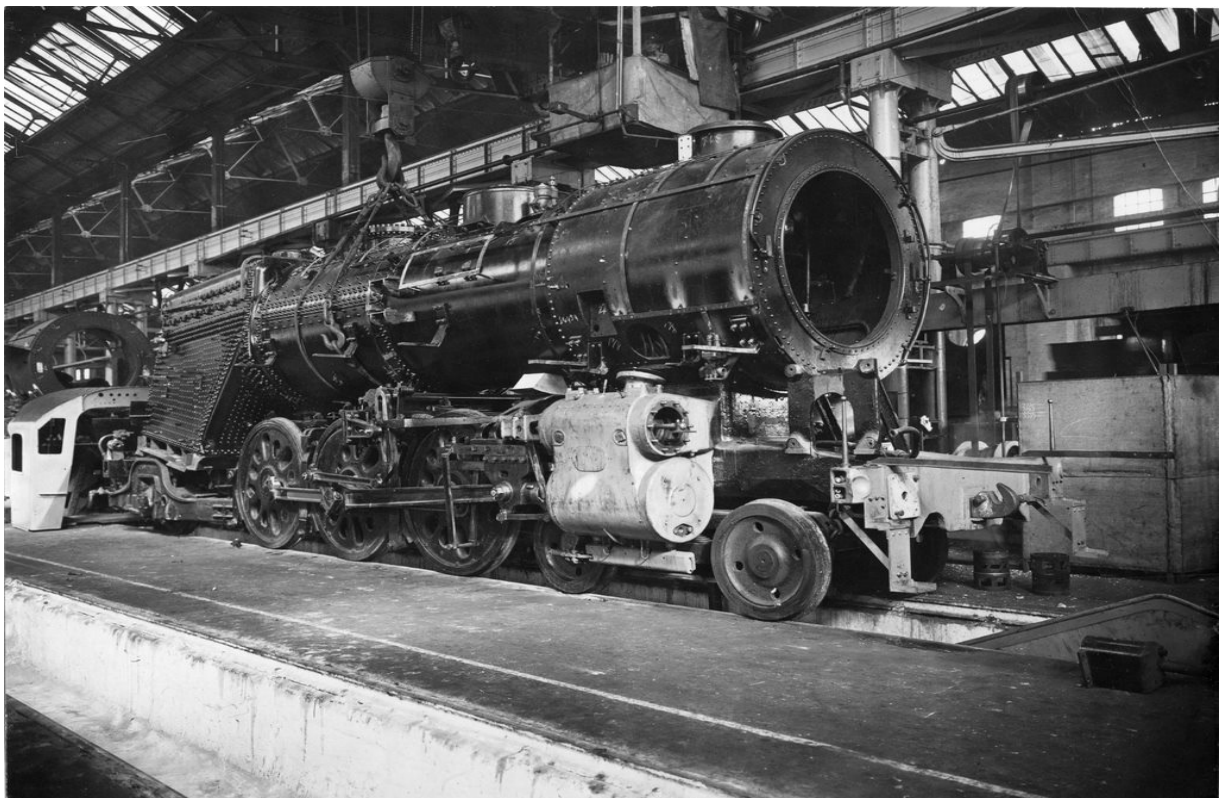


Figure 3.3: 1945 photograph of a locomotive having its boiler lifted off its bogie and wheelset by an overhead crane. Source: State Rail Authority Archives, NSW State Archives, 21822, B45594, NID 449/5.

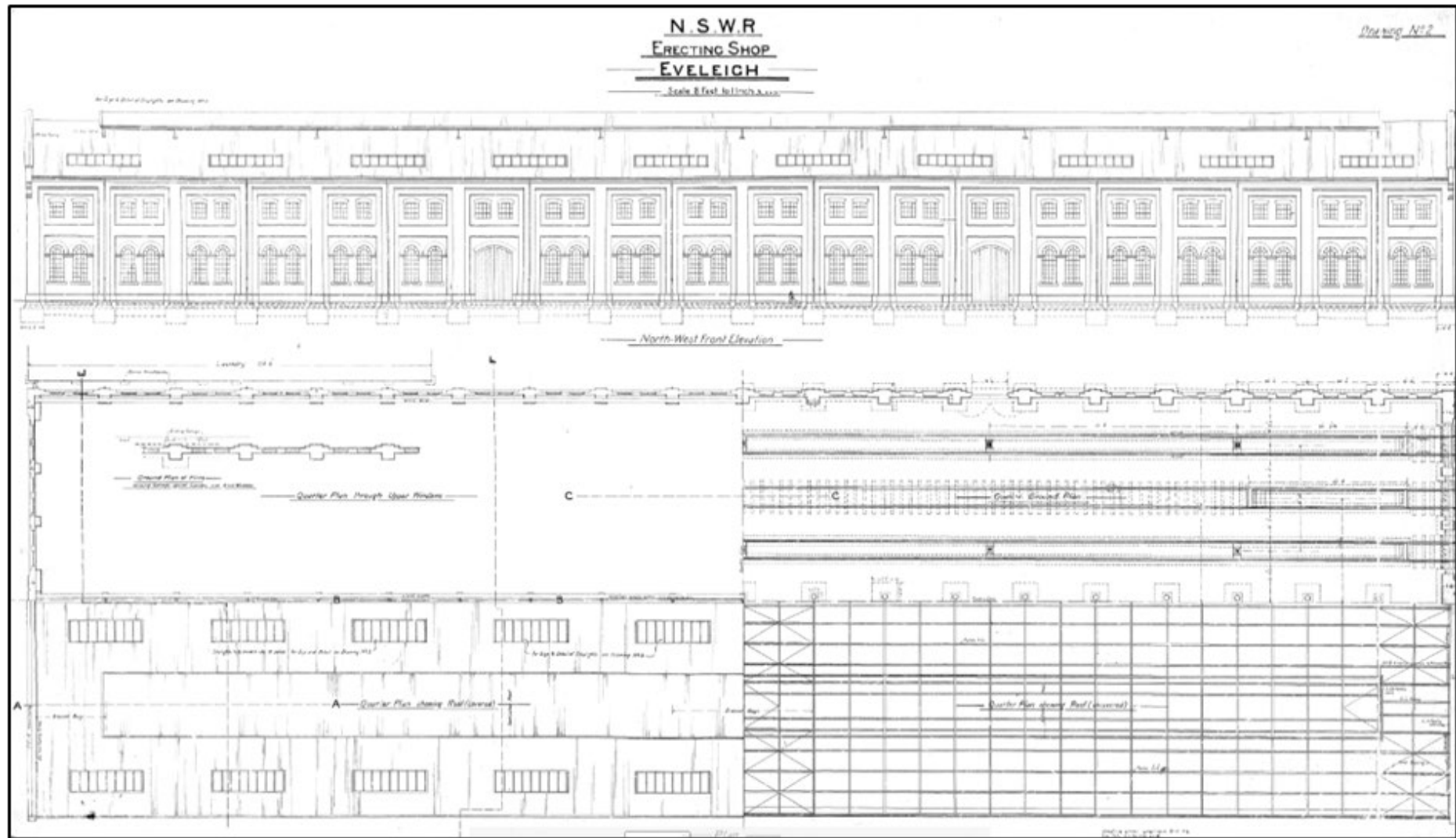


Figure 3.7: Drawings for the LES Building, undated. Source: State Rail Authority, NSW State Archives, NRS-20550-2-24-COR/529P2, 38-2.



38-1.

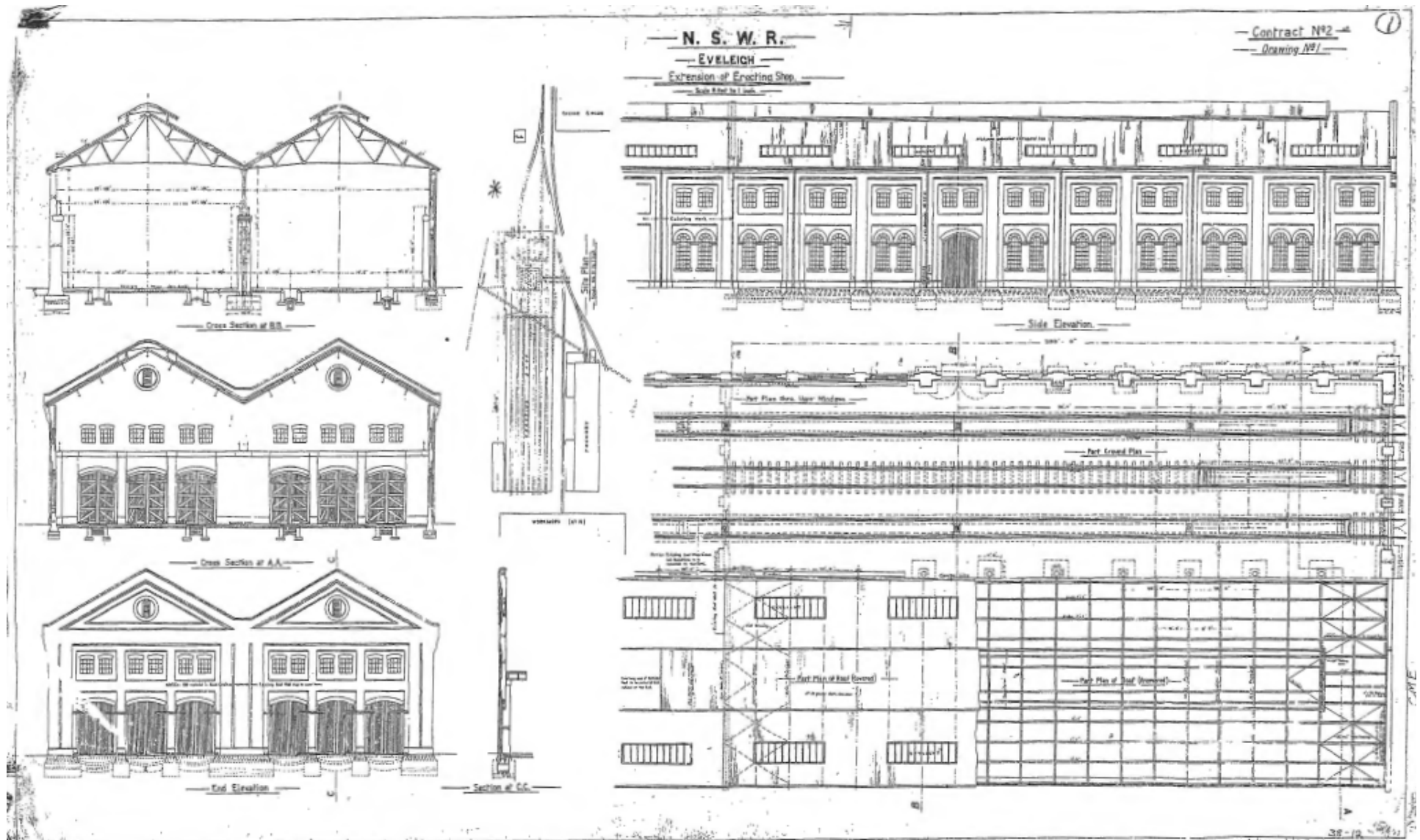


Figure 3.9: Drawings for the LES Building (formerly known as Erecting Shop), undated. Source: Source: State Rail Authority, NSW State Archives, NRS-20550-2-24-COR/529P2, 38-12.

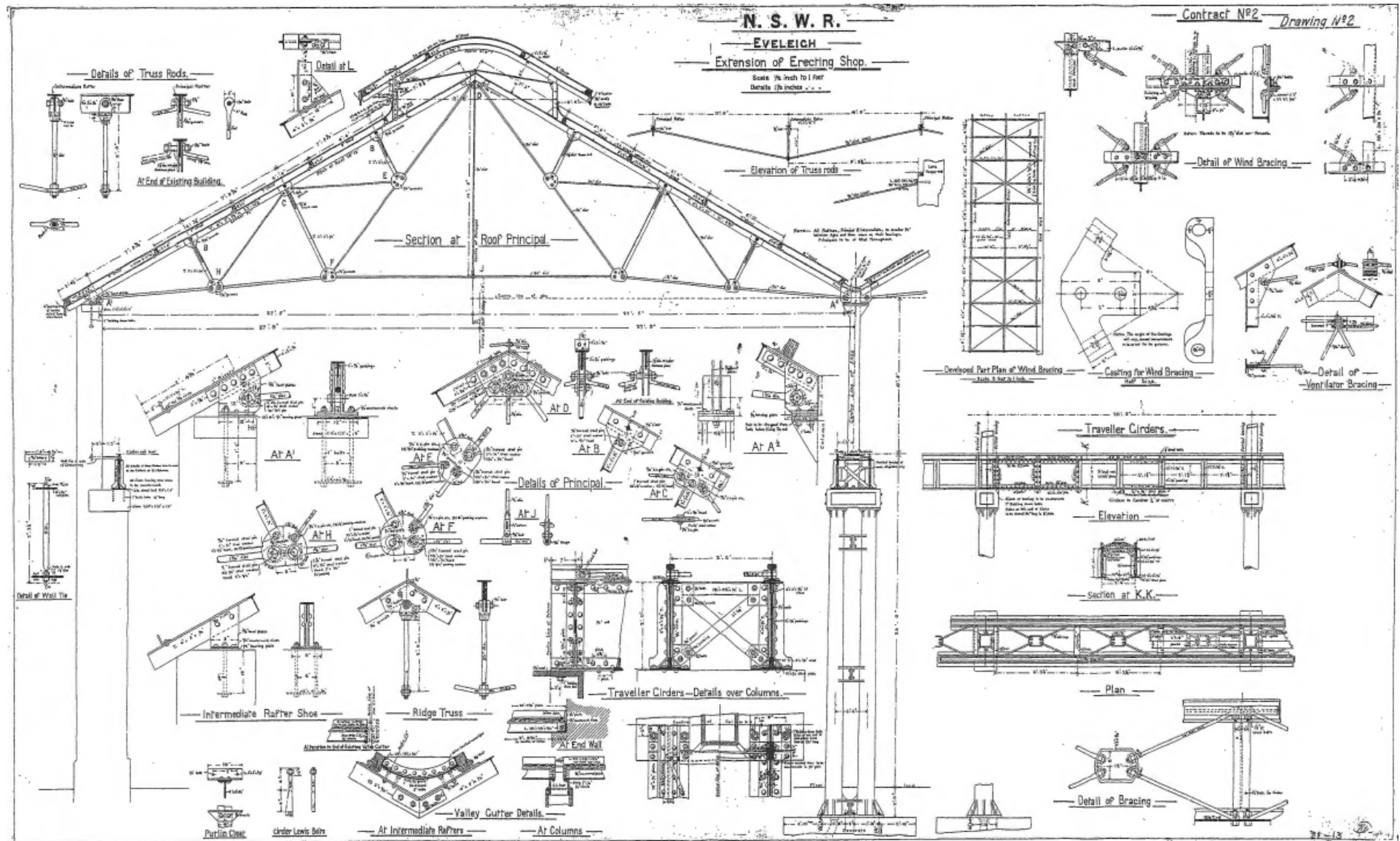


Figure 3.10: Detailed Drawings for the LES Building (formerly known as Erecting Shop), undated. Source: State Rail Authority, NSW State Archives, NRS-20550-2-24-COR/529P2, 38-13.

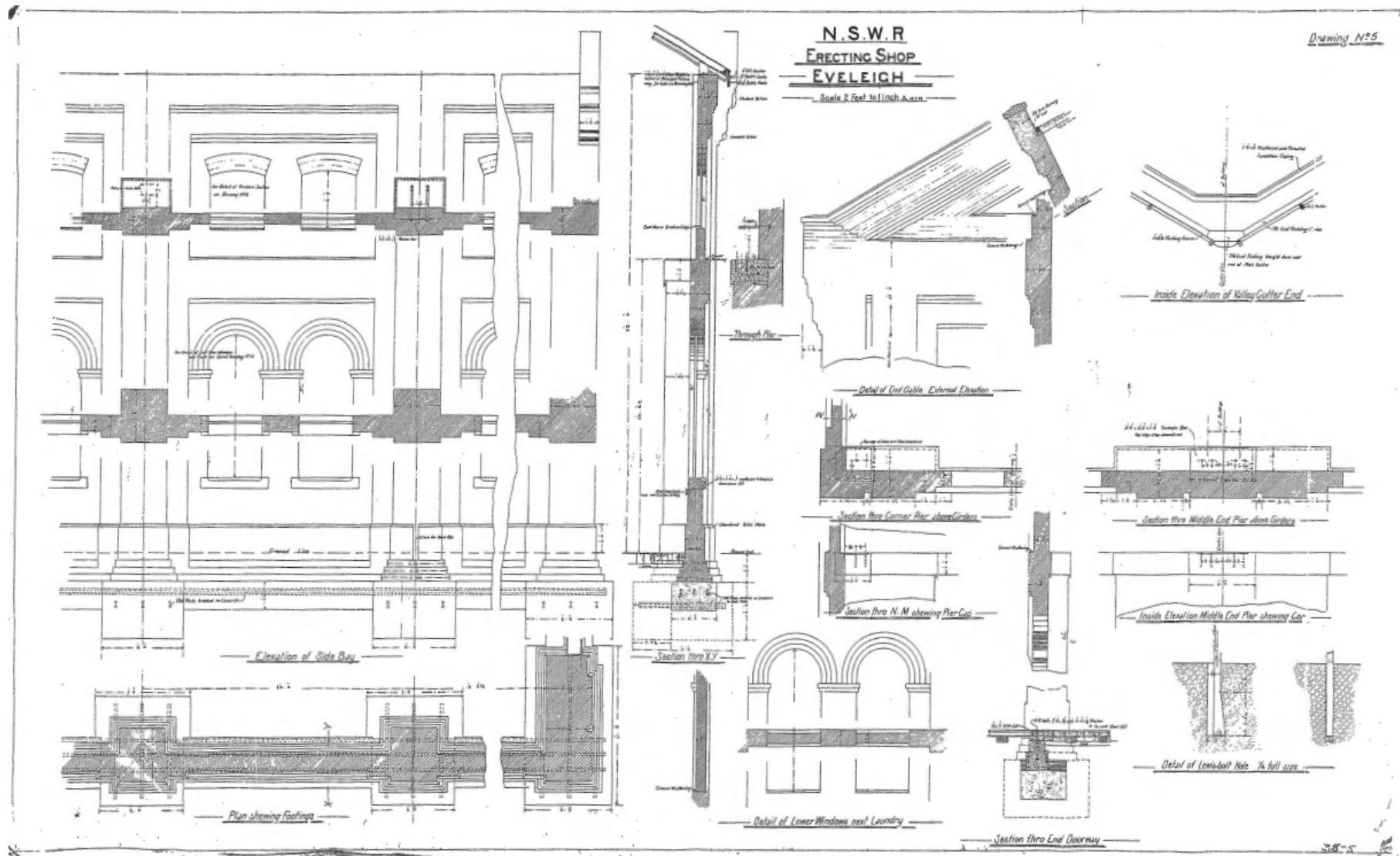


Figure 3.11: Detailed Drawings for the LES Building (formerly known as Erecting Shop), undated. Source: State Rail Authority, NSW State Archives, NRS-20550-2-24-COR/529P2, 38-5.

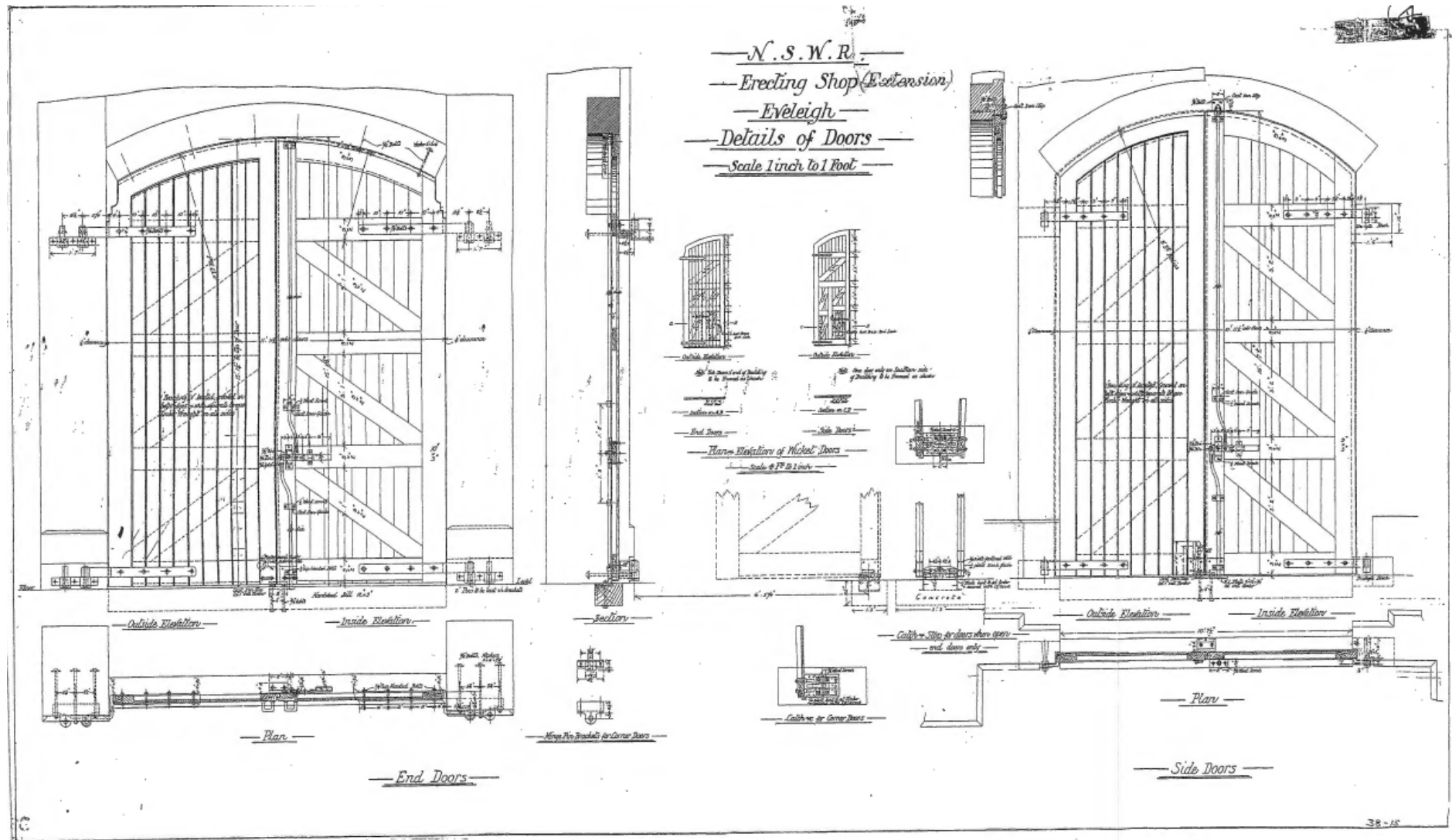


Figure 3.12: Detailed Drawings for the LES Building (formerly known as Erecting Shop), undated. Source: State Rail Authority, NSW State Archives, NRS-20550-2-24-COR/529P2, 38-4.

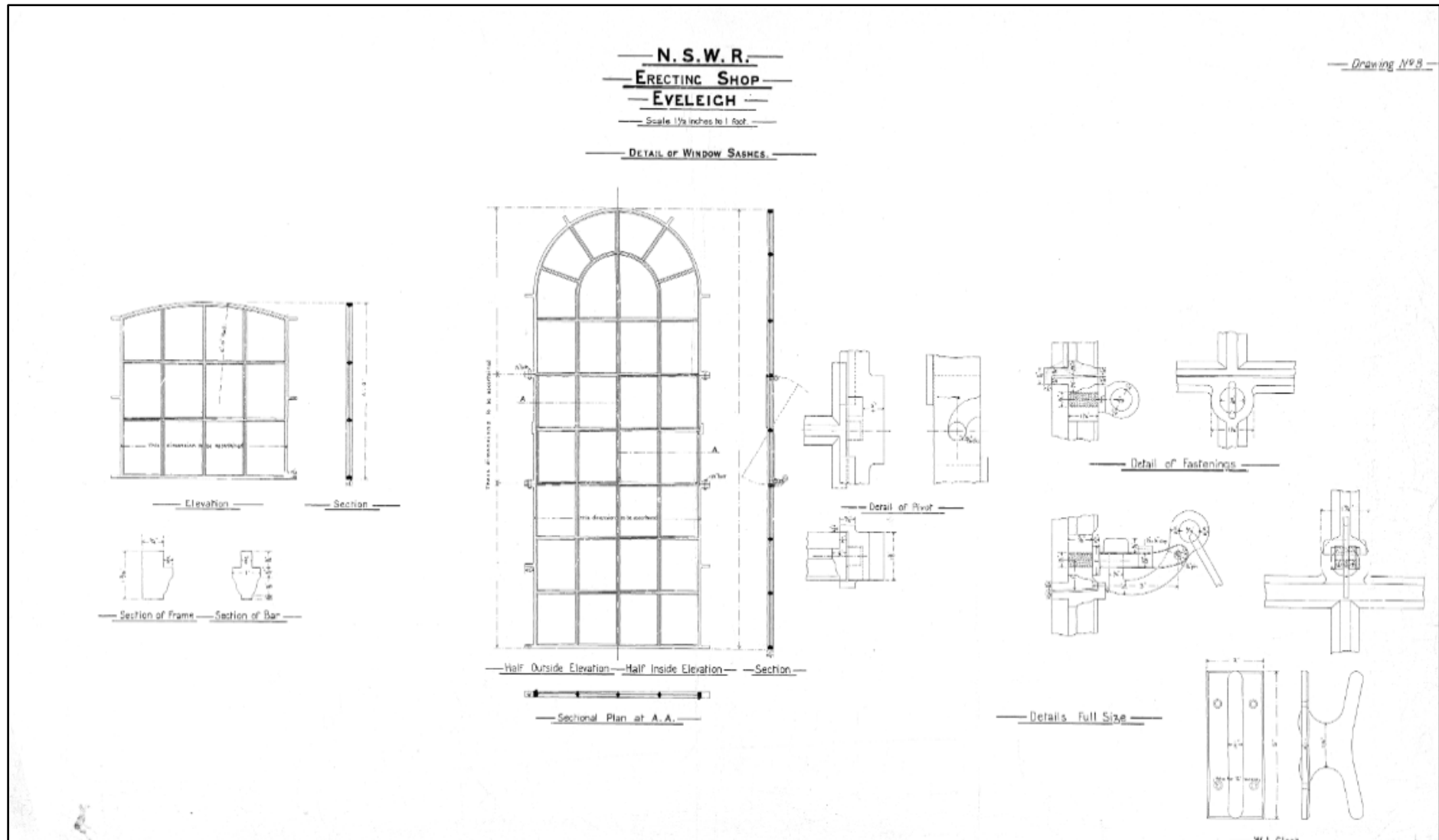


Figure 3.13: Detailed Drawings for the LES Building (formerly known as Erecting Shop), undated. Source: State Rail Authority, NSW State Archives, NRS-20550-2-24-COR/529P2, 38-4.

3.6. Historical Summary Timeline – Large Erecting Shop

Year	Event
Pre-1788	Land that would become the LES is occupied and maintained by the Aboriginal people of the Gadigal Clan.
1835	Land is given via land grant to James Chisholm.
1855	NSW first rail line was constructed, bisecting Chisholm land at Eveleigh.
1887	Eveleigh Railway Workshops were constructed, consisting of the Eveleigh Locomotive Workshops (ELW) to the south of the rail line and the Eveleigh Carriage Workshops to the north.
1899	Large Erecting Shop completed.
c1901	Traverser No. 1 was installed between Locomotive Workshop and Large Erecting Shop, following removal of earlier steam Ground Traversers from Bay 17 and 23 of Carriage Workshops Building.
1906	Large Erecting Shop is extended, constructing an additional 10 bays from the original 20, and increasing floorspace by 50%.
1907	New Locomotive Shop is constructed at the northern end of the Locomotive Workshop. The 'New' Erecting Shop is renamed the 'Large Erecting Shop' to deter any confusion.
1910	By this date, all but two of the overhead cranes are converted from ropes and pulleys to electric power.
1919	New foundry building was constructed to the South-East of the LES.
1924	Two additional overhead travelling cranes are installed into the LES.
1937	'Old' Erecting Shop in Locomotive Workshop is closed and facilities transferred to new Workshops in Chullora
1952	Two offices for sub-foremen with power points and lighting were installed and some pits were deepened 'after a trial'.
1957	In the 1950s, lighting was significantly improved and installed into the LES, as well as five additional power points for equipment and machines.
1965+	LES is altered for the repair and overhaul of diesel engines in addition to steam engines.
1970	LES now exclusively repairs diesel engine locomotives.
1981	All state-owned locomotive work is moved to the Chullora workshops.

Year	Event
1985	Eveleigh Locomotive Workshops are closed, and LES is leased to the heritage steam train operator company, 3801 Limited.
2009	Eveleigh Locomotive Workshops are redeveloped into Australian Technology Park. The LES is not part of the redevelopment.
2017	Limited 3801 terminates its use of the LES.
2017+	LES is owned by TAHE and managed by Transport Heritage NSW. The building sits within the precinct known as the 'Eveleigh Precinct: Fleet Maintenance Division' controlled by Sydney Trains.
2018 - present	Heritage assets across NSW, including a number of items associated with the LES, started to be moved to a new undercover facility at Chullora.

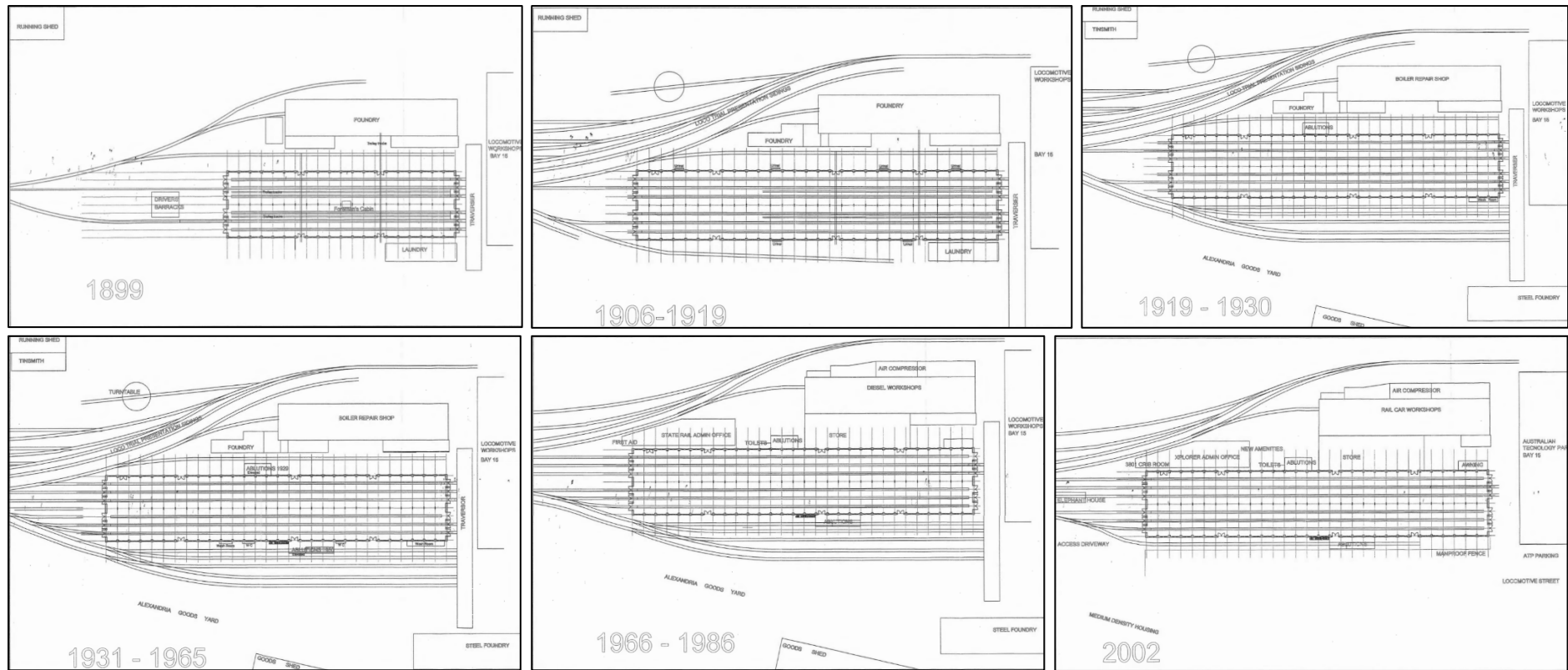


Figure 3.14: Site development plans of the LES from its original construction in 1899 to 2002. Source: 2003 SDA CMP, p.31-36.

4. Physical Analysis

4. Physical Analysis

This chapter provides a detailed summary and physical analysis of the existing structures and features within the subject site, as well as context and overview of the immediate surroundings, including places of importance essential to the understanding of the physical context of the site.

4.1. Eveleigh Precinct (TfNSW)

The LES building is owned by TAHE and managed by Transport Heritage NSW. The building is adjacent to several buildings associated with a variety of activities related to the Sydney Trains operations, and the area is identified as the Eveleigh Precinct. The adjacent buildings to the LES are as follows (Figure 4.1):

- LES Building (subject site)
- Oscar Maintenance Centre (OMC)
- Eveleigh Maintenance Centre (EMC)
- Downer Maintenance Centre (DMC) or Intercity Maintenance Centre (IMC) / RailConnect NSW
- United Group Limited Unipart (UGLU) (Stores)

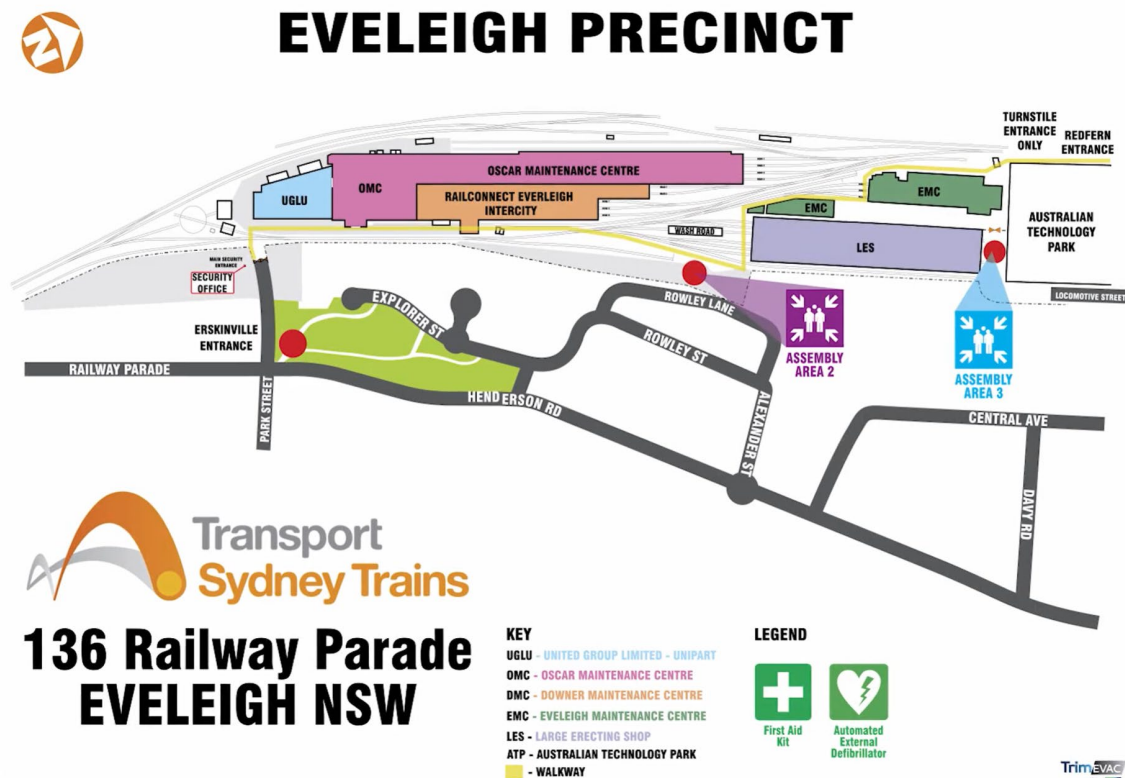


Figure 4.1: Eveleigh Precinct map. The subject site is indicated in lilac. Source: Sydney Trains, 2022.

The Eveleigh Precinct is delineated by the following sites:

- North: Railway corridor and the North Eveleigh Precinct
- West: Railway corridor
- South: Locomotive Street, Channel 7 Global Television and Pacific Magazines Building (South Eveleigh Precinct), residences along Explorer Street and Rowley Lane
- East: Locomotive Workshop Building (South Eveleigh Precinct)

4.1.1. LES Building

The LES building (Figure 4.2 and Figure 4.3) is located in the eastern portion of the Eveleigh Precinct and has a total area of approximately 8300m² with a perimeter of roughly 474m. The building is bounded by Locomotive Street (Figure 4.4) and the Channel 7 Global Television and Pacific Magazines Building to the south; the Eveleigh Maintenance Centre (Figure 4.5), including the Administration Office (Figure 4.6), and an active rail corridor to the north; the Oscar Maintenance Centre (OMC) (Figure 4.7) and the Intercity Maintenance Centre (IMC) / RailConnect NSW building (Figure 4.8) to the west; and the Locomotive Workshop building immediately to the east (Figure 4.9).

The surrounding context of the subject site is characterised by a mix of residential, commercial, educational, and retail uses, located within the suburbs of Alexandria to the south and southeast, Erskineville to the west and Redfern to the northeast.



Figure 4.2: View of the LES western and southern facades. Source: Curio Projects, 2022



Figure 4.3: View of the LES western facade. Source: Curio Projects, 2022.



Figure 4.4: Southern façade of the LES along Locomotive Street and Road 7 Siding. Source: Curio Projects, 2022.



Figure 4.5: Eveleigh Maintenance Centre (EMC). Source: Curio Projects, 2022



Figure 4.6: Eveleigh Maintenance Centre Administration Building. Source: Curio Projects, 2022



Figure 4.7: Oscar Maintenance Centre. Source: Curio Projects, 2022.



Figure 4.8: Intercity Maintenance Centre (IMC). Source: Curio Projects, 2022



Figure 4.9: Locomotive Workshop Building. Source: Curio Projects, 2022

(a) Broader Setting

The LES is located on the southwestern side of the former ERW, adjacent to the South Eveleigh Precinct. The LES building is separated from Bay 15, the western extremity of the Locomotive Workshop, by an open carriageway providing access to the current EMC yard. This area was formerly occupied by a traverser used for transporting locomotives between the Locomotive Workshop and the LES²⁷. The building was designed to accommodate the fabrication, repair, and maintenance of the steam engines of the period in which it was built. The LES remains a prominent single purpose structure surviving on the ERW site. It is the most intact in terms of original fabric and functional layout, despite detracting extensions and redundant services attached to the building²⁸.

The areas to the north and west of the building remain operational for Sydney Trains. Modern workshops and store buildings share the northern wall of the LES for part of its length (Figure 4.5). The LES windows in this section have been bricked over (Figure 4.18). The EMC Administration building for the rail car workshops is adjacent to the north side of the building towards the west end (Figure 4.6). A tall open-ended shed known as the Elephant House spans Road 3 near the western side of the LES²⁹ (Figure 4.10). This shed consists of a structural steel portal frame that is clad with a clip lock style metal external sheeting to the walls and a low pitch gable roof. Built in the 1980s, this

²⁷ SDA 2003 CMP p. 30

²⁸ Ibid p. 35

²⁹ Ibid p. 30

is used as an inspection facility for rail cars by Sydney Trains and has high level catwalks and accessways to facilitate access to the upper reaches of the trains.



Figure 4.10: View of the Elephant House to the west of the LES Building. Source: Curio Projects, 2022.

(b) Exteriors

The structure of the LES remains substantially as when finally completed in 1905 and, consequently, its original fabric and interior configuration are largely intact. The building measuring 184 metres long and 35 metres wide³⁰, externally consists of bare sandstock load-bearing brickwork laid in an English bond pattern with the lowest 9 courses expressed.

The building is divided into 30 bays which are recessed in between engaged and expressed brick piers that run along its length. Corbelled brickwork mid-height transom and top head brickwork span between the piers. Each bay between the piers has four windows. A lower pair of taller semicircular head windows with weathered dressed sandstone sills contain mostly obscure wired glass in a 6 x 4 grid pattern cast iron frame. Six panes at the top of the cast iron frame are separated by cast iron radial muntins that follow the curved head around a muntin split semicircular pane. An openable cast iron sash with two rows of panes high midway pivots horizontally to provide airflow. Above the mid-height brick transom, a pair of smaller flat top arched head windows contain mostly obscure wired glass in a 3 x 4 grid pattern cast iron frame. The three courses of brickwork around the head of the windows are laid radially and are highlighted by their contrasting red colour against the dirty blackened patina of the cream-coloured bricks that make up the majority of the building. A decorative expressed brickwork detail defines the transition point at the base of the semicircular red brick curve on the larger lower windows (Figure 4.11).

³⁰ SDA 2003 CMP p. 31-35



Figure 4.11: LES building facade configuration showing brickwork, engaged columns, and upper and lower window examples.
Source: Curio Projects, 2022

The eastern (Figure 4.12) and western ends (Figure 4.3) of the LES are double gabled with detailed pedimented brickwork and a stone top capping. Each tympanum contains a central ocular window with a 3-course red brick perimeter. These central windows have a vertical band of openable horizontal glass louvres with a fixed pane on either side. 12 flat top arch head windows are matching the style and height of the upper bays on the sides of the building. A centrally located straight ladder supported by cross-bracing back to the façade provides maintenance access to the valley of the roof. Entry to the western end is via six large pairs of arched head heavily braced timber doors that outwardly open on external heavy steel strap hinges. These doors were installed to replace the original doors that were heavily damaged and deteriorated. Six tracks (Roads 1-6) enter the building with a seventh track (Road 7) forming a siding that extends almost the full length of the southern side of the LES and is used to store rolling stock. This siding is bounded by a security fence running parallel between the LES and Locomotive Street.



Figure 4.12: Eastern façade of the LES showing the corrugated iron-clad doors and infills. Source: Curio Projects, 2022

The eastern end of the LES has had all of the doors deteriorate to the extent that they have been sheeted with corrugated iron and held together with temporary repairs³¹ or removed and infilled with a timber frame clad in corrugated iron; however, their physical integrity remains highly compromised. A war memorial Roll of Honour that was ‘Erected by the Large Erecting Shop’ is centrally placed on the eastern facade.

In 1939, corrugated iron sheeted ablution facilities were built on a concrete suspended slab on outrigger brackets about five metres above ground and attached to the outside of the northern wall (Figure 4.13). They were accessed by a set of stairs below which have since been removed. As a result, the facilities are now inaccessible. There are 13 northerly facing windows assumed to be one for each stall and one westerly facing window at a high level. The underside of the ablution block has been partially filled in with brickwork and corrugated iron to form another storeroom below with three lower-level northerly facing windows. The room is currently inaccessible.

The LES has been subject to intermittent ad-hoc modification, with the facade bearing evidence of past minor alterations and extensions (Figure 4.14). Original fabric has been generally left in place or built over³², however, minor changes to brickwork, windows and openings, evidence of roof flashings, sectionally painted walls and remnant fixings, bracketry and redundant services remain scattered across the facades. Three pairs of arched timber side doors are located within the southern wall (Figure 4.15) along with a series of single escape and access doors that appear to have been added to the building over time (Figure 4.16 and Figure 4.17).

³¹ SDA 2003 CMP p. 35

³² Ibid

Corresponding doors are located in the northern wall; however, several have been bricked over to close off the store behind. This is also the case for a few of the northern windows that have been modified into doors or bricked over to close them off (Figure 4.18).



Figure 4.13: Northern Ablutions Facility and Store. Source: Curio Projects, 2022.



Figure 4.14: Evidence of past ad hoc modification. Source: Curio Projects, 2022.



Figure 4.15: South wall double entry doors. Source: Curio Projects, 2022.



Figure 4.16: South wall single entry doors and modified window. Several glass panels of the existing windows are damaged and require replacement. Source: Curio Projects, 2022

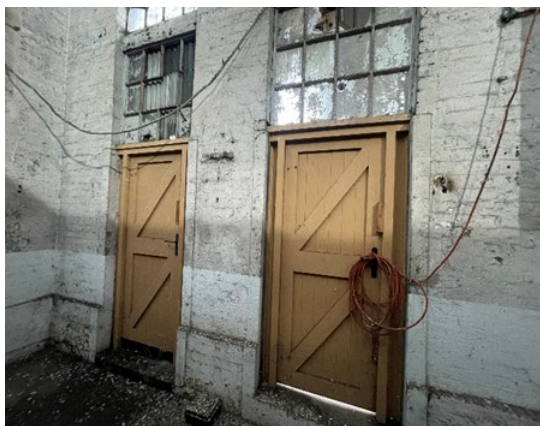


Figure 4.17: South wall single entry doors. Source: Curio Projects, 2022.

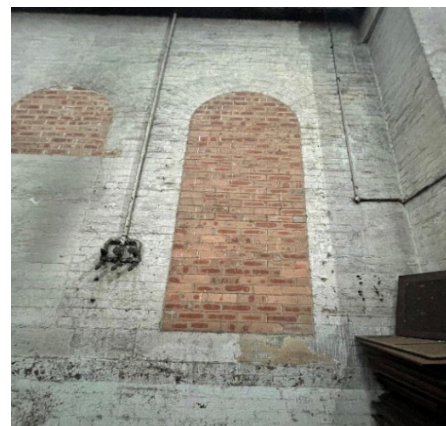


Figure 4.18: South wall double entry doors. Source: Curio Projects, 2022.

Roof and Roof Structure

The LES was reroofed in 1997 with metal gutters and downpipes also replaced³³. The contemporary detail for the reroofing departed from the original and distinctive design of a longitudinal (east-west) raised and curved roof monitor along the two ridges. The new metal roof now has transversely placed Perspex skylights spaced along its length (north-south) and a row of extraction fans with raised circular cowlings spaced evenly along the innermost side of the two ridges providing ventilation. The structure of the double gable roof consists of fabricated steel trusses comprising rail track sections for roof beams and top chords³⁴ and bolted junction plate bracketry joining round steel cross braced framing members and bottom chords.

The roof is supported midspan by a paired row of cast iron columns (Figure 4.19) that also serve for roof water drainage³⁵ and were the support for the central transmission shaft, the overhead crane inner rails and a variety of compressed airlines, steampipes and cabling runs. Electric high bay lights are hung from the roof structure to provide minimal supplementary light when the main natural light source from the skylights is not effective.



Figure 4.19: Cast Iron Columns Supporting the Roof Structure. Source: Curio Projects, 2022

³³ SDA 2003 CMP p. 32

³⁴ Ibid

³⁵ Ibid

(c) Interiors

Internally, the LES is divided into two parallel galleries longitudinally along its central axis by a row of cast-iron pillars which support the roof and the innermost rails for the seven travelling overhead cranes. Each gallery has three parallel rail lines (Roads 1-6) laid along the floor in concrete. The outer two lines in each gallery (i.e., on roads 1, 3, 4 and 6) have concrete access stairs leading to undercarriage inspection pits between the rails to facilitate repair work beneath trains on the respective tracks (Figure 4.20). The full-length pits to the four pit roads and short pits to the clear centre road at the western end remain as originally constructed beside the replacement of original timber slab floors with concrete paving. Several smaller 610mm (2ft) gauge 'tramway' tracks run parallel to the 'pit' tracks³⁶. Men used these tramways to push trolleys loaded with equipment and parts to and from the 'pit areas' to the various other workshops where they were manufactured or repaired.



Figure 4.20: North Eastern View of the Road 1 Pit. Source: Curio Projects, 2022



Figure 4.21: Eastern end of the Southern Gallery. Source: Curio Projects, 2022

Overhead Cranes

Overhead cranes on high level rails with glass windowed underslung operator cabins are spread throughout (Figure 17). The operational status of these cranes is unknown however previous studies indicate that some of them are no longer operational.

The southern gallery contains two cranes:

- No. L 28, installed 1904, DC electric operation, Craven, truss/girder type, located at the western end.
- No. L 25, no date, DC electric operation, two truss type also located at the western end.

The northern gallery Contains five cranes:

- No. L 20, installed 1899, DC electric operation, Craven, cannibalised for DC spares, truss/girder type, located at the west end.
- No. L 23, installed 1904, upgraded to AC electric operation, Craven, girder type 40/12 tonnes lifts, located at the west end.
- No. L 27, installed 1899, upgraded to AC electric operation, girder type 40/12 tonnes lifts, located 1/3 midway³⁷.
- No. LC 829, no date, DC, electric operation, truss type, located 2/3 midway.

³⁶ SDA 2003 CMP p. 14; 34

³⁷ SDA 2003 CMP p. 13



Figure 4.22: North eastern view of the Road 1 pit. Source: Curio Projects, 2022

Internal Fixtures, Fittings and Finishes

The original main switchboards serving the building are located in several places around the perimeter walls. Conduits extend throughout the building in an ad-hoc fashion with cast iron switch boxes and power outlets in various locations throughout. Redundant electrical insulators on metal brackets under the bottom truss chord evidence early primitive power running throughout the building. Compressed airlines and water pipes are run throughout mostly at a high level with dropper outlets abundantly located on the perimeter walls and central columns throughout providing access for the workers.

A series of small moveable timber cabins are scattered around the building most likely placed to suit the operational needs of the time (Figure 4.23). Some have been utilised as offices and others are used as amenities, for storage of archives and records and storage of small parts³⁸.

Various metal locker-style storage cupboards, timber and metal workbenches, drums, train parts and tools are scattered throughout the building, mostly around the perimeter walls and against the central columns (Figure 4.24 and Figure 4.25). A Large cast iron radial arm drill press and a plate roll former are all that remains of the heavy machinery that would have once littered the workshop.

The patina, age, and aesthetics of remnant and/or redundant fixtures, fittings and finishes, including the elements described above add to the overall character of a place and are evidence of the site's former uses. They often add to the interpretative experience of a site and can be utilised to demonstrate the evolution of the site, both in terms of technological advances and change of use.

³⁸ SDA 2003 CMP p. 33



Figure 4.23: Workshop cabin between Road 3-4 East.
Source: Curio Projects, 2022.



Figure 4.24: Metal tool storage cupboard example. Source: Curio Projects, 2022.



Figure 4.25: Workbench located between Road 3-4 West.
Source: Curio Projects, 2022.



Figure 4.26: Plate rolling machine located on Road 4 East.
Source: Curio Projects, 2022

(d) Moveable Heritage

Table 4.1 provides a preliminary list of moveable heritage items that have been listed on the Section 170 Heritage Conservation Register (2013) and have been identified as being located within the LES. During Curio's site visit on 13 May 2022, the majority of these items were not located within the building, however, a number of heritage-listed items noted to be located in different precincts were situated within the subject site.



A drilling machine produced by William Asquith Ltd of Halifax, England, was also noted during the site visit. The machine was likely to be fabricated in the 1920s and could be part of the original machinery related to the ERW activities. Although not listed on the ATP or the TAHE S170 Registers, further investigation is recommended to assess the machine's use, significance, and detailed historical background.








Figure 4.27: Drilling machine produced by William Asquith Ltd of Halifax, England, likely fabricated in the 1920s. The item is not listed as part of the ATP or Railcorp S170 Registers. Source: Curio Projects, 2022.

Further, it is recommended that an overall revised and updated survey of all the moveable heritage items and associated collections within the subject site be undertaken as part of the next phase of works for the project.



Table 4.1: Moveable Heritage Items within the LES Building noted from Curio's site visit



Item Name SHI #	Current Location	Images
TAM 1883 – Main-line Sleeping Car SHI #4807079	Within the LES.	 
FRN 2186 – Second-class Sitting / Buffet Car SHI #4807101	No longer within the LES.	N/A
Locomotive, Diesel 4401 SHI #4807242	No longer within the LES.	N/A
Locomotive, Diesel Shunting 7344 SHI # 4807250	No longer within the LES.	N/A
MFS 2028 – Second-class Sitting Car SHI #4807097	No longer within the LES.	N/A
MFS 2096 – Second-class Sitting Car SHI #4807089	No longer within the LES.	N/A
MFS 2121 – Second-class Sitting Car SHI #4807091	No longer within the LES.	N/A

Item Name SHI #	Current Location	Images
MFS 2145 – Second-class Sitting Car SHI #4807095	No longer within the LES.	N/A
AAH 8 – Officer's Inspection Carriage SHI # 4807010	Within the LES. According to the s170 Register, the item was supposed to be located in the Broadmeadow Locomotive Depot.	
HX 1006 – Pullman Sitting Car SHI #4807004	Within the LES. According to the s170 Register, the item was supposed to be located in the Thirlmere Railway Precinct.	 

Item Name SHI #	Current Location	Images
CBC 1089 – Pullman Sitting Car SHI # 4807005	Within the LES. According to the s170 Register, the item was supposed to be located in the Thirlmere Railway Precinct.	
CBC 1090 – Pullman Sitting Car SHI # 4807006	Within the LES. According to the s170 Register, the item was supposed to be located in the Thirlmere Railway Precinct.	

4.1.2. Other Buildings within the Eveleigh Precinct

Building	Location & Description	Images
Oscar Maintenance Centre (OMC)	<p>The OMC is located to the north west of both the LES and South Eveleigh Precincts and within the rail corridor that creates the north and western borders of the site, the OMC is of structural steel portal frame construction with precast concrete decorative façade panelling bays separated by expressed precast columns that are contrasting in colour to the bays. The majority of the bays have a corrugated fibreglass light panel in the upper half. The OMC has a metal clip lock style roof with transversely located Skylight panels and metal gutters and downpipes.</p>	
Eveleigh Maintenance Centre (EMC)	<p>The EMC is located to the north of the LES at the eastern end and outside the north western corner of the South Eveleigh Precinct. The EMC consists of a large main shed which is of metal construction with metal clip lock style sheeting, integrating a store that is attached to and shares a common wall with the north eastern side of the LES. The main shed has a gable roof with three large openings on both the eastern and western ends with tracks running through. The store has a flat roof and two smaller vehicular entries on each end. The eastern end of the store has an open yard that contains a small awning against the Northern wall of the LES. The EMC is accessed from Locomotive Street via a vehicular carriageway between the eastern end of the LES and Bay 15 of the Locomotive Workshop or via the rail corridor to the west.</p> <p>A second building used as the EMC administration building is fabricated from brick with a flat metal roof and is located to the north of the LES at the eastern end. The building has a singular story at its western end and is a double story at its eastern end.</p>	

Building	Location & Description	Images
<p>Downer Maintenance Centre (DMC) or Intercity Maintenance Centre (IMC) / RailConnect NSW</p>	<p>Located to the west of both the LES and South Eveleigh Precincts and within the rail corridor that creates the north and western borders of the site, the IMC adjoins the Southern wall of the OMC Facility at the most western end. The IMC is of structural steel portal frame construction with precast concrete decorative façade panelling bays separated by expressed precast columns that are contrasting in colour to the bays. The majority of the bays have a corrugated fibreglass light panel in the upper half. The IMC has a metal clip lock style roof with transversely located Skylight panels and metal gutters and downpipes.</p>	
<p>United Group Limited Unipart (UGLU) (Stores)</p>	<p>The UGLU is located at the western end of the Eveleigh Precinct. This building signposted as the Administration and Production HUB is attached to the OMC at the western end. The building is fabricated from brick with a flat metal roof and is located to the north of the LES at the Eastern end. The building is a double-storey with a single-storey attachment to the north adjoining an awning.</p> <p>A curved roof shed fabricated of clip lock style metal sheeting with two large roller doors on its eastern end is the most western building of the Eveleigh Precinct. It is located across a vehicular carriageway to the west of the UGLU.</p>	

4.2. South Eveleigh Precinct

The South Eveleigh Precinct is adjacent to the Large Erecting Shop to the northwest and takes up what was once the southern side of the wider ERW. Three key heritage items remain within the Southern Eveleigh Precinct curtilage. These include:

- Locomotive Workshop
- Former Works Managers Office (currently The Bell Tower)
- Water Tower
- Former New Locomotive Workshop (currently National Innovation Centre)

Modern Buildings on Site include the

- The Axel Building
- The Foundry Building
- Channel 7 Global Television and Pacific Magazines (Media City) Building
- South Eveleigh Community Building
- National Information and Communication Technology Australia Ltd (NICTA)
- Biomedical Building
- NSW Transport Management Centre / Sydney Ambulance Centre

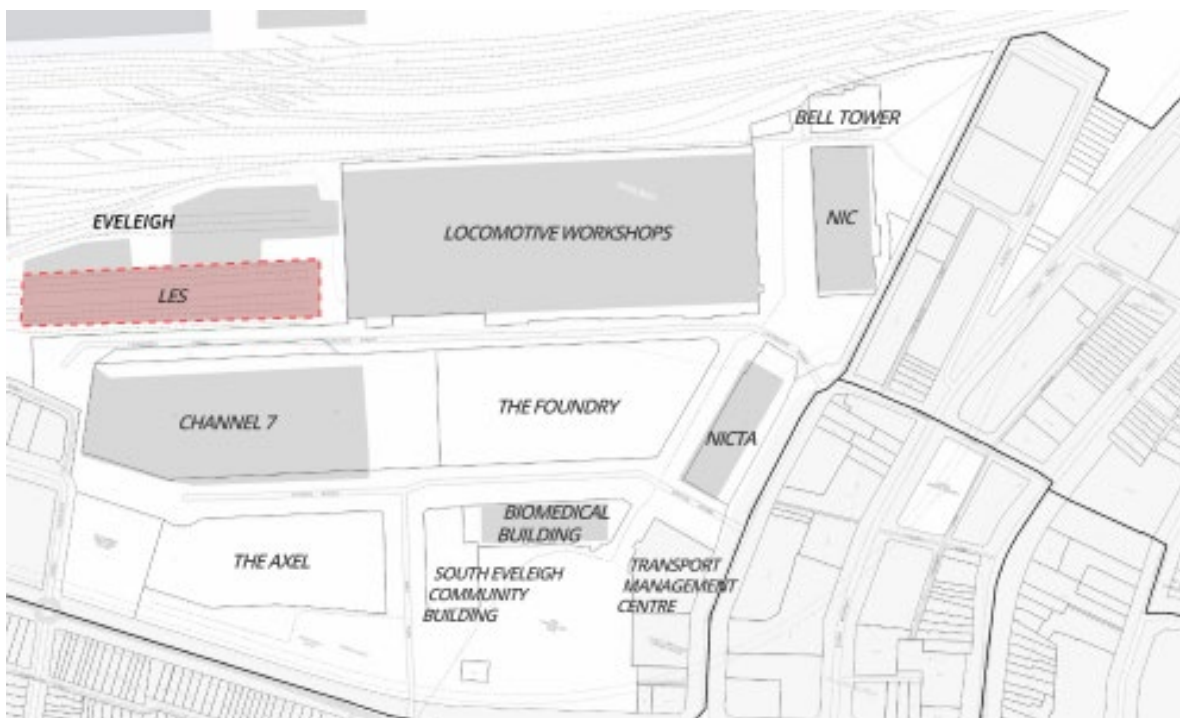


Figure 4.28: The subject site and the existing buildings within the South Eveleigh Precinct. Source: Curio, 2022.

4.2.1. Locomotive Workshop

The Locomotive Workshop (Figure 4.29) comprises a two-story structure made of sandstone bricks and contains sixteen work bays of equal size (Bays 1-15). Built in 1887, the Locomotive Workshop was responsible for the manufacture of the iron and steel components of steam locomotives and contained areas for construction, repair, and dismantling as well. The building has been modified in use and appearance several times, including its redevelopment in 1993 as part of the Australian Technology Park, and its subsequent transferal to Mirvac from 2016 onwards. As part of Mirvac's South Eveleigh Precinct, the Workshop is now an industrial-themed multi-use space hosting a variety

of commercial and retail tenancies, as well as an interpretive exhibition comprising a substantial portion of the heritage moveable collection, including equipment, machinery, tools and other interesting objects uncovered during the construction stage.



Figure 4.29: The Locomotive Workshops building. Source: Mirvac, 2022, accessed on 17 March 2022 from <https://www.mirvac.com/office-and-industrial/office/nsw/locomotive-workshop>.

4.2.2. The Bell Tower – Former Workers Managers Office

The former Works Managers Office is in the northeast corner of South Eveleigh Precinct and is part of the heritage-listed curtilage of the ERW, being located to the northeast of the Locomotive Workshop and opposite the National Innovation Centre. The building was renamed The Bell Tower in 2021 and consists of a two-storey rendered masonry building, painted light grey with maroon trimmings and details. It was also subject to relatively minor changes in 2021 to allow the building to be adaptively reused for small-scale offices and meeting rooms³⁹.

The Works Managers' Office served as the main office for the managerial and administrative staff of the ELW, including timekeepers, paymasters, and a variety of clerks. Completed in 1887, the original building was expanded in 1922 and again in 1944⁴⁰. The latter addition gave the building its signature T shape and is retained in the building's current external form. The building operated as commercial and management offices from 1994-5, known as the International Business Centre, receiving several interior renovations and reconfigurations, before being refurbished and renamed The Bell Tower in 2022 for its continued use as office space. The name for the 'Bell Tower' came from the roof-mounted bell which was used to alert the workers to the start of the day, breaks and end of the day. The bell was so accurate that residents from Redfern all the way to Bondi set their watches by the ring of the daily bell⁴¹.

³⁹ Weir Phillips, 2020. *Heritage Impact Statement – The Former Work Manager's Office, The International Business Centre*.

⁴⁰ Gazzard Sheldon Architects, 1992. *Conservation Plan for the Work Managers Office Eveleigh Railway Workshops*, p.5-6

⁴¹ Australian Railway Historical Society, R.G Preston, 1997. *The Eveleigh Locomotive Workshops Story*, p.4



Figure 4.30: Western and southern facades of the former Work Managers Office. Source: Curio Projects, 2022.



Figure 4.31: Eastern facade of the former Works Managers Office, now The Bell Tower. Source: Curio Projects, 2022.

4.2.3. Water Tower

The Water Tower, located opposite The Bell Tower, is a square, wrought iron and steel structure that served as a water reservoir for the Eveleigh area, and now serves as a landmark and wayfinding aid⁴². It was recently subject to major conservation and safety upgrade works to ensure its preservation for the future⁴³.



Figure 4.32: Water Tower. Source: Curio Projects, 2022.

4.2.4. National Innovation Centre – Former New Locomotive Workshop

The New Locomotive Workshop building, currently the National Innovation Centre (NIC) (Figure 4.33 and Figure 4.34), served as one of the later buildings in which the new locomotives were constructed on site and is located to the east of the Locomotive Workshop building⁴⁴. Constructed as a masonry building with steel columns and a corrugated iron roof, the building once consisted of two bays fitted out as a workshop for locomotive construction. It also served as one of the workshops' only proper washing facilities. The building has since been retrofitted into a multi-purpose office space for Cicada Innovations, consisting of three levels and a central atrium⁴⁵.

⁴² Godden Mackay Logan 2013, Australian Technology Park CMP Vol.1, p.48-49

⁴³ Ibid p.49

⁴⁴ Ibid p.48

⁴⁵ Ibid



Figure 4.33: View of the northern and eastern facades of the NIC building to the left and The Bell Tower building to the right.
Source: Curio Projects, 2022.



Figure 4.34: Former New Locomotive Workshop, now National Innovation Centre. Source: Cicada Innovations, 2022, accessed 20 May 2022 from <https://www.cicadainnovations.com/about-2>

4.2.5. The Foundry

The Foundry is an eight-storey building located directly to the south of the Locomotive Workshop across Locomotive Street (Figure 4.35). Constructed in 2020, the building stands on what was once the Foundry for the Locomotive Workshop and was responsible for the casting of tools and locomotive parts from a variety of moulds. The building now serves as major office space for the Commonwealth Bank of Australia, as well as housing a variety of commercial and retail tenancies⁴⁶.



Figure 4.35: The Foundry Building. Source: Mirvac, 2019, accessed 19 May 2022 from <https://www.sustainablebuildingawards.com.au/commercial-architecture-large-2020/the-foundry/>

4.2.6. The Axel Building

The Axel Building is nine stories high and located directly to the south of the Channel 7 building and west of the SEBC, making use of glass and orange coloured panelling (Figure 4.36). Similar to The Foundry building, the majority of the Axel building houses commercial offices connected with the Commonwealth Bank of Australia with retail tenancies located on the ground floor.

⁴⁶ Godden Mackay Logan 2013, *Australian Technology Park CMP* Vol.1, p.12, 45



Figure 4.36: The Axel Building. Source: FJMT Studio, 2020, accessed 19 May 2022 from <https://fjmtstudio.com/projects/axle-south-eveleigh/>

4.2.7. The Channel 7 Global Television and Pacific Magazines (Media City) Building

The Channel 7 Global Television and Pacific Magazines (Media City) Building is located directly to the south of the Large Erecting Shop and west of The Foundry and consists of an eleven-storey media complex, which houses the head offices for the Seven Network, Pacific Magazines, and Global Television. Built in 2010, the building is made predominantly of reinforced concrete with external steel shading which alludes to the wider precinct's industrial nature. The southwestern wing of the building is much lower, with only 4 stories, to mirror the height of the adjacent LES and Locomotive Workshop, softening the transition between the heritage and the contemporary buildings⁴⁷.



Figure 4.37: Channel 7 Global Television and Pacific Magazines (Media City) Building. Source: Google Maps, 2022.

⁴⁷ Godden Mackay Logan 2013, Australian Technology Park CMP Vol.1, p.49

4.2.8. South Eveleigh Community Building

In the centre of the South Eveleigh Precinct, to the southwest of The Foundry Building and east of The Axel building, is the precinct's community centre, the South Eveleigh Community Building (Figure 4.38). Completed in 2019, the four-storey building contains a variety of community spaces, including a leisure space and a childcare centre, as well as a number of retail tenancies. The rooftop of the SEBC also comprises a public accessible native rooftop garden, using principles of traditional indigenous plant knowledge to cultivate and manage over 2,000 edible and medicinal native plants⁴⁸ (Figure 4.39).



Figure 4.38: South Eveleigh Community Building. Source: Architecture and Design, accessed 19 May 2022
<<https://www.architectureanddesign.com.au/projects/landscape-urban-design/yerrabingin-indigenous-rooftop-farm#>>

⁴⁸ Yerrabingin 2020, South Eveleigh Native Rooftop Farm, accessed 20 May 2022 from <https://www.yerrabingin.com.au/projects/rooftop-farm>



Figure 4.39: South Eveleigh Community Building Rooftop Garden. Source: Yerrabingin website, accessed 19 May 2022
<<https://www.yerrabingin.com.au/projects/rooftop-farm>>

4.2.9. National Information and Communication Technology Australia Ltd (NICTA)

The National Information and Communication Technology Australia Ltd (NICTA) research facility is located directly south of the National Innovation Centre and was completed in 2008 (Figure 4.40). Constructed of weather steel cladding to connect to the industrial context of the precinct, the research facility currently houses the offices and research space for CSIRO Data61 as well as the Defence Science and Technology Group⁴⁹.



Figure 4.40: NICTA building. Source: Google Maps, 2022

⁴⁹ Godden Mackay Logan 2013, *Australian Technology Park CMP* Vol.1, p.49

4.2.10. Biomedical Building

The Biomedical Building is among the oldest of the precinct's non-heritage buildings and was completed in 2000 (Figure 4.41). The four-storey purpose-built research facility is located directly east of the SECB House and abuts the northern side of the Eveleigh Green and was also designed with materials that reflected the industrial feeling of the wider South Eveleigh Precinct⁵⁰. Current tenants include the Australian National Institute of Management and Commerce, the ACRF Image X Institute, and Charlotte's Little Sister café.



Figure 4.41: Biomedical Building. Source: Google Maps 2022.

4.2.11. Transport Management Centre / Sydney Ambulance Centre

Finally, abutting the western end of the Eveleigh Green, the Sydney Ambulance Centre is also among the oldest buildings within the South Eveleigh Precinct (Figure 4.42). The three-storey red brick building now houses the NSW Ambulance Service as well as the Transport Management Centre⁵¹.



Figure 4.42: The Sydney Ambulance Centre. Source: Google Maps, 2022.

⁵⁰ Ibid p 48-49

⁵¹ Godden Mackay Logan 2013, *Australian Technology Park CMP* Vol.1, p.49

4.3. North Eveleigh Precinct

The North Eveleigh Precinct is delineated to the north by Wilson Street and to the south by the railway corridor. North Eveleigh includes a number of extant significant heritage buildings and structures, as well as a number of later and modern structures that occupy the precinct.

Many of the main built items in North Eveleigh of the former ERW are individually identified as heritage items within the State Heritage Listed Eveleigh Workshops. They are identified as the:

1. Carriage Workshops (known as Carriageworks) (Figure 4.43)
2. Blacksmith's Workshop (Figure 4.44)
3. Paint Shop (Figure 4.45)
4. Scientific Services Building No. 1 (Figure 4.46)
5. Chief Mechanical Engineer's Office Building (Figure 4.47)
6. Telecommunications Equipment Centre (Figure 4.48)
7. Clothing Store (Figure 4.49)



Figure 4.43: North facade of the Carriageworks, North Eveleigh. Source: Curio Projects, 2021.

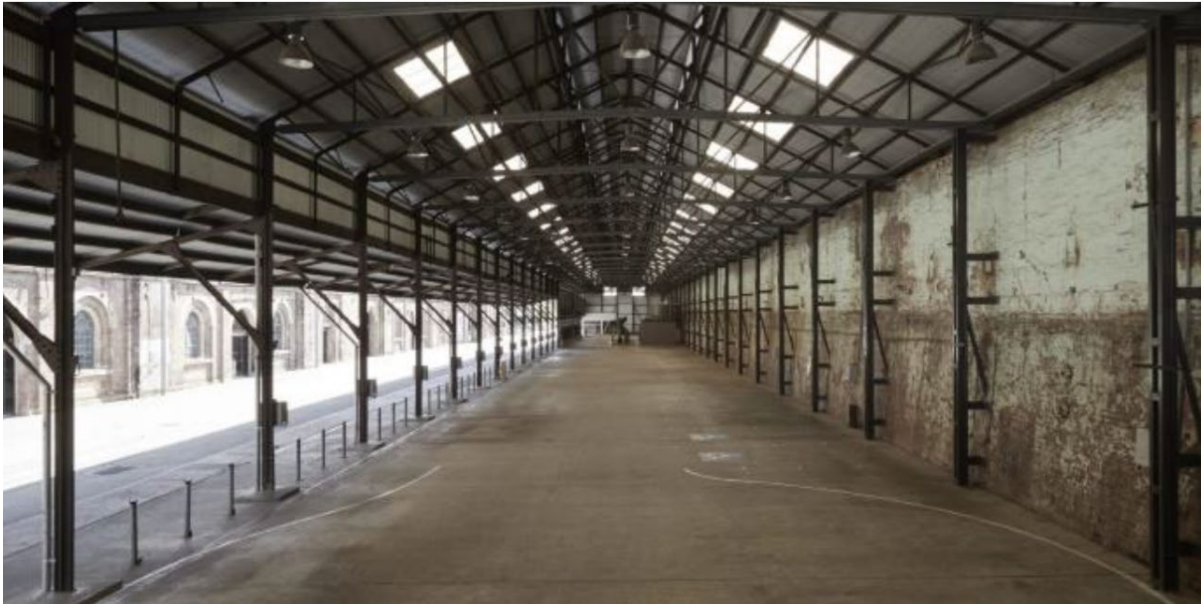


Figure 4.44: Internal view of Blacksmith Workshops looking towards the west, North Eveleigh. Source: Curio Projects, 2021.



Figure 4.45: Eastern facade of the Paint Shop and Suburban Car Workshops from the Fan of Tracks. Source: Curio Projects, 2021.



Figure 4.46: Northwestern facade of the Scientific Services Building No.1. Source: Curio Projects, 2021.



Figure 4.47: Northern facade of the Chief Mechanical Engineers Building (CME Building) from Wilson Street. Source: Curio, Projects, 2021.



Figure 4.48: Western facade of the TEC building. Source: Curio Projects, 2021.



Figure 4.49: Northern facade of the Clothing Store building. Source: Curio Projects, 2021.

4.4. Neighbourhood Context

The Large Erecting Shop is located to the southwest of the South Eveleigh Precinct's Locomotive Workshop, along the western end of Locomotive Street. To the north sits the Sydney train line, along which nearly every major line passes before or after it reaches Redfern Station⁵² (Figure 4.50).

The corridor between the LES and the Locomotive Workshop once held a traverser to transport locomotives between the two buildings⁵³ (Figure 4.51). It is now an access lane for Sydney Trains Eveleigh Precinct to access the buildings adjacent to the northern facade of the LES, known as the 'Eveleigh Precinct: Fleet Maintenance Division'. The access lane is frequented by trucks and cars coming in and out of the Fleet Maintenance Division. The access lane also houses a large-scale plant room for the Locomotive Workshop (to the east of the LES) and provides a main entry point into the subject site. The introduction of the c.1990s plant room to facilitate the upgrades to the Locomotive Workshop and the loss of the traverser from this location have impacted the relationship between the LES and the Locomotive Workshop and the grandeur and character of the entrance into the LES.

To the southeast of the LES, sits the South Eveleigh Village Square and The Foundry building (Commonwealth Bank of Australia). The square contains a large community and park area with a variety of art installations and seating areas with direct access to Central Avenue. This provides the LES with direct access to a community area as well as access to the rest of the South Eveleigh Precinct⁵⁴.

Opposite the LES, to the south along Locomotive Street, is the headquarters for Seven Western Media (SWM) (Channel 7), which continues west until Rowley Lane (Figure 4.52 to Figure 4.54). This eleven-storey building is generally accessible through Locomotive Street itself or via the South Eveleigh Village Square, which links the LES to Central Avenue to the south and contains a variety of sculptural and community-minded structures and trees.

The publicly-accessible areas in-between the LES and SWM building along Locomotive Street is a trafficable zone that consists of a concrete roadway, lined with gum trees and security fencing on either side, which ends in a cul-de-sac with a turning circle (Figure 4.55 to Figure 4.57). The southern facade of the LES is visible along the entire road, although it is somewhat obstructed by the security fencing, as is the northern facade of the Channel 7 Global Television and Pacific Magazines (Media City) Building.

Beyond this turning circle sits a small park-lined footpath leading to Rowley Lane, which contains a row of five to six storey terraced apartment buildings (Figure 4.58 and Figure 4.59). These apartments have a clear line of the site towards the western facade of the LES, including the tracks leading out from the LES into the main train line.

⁵² Simpson Dawbin Architects, Large Erecting Shop CMP, 2003, p.10

⁵³ Ibid p.22

⁵⁴ Godden Mackay Logan 2013, *Australian Technology Park CMP* Vol.1, p.12, 45



Figure 4.50: View of the wider Sydney rail line and maintenance site from the western end of the LES. Elephant house to the left. Source: Curio Projects 2022.



Figure 4.51: View of the access lane between the eastern facade of the LES and the western facade of the Locomotive Workshop. Source: Curio Projects, 2022.



Figure 4.52: View of the Commonwealth Bank Building and the Channel 7 Global Television and Pacific Magazines (Media City) Building from the southern facade of the LES. Source: Google Maps, 2022.



Figure 4.53: View of the Channel 7 Global Television and Pacific Magazines (Media City) Building to the left and the eastern facades of the LES and Locomotive Workshop from the South Eveleigh Village Square. Source: Curio Projects, 2022.



Figure 4.54: View of the Locomotive Street continuing in between the LES and the Channel 7 Global Television and Pacific Magazines (Media City) Building. Source: Curio Projects, 2022.



Figure 4.55: Continuing view down Locomotive Street towards Rowley Lane. Source: Curio Projects, 2022.



Figure 4.56: Western end of Locomotive Street ending in an open cul-de-sac. Source: Curio Projects, 2022.



Figure 4.57: View of the Locomotive Street cul-de-sac with a view towards Rowley Lane and apartments. Source: Google Maps, 2022.



Figure 4.58: View of Rowley Lane apartments. Source: Google Maps, 2022.



Figure 4.59: View of Rowley Lane facing the LES. Source: Google Maps, 2022.

5. Non-Aboriginal Archaeology

5. Non-Aboriginal Archaeology

This chapter has been drawn from the *Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal* prepared by Curio (2022). For a full overview of the archaeological background of the subject site, reference should be made to the original report.

The extent to which physical remains of past occupation may survive across any site is dependent on two main factors: firstly, the nature of the archaeological resource; and secondly, the nature and extent of subsequent development and modifications at a site that may have impacted the deposition or conservation of the archaeological resource. While each subsequent phase of development and occupation may contribute new deposits and features to the archaeological record, it may also remove or disturb deposits and features associated with previous phases of occupation.

5.1. NSW Heritage Act 1977

While the *Heritage Act 1977* protects items listed on the State Heritage Register across NSW, historical (non-Aboriginal) archaeological remains in NSW are additionally protected from being moved or excavated through the operation of the ‘relics’ provisions of the Act. An archaeological site is an area of land which is the location of one or more archaeological ‘relics’. A ‘relic’ is defined as:

any deposit, artefact, object or material evidence that:

- (a) Relates to the settlement of the area that comprises New South Wales, not being Aboriginal settlement*
- (b) Is of State or local heritage significance.*

As the subject site (LES) is listed on the SHR as part of the overall ERW complex, all archaeological deposits are further protected from harm as part of that gazetted listing. Impact to any archaeological deposits within the curtilage of the ERW SHR listing generally requires an approval under the s.57 provisions of the Heritage Act to be in place⁵⁵.

5.1.1. ‘Relics’ vs ‘Works’

In the management of historical archaeology at the subject site, it is appropriate to consider and acknowledge the archaeological management process and approach that has recently been applied with success to the South Eveleigh Precinct during Mirvac’s redevelopment of the precinct (former ATP) since 2015.

Consultation between Curio Projects and Heritage NSW archaeologists in 2015-2016 in relation to the management of historical archaeological resources within the South Eveleigh Precinct focused substantially on discussions involving how to address, define, and differentiate between the nature of archaeological ‘relics’, and in or below ground remains or structures pertaining to existing structures and heritage buildings. This resulted in the following definition being established and adhered to for the management of ‘relics’ vs ‘works’ throughout South Eveleigh redevelopment works.

In situ industrial archaeological resources (such as roadways, railway tracks, tram tracks, kerbstones, culverts, milestones, remnant flues, and other related below-ground infrastructure) are managed

⁵⁵ With the exception of developments approved as SSD or SSI provisions of the EP&A Act, in which case the process that would normally be undertaken to seek archaeological approvals under the Heritage Act remains generally the same, however the requirement for a final approval under s57 or s60 of the Heritage Act is not required for an SSD or SSI development after SSD approval.

and defined by Heritage NSW as ‘works’, as opposed to as archaeological ‘relics’ (as defined by the relics provision of the Heritage Act).

For example, this approach was applied in the consideration and management of the remnant Foundry walls at South Eveleigh, which, while sections of the former building remained on the site following the demolition of the bulk of the building in the 1990s as part of the former ATP development, were above ground remnant fabric, and not an archaeological deposit. The Foundry walls were thus subject to archival recording prior to removal, with remnant fabric to be retained and stored where possible for potential use in an interpretative context at a later stage of the development. A similar approach was applied to the management and recording of features uncovered in the floor of the Locomotive Workshops once the modern concrete floor poured as part of the 1990s former ATP development had been removed.

As the LES and South Eveleigh are both elements of the same overarching site of the former ERW, it is therefore considered appropriate that a consistent approach be applied to the management of archaeological resources between the two precincts. Therefore, it is proposed that a similar differentiation between ‘relics’ and ‘works’ (where relevant) be applied to the LES study area, as while both relics and works may have the potential to be present, the way in which they are required to be managed may differ. This topic is further discussed and detailed in Section 5.4 below.

5.2. Previous Archaeological Investigations

Context and information for this section regarding historical archaeological context and potential for the LES building have been primarily drawn from Curio’s 2022 Redfern North Eveleigh Non-Aboriginal Heritage Study as a comparative analysis of studies was required, and it has been referenced when appropriate.

5.2.1. South Eveleigh Archaeological Monitoring (Curio Projects 2017-2020)

When the South Eveleigh Precinct (former Eveleigh Locomotive Workshop) underwent redevelopment, Mirvac commissioned Curio Projects to prepare a number of reports to support the Stage 1 Concept Plan SSDA for the redevelopment of the precinct. A Historical Archaeological Research Design and Excavation Methodology (ARD +EM)⁵⁶ report was developed and outlined a site-specific program of archival recording and controlled archaeological excavation to be undertaken and included in the proposed remediation, demolition and ground preparation works for the South Eveleigh precinct.

From 2017-2020, Curio Projects undertook a series of archaeological monitoring activities within the South Eveleigh Precinct. Activities included archaeological monitoring and controlled excavation of works and relics across the following primary locations at South Eveleigh:

- “Building 2” (location of the former Eveleigh Foundry), now known as The Foundry; and
- Within the Locomotive Workshop building.

Below provides a summary of the results and findings that were uncovered during these archaeological monitoring works to offer context for similar archaeological resources and historic works that are likely to be encountered within the LES study area.

⁵⁶ Curio Projects 2016, *Historical Archaeological Research Design and Methodology for Australian Technology Park*, Prepared for Mirvac.

Building 2 (The Foundry)—Foundry Remains

Between February and June 2017, a site-specific program of archaeological monitoring and archival recording took place as part of pre-construction works for South Eveleigh “Building 2” (now completed building, referred to as “The Foundry”).

Before excavations took place, the Building 2 impact area was made up of terraced car parking of asphalt and concrete with grassed islands between terraces. Significant portions of the old foundry were kept in situ between the levels of the car park and were backfilled in to create the terracing. The southern foundry wall and the retaining wall which was connected to the foundry dressing shop were identified within the car park before excavation commenced. As part of the development of Building 2, archival photographic recording of the Foundry Wall was undertaken⁵⁷.

During the excavation works for Building 2 footprint, archaeological monitoring and recording focused on recording the former foundry features that were uncovered by excavation works (Figure 5.1), including remains of the wheel press and tinsmith workshops located in the north of the building footprint and in the south, parts of the main steel, brass and iron foundry, as well as remains of the Blower House (Figure 5.3), iron foundry core oven, cupolas, furnaces and other associated foundry features and associated artefact deposits (Figure 5.2)⁵⁸.

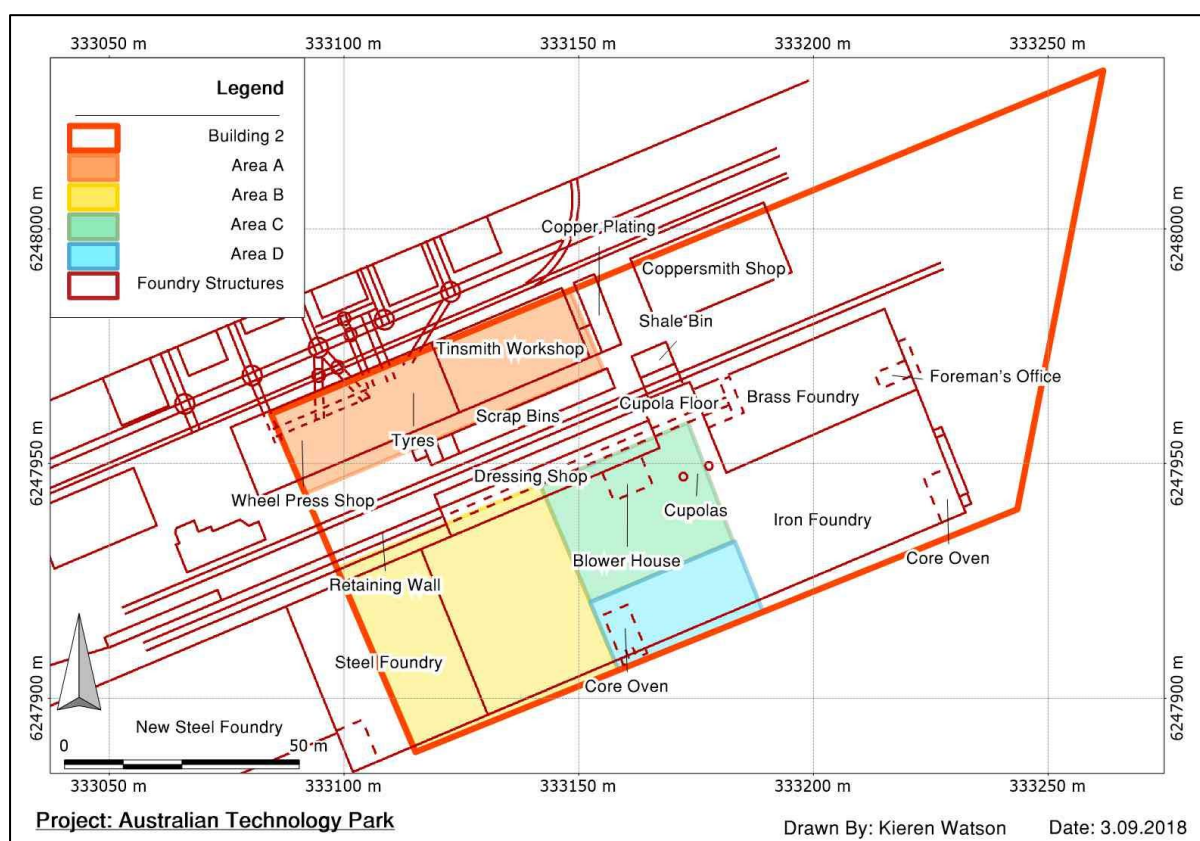


Figure 5.1: The Foundry Excavation and Monitoring zone with the former Foundry plan overlaid. Source: Curio Projects, 2018.

⁵⁷ Curio Projects 2019, *Former Foundry, South Eveleigh- Archaeological Monitoring and Monitoring Works*, Prepared for Mirvac.

⁵⁸ Ibid



Figure 5.2: Example of structural remains of the former Foundry uncovered within The Foundry footprint, archaeologically recorded during construction works. Source: Curio Projects, 2017.



Figure 5.3: Overview of remains of the foundry blower house showing the extent of the structure. Source: Curio Projects, 2018.

Locomotive Workshop Building

Curio Projects undertook archaeological monitoring and recording of sub-floor features uncovered during development and construction works within the Locomotive Workshop building, detailed in several reports as part of the Conditions of Consent under the SSDA for the works⁵⁹. During these archaeological works, archaeological monitoring, removal and recording of below ground impacts and features was undertaken.

The majority of impacts across the ground floor of the Locomotive Workshop building were for services, piles, elevator pits, a new floor level in Bay 1 South Annexe and the bulk excavation for the construction of a travelator in Bay 4 South to link the underground to the new basement car park below Building 2 (“The Foundry”). The groundworks undertaken within the Locomotive Workshop building for the 1990s internal fit-out and upgrade of the Locomotive Workshop were generally concentrated in Bays 3 to 15, which involved the installation of new services, electrical substations, service pits, and water mains that then extended out to and along Locomotive Street, and lastly general levelling and pouring of a concrete slab across Bays 3-15. Bays 1 and 2 of the Locomotive Workshop were left as the most intact within the Workshop building, as the works during the 1990s to the ground floor remained relatively minor⁶⁰.

In 2020, installation of new service trenches was undertaken across the Locomotive Workshop as part of Mirvac’s South Eveleigh redevelopment which encountered industrial works and archaeological relics from a range of stages of use at the Workshops, some dating back to the construction and establishment of the building in 1887. As part of the 2020 works, the subsurface features and works encountered and recorded include large pipes, machine footings, brick arch footings, machine pits, remnant railway tracks, flue systems, underground brick structures, wooden support beams supporting traversers, and the brick support arches of the main building façade⁶¹.

Besides the brick arch footings, the sub-surface historical features uncovered within the Locomotive Workshop during 2019 and 2020 as part of the construction works were not considered to be rare, as they were associated with the daily working activities performed on-site during its industrial period⁶². After appropriate recording and documentation of the sub-surface features had been completed by archaeologists, development works were able to carry on in each location. On-site, many elements are being reinterpreted, including some foundry mouldings and several of the pipes. Digital recordings from excavations, as well as archaeological recording work undertaken will be used to inform the final school education kits, cultural education tours and digital overlays for the South Eveleigh site.

⁵⁹ Curio Projects 2019b, *Potential Sub-Surface Features, Locomotive Workshops, Australian Technology Park*, Prepared for Mirvac; Curio Projects 2020a, *Archival Recording Report for the Condition of Consent C.21 of the redevelopment of the Locomotive Workshops Building (Bays 1-4A)*, prepared for Mirvac; Curio Projects 2020b, *Archival Recording Report for the Condition of Consent C.21 of the redevelopment of the Locomotive Workshops Building (Bays 5-15)*, Prepared for Mirvac.

⁶⁰ Ibid

⁶¹ Ibid

⁶² Ibid



Figure 5.4: Photogrammetry model of brick footings and arch of Locomotive Workshop uncovered and recorded during archaeological monitoring of construction works. Source: Curio Projects, 2019.

5.2.2. Carriageworks Test Excavation and Monitoring (Casey & Lowe and OCP 2005)

In 2004, archaeological testing within the Carriage Workshop building was undertaken under a Section 60 Excavation Permit to inform the design of what is now known as the Carriageworks Theatre and Performance space. Archaeological testing took place within the building in an area that overlapped with the former traverser pit in order to establish the location and nature of the traverser wall and extent of the footings to understand if the traverser would be impacted by the proposed excavation works to lay a new floor. Archaeological excavation exposed the sides and base of the traverser pit⁶³. Archaeological monitoring was undertaken in conjunction with the development works of the Carriageworks Theatre space and discovered additional features including machinery pits, rail alignments, as well as discarded equipment and rubbish dumped in brick pits. Integrity of the material encountered during development works was such that sub-surface wooden sleepers remained visible within the debris during excavation works.⁶⁴



Figure 5.5: Example of brick pits and machinery (a line or counter shaft) uncovered beneath the floor of the Carriage Workshop building during archaeological monitoring and investigation works in 2004. Source: AHMS 2008: 17.

⁶³ Casey & Lowe & Otto Cserhalmi + Partners 2005a, Carriageworks at Eveleigh excavations for construction: Excavation permit application.

⁶⁴ Ibid

5.2.3. Redfern North Eveleigh - Paint Shop Precinct Non-Aboriginal Heritage Study (Curio 2021)

In 2021, Curio undertook a Non-Aboriginal Heritage Study at the Redfern North Eveleigh Precinct for Transport for NSW. This study provided the current heritage context within the Paint Shop Sub-Precinct, with respect to its historical background, existing site conditions, current heritage legislative requirements/guidelines in NSW and overarching recommendations with respect to the management of the heritage values within the Paint Shop Sub-Precinct.

A historical archaeological assessment was included as part of this assessment and stated the key historical activities within the study area that would have influenced and/or impacted the survival of archaeological resources include the following⁶⁵:

- The cut down of the original landscape level to the railway level in 1882 for the construction of the main workshop buildings and Fan of Tracks. Generally, a four to five metres difference in level between Wilson Street (north) and the main rail line (south).
- Ongoing evolution and expansion of the Eveleigh Carriage Workshops throughout its lifespan, including continuous demolition and construction of built structures and features
- Mid to late twentieth-century use of the site including the construction of modern buildings and adaptive reuse of historically significant buildings on site (Carriageworks Theatre).

Potential archaeological deposits across the study area at the railway level stated to likely consist of artefacts, footings, and deposits associated with former sheds, brick piers, and former ground services. Sub-surface features related to rail and carriage workshop function would potentially include inspection pits, machine bases and remains of machines, plant, trails, traversers and turntables.

Underfloor historical archaeological deposits were noted to potentially be retained at the Wilson Street level of the study area associated with the CME Building and Scientific Services Building.

Overall assessing the Paint Shop Sub-Precinct study area in North Eveleigh to contain moderate to high potential for historical archaeological deposits to be present.

The study also assessed the potential historical archaeological resource of the Paint Shop Sub-Precinct to likely have research potential that would contribute to the understanding of the significance of the ERW site at a local level⁶⁶.

5.3. Historical Archaeological Potential — LES

The main historical activities within the study area that would have influenced and/or potentially impacted the survival of archaeological resources within the LES study area are summarised as follows:

- Land clearing and farming.
- Construction of the LES building in the 1890s, including installation of services, building foundations etc.
- Any upgraded services throughout the 20th century (especially sewer services along the northern and southern façade of the LES).

⁶⁵ Curio 2021, *Redfern North Eveleigh Precinct Non-Aboriginal Heritage Study*, prepared for TfNSW.

⁶⁶ Ibid

- Installation of a traverser along the eastern boundary of the study area between the Locomotive Workshop and the LES building.
- Construction of Ablution buildings (one located abutting the northern façade and the other located along the southern façade of the LES).
- Installation of railway track along the southern boundary.

Although the topography of the landscape is relatively flat, the LES study area likely required minor land grading and levelling in preparation for the construction of the LES.

Former railway related structures (now demolished) were previously located within and/or along the boundary of the study area. These included:

- **Laundry** – pre-1899 to c1919 (previously located along the southern boundary of the study area)
- **Driver's Barracks** – pre-1899 to 1906 (previously located within the western half of the LES before the 1906 extension of the LES building).
- **Traverser** – pre-1899 to c1986 (located between the eastern façade of the LES building and the Locomotive Workshop)
- **Southern Ablution building** – 1931 to c2002
- **Urinals and Water closets** – 1906 to 1965 (located along the northern and southern façade of the LES).

Potential archaeological deposits located across the study area may consist of artefacts, footings, and deposits associated with former structures such as post holes, brick piers, services, and former ground services. Sub-surface features associated with rail and LES functions and uses may include features such as inspection pits, machine bases and remains of machines and traversers. The LES building has a high potential for sub-surface historical 'works' including sub-floor inspection pits, machinery, and rail tracks.

Overall, it is likely that the LES study area would have potential for archaeological deposits to be present in a sub-surface capacity, comparable to the features encountered within previous archaeological monitoring and excavation works at North and South Eveleigh. Examples of deposits likely to be present beneath the floor level of the LES are likely to include machine pits, foundations, rail lines etc. Archaeological deposits and resources of former buildings and associated features and deposits have potential to be present within other areas of the precinct.

Based on the results from previous historical archaeological excavations around the wider Eveleigh Precinct, the LES study area is assessed as having a **moderate to high potential for historical archaeological deposits** to be present, as illustrated in Figure 5.6 below.

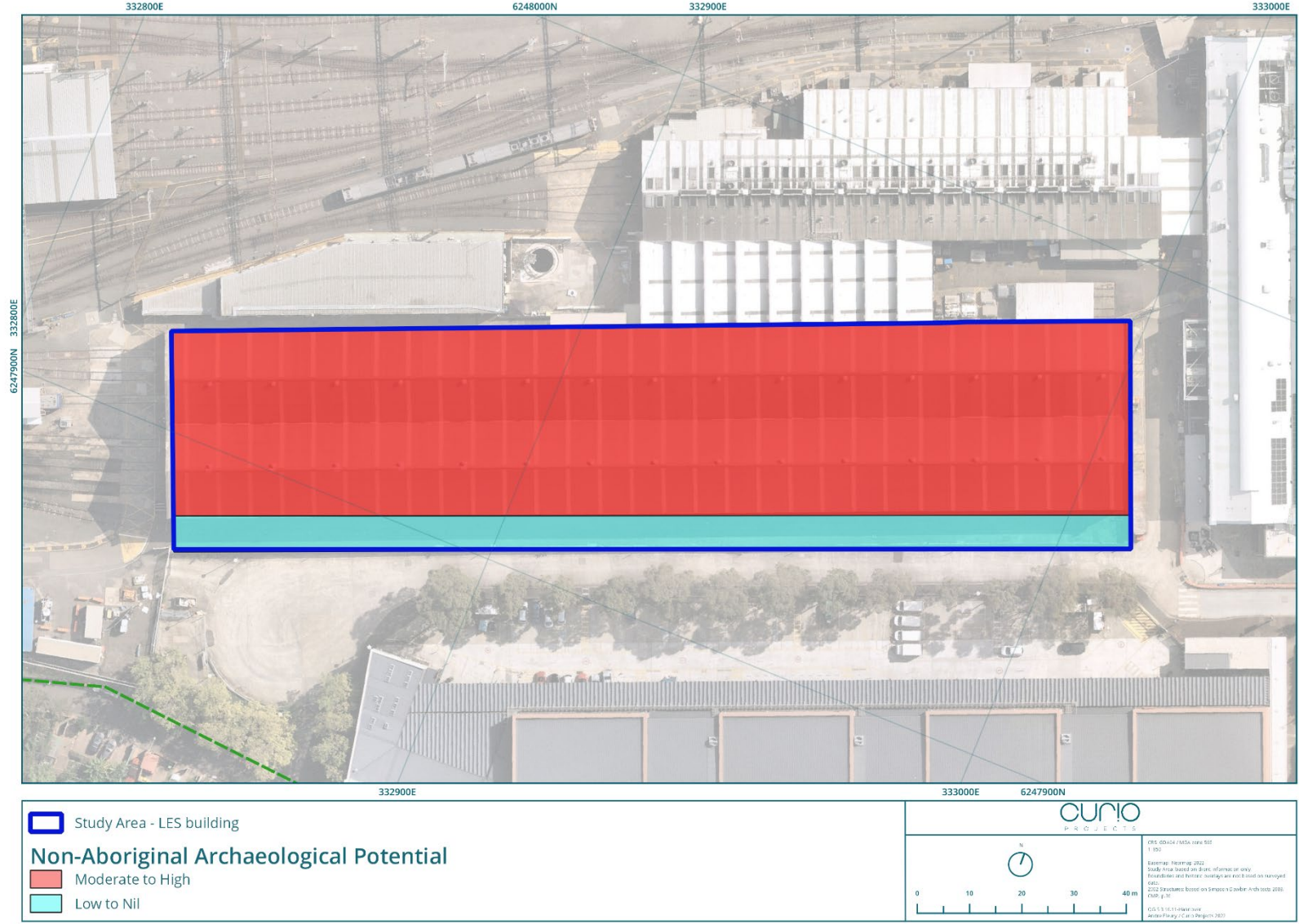


Figure 5.6:: Historical Archaeological Potential across the LES. Source: Curio Projects, 2022.

5.4. Archaeological Management Plan and Framework Strategy

The following historical archaeological management framework has been developed in light of the assessment of historical archaeological potential and significance for the LES study area (as presented in the sub-sections above). This will include overarching recommendations, potential mitigation measures, and process recommendations developed for the study area.

As noted in Section 5.1.1 above, in situ industrial archaeological resources (such as roadways, railway tracks, tram tracks, kerbstones, culverts, milestones, remnant flues, and other related below-ground infrastructure) were managed and defined for the South Eveleigh development in consultation between Curio Projects and Heritage NSW archaeologists as ‘works’, as opposed to as archaeological ‘relics’. It is considered appropriate that a comparable management approach be applied to the LES study area as a component of the same wider ERW complex.

Therefore, the archaeological management of the LES study area proposes to maintain this differentiation between ‘relics’ and ‘works’ where relevant, as while both relics and works may have the potential to be present, the way in which they are assessed and managed may differ.

In situ features associated with the function of the site, such as railway tracks, turntables, kerbstones, and other related rail infrastructure that exists below-ground in association with the former railway uses of the site have the potential to be present within the LES study area and would be considered historical ‘works’.

Standing footings and walls structures etc., that are associated with remnant standing structures and buildings, are not technically considered archaeological ‘relics’, as they are remnant, standing footings and infrastructure associated with the existing heritage buildings.

Features that have potential to be located within the LES study area, such as former footings and deposits associated with former railyard structures (e.g., post holes, brick piers, services, former ground services, and artefact deposits) would be considered archaeological relics.

Methodologies for management and recording of non-Aboriginal historical objects (‘works’) and/or archaeological deposits (‘relics’) should be relatively consistent with those applied to management of similar deposits at South Eveleigh.

5.5. Aboriginal Archaeology & Cultural Heritage

An Aboriginal Cultural Heritage Study and Statement of Impact has been undertaken by Curio Projects in 2022. The report provides background research and assessment of evidence and information about material traces of Aboriginal land use in the study area and surrounds, significance assessment of potential Aboriginal sites, places, landscapes, and/or other values, as well as an impact assessment and management recommendations to assist TAHE with their future responsibilities for Aboriginal cultural heritage within the study area.

The report states that the study area has low to moderate potential for intact Aboriginal archaeological deposits to be present, most likely in the form of low density or isolated stone artefact sites representative of general Aboriginal movement across and use of the Redfern landscape. The report also notes that the Rezoning Proposal and future development at the LES site represent a significant opportunity to have an overall positive impact to Aboriginal cultural heritage values through the proposed introduction of meaningful heritage interpretation initiatives, as well as the integration of Aboriginal cultural heritage values and outcomes into the project design.

6. Heritage Significance

6. Heritage Significance

6.1. Assessing Significance Methodology and Criteria

The Burra Charter Australia (Australia ICOMOS 2013) defines cultural significance as:

...aesthetic, historic, scientific, social, or spiritual value for past, present or future generations. Cultural significance is embodied in the place itself, its fabric, setting, use, associations, meanings, records, related places, and related objects. Places may have a range of values for different individuals or groups. (Australia ICOMOS 2013: 2).

The assessment of significance is based on the principles of the Burra Charter (Australia ICOMOS, 2013); the Interpretation and Intangible Cultural Heritage and Place Practice Notes (Australia ICOMOS, 2017); guidelines contained within the NSW Heritage Manual (Heritage Office and DUAP 1996); and the 2001 Assessing Heritage Significance: A NSW Heritage Manual Update – prepared by the former Heritage Branch, NSW Office of Environment and Heritage (government department now known as Heritage NSW).

The Assessing Heritage Significance: a NSW Heritage Manual Update is used to compare the significance of a place with the NSW Heritage assessment criteria, in which a place can meet more than one criterion. Such processes determine the level of significance of a place – either for the local government area, for the State of NSW or the broader Australian community. It results in a succinct statement of heritage significance.

For the assessment of local or State significance of an item, the NSW Heritage Guidelines indicate that the item must meet one or more of the following criteria:

Criterion A—Historical Evaluation

An item is important in the course or pattern of NSW's cultural or natural history (or the cultural or natural history of the local area)

Criterion B— Historical Association

An item has strong or special associations with the life or works of a person, or group of persons, of importance in NSW's cultural or natural history (or the cultural or natural history of the local area)

Criterion C — Aesthetic Value

An item is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in NSW (or the local area)

Criterion D— Social/ Cultural Value

An item has strong or special associations with a particular community or cultural group in NSW (or the local area) for social, cultural, or spiritual reason

Criterion E— Technical/Research Potential

An item has potential to yield information that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area)

Criterion F—Rarity

An item possesses uncommon, rare or endangered aspects of NSW's cultural or natural history (or the cultural or natural history of the local area)

Criterion G—Representativeness

An item is important in demonstrating the principal characteristics of a class of NSW's cultural or natural places; or cultural and natural environments

6.1.1. LES – Assessment of Significance

The 2003 CMP provides an assessment of significance for the LES building against the NSW heritage criteria. Since the CMP was prepared in 2003, the precinct (formerly identified as ATP) has been redeveloped (2015-present) and is now known as the South Eveleigh Precinct. CMPs are generally required to be updated every 5-10 years, yet the 2003 CMP has not been revisited in almost 20 years. Therefore, in order to comply with current Heritage NSW recommendations and the requirements of the SEARs, a new draft CMP that provides guidance on the management of the LES building within the overall management of the redeveloped South Eveleigh Precinct is being currently developed by Curio.

Therefore, Table 6.1 presents the 2003 CMP assessment of significance against the updated assessment extracted from the draft South Eveleigh Precinct CMP in accordance with the NSW heritage criteria.

Table 6.1: 2003 CMP Assessment of Significance & Updated Curio Assessment against the NSW Heritage Criteria

Criterion	2003 CMP Assessment	Updated Curio Assessment
Criterion (a) Historical Evaluation	<p><i>The Large Erecting Shop has the distinction as the only remaining facility of the original Eveleigh complex performing much the same functions for which it was constructed, and in continuous use for a similar function since original construction.</i></p> <p><i>The building remains an intact example of the longitudinal pit erecting shop, the layout and structure of the building fundamentally unchanged since original construction. It is physical evidence of a past era of steam traction and the important process of locomotive construction and overhaul is embodied in the fabric and design of the building.</i></p> <p><i>The Department of Railways in parallel with private contractors and overseas suppliers undertook the construction of locomotives of many classes which became the mainstay of motive power on the railways until the end of the steam era.</i></p>	<p>The South Sydney area is culturally significant to First Nations Peoples and is home to the Gadigal people of the Eora nation, who have maintained their connection to Country despite the major impacts brought by colonisation.</p> <p>The Large Erecting Shop (LES) within the wider context of the former Eveleigh Railway Workshops (ERW) reflects a key, expansion of the ERW for locomotive construction, technology, overhaul, and maintenance in NSW during the 19th and 20th centuries and in the instance of the LES, continuing maintenance into the 21st century.</p> <p>The growing operation of the ERW site directly contributed to the workforce of NSW planning, design and expansion of the surrounding suburbs (e.g., Alexandria, Eveleigh, Redfern, etc), which were developed in response to precinct activities and workers' demands.</p> <p>Further, the ERW site, the LES inclusive, was a key location in the development of the union movement in NSW which, through many strikes and protests, including the Great Strike of 1917, resulted in the improvement of working conditions during the early twentieth century⁶⁷.</p>
Criterion (b) Historical Association	No assessment under the 2003 CMP.	<p>The LES and the wider ERW are associated with the early careers of prominent Australian and NSW historical figures, including the former Prime Minister Ben Chifley, former NSW Premiers James McGowen, JJ Cahill, and the former Federal Member for Sydney, Eddie Ward.</p> <p>The site and the wider precinct are also associated with the establishment and development of the trade unions in NSW and the broader union movement Australia-wide.</p>

⁶⁷ GML 2013 p.89

Criterion	2003 CMP Assessment	Updated Curio Assessment
Criterion (c) Aesthetic	<p><i>The Large Erecting Shop is a prominent visual element on the Eveleigh Workshops site. The full impact of the massive 180 metre length of the building can be appreciated from the southern area of the site, the ATP carpark and Henderson Road.</i></p> <p><i>Architecturally the building represents the highest standards of building design and detailing typical of the most important industrial facilities of the late Victorian period. Elements of the facade treatment repeat the design of the workshops complex constructed 13 years earlier, contributing a cohesion and integrity to the overall site, a quality not sustained with later development on the site.</i></p> <p><i>The exterior featured gabled and parapeted end elevations with stone copings, load bearing brick facades with recessed window bays, cast iron arched windows, and sandstone sills. It was one of the last of the buildings in the Eveleigh complex of masonry construction aspiring to quality architectural design and finish, reflecting a change to a more utilitarian and ad hoc provision of railway facilities characteristic of later development on the site.</i></p> <p><i>The building also represents an accomplished Australian interpretation of the design and layout of several of the great locomotive workshops in Britain at the time, including Crewe, Derby and Bow.</i></p>	<p>The LES forms part of an area that is culturally significant to the Aboriginal community and any Aboriginal archaeological deposits discovered within the LES site would have the potential to hold a strong association with their community.</p> <p>The LES is a large-scale, prominent building within the former EWR site. The building is an important element within significant view lines from Locomotive Street and the overall South Eveleigh precinct and remains prominent in the landscape in views from North Eveleigh and the railway line that separates North and South Eveleigh.</p> <p><i>Architecturally, the building represents the highest standards of building design and detailing typical of the most important industrial facilities of the late Victorian period. Elements of the facade treatment repeat the design of the [adjacent Locomotive] Workshops complex constructed 13 years earlier, contributing to the cohesion and integrity of the overall site.</i></p> <p><i>The exterior features gabled and parapeted end elevations with stone copings, load-bearing brick facades with recessed window bays, cast iron arched windows, and sandstone sills. It was one of the last of the buildings in the Eveleigh complex of masonry construction aspiring to quality architectural design and finish, reflecting a change to a more utilitarian and ad hoc provision of railway facilities characteristic of later development on the site.</i></p> <p><i>Internally, the building remains an intact example of the longitudinal pit erecting shop, the layout and structure of the building fundamentally unchanged since original construction. Remnant internal fabric, including the gantry cranes, cast iron columns, and the patina of age which is represented in the fabric, adds to the authenticity and aesthetics of the space.</i></p> <p><i>Further, the building also represents an accomplished Australian interpretation of the design and layout of several of the great locomotive workshops in Britain at the time, including Crewe, Derby and Bow.</i></p>

Criterion	2003 CMP Assessment	Updated Curio Assessment
Criterion (d) Social	<p><i>At its peak of operation in 1917, Eveleigh workshops employed a total of over 3,300 staff. The social and industrial significance was immense as one of the largest single work places in Australia. Up to 1,100 employees worked in the Erecting Shop alone, and as a single workspace, this building highlighted the work practices of railway workshops and gradual improvement in conditions. Lighting within the work areas depended on natural light from skylights and windows, ventilation with minimal task lighting or effective artificial lighting. Ventilation depended on the ridge monitors, totally inadequate in this smoky and dusty environment. Three generations of improving sanitary facilities are reflected in the early slate urinals fixed around the external walls supplemented by earth closets in outbuildings; the 1930 elevated WCs with occupants monitored and supervised by trusted attendants; the 1950 elevated WCs and showers.</i></p> <p><i>Until the 1970s most employees used buckets for washing, and hot and cold running water in amenities was not widely available on the site until the mid-1970s.</i></p>	<p>Due to the magnitude of the precinct, which includes the LES, the ERW were one of the largest employers in NSW during the late nineteenth and twentieth centuries.</p> <p>The LES and the wider precinct are greatly significant to the former and current railway workers and the local community, in particular to the groups involved in the NSW railways, as the site is home to a multitude of important and meaningful stories associated with the industrial working environment, a multicultural workplace and social clubs and activities. Although no longer in operation, the precinct is also a source of pride to these groups as it represented the capacity of Australian industry at the time and is demonstrative of the high level of trade and technical skills of the workers.</p> <p>Further, the organisation and development of the union movements within the precinct managed to improve work conditions through multiple strikes and protests and was highly contributory to shaping the future of Australia's working rights.</p>
Criterion (e) Technical/ Research Potential	<p><i>The building contains a number of intact features of technological significance which enable clear interpretation of industrial processes which took place during the working life of the building.</i></p> <p><i>The overhead cranes are highly significant. All 7 cranes represent the technology of travelling cranes dating from various period of installation and adaptation. The three original Craven cranes of 1899 are relics of the rope driven system initially installed in the building. Although now heavily modified, it is significant that two of these cranes remain operational 103 years after installation. Several non-operational cranes dating from 1885 remain within the adjacent building, the former Locomotive Workshops complex preserved as static relics as part of the ATP adaptive reuse. The manufacture of these relics predates construction of the building in which they are housed.</i></p> <p><i>Remnants of the 2'-gauge trolley tracks are relics of the extensive transportation system for heavy components and tools within the building</i></p>	<p><i>The building contains a number of intact features of technological significance which enable clear interpretation of industrial processes which took place during the working life of the building.</i></p> <p><i>The overhead cranes are highly significant. All 7 cranes represent the technology of travelling cranes dating from various periods of installation and adaptation. The three original Craven cranes of 1899 are relics of the rope-driven system initially installed in the building. Although now heavily modified, it is significant that two of these cranes remained operational 103 years after installation.</i></p> <p><i>Remnants of the 2'-gauge trolley tracks are relics of the extensive transportation system for heavy components and tools within the building providing the essential link between the [LES, the former] foundry and workshops.</i></p> <p><i>Evidence remains in the structure of the central transmission drive shaft, supplying power to all equipment within the work areas. Traces of switch boxes, compressed</i></p>

Criterion	2003 CMP Assessment	Updated Curio Assessment
	<p>providing the essential link between the Erecting Shop and the foundry and workshops.</p> <p>Evidence remains in the structure of the central transmission drive shaft, supplying power to all equipment within the work areas. Traces of switch boxes, compressed air outlets, brackets for steam pipes and electrical insulators all express the layering of technological change throughout the life of the building.</p>	<p>air outlets, brackets for steam pipes and electrical insulators all express the layering of technological change throughout the life of the building.</p>
Criterion (f) Rarity	<p>The Large Erecting Shop is unique in Australia, not only as a complete example of this type of facility, but also continuing to perform the function of its original design.</p> <p>It is the only element of the original Eveleigh Workshops remaining within its original railway context and continuing connection to the rail network.</p> <p>It is a rare example in the world of an erecting shop of this scale to remain in public ownership continuing to fulfil a railway function.</p>	<p>The LES no longer meets the Rarity Criterion in terms of continuity of use as the building no longer fulfils its original railway functionality.</p> <p>The ERW as a whole provides a rare composition of large-scale railway workshop buildings remaining in NSW. The former precinct still retains several intact buildings, including the LES, the Locomotive Workshop, the former Works Managers' Office and the New Locomotive Workshop in its southern portion, and the Paint Shop, Carriage Workshops, the Chief Mechanical Engineer's building, the Clothing Building, the Scientific Services Building, and the Telecommunications Building in its northern portion.</p>
Criterion (g) Representativeness	<p>No assessment under the 2003 CMP.</p>	<p>The wider ERW, including the LES, is representative of the large-scale industrial and technical working environment and railway workshop operations in the nineteenth and twentieth centuries.</p>

6.2. Statement of Significance

6.2.1. ERW

The following Statement of Significance for the overall ERW SHR site has been extracted from the OCP 2022 Overarching CMP updated by Curio:

The ERW complex is of exceptional heritage significance to the state of NSW for its major contribution to the establishment, operation and growth of the NSW railways, which was essential to the growth and development of NSW from the late nineteenth century onwards. The operation of the railway workshops and stores at Eveleigh is associated with the phenomenon of railway networks that allowed the unprecedented development of Sydney suburbs and rural NSW at the end of the nineteenth century and the early twentieth century. The Workshops complex is significant as a rare remaining example of a relatively intact, large-scale nineteenth century railway workshops that retains unity of character as well as continued links to railway operations for over one hundred years to this day.

The complex is significant as one of only a limited number of such facilities within the country, with individual states within Australia generally having established a single major railway workshop facility for maintenance and also manufacture of rolling stock and engines, supplemented by smaller workshops. As such, the ERW represent one of the largest industrial enterprises in Australia and the large-scale infrastructure demonstrates Government confidence in establishing and expanding rail networks in the late nineteenth century. The site retains the ability to demonstrate a range of characteristics that are typical of major railway construction and maintenance workshops in Australia, including the range of building types providing similar functions, aesthetic qualities and comparable history in terms of growth and expansion, involvement in wartime production, subsequent decline and adaptive reuse. There are opportunities to undertake further detailed research to identify potential national values, particularly in the context of the integrity of former railway workshop sites in Australia. Similarly, there are opportunities for further investigation of international railway workshop facilities to clarify the potential significance of the ERW in an international context.

Historically the site is important for its links to an early phase of railway development in NSW, with onsite evidence remaining intact from as early as 1887. Though many structures and items have been removed, the remaining site evidence reads as a living interpretation of the technological, administrative, social and cultural developments in over 100 years of railway operations in NSW, including the major transition from steam to diesel and electric powered train operation. The layout of the extant site elements is also indicative of the functional and administrative arrangements during the period of the site's operation.

The ERW site is associated with the life and work of the early railway engineers John Whitton and George Cowdery, individuals whose life and work made significant contributions to the establishment, development and operation of railways in Australia and NSW. Whitton, Chief Engineer of the NSW Railways between 1856 and 1899, was responsible for the major restructuring of the rail system which resulted in the resumption of land at Eveleigh and the relocation of the old Redfern Workshops (Sydney's first railway yards) to Eveleigh. George Cowdery, Engineer for Existing Lines, executed the detailed design at Eveleigh.

The ERW have considerable aesthetic significance as an industrial landscape formed by the complex of functional buildings and associated infrastructure. Aesthetic and technical significance is demonstrated in the high quality design and construction of the original buildings, which are substantially intact and display finely detailed polychrome brickwork and well-articulated facades that embody the pride of the late Victorian era. The simple, strong functional forms of the workshop buildings have landmark quality, not only as important townscape elements in the Redfern/Eveleigh area, but as part of the visual train journey of thousands of passing commuters. The combination of the southern locomotive sheds at the Australian Technology Park and the former Carriage and Wagon Workshops provide a distinctive landmark in the Sydney landscape and define views to and from the site.

The Workshops are of social value to generations of railway employees past and present as a workplace producing high quality craftsmanship utilising state-of-the-art technology. The place served as a training ground for thousands of apprentices, tradesmen and engineers and was one of the biggest employers in New South Wales. Workers of the ERW centred their social activities on the workplace and social events were organised by and for workers both on the site and beyond. The site also has social value as a heritage icon for current local communities, which is reinforced by ongoing community interest in the place.

The remaining tangible evidence and intangible site values reflect the technological, social and cultural development of the NSW railways, as well as broader important historical events. The ERW has a strong historical association with union activities and is credited as being pivotal in the Australian Labour Movement, with the formation of the Amalgamated Railway and Tramway Service Association (ARTSA) in 1886. Eveleigh was seminal in many major industrial strikes, the ramifications of which were felt throughout the nation. In addition, several significant figures in the labour movement worked at Eveleigh, including James McGowan, the first Labour Premier of NSW.

The Workshops represent significant research potential for their ability to inform through remaining physical, documentary and oral evidence the functions and operations of a large-scale nineteenth and twentieth century railway workshops. The site also retains an exceptional and rare collection of historically and technically significant heavy machinery, although many items have been removed in the process of modern site development⁶⁸.

6.2.2. LES Building

The Statement of Significance prepared by Simpson Dawbin Architects for the 2003 CMP has been revised and updated by Curio in order to reflect the changes to the building's use and any impacts this has had on the statement of significance. The following statement has been extracted from the draft South Eveleigh Precinct CMP currently being prepared by Curio:

The natural landscape and pre-colonial environment of the Eveleigh site; the sandy and rich Blackwattle Swamp and O'Shea's Creek were significant resources for the Gadigal people of the Eora Nation.

The Large Erecting Shop was one of the largest and most imposing single structures on the Eveleigh site, and its role within the workshops complex was pivotal in the

⁶⁸ OCP 2022 (Curio update) CMP: 95-96

*process of locomotive overhaul and the construction of new locomotives. It was of equal importance as any other facility within the Eveleigh Workshops complex but is unique in retaining its railway context*⁶⁹. The building contributed to the expansion of the railway system in NSW until the end of the steam era and the conversion to diesel maintenance until it ceased operation in 1985.

The site forms part of the former Eveleigh Railway Workshops precinct, which directly contributed to the expansion of the surrounding suburbs. The precinct is strongly associated with the development of union movements in NSW and subsequent strikes and protests that allowed the workers to improve working conditions during the early twentieth century. The precinct is associated with the early careers of prominent Australian and NSW historical figures including a former Prime Minister and State Premiers.

The area is culturally significant to the Aboriginal community and further archaeological investigations would potentially reveal additional associations with the community as well as the existing social, historical, and scientific values.

The Large Erecting Shop represents the ambitious and important industrial facilities of the late Victorian period and remains a prominent building within the former EWR precinct and a highly significant element within site view lines and the industrial landscape of the precinct. The exterior of the building retains most of its original elements, including the gabled and parapeted end elevations with stone copings, load bearing brick facades with recessed window bays, cast iron arched windows, and sandstone sills. Internally, the building remains an intact example of the longitudinal pit erecting shop. The layout and structure of the building remains fundamentally unchanged since its construction. Remnant internal fabric, including the gantry cranes, cast iron columns, and the patina of age which is represented in the fabric, adds to the authenticity and aesthetics of the space.

*The changing technology and work practices associated with the functions of the facility remain visible in the fabric and are an intact resource for interpretation of the process of a locomotive erection and overhaul from 1899 to the present day. It is the only example in Australia of the longitudinal pit erecting shop design, closely modelled on contemporary British practice as demonstrated at Crewe and Derby Railway Workshops*⁷⁰.

6.3. Grading of Significance

The grading of significance provides a further context of the heritage significance of each element of the site and provides guidance for appropriate heritage management and retention/tolerance for change.

The draft South Eveleigh Precinct CMP, currently being prepared by Curio, provides the following updated grading of significance of the key elements of the subject site, which are consistent with the gradings of significance in the GML 2013 *Conservation Management Plan for Australian Technology Park*. An additional criterion, *Neutral Significance*, which is defined as 'Modern elements that are required as part of the ongoing use of the site that neither add nor detract from the overall significance of the site has been included as part of the grading of significance.

⁶⁹ SDA 2003 CMP p.57

⁷⁰ Ibid

EXCEPTIONAL SIGNIFICANCE

Rare or outstanding element directly contributing to an item's local and State significance.

HIGH SIGNIFICANCE

High degree of original fabric. Demonstrates a key element of the item's significance.

Alterations do not detract from significance.

MODERATE SIGNIFICANCE

Altered or modified elements. Elements with little heritage value, but which contribute to the overall significance of the item.

LITTLE SIGNIFICANCE

Alterations detract from significance. Difficult to interpret.

NEUTRAL SIGNIFICANCE

Modern elements that are required as part of the ongoing use of the site that neither add nor detract from the overall significance of the site.

INTRUSIVE

Damaging to the item's heritage significance.

Table 6.2: Grading of Significant Elements

Element Description	Grading of Significance
East Elevation	
Traverser pit	EXCEPTIONAL
Shipping containers in yard	INTRUSIVE
Locomotive doors, Roads 1-5	HIGH
Doors, Road 6	EXCEPTIONAL
Round Windows	EXCEPTIONAL
South Elevation	
Road 7, siding	EXCEPTIONAL
Chainwire enclosure	INTRUSIVE
Stone corbel brackets, east end	EXCEPTIONAL
Paint remnants to walls to demolished washrooms	NEUTRAL
Bracket enamel light fittings	EXCEPTIONAL
Elevated amenities block and stair	INTRUSIVE
RSJ brackets, to the west of amenities	HIGH
PVC downpipes	INTRUSIVE
Double entry doors, foot of stairs	EXCEPTIONAL
Remnant wall urinal	MODERATE
Windows above urinal	EXCEPTIONAL
West Elevation	
Elephant House	LITTLE
Six sidings	EXCEPTIONAL

Element Description	Grading of Significance
Locomotive doors, roads 1-6	HIGH
Surface mounted conduits	INTRUSIVE
Rail bollards	MODERATE
Steel ladder	HIGH
Modem bracket lamps	NEUTRAL
Fire hose reel cabinets	INTRUSIVE
Bank of electrical switches, centre pier	INTRUSIVE
Crib Room, (First aid room)	MODERATE
North Elevation	
Engraved stone sill and datum in brickwork	EXCEPTIONAL
Tank platform	EXCEPTIONAL
Toilet block	INTRUSIVE
Elevated amenities block	INTRUSIVE
Attached stores building	INTRUSIVE
Lean-to awning, east end	INTRUSIVE
Painted wall section	INTRUSIVE
Interior (<i>commencing at the northwest corner, road no 1</i>)	
Cabin, safe working	EXCEPTIONAL
Weighbridge	EXCEPTIONAL
Double doors	EXCEPTIONAL
Trolley bridges to pits	EXCEPTIONAL
Bricked up double doors	NEUTRAL
Switchboard	INTRUSIVE
Remnant trolley tracks	HIGH
Inspection pit	HIGH
Short Inspection pit	HIGH
Assembly point	HIGH
3801 office cabin	MODERATE
Office, store	MODERATE
Office	MODERATE
Office, Machine shop	MODERATE
Powerhouse Museum, office	NEUTRAL
Road 1 to 3	EXCEPTIONAL
Road 4, track bed and pit	MODERATE
Roads 5 & 6	EXCEPTIONAL
Transmission shaft bearings	EXCEPTIONAL
Machinery support beam	HIGH

Element Description	Grading of Significance
Overhead Cranes	
L20	MODERATE
L23	EXCEPTIONAL
L25	EXCEPTIONAL
L27	EXCEPTIONAL
L28	EXCEPTIONAL
L829	HIGH
L830	HIGH
Structure	
Cast iron centre columns	EXCEPTIONAL
Roof trusses	EXCEPTIONAL
Rail section purlins	EXCEPTIONAL
Curved gutter	EXCEPTIONAL
Ridge and monitor	LITTLE
Ventilation cowls	NEUTRAL
Roof sheeting	NEUTRAL
Clear roof lights	NEUTRAL
Services	
Suspended light fittings	MODERATE
Selected switch boxes	EXCEPTIONAL
Compressed air outlets	EXCEPTIONAL
Switchboards	INTRUSIVE
Power outlets	INTRUSIVE
Surface mounted conduits	INTRUSIVE
Bracket lights on cranes	MODERATE

7. Rezoning Proposal

7. Rezoning Proposal

The LES is a large industrial building with a footprint of approximately 6,000sqm located at the northwest of the South Eveleigh Precinct. The LES ceased formal operation in 1985 and has been largely unoccupied since approximately 2017.

The NSW Government is committed to working with the local community to develop the biggest innovation district of its kind in Australia, being Tech Central. The South Eveleigh Precinct is a key neighbourhood within Tech Central, delivering workplaces and collaboration spaces that support the vision for a new tech and innovation ecosystem. The inclusion of the LES within the broader South Eveleigh Precinct has the potential to support further innovation, collaboration and jobs for the future.

Transport for NSW (TfNSW) is therefore seeking to adaptively use the LES for a mix of uses, including commercial office and retail premises. Such land uses are currently not permitted within the planning controls that apply to the LES building, which still reflects its former infrastructure function. As such, a State-led rezoning application is being proposed to the Department of Planning and Environment (DPE) to amend the planning controls that currently apply to the site under State Environmental Planning Policy (Precincts – Eastern Harbour City) 2021 (Precincts SEPP).

The proposed new planning controls are intended to facilitate the following:


- Alterations within the existing LES to convert the ground level into commercial office and retail premises,
- Creation of two new 'internal' storeys within the existing LES building envelope for use as commercial office premises,
- External upgrade and conservation work to the existing LES building to ensure it is fit for purpose and environmentally sustainable,
- Heritage interpretation and conservation work generally throughout the LES site,
- Services augmentation, and
- Publicly accessible space upgrades.

While external works are required, the proposal does not seek to significantly alter the existing building footprint of the LES. Further, it is proposed that the general form of the existing building and key architectural features of the existing building are retained in any future reuse of the building for commercial purposes, noting that the LES is part of the Eveleigh Railway Workshops complex included in the State Heritage Register.

Since 2015, Mirvac has successfully developed the South Eveleigh Precinct. With the completion of the Locomotive Workshop project, which also involved the adaptive use of industrial buildings listed on the State Heritage Register, it is now considered a logical time to adapt and integrate the LES into the broader technology precinct. TAHE is therefore preparing documentation to support the State-led rezoning application.

Figure 7.1 to Figure 7.19 present the proposed works related to the Rezoning Proposal for the LES.

LARGE ERECTING SHED (LES) SOUTH EVELEIGH REZONING PLAN

 PART OF LOT 5 DP 1175706 TO BE REZONED FROM SPECIAL PURPOSE ZONE - INFRASTRUCTURE TO BUSINESS ZONE - BUSINESS PARK

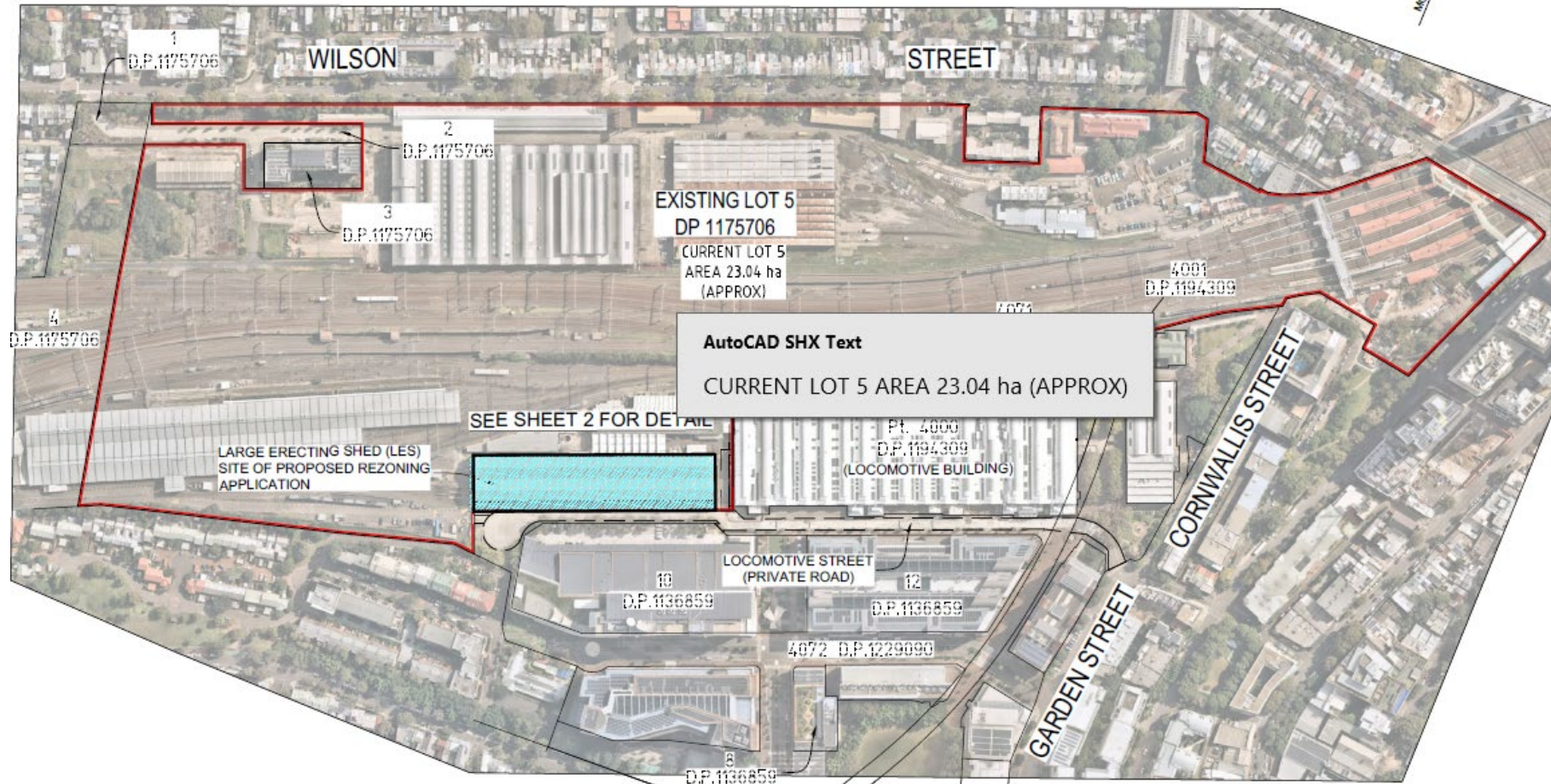


Figure 7.1: Proposed Rezoning of part of Lot 5 in DP 1175706 containing the LES. Source: Tasy Moraitis, Reference 196-21G I02, 2022.

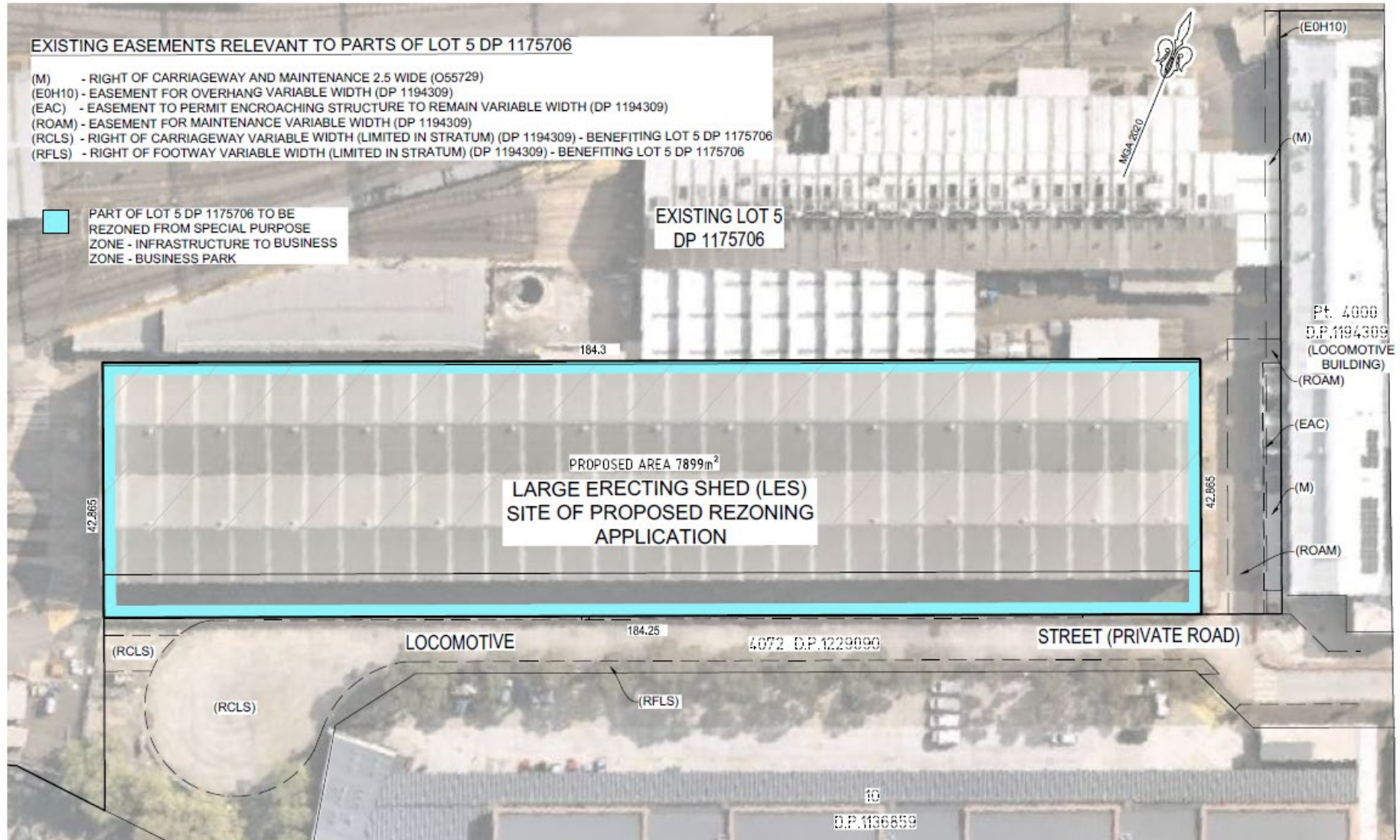


Figure 7.2: Detail of the Proposed Rezoning of part of Lot 5 in DP 1175706 containing the LES. Source: Tasy Moraitis, Reference 196-21G I02, 2022.

LARGE ERECTING SHED (LES) PLAN OF PROPOSED SUBDIVISION

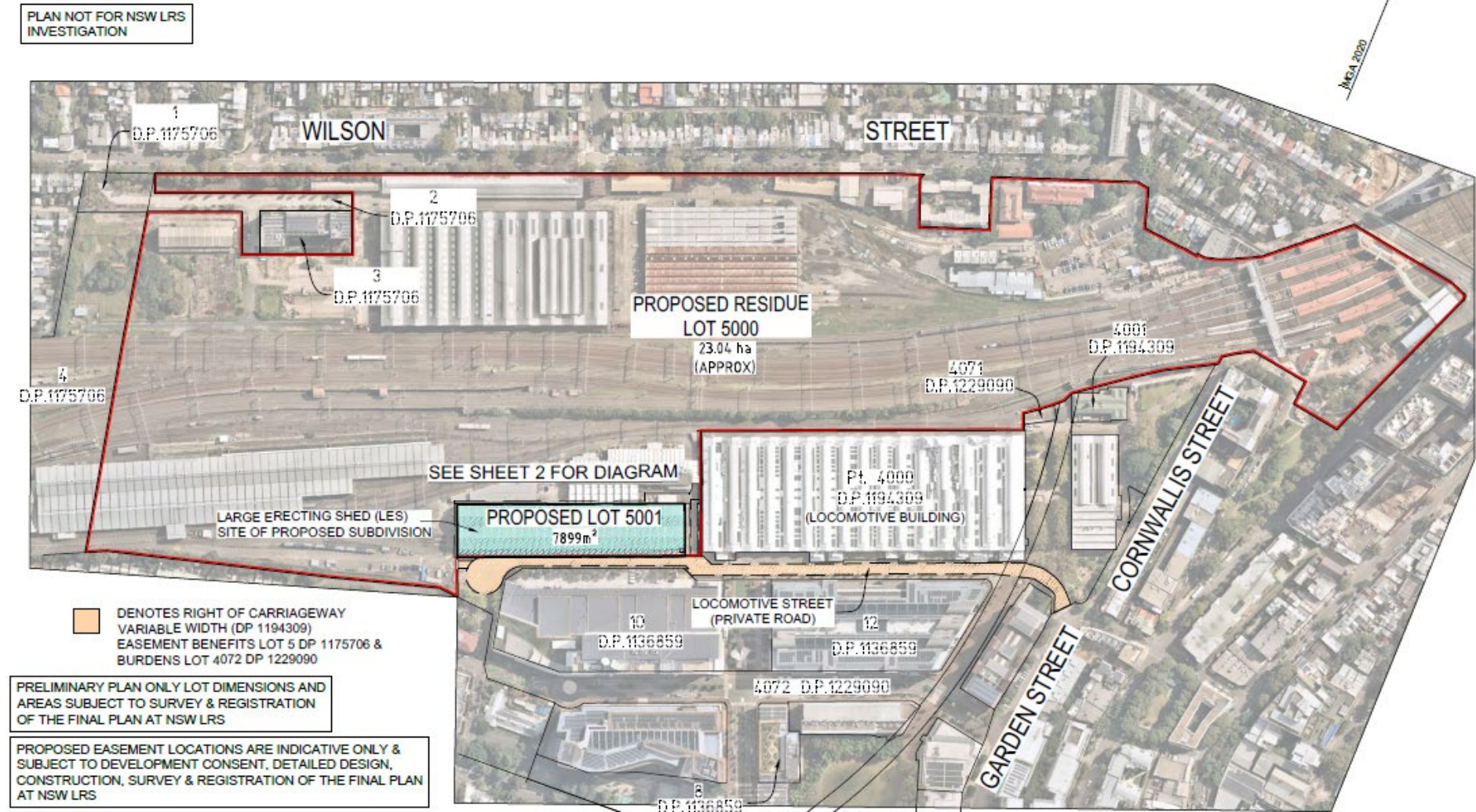


Figure 7.3: Proposed subdivision of part of Lot 5 in DP 1175706 containing the LES. Source: Tasy Moraitis, Reference 196-21G S01, 2022.

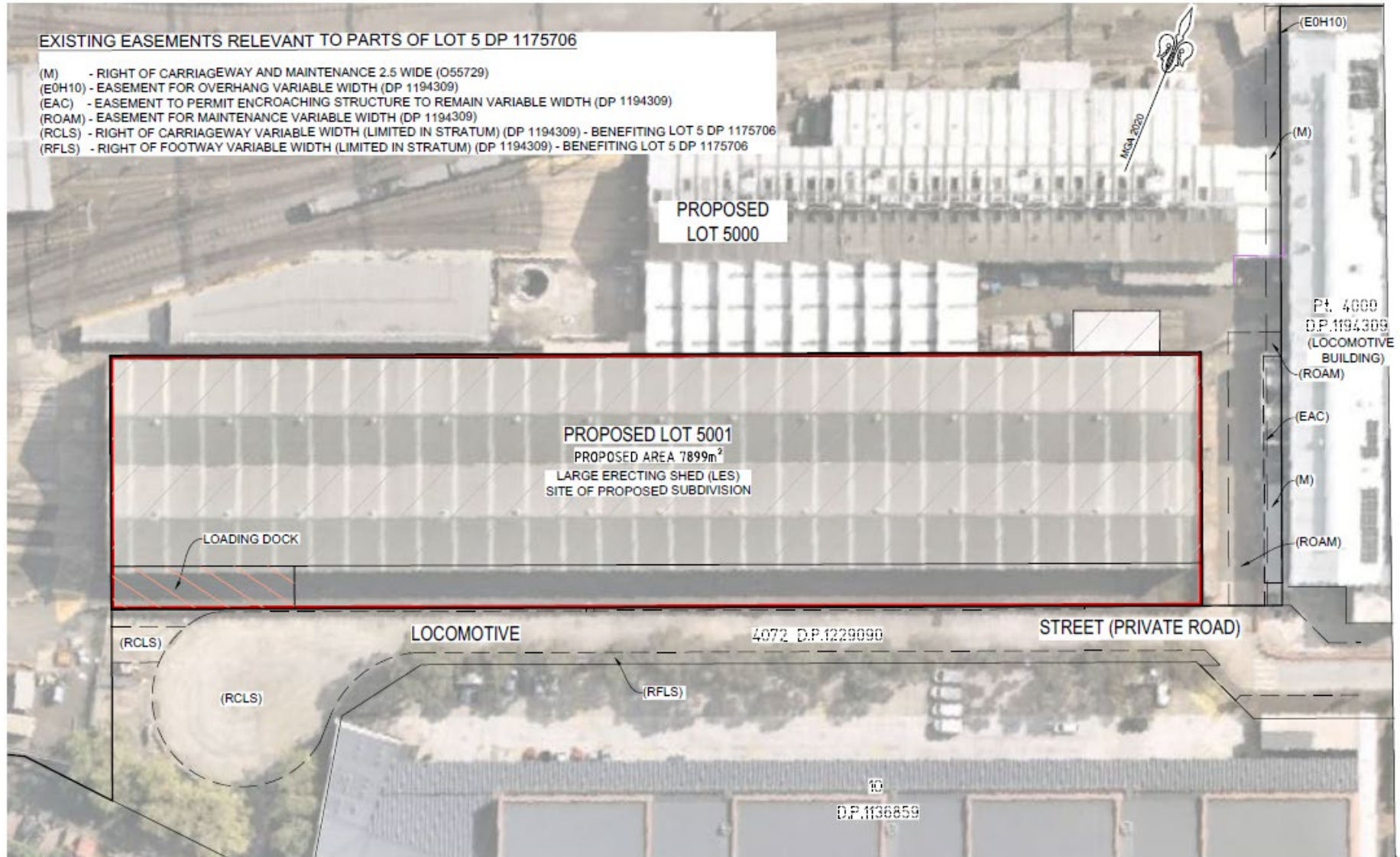


Figure 7.4: Detail of the proposed subdivision of part of Lot 5 in DP 1175706 containing the LES. Source: Tasy Moraitis, Reference 196-21G S01, 2022.

SCHEDULE OF PROPOSED CONCEPTUAL EASEMENTS

- (PE1) - RIGHT OF ACCESS VARIABLE WIDTH (OVER SHARED DRIVEWAY)
- (PE3) - EASEMENT MAINTENANCE 2.5 WIDE, 1 WIDE & VARIABLE WIDTHS
- (PE6) - EASEMENT FOR BUFFER STOP VARIABLE WIDTH
- (PE7) - EASEMENT FOR ACCESS AND USE
- (PE8) - EASEMENT FOR ACCESS, ATTACHMENT, SUPPORT AND USE VARIABLE WIDTH
- (PE9) - EASEMENT FOR WATER METER SERVICE VARIABLE WIDTH
- (AC) - EASEMENT FOR ACCESS CORRIDOR VARIABLE WIDTH

IT IS ANTICIPATED THAT EASEMENTS FOR SERVICES, ENCROACHMENT & OVERHANG MAY BE REQUIRED SUBJECT TO FINAL SURVEYS BEING COMPLETED

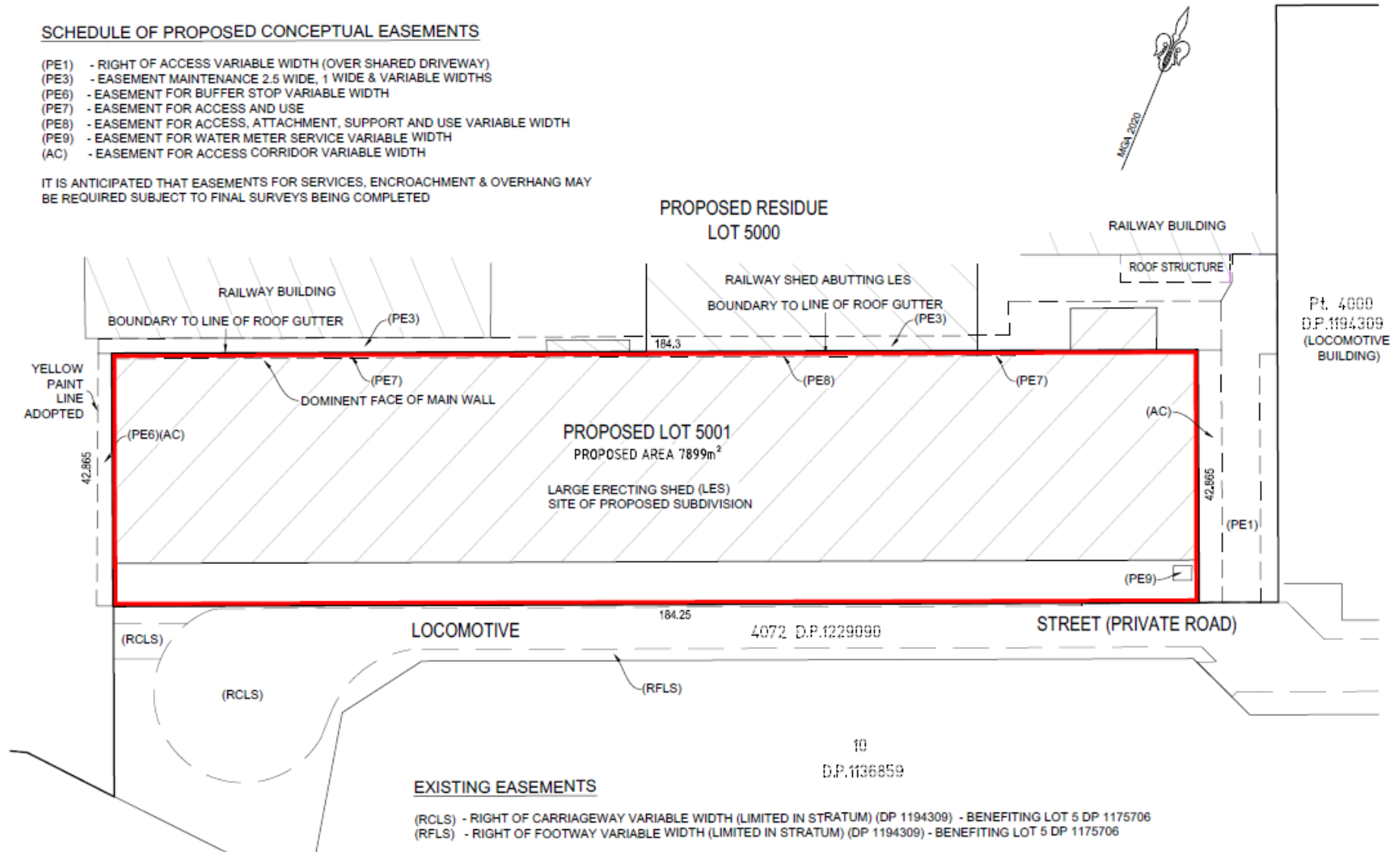


Figure 7.5: Detail of the proposed subdivision of part of Lot 5 in DP 1175706 containing the LES. Source: Tasy Moraitis, Reference 196-21G S01, 2022.

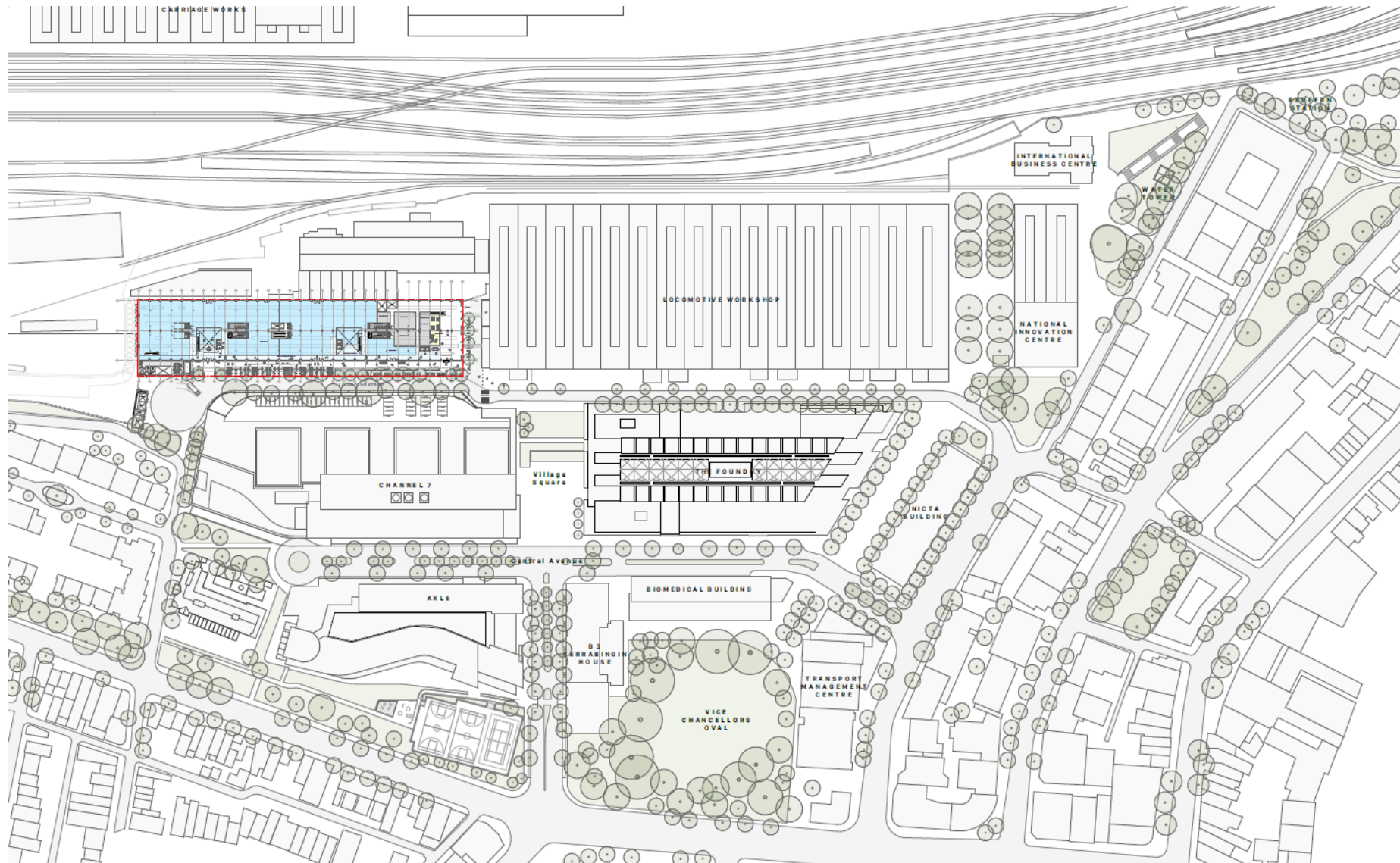


Figure 7.6: Proposed Site Plan. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022.

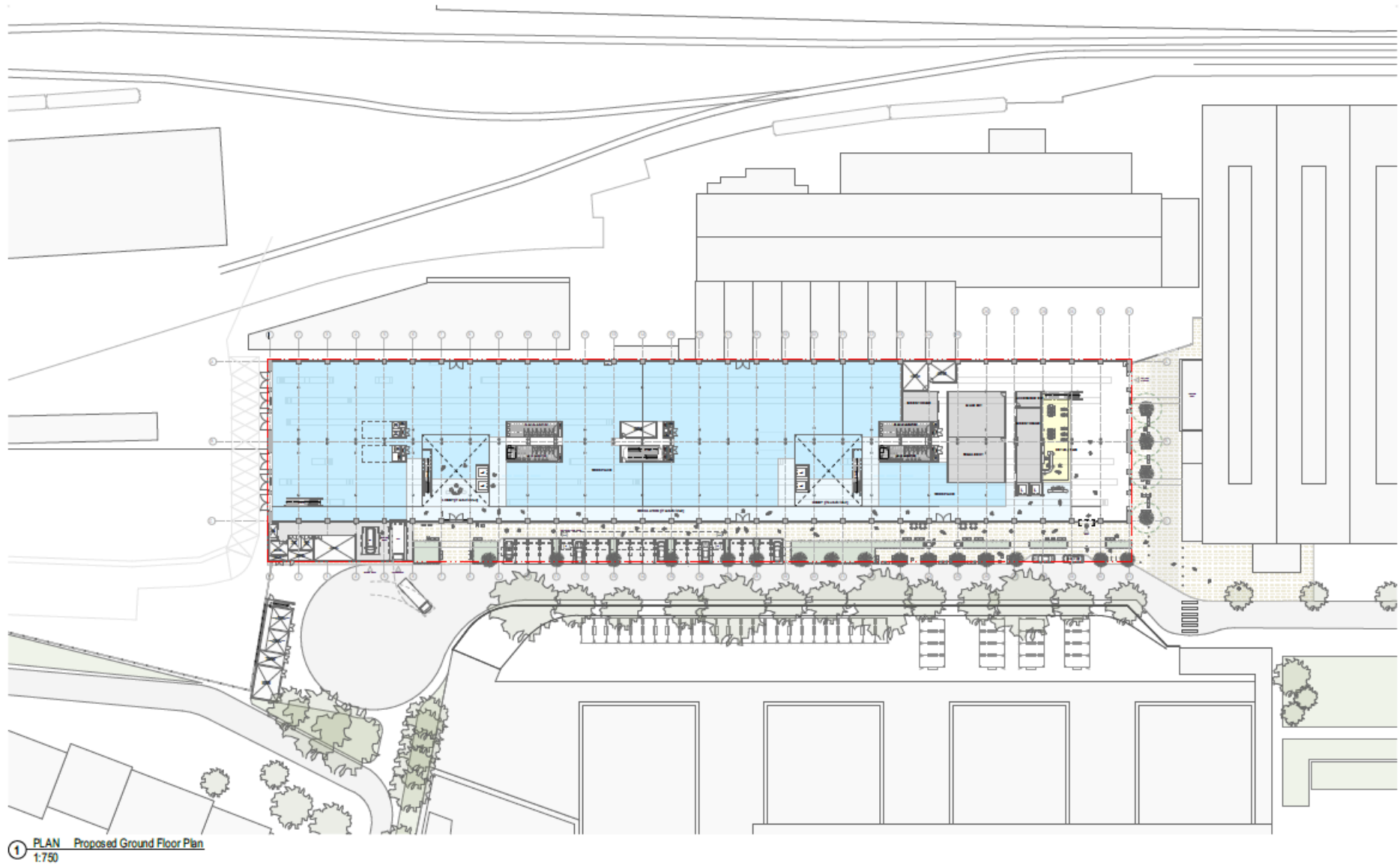


Figure 7.7: Proposed Ground Floor Plan. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022.

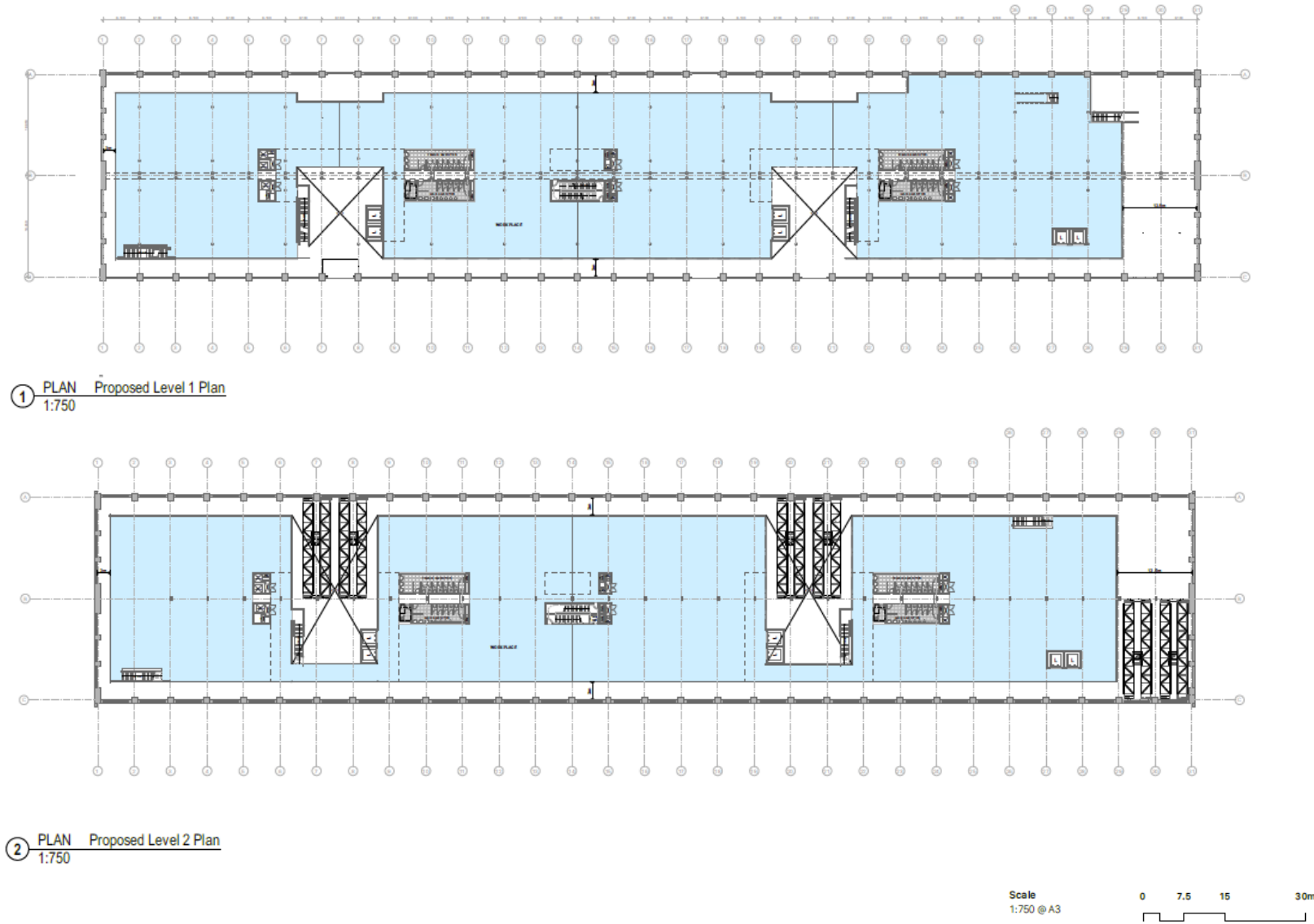


Figure 7.8: Proposed Level 1 & Level 2 Plans. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022.

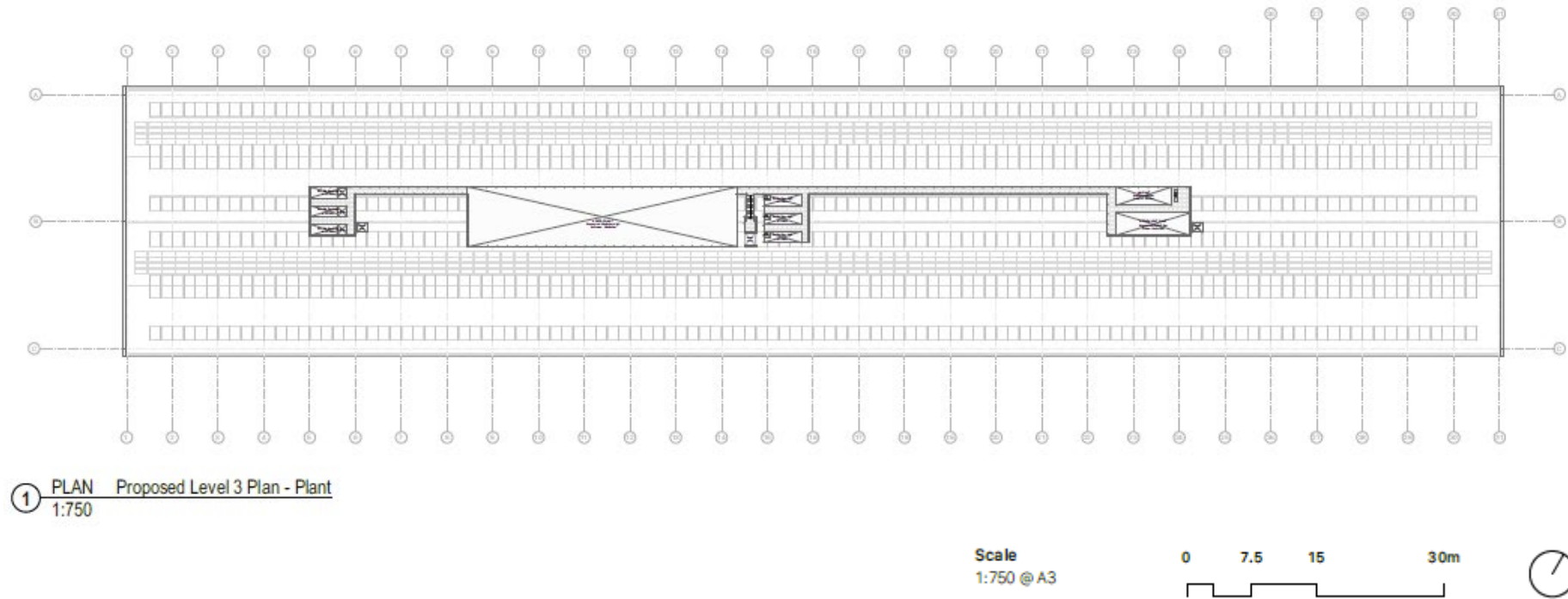


Figure 7.9: Proposed Roof Plan. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022.

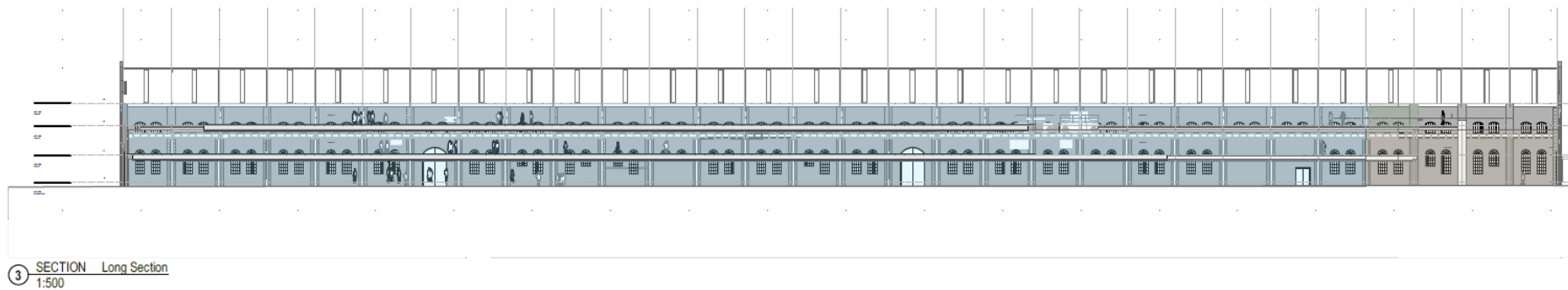


Figure 7.10: Proposed Long Section. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022.

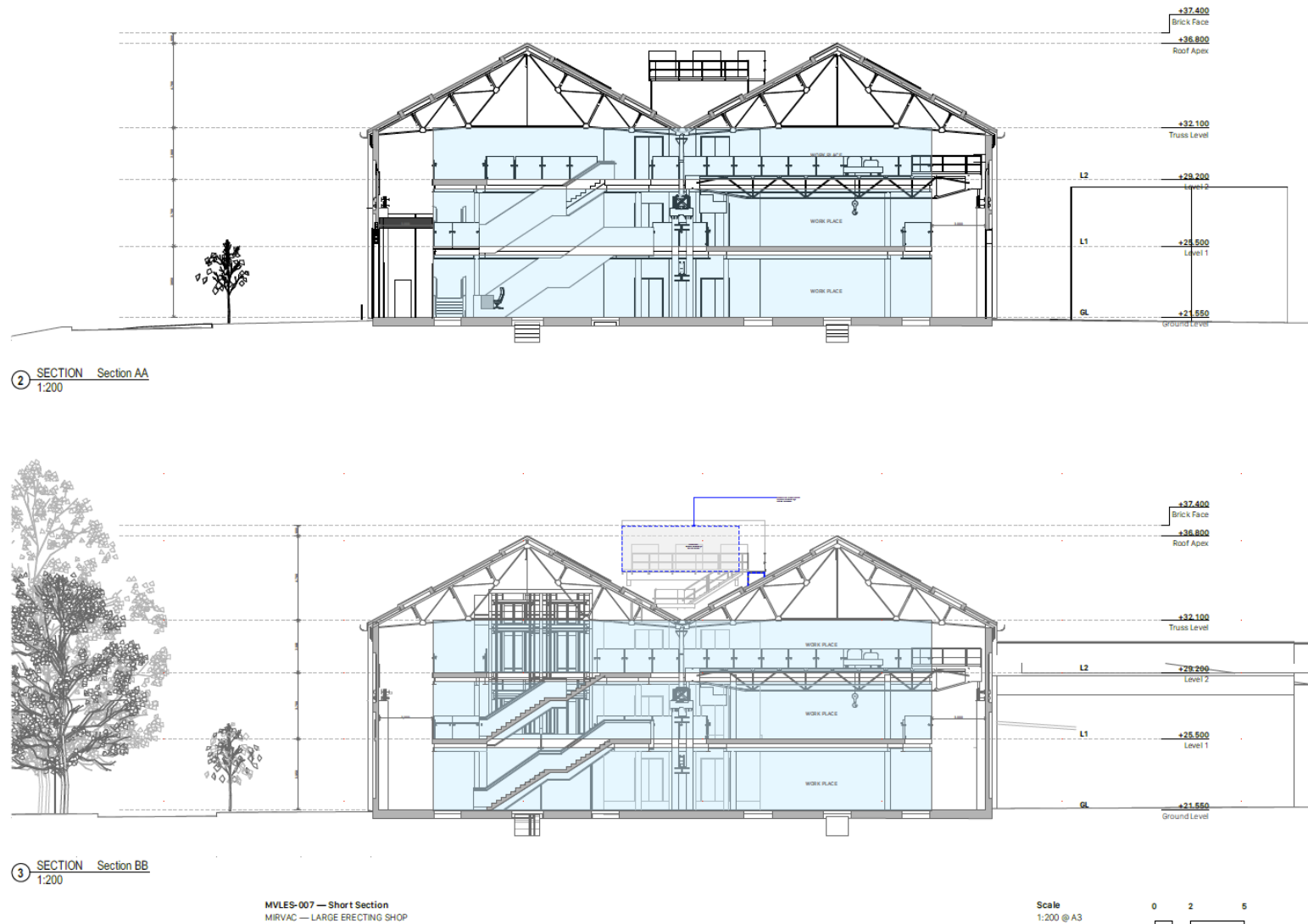


Figure 7.11: Proposed Sections AA and BB. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022

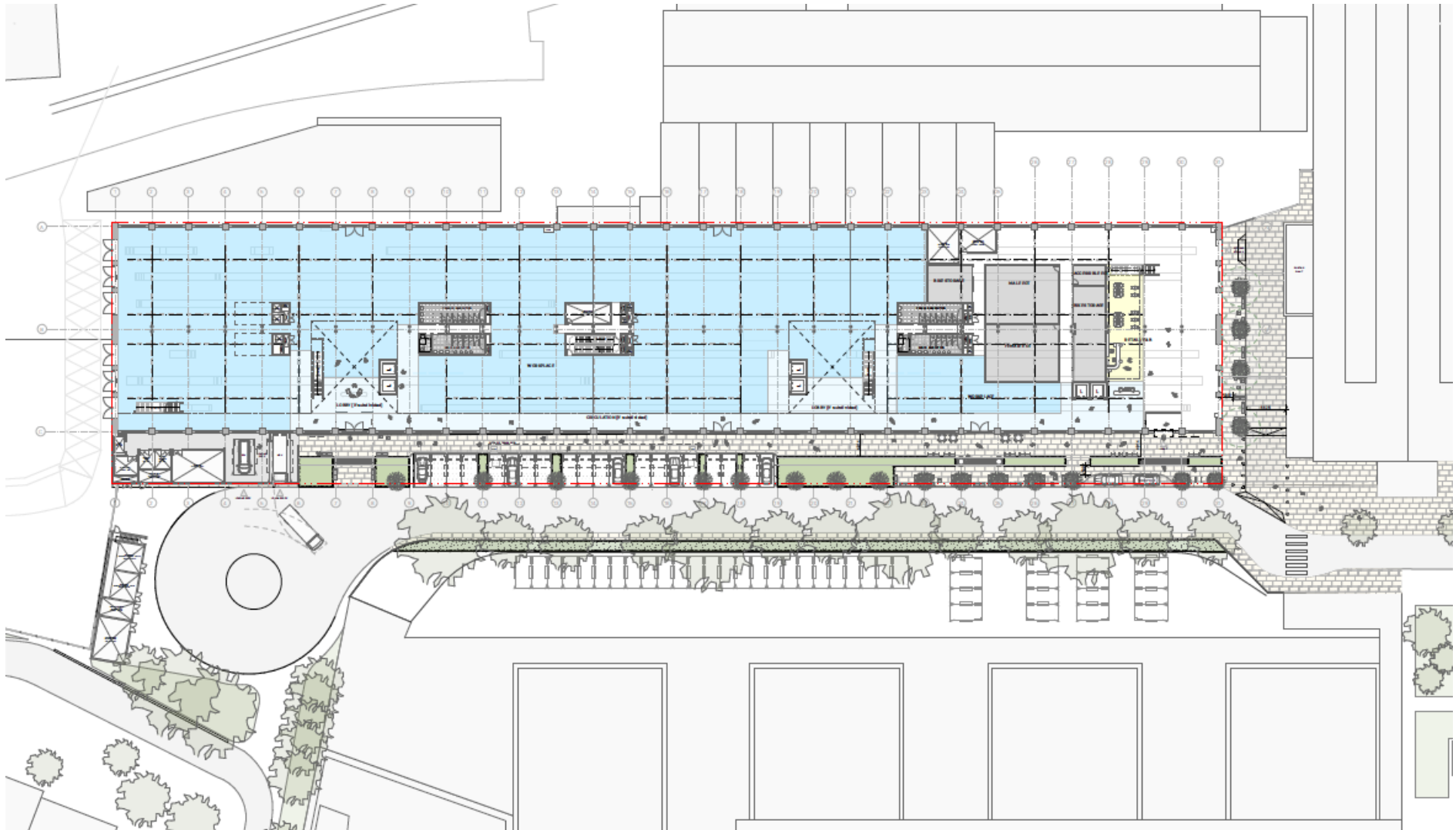


Figure 7.12: Proposed Landscaping Plan. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022.

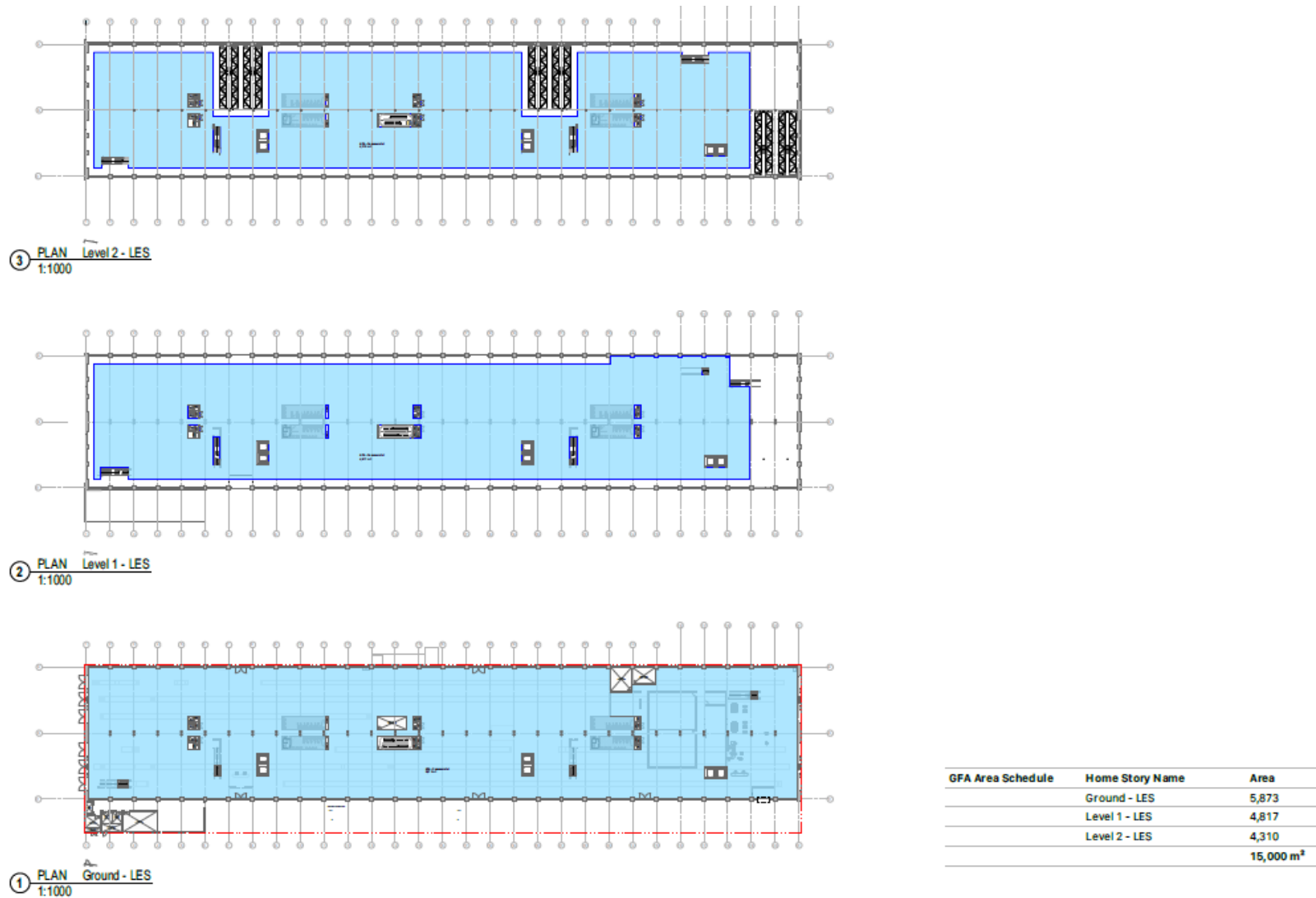
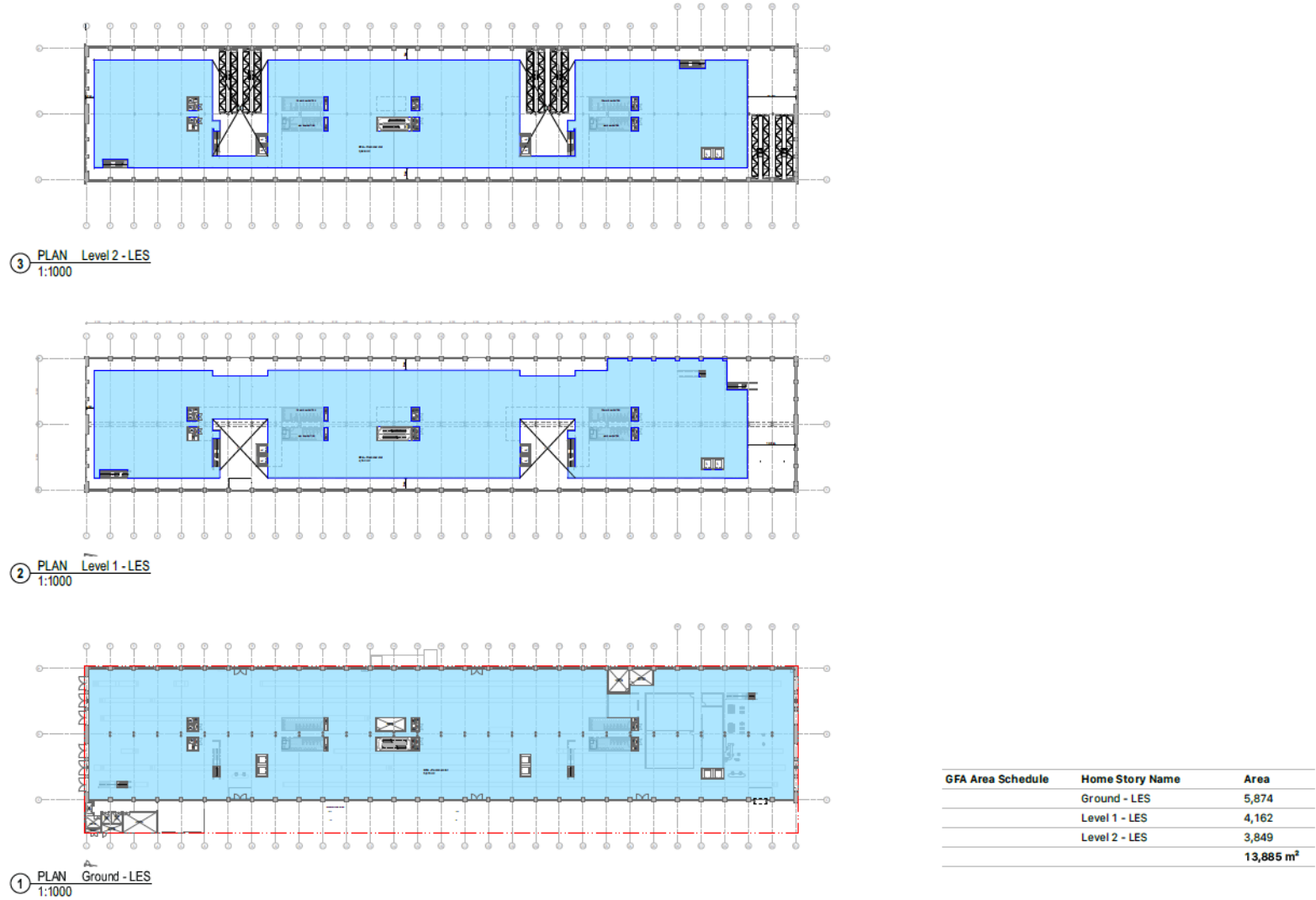


Figure 7.13: Proposed Rezoning Scheme – GFA Plans. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022.



V

Figure 7.14: Proposed Reference Scheme – GFA Plans. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022.



Figure 7.15: External view – Locomotive Street East. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022



Figure 7.16: Aerial view of the LES. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022



Figure 7.17: Internal view of the proposed communal space at the eastern end of the LES building. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022



Figure 7.18: Internal view of the proposed communal space and retail at the eastern end of the LES building. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022



Figure 7.19: Internal view of the proposed full height atrium within the commercial space. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT Studio, 2022

8. Assessment of Heritage Impact

8. Assessment of Heritage Impact

This section provides a preliminary assessment of the potential impact that the Rezoning Proposal for the subject site would have on the non-Aboriginal cultural and archaeological values of the subject site.

The detailed design for the proposed buildings and other areas including how new structures interface with heritage buildings has not been undertaken yet and therefore is not able to be considered as part of this heritage impact assessment.

Detailed design will take place during a future stage of the planning and redevelopment process and will require an accompanying additional heritage impact assessment (subsequent and additional to this report), to specifically respond to and assess its impacts, once developed.

8.1. Change of Use

Since the LES CMP was prepared in 2003, the precinct (formerly identified as ATP) has been redeveloped (2015-present) and is now known as the South Eveleigh Precinct. CMPs are generally required to be updated every 5-10 years, yet the 2003 CMP has not been revisited in nearly 20 years. In order to comply with current Heritage NSW recommendations and the requirements of the SEARs, a new draft South Eveleigh Precinct CMP providing guidance on the management of the LES building within the overall management of the redeveloped South Eveleigh Precinct is currently being prepared. The majority of the aspirations, intended partnerships (with the Powerhouse Museum, for example), objectives and future visioning which underpinned the 2003 CMP have never been realised for the LES. Therefore, a re-examination of the building and its adaptability for change and sustainable future use is being closely considered as part of the ongoing future management of all heritage assets within the South Eveleigh Precinct, as part of the redrafting of the South Eveleigh Precinct CMP.

The LES building was first constructed in 1899 adjacent to the Locomotive Workshop to be of supplementary use for the rolling stock overhaul services provided within the ERW precinct, however, those activities ceased in 1985. Although the LES was used for the maintenance of trains under the management of Locomotive 3801 Ltd (1985-2017), the building was only accessible to the public through scheduled tours during this period. Since Locomotive 3801 departed, the building has no longer been accessible to the general public and a substantial amount of the heritage moveable collection has been moved to a different location.

The redevelopment of the adjacent Locomotive Workshops was carefully studied and executed to ensure a sensitive and respectful outcome for its heritage values and the community, balancing commercial and retail uses with interpretive exhibitions and cultural heritage tourism opportunities. The development of the site has demonstrated a positive approach to adaptive reuse, seeing both the historical fabric and significance of the site retained through the introduction of sympathetic solutions and primarily reversible methodologies, ensuring the new and introduced fabric remains recessive in relation to the retained historical fabric, thus highlighting the original elements and characteristics within the precinct and creating a respectful composition overall.

Similarly, the NSW Government is investing in the renewal of the Redfern North Eveleigh Precinct to create a unique mixed-use development, located within the important heritage fabric of North Eveleigh, part of the former ERW curtilage. The precinct is intended to be redeveloped and

adaptively reused to create a destination for living, working and an inclusive, active and sustainable place for the community⁷¹.

Even since its first iteration in the 1990s, the adjacent Locomotive Workshop building has demonstrated that it can retain character, interpretation and meaning even when reused. Despite redevelopment, the clever architectural design and sympathetic fit-outs allow the new fabric to be recessive and primarily reversible with the fabric of the original building remaining the prominent storyteller. This, combined with a new overlay of thoughtful, creative, and engaging interpretation further helps visitors understand the complex and significant history of the site.

As a result of those positive outcomes, TAHE intends to extend the South Eveleigh vision to the LES, restoring its functional connection with the Locomotive Workshop and expanding the opportunities for public interaction with the history and legacy of the site without negatively impacting the integrity of its fabric or compromising its spatial qualities.

The objective is to integrate the LES into the broader South Eveleigh Precinct with the upgrade of the LES being the final building in the precinct to be adaptively reused to support the vision for a new tech ecosystem and to provide an activated precinct for locals, day to day workers and cultural heritage tourism visitors alike.

The intrinsic values of the ERW are not only the structural and technical assets but clearly also demonstrate the spiritual and social values borne by human use, work on-site, camaraderie and the myriad of stories, and activities that occurred across the site throughout time. With its significant pre-colonial associations and use, industrial heritage splendour, complex layered history, offerings of close encounters with industrial architecture, landscapes, traditional trades and storytelling, the ERW has and will draw generations of visitors who seek an immersive experience steeped in rich heritage, while offering all of the modern conveniences that an urban environment can offer.

After the successful adaptive reuse and ongoing activation of the South Eveleigh precinct, TAHE has a clear understanding of the possible outcomes for the site and fully supports the heritage character of the building, including its original fabric and patina, to remain prominent and appreciated. They aim to achieve the following objectives:

1. Transform the existing heritage building into an adaptively re-purposed workplace, incorporating primarily commercial use, complemented by an activation/communal space and retail amenity.
2. Provide a positive contribution to the South Eveleigh precinct through the reactivation of a redundant asset.
3. Celebrate the heritage significance of the building through interpretative measures that compliment, rather than compete with the interpretative approaches across the rest of South Eveleigh, Redfern Station and North Eveleigh.
4. Provide an interesting, unique heritage workspace to appeal to a diverse range of tenants, where the authenticity of the history of the site is celebrated and embraced on a daily basis.

These principles will help to create an economic, environmental, and socially sustainable development not only for the building users but also for the local community and key heritage stakeholders.

⁷¹ NSW Department of Planning and Environment, 2021. *Employment Lands: 2021 report on business parks*

According to the guidelines described in *New Users for Heritage Places: Guidelines for the Adaptation of Historic Buildings and Sites* prepared by NSW Heritage Council (now Heritage NSW) and The Royal Institute of Architects:

Many of our heritage buildings were built for a use that no longer exists today. If we want to conserve these buildings, then viable new uses must be found that retain and sustain them into the future. Redundant buildings are vulnerable to neglect, decay and eventually demolition.

The best way to conserve a heritage building, structure or site is to use it. Adaptation or adaptive reuse offers new uses for old places.

The new use needs to be compatible with the building, retain its historic character and conserve significant fabric, but it can still introduce new services, as well as modifications and additions. Each generation contributes to the constantly evolving historic environment in its own way. Architects, building designers and developers are crucial to the outcome of such change⁷².

Therefore, adaptive reuse is an efficient method to ensure heritage places remain relevant and activated, especially for highly significant sites such as the LES and the former ERW setting. The significance of a heritage place does not prevent it from being derelict and eventually neglected, and the costs to maintain and protect the place are often high and not sustainable, especially if it remains inaccessible to the community. In contrast, the adaptive reuse strategy will preserve the building's physical integrity while creating opportunities to draw thousands of workers and visitors daily and reviving the historical importance of the site as a symbol of Australia's economic and labour capacity.

Moreover, the adaptive reuse of the LES to accommodate technology-based companies is consistent with the Government's prioritisation of tech-based investment, in terms of growing innovation, and employment and strengthening NSW's position and global recognition in the innovation and technology sector. The proposal is also consistent with the SEPP strategy for Business Parks as it enables a mix of uses to function in one place to meet the day-to-day demands of workers in the immediate area, providing significant employment opportunities to the community.

Overall, the proposed strategy will enable the LES, the South Eveleigh and Redfern North Eveleigh precincts to be a cohesive and sustainable area that retains and appreciates its historical background and architectural setting while strengthening the economy and investing in innovative sectors. The proposal demonstrates best practice in the reuse and activation of the heritage building to retain its longevity and significance without adversely impacting the building's heritage values or physical integrity and investing in innovative and meaningful interpretive solutions to attract users and visitors to the site.

8.2. Building Interiors

The scope of the Rezoning Proposal does not yet include a detailed layout of the intended internal configuration of the building. Therefore, the following assessment relates to the indicative proposed layout only.

⁷² NSW Heritage Council and The Royal Institute of Architects, 2008. *New Uses for Heritage Places: Guidelines for the Adaptation of Historic Buildings and Sites*, p. 4

8.2.1. Internal Configuration

Commercial

The majority of the internal space of the LES will be dedicated to commercial activities and offices, targeting up to 3 tenants only. To make the redevelopment financially viable, TAHE and their design team have targeted a 15,000 sqm of GFA, requiring the internal space to be divided into three new levels⁷³ (Figure 8.1).

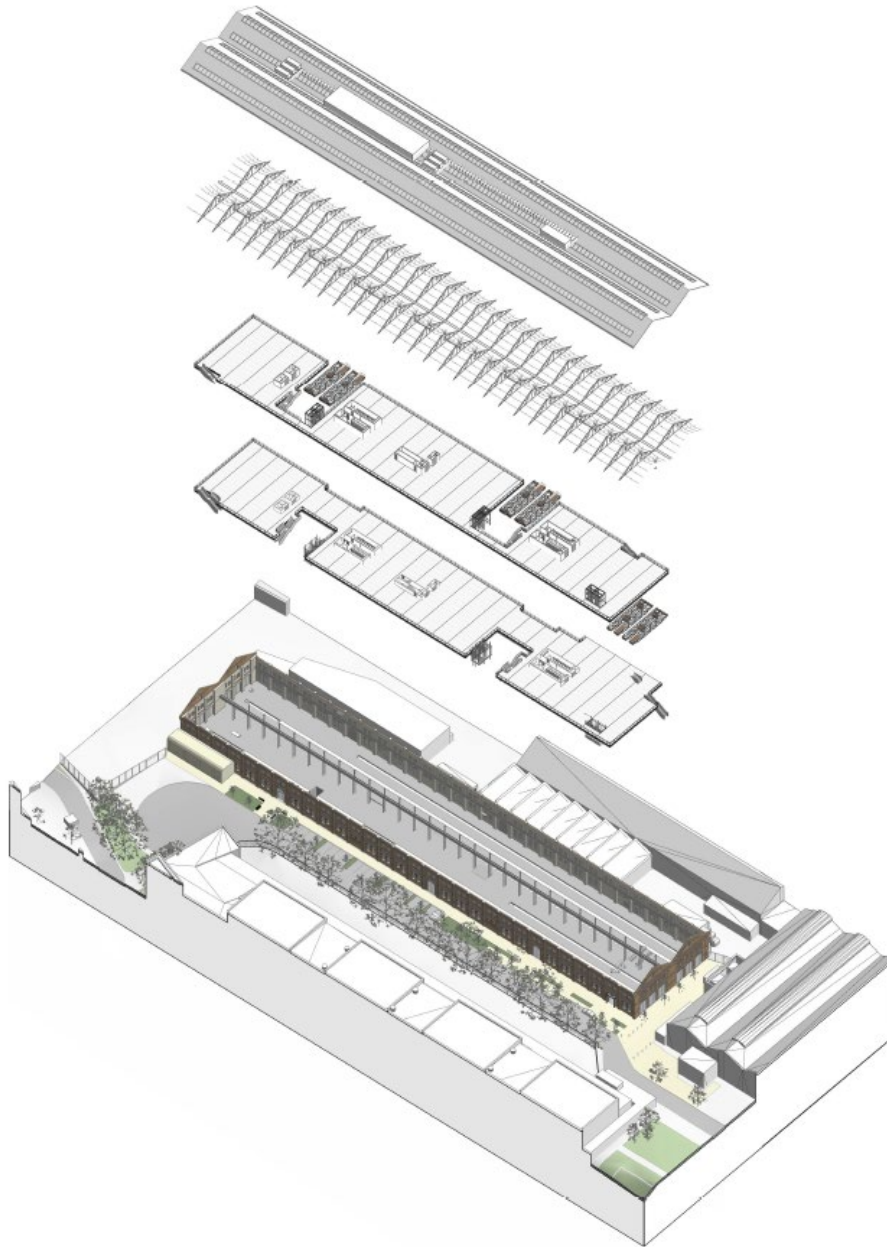


Figure 8.1: Axonometric View of the proposed subdivision for the LES interiors. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT, 2022.

⁷³ Mirvac, 2022. *Project Brief (LES) - Summarised high-level brief for Rezoning*.

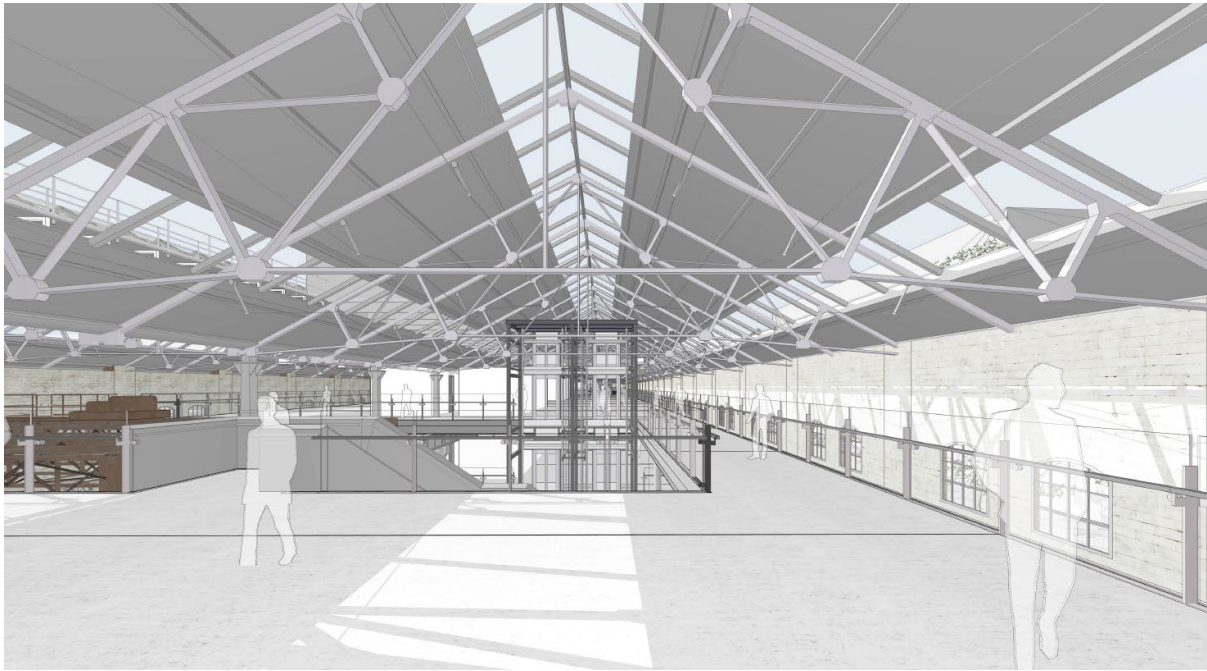


Figure 8.2: Proposed commercial space. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT, 2022.

The two new levels will be inserted within the building without physically interacting with the original fabric of the external walls. The structure will be offset from the internal facade of the building and will be protected by an internal balustrade, to retain the integrity of the physical fabric and to allow for uninterrupted views of the full-height masonry walls. The spatial separation between the internal walls of the façade and the modern insertion will also help to visually define the proposed structure as a modern insertion within the original LES structure, creating a clear distinction between modern and heritage fabric (Figure 8.3).

This is very similar in approach to the management of Bay 15 of the Locomotive Workshops, which has been very successful in allowing maximum utilisation of Net Lettable Area (NLA), whilst still providing a very clear interpretation and understanding of the original internal scale and size of the bay (Figure 8.4).

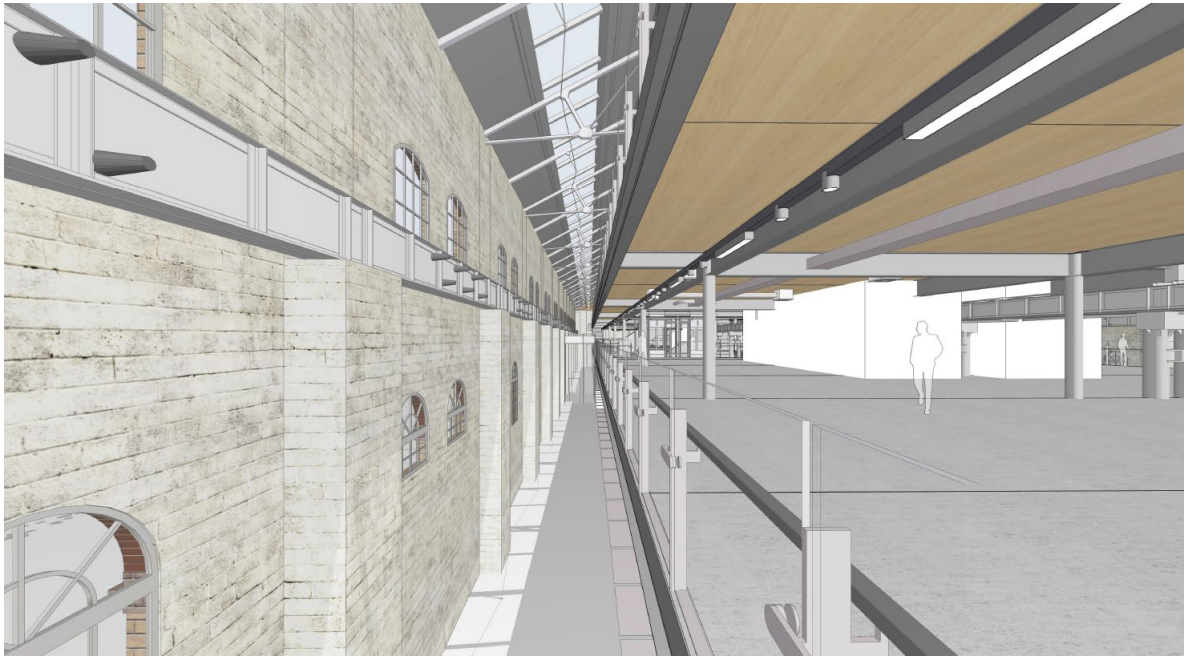


Figure 8.3: View of the proposed spatial separation between the original masonry walls and the modern floorplates. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT, 2022.

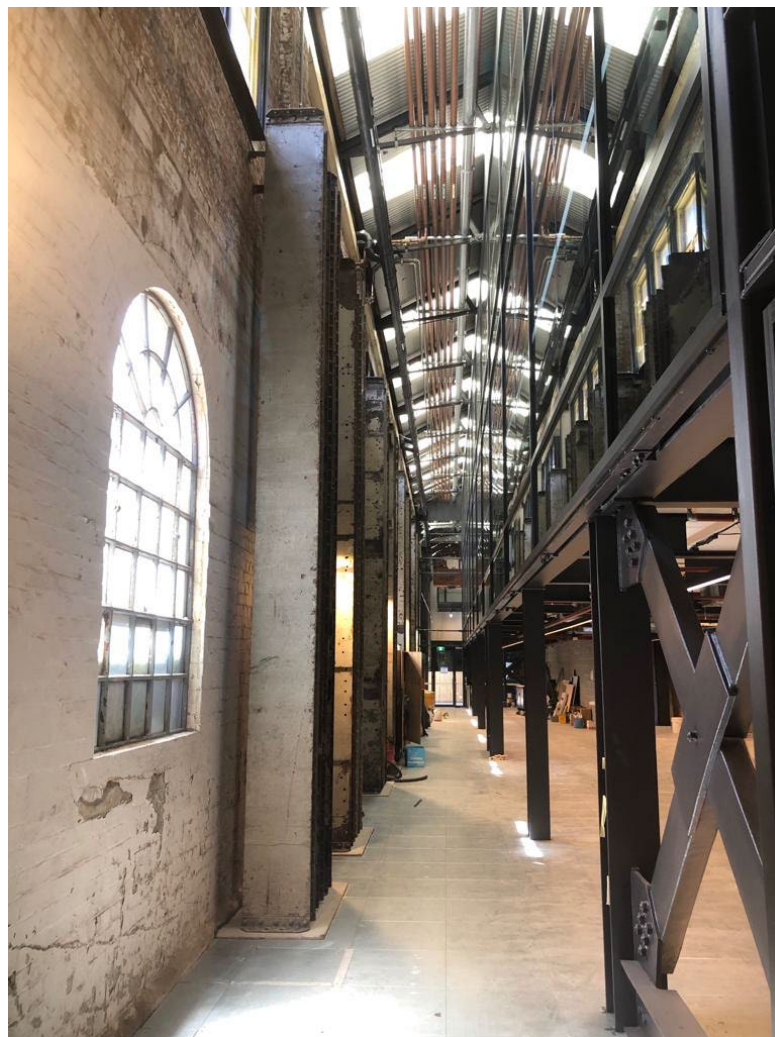


Figure 8.4: Existing spatial separation between the original masonry wall and the modern mezzanine structure in Bay 15 of the Locomotive Workshop. Source: Curio, 2022.

Two full-height atriums have also been designed to further minimise the visual impact of the new floorplates. Each will provide a sample of the original spatial characteristics of the building, with uninterrupted views to the original roof trusses, cast-iron columns, and beams (Figure 8.5). The insertion of these spaces will help to break up the solidity of the floorplate, allowing the original scale of LES to remain prominent. In addition, the vertical circulation and lobbies of the tenancies will also be located in the atria area, encouraging users and visitors to appreciate the historical fabric whether it be as part of spill-out functions, waiting for clients, or as part of the day-to-day entry experience to the site.

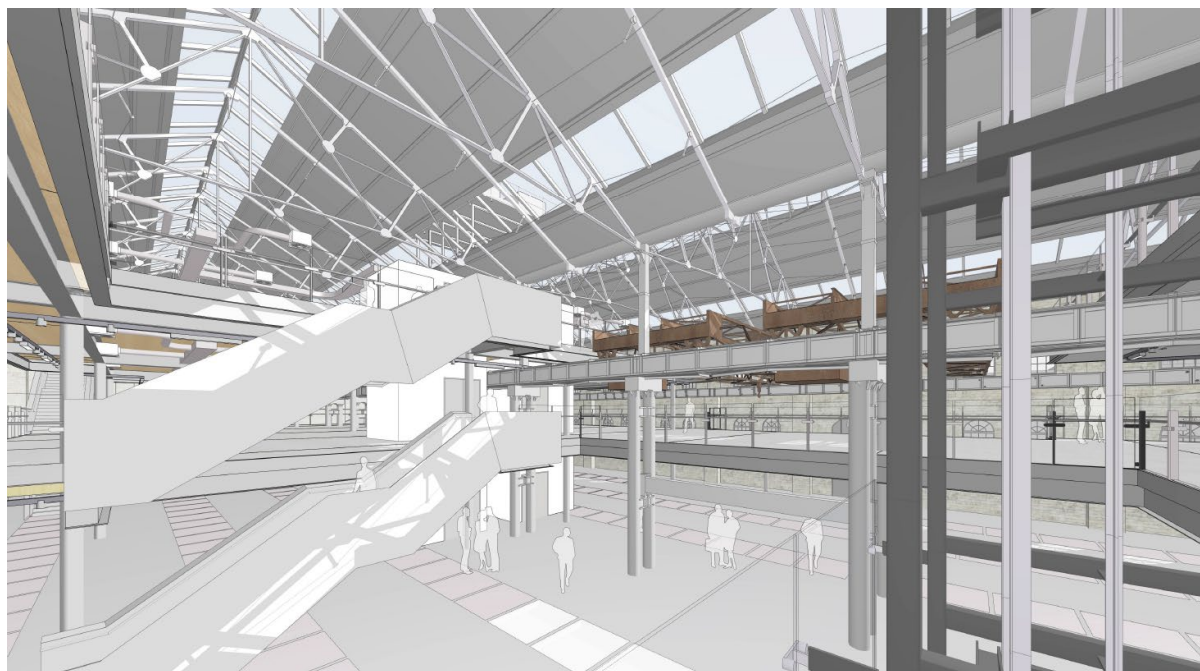


Figure 8.5: View of the proposed full height atrium. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT, 2022.

Activation / Communal Space

The activation/communal space will be located at the eastern end of the LES and will be accessible during its operational hours. The location has been strategically chosen as it is the area closest to the Locomotive Workshop and other buildings along Locomotive Street, encouraging users to explore the communal space during their walk along the street.

This strategic location will also enable the creation of an interpretive/public art zone between the LES and the Locomotive Workshop, extending the heritage exhibition route towards the LES and reinforcing the proposed incorporation of the building into the South Eveleigh precinct (Figure 8.7).

Internally, the communal space will function as a welcoming area, comprising the main lobby, retail operators, end-of-trip facilities, and storage. In addition, the area should be used to display multiple heritage items.

The communal space has also been designed to retain the original scale and spatial configuration of the building. The retail zone, end-of-trip facilities, and the building manager's office will be aligned with or behind the amphitheatre bleachers in order to retain the eastern lobby free from any partitions or subdivisions. By preserving the original height and openness of the eastern bays within the main communal area, users will be able to experience a sample of the heritage space and appreciate its original proportions and elements such as the original masonry walls, cast-iron double columns, and roof trusses.



Figure 8.6: View of the proposed communal space. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT, 2022.

Retail

The retail zone has been designed to allow for food and beverage operators, including a potential bar, to attend to the users' and visitors' everyday demands as well as potential events. The retail tenancies will be located on the ground floor behind the amphitheatre bleachers to ensure the full-height communal space remains public and all back-of-house facilities are hidden.

Overall, including a retail zone is highly encouraged from a heritage perspective as it will help to attract visitors to the building, encouraging them to occupy the space during and outside business hours and contemplate the heritage setting.

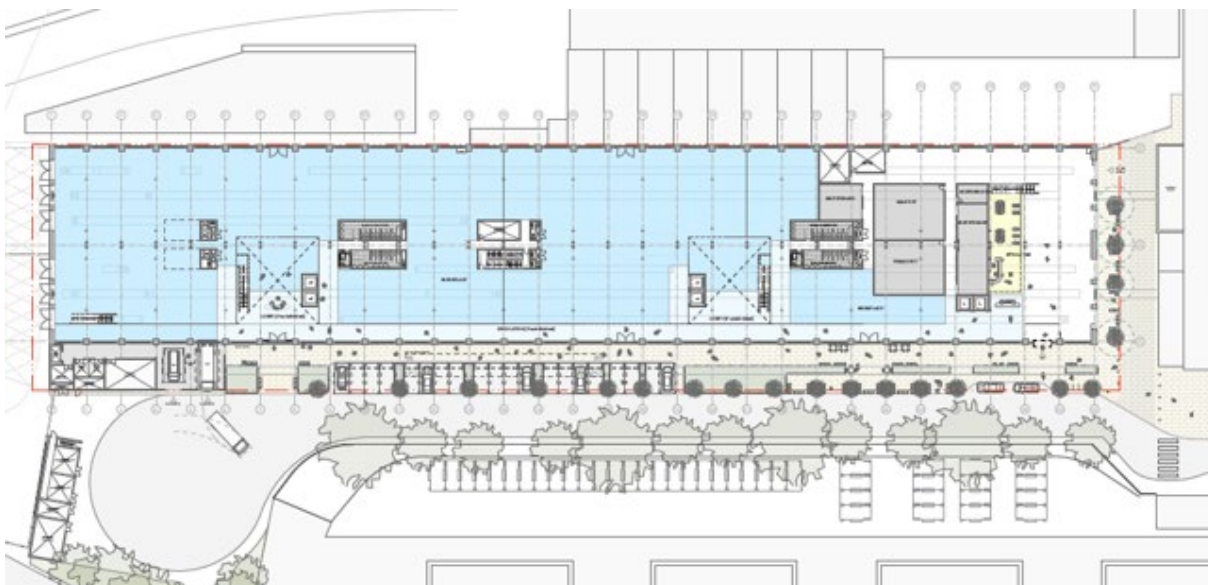


Figure 8.7: Proposed ground floor plan indicating the potential zone for retail in yellow. Reference scheme only for the Rezoning Application. Design is subject to detailed design, SSDA and planning approvals. Source: FJMT, 2022.

Overall Look and Feel

As the concept design and detailed materiality will be subject to future design, the Rezoning Proposal includes an intentional look and feel for the space represented by several benchmarking projects (Figure 8.8 to Figure 8.13). These projects have been selected for their relevance in terms of use, typology, scale, adaptive reuse, and approach to heritage conservation and interpretation.

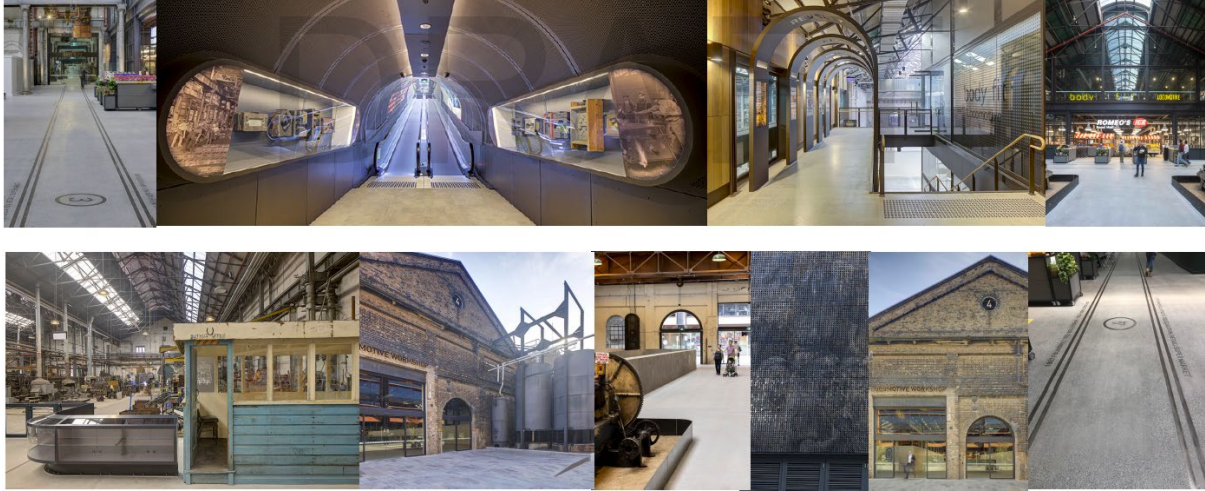


Figure 8.8: The Locomotive Workshop. The building is adjacent to the LES and they both form part of the ERW curtilage. The Locomotive Workshop shares similar historical use, typology and scale with the LES and consists of a positive example of adaptive reuse, conservation and interpretation strategies. Source: FJMT, 2022.

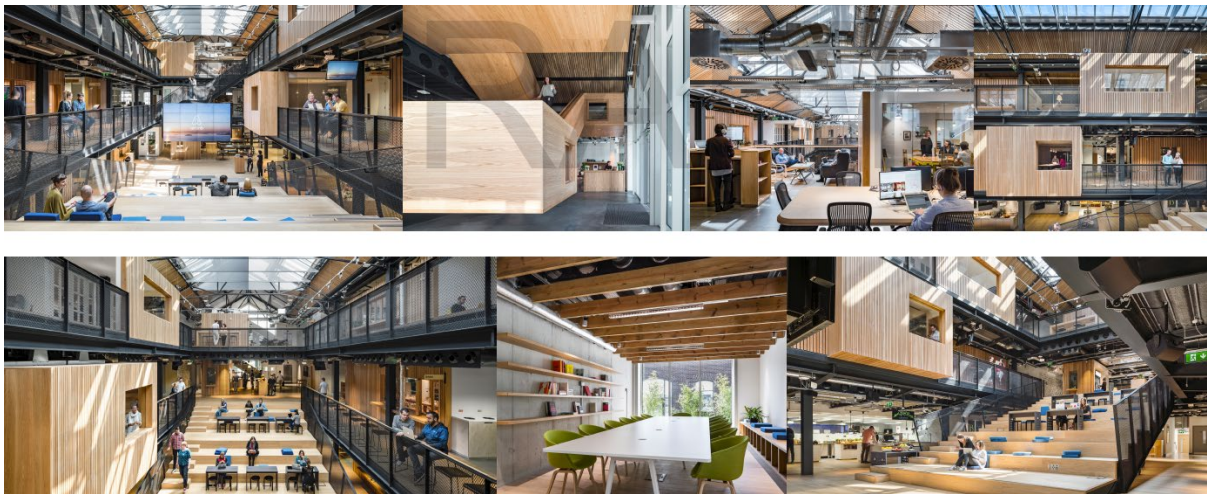


Figure 8.9: Airbnb Headquarters in Dublin, Ireland. Source: FJMT, 2022.

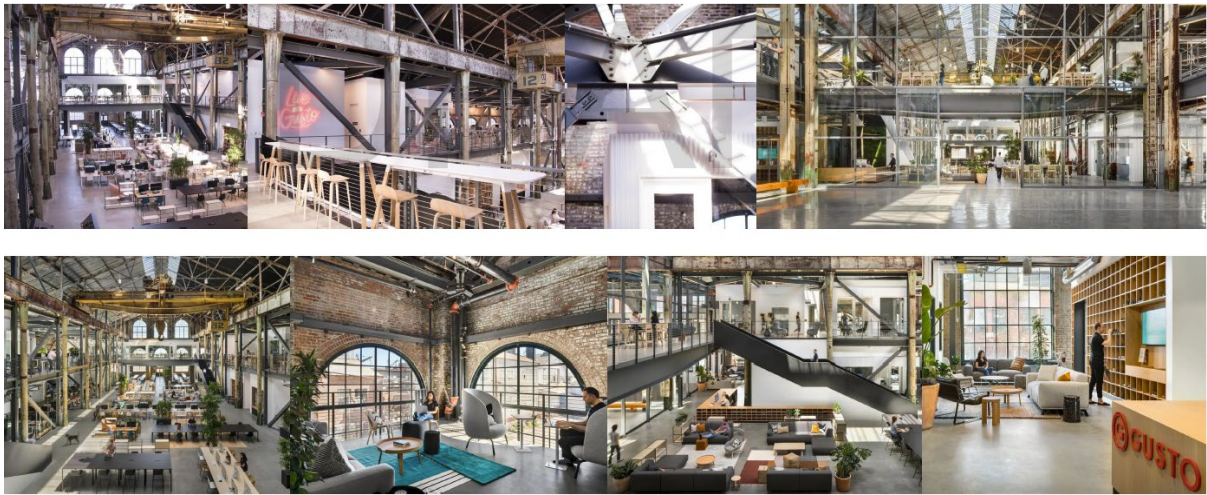


Figure 8.10: Gusto Headquarters in San Francisco, California. Source: FJMT, 2022.

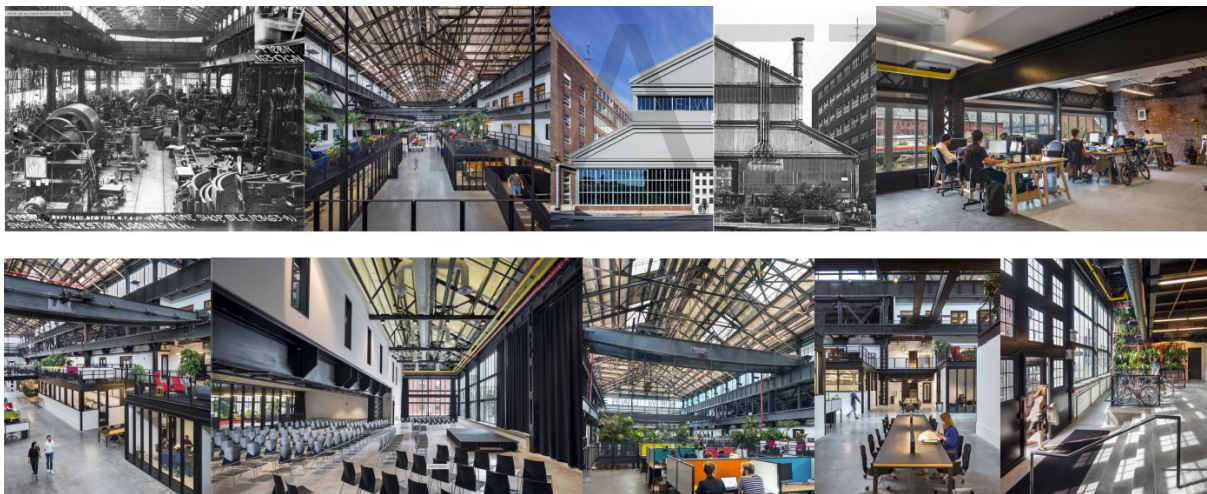


Figure 8.11: New Lab in Brooklyn, New York. Source: FJMT, 2022.

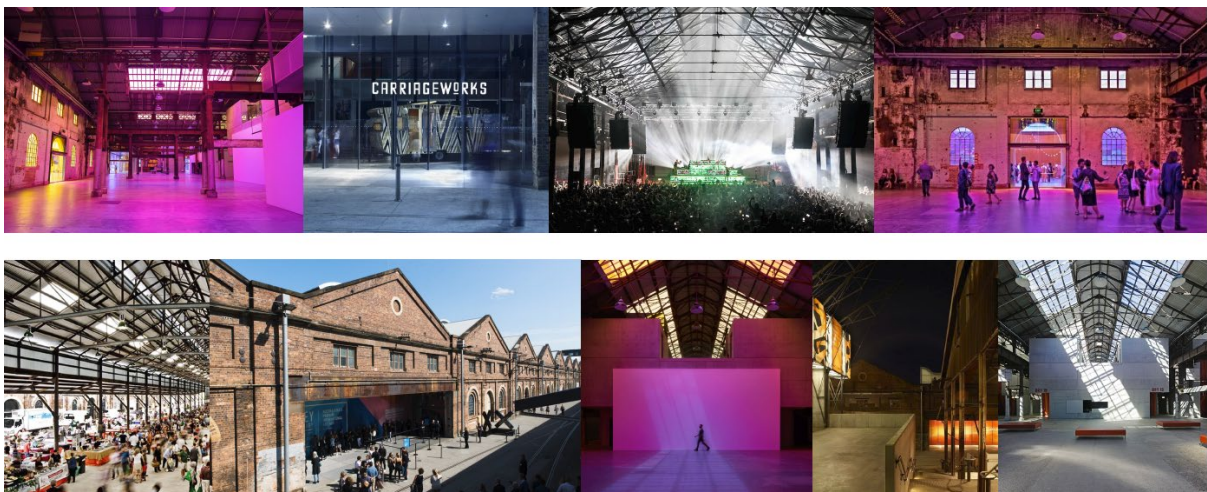


Figure 8.12: Carriageworks. The building is on the opposite side of the LES and Locomotive Workshop and forms part of the ERW curtilage. Carriageworks share similar historical use, typology and scale with the LES and consist of a positive example of adaptive reuse and conservation. Source: FJMT, 2022.

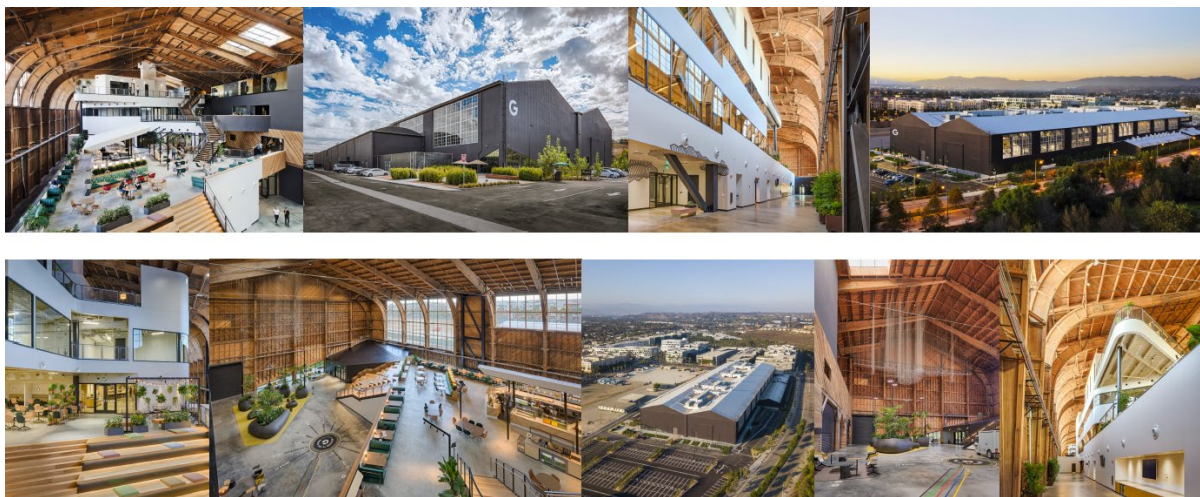


Figure 8.13: Spruce Goose in Playa Vista, California. Source: FJMT, 2022.

Overall, all benchmark projects demonstrate best practice in associating commercial and retail uses with industrial, and historical settings. The open plan fit-outs with contemporary and minimal additions are a consistent characteristic of such places as they allow the heritage fabric to remain prominent and relevant. Materiality and colour palettes are sympathetic and commensurate with the industrial character of the building and, where possible, glazing partition walls are used to enclose spaces without visually blocking views to any original elements. In addition, the large scale of the communal spaces within shed-like buildings encourages them to be reused as exhibition areas, creating an engaging and innovative experience inside their everyday workplace.

8.2.2. Overhead gantry cranes

Although the scope of the Rezoning Proposal does not yet include a detailed design depicting alterations to any original elements, the proposal outlines intentions to retain, restore, modify, or move specific items to improve their physical condition, improve their visibility and/or adapt them to current standards.

Due to the proposed subdivision of the internal space into three levels, the 7 existing overhead gantry cranes will be relocated from their current positions to meet head clearance requirements and in order to maximise the continuity of floorspace. Although subject to future detail, it can be anticipated that the cranes will be relocated to the full height atria and potentially to the communal area at the eastern end of the building to not only meet the head clearance requirements but also to place them within areas that retain original characteristics of the heritage building (e.g. scale, openness, height, etc.). Like the communal space, the atria will most likely house the tenancies lobbies, similar to the atrium spaces throughout the Locomotive Workshop, which will facilitate and encourage users to appreciate the retained overhead cranes during their work breaks.

8.2.3. In Ground Pits

The existing in-ground pits are highly significant elements due to their importance during the locomotive overhaul process. The existing pits present the opportunity of being reinterpreted and could be converted into unique displays to showcase salvaged items or be kept as exemplars of the work conditions at the time, in particular the ones located in the communal space. Section 8.6 *Heritage Interpretation* further describes the intentions for Heritage Interpretation initiatives for the site.

8.2.4. Arch doors

The proposal intends to remove the arch doors on the eastern and western facades and replace them with glass infill panels to maximise natural light in the space.

The north and south ends of the LES originally comprised six arched timber double doors of heavily braced and ledged construction, however, most of these doors have deteriorated and have temporary repair work to keep them operational. Only the door located at Road 6 east end is largely intact and provides evidence of original detailing.

Therefore, the proposal's intention will not have an adverse visual or physical heritage impact on the modern doors as they are not part of the original fabric of the building. It is recommended that the only original door left (Road 6 – east end) is either kept in place or reused as part of the Heritage Interpretation Strategy for the site (Section 8.6 *Heritage Interpretation*).

8.2.5. Facade arch windows

The original frames of the arched windows on the building facade are intended to be retained within the concept design of the Rezoning Proposal. A condition assessment report of the existing original panes will need to be prepared prior to any future DA for the site, in order to ensure that the character of the original windows is retained, as much as possible.

It is recommended that only damaged glazing be replaced with clear and transparent glass. The intact frosted panels should be retained. The contrast between two glass (modern glazing and original) will create a clear distinction between historical and contemporary fabrics, similar to the arched windows on the Locomotive Workshop building. As a result, this will require further detailed consideration as part of the next stage of the detailed design.

Overall, the proposal is intended to have a positive visual and physical impact on the arch windows as it will retain the original fabric and replace only damaged panels.

8.2.6. Roof Materiality & Skylights

The proposal aims to remove and replace the existing roof sheeting with new roofing to comply with current standards (e.g. water tightness, acoustic performance, etc.). The new roof sheeting will be designed with the skylights running longitudinally to maximise natural lights within the building as well as to allow the rooftop plant to be installed within the gully of the roof.

The existing roof sheeting has been recently replaced (1997) and does not form part of the original fabric. Therefore, the proposed alteration will not have any impact on the physical fabric of the LES. The design, style, colour, and materiality of the new sheeting should be chosen in accordance with the industrial character of the building to avoid any detrimental impact.

The new orientation of the skylights will have a positive visual impact on the overall heritage fabric and significance of the LES as it will restore the original longitudinal configuration of the skylights.

8.2.7. Heritage Moveable Collection

The scope of the Rezoning Proposal does not include specific details about the proposed placement, treatment, management, and incorporation of the significant moveable heritage collection associated with the LES building and the former ERW precinct. This subject will be further developed in future detailed phases and will play a significant part in the design development as per the example of the adjacent Locomotive Workshop.

It is noted that, as previously mentioned in Section 4.1.1, the majority of the items listed on the Section 170 Register as being located within the LES listed (S170 Register) have been moved offsite to a new location. Therefore, it is recommended that a comprehensive moveable heritage survey is

undertaken and a current Moveable Collections Catalogue that records and catalogues all moveable heritage items across the LES is developed in order to update relevant heritage listings with the details of the current inventory. The inventory will form a sub-section of the broader South Eveleigh Moveable Heritage Catalogue being prepared by Curio and ICS and should be included as a sub-section of the Moveable Collections Management Plan for South Eveleigh (currently in progress).

8.3. Public Domain

8.3.1. Extension of Locomotive Street

The proposed extension of Locomotive Street serves four main purposes:

1. Integration of Locomotive Street as a connective element.
2. Provision for connection of Locomotive Street to Rowley Lane to improve vehicular and pedestrian connectivity.
3. Seamless connectivity to the South Eveleigh Precinct.
4. Creation of a new gateway to South Eveleigh.⁷⁴

The extension will facilitate Locomotive Street to be set as the primary pedestrian access to the LES building, as it is now for the Locomotive Workshop. This connection between the two buildings will enable the LES to be fully integrated into the broader South Eveleigh precinct, strengthening the connection with the precinct and creating a continuous heritage experience throughout the street.

The proposal will also enable a new entrance to the precinct by connecting Locomotive Street with Rowley Lane, creating opportunities to activate the western end of the site by adding landscaping solutions and public amenities to the area.

Overall, the proposed extension of Locomotive Street will have a positive impact on the heritage significance of the LES as it will attract users to explore the full extent of the building, enhancing the visual connectivity between the subject site and the remaining South Eveleigh Precinct, in particular the Locomotive Workshop. Likewise, the creation of a new gateway on the western end of the precinct will help to activate and define it as an important portal to South Eveleigh.

8.3.2. Car Parking

The proposed western extension of Locomotive Street will enable the creation of a car parking area on the northern side of the street, along the central portion of the LES. The car parking will attend to the future demand for parking risen from the redevelopment of the subject site and its chosen location is consistent with the existing parking zone on the southern side of Locomotive Street.

The location of the car parking has been carefully studied to not impact the overall experience along Locomotive Street. Due to the proximity to the South Eveleigh at the eastern end of the LES, in particular to the Locomotive Workshop, the eastern zone of the public domain along the street has been designated to accommodate landscaping solutions to improve and extend the continuous heritage experience (see Section 8.3.3 *Landscaping*). Therefore, the new car parking has been positioned to the west of the seating zone to avoid any interruptions to the pedestrian route. In addition, with the new loading dock annexe at the western end of the southern facade (see Section 8.4 *External Annexes*), the car parking area will help to visually soften the impact of the new volume, being assessed as an appropriate solution.

⁷⁴ FJMT 2022. *LES Rezoning Report*. Pg 14

8.3.3. Landscaping

The Rezoning Proposal does not include specific details about landscaping design; however, it is anticipated that the proposed landscaping will be designed to not detract from or reduce the legibility of the original façade. The future detailed design will be developed based on the following overarching principles to upgrade the public domain area, especially along the eastern and southern facades of the LES:

Connection

- *Ensure clear vehicle and active transport connections are integrated with existing infrastructure.*
- *Enhance and extend existing pedestrian and shared circulation routes through and around ATP including connections to existing transport nodes, such as Redfern Station and bus stops on Henderson Road.*
- *Design the public domain to be in line with the established South Eveleigh Public Domain.*

Activation

- *Create public spaces which are attractive, comfortable and usable.*
- *Further integrate suite of public realm furniture, including lighting, bench seating and other social amenities.*
- *Extend the public realm to ensure activation day and night, seven days a week.*

Diversity

- *Create outdoor places that encourage flexibility of use.*
- *Provide a range of streetscape spaces from small and intimate gatherings and outdoor working opportunities.*

Identity

- *Reveal heritage elements of the site to create a strong sense of place. Provide interpretation to educate and enhance the unique experience of place.*
- *Identify opportunities to integrate public art into the public domain design.*
- *Retain and reinforce the existing site character through the preservation of existing mature trees.*

Sustainability

- *Propose high quality, low maintenance, robust softscape and hardscape materials.*
- *Use native flora and encourage biodiversity and habitat for local fauna.*
- *Reuse existing materials in new site features where possible;*
- *Incorporate Water Sensitive Urban Design Initiatives (WSUD) into the streetscape.*

Safety

- *Create safe, low-speed, pedestrian friendly accessible spaces.*
- *Design the public realm with regard to crime prevention (CPTED) principles⁷⁵.*

Such principles will help to establish a core foundation to develop the conceptual design, ensuring the result is both functional and respectful to its heritage setting.

⁷⁵ FJMT 2022. LES Rezoning Report. Pg 35

As mentioned in Section 8.3.2 *Car Parking*, the addition of landscape solutions, including public domain improvements such as the creation of seating zones, along Locomotive Street reinforces the activation strategy used for the South Eveleigh precinct. Such areas will not only encourage users to occupy the street but also create opportunities for public art and interpretation products to be displayed, attracting more visitors to interact and learn about the cultural significance of the site.

8.4. External Annexes

Loading Dock and Plant & Equipment

The scope of the Rezoning Proposal does not include specific details about the loading dock and plant & equipment room; however, the potential locations of these two areas have been carefully studied at this initial design stage as additional volumes within or adjacent to the main building could potentially result in a negative visual impact on the heritage significance of the LES. Therefore, any design solutions that propose the loading dock and the plant & equipment room to be located within the building have been rejected, as they would have a major impact on the spatial quality and readability of the interiors as well as block views of the retained significant elements throughout the space.

As a result, the loading dock has been located at the western end of the southern façade of the building, accessible via Locomotive Street. Likewise, the plant & equipment room have also been positioned at the western end of the site, however, the proposed annexe will be fully detached from the LES building and will be at the southwestern corner of the existing cul-de-sac at the termination of Locomotive Street, on land currently controlled by Mirvac.

Further, to ensure the plant & equipment annexe structure is the smallest possible, part of the services is proposed to be located on the existing roof under the conditions that they are no higher than the roof apex, not visible from the ground level, and are accessible and easily maintainable.

Overall, the proposed locations of the loading dock and plant & equipment have been carefully developed and will not have any physical impact on the heritage fabric of the building as they will be self-standing, independent structures. In addition, the external locations proposed for the two annexes will avoid any visual interference on the internal viewlines within the LES and, by detaching one of the annexes from the building, the proposed new volumes will not have a detrimental visual impact on the legibility of the southern facade of the LES from Locomotive Street.

Further design details will be required to properly assess the proposed annexes' impact on the heritage significance and fabric of the building; however, interpretive solutions are encouraged to mitigate the visual impact of the new structure on the southern facade. Similar strategies have been undertaken for the modern annexes of the Locomotive Workshop and have resulted in outstanding interpretation products. The Workers Wall and the Timecard Wall Display have been developed to envelop the new annexes with innovative interpretative solutions that retell interesting stories and honour the site workers' legacy (Figure 8.14Figure 8.14).



Figure 8.14: The Workers Wall and Timercard Wall Display have been developed to envelop the modern annexes with innovative interpretation solutions that allude to the historical background of the Locomotive Workshop. Source: Curio, 2022.

8.5. Potential Archaeological Impacts

The following assessment of potential archaeological impacts has been drawn from the *Non-Aboriginal Heritage Study: Large Erecting Shop – Rezoning Proposal* prepared by Curio (2022). For a full overview of the archaeological background of the subject site, reference should be made to the original report.

8.5.1. Historical Archaeology

As the LES site is adjacent to South Eveleigh and both are elements of the same overarching site of the former ERW, a consistent approach will be applied to the management of archaeological resources. Therefore, it is proposed that a similar differentiation between ‘relics’ and ‘works’ (where relevant) be applied to the LES study area (the context in which archaeological relics and works have been successfully managed at South Eveleigh previously), while both relics and works may have the potential to be present, the way in which they are required to be managed may differ.

The proposed development is likely to have minimal impact to the historical archaeological potential within the LES study area. Future development works that will impact the ground surface within areas of moderate or high archaeological potential as well as works that will impact the ground level within the LES subject site will have potential to encounter and impact historical archaeological features, resources, and works but not as many relics.

The Rezoning Proposal does not include substantial bulk excavation works and therefore targeted areas may include areas of excavation of lift pits and piling. Depending on the depth and location of excavation proposed, these works have potential to be a moderate to major archaeological impact, potentially removing a proportion of the historical archaeological resource remaining within the LES study area.

The archaeological impact of the project will require management as part of the future stages of the development, following the finalisation and approval of the proposed works, and commencement of detailed design phases that will determine the exact nature and extent of the potential archaeological impact in each area of the site.

The following overarching recommendations are made regarding the management and mitigation of the removal of historical archaeology within the LES study area:

- Historical archaeology within the LES study area should be managed in accordance with the archaeological potential and management framework outlined in Section 5 of this report, which should be further developed through a future Archaeological Research Design (ARD), specific to the impacts and final design of the precinct renewal development. It should also be managed in accordance with the archaeological provisions of the Draft South Eveleigh Conservation Management Plan.
- Archaeological works carried out within the study area should be undertaken under a Section 60 Excavation Application under the NSW Heritage Act 1977, depending on the assessed significance of the potential archaeological deposit and location in question. The appropriate approvals pathway (i.e., a s60 Application, or a s60 Fast Track Application) would depend on the nature and extent of works proposed, and the corresponding impact to potential historical archaeological resources. This would be determined and confirmed during the preparation of the ARD.
- Archaeological mitigation program developed through the ARD should be specific to the location and nature of ground-penetrating works at the site, but may include:
 - Targeted archaeological test excavation undertaken in the key areas identified as having moderate-high archaeological potential before construction works commence.
 - Unexpected finds protocol.
 - Archaeological monitoring in areas with nil - low archaeological potential.
 - Archival recording, surveying, photogrammetry, and 3D scanning should be performed to selected features for future heritage interpretation products if any archaeological features are uncovered during monitoring and testing.
 - Significant archaeology uncovered will need to be incorporated into the heritage interpretation on site.
- Landscaping may also be redesigned to avoid physical impacts on the archaeological resource by changing the nature of the intended planting, i.e., low-impact shallow-rooted bushes rather than mature trees with the potential to create future damage through expansion of the root ball. Where it is not possible to avoid archaeology through redesign or use of previously disturbed areas, further archaeological assessment and investigation will be necessary.
- Where possible, detailed design options and solutions should be investigated to reposition structural and built elements that have potential to impact archaeology, for example, minor relocation of support columns or options to span across areas identified as having high archaeological potential with two or more columns. A similar avoidance approach may also be employed with the installation of services, either avoiding areas of the archaeological resource and redesigning the route of the services or identifying existing service corridors, trenches etc and laying new services within areas that have previously been subject to excavation disturbance.

8.5.2. Aboriginal Archaeology

In relation to Aboriginal archaeological potential, Curio's 2022 *Aboriginal Cultural Heritage Assessment: Large Erecting Shop – Rezoning Proposal* predicts the LES subject site has low to moderate potential for Aboriginal archaeology for the following reasons:

1. Historical development within the subject site has resulted in disturbance and impact to upper natural soil profiles, however, the subject site has not previously been subject to extensive bulk excavation.
2. The generally lower lying and swampy landscape of the subject site and surrounds were not likely preferable or suitable locations for intensive Aboriginal occupation and camping prior to 1788. However, the subject site retains potential for low-density artefact scatters and isolated artefacts to be present within natural undisturbed soil profiles, representative of ephemeral movement of people through the landscape and use of the area as a natural resource zone.
3. Should such deposits be found to be present within remnant natural soil profiles across the subject site, these may have potential for moderate to high social, historical, and scientific significance, depending on the nature of the resource present.
4. The subject site has low to moderate potential for intact Aboriginal archaeological deposits to be present, most likely in the form of low density or isolated stone artefact sites representative of general Aboriginal movement across and use of the Redfern landscape.
5. The Rezoning Proposal and future development at the LES site represent a significant opportunity to have an overall positive impact to Aboriginal cultural heritage values through the proposed introduction of meaningful heritage interpretation initiatives, as well as the integration of Aboriginal cultural heritage values and outcomes into the project design.

It has been recommended that geotechnical investigations should be undertaken across the footprint of the subject site. This will allow a better understanding of any intact natural soil deposits within the subject site and help in determining if Aboriginal archaeological test excavations will be required. If geotechnical results identify any remnant soil landforms, a program of Aboriginal archaeological test excavations should be undertaken at the subject site prior to any ground disturbing works to determine whether subsurface Aboriginal archaeological deposits are present. Specifically:

- An archaeological test excavation methodology and archaeological research design (ARD) document should be prepared in accordance with the Code of Practice, establishing the research framework and methodological approach to guide the test excavation.
- Any Aboriginal archaeological test excavation undertaken at the subject site should work in collaboration with, or acknowledgement of, any required historical archaeological investigation, should it be required (subject to the recommendations of a separate report).

8.5.3. Assessment of Intangible and Social Impacts

As part of the former ERW, the heritage buildings, values, and character of the LES are emblematic of a type of work no longer common in NSW and the remaining buildings are seen as a testament to the many thousands of workers and their families that made their living within the walls of the Workshops.

Relevant to the development of the Large Erecting Shed is the previous and extensive consultation programs and reporting undertaken in association with the South Eveleigh developments of the Public Domain, Buildings 1-3 and the Locomotive Workshop at South Eveleigh. The detailed consultation programs, undertaken by Ethos Urban in collaboration with the broader consulting teams on behalf of TAHE were carried out during different stages in the design and development of the sites, both pre and post-DA submissions.

Stakeholder groups were identified and invited to participate in consultation activities using existing relationships and networks, public advertisement (via newspapers and Eventbrite), social media (including Instagram and Facebook) as well as a postcard letterbox drop to 12,000 surrounding businesses and residents. The Conditions of Consent associated with SSDAs 7317, 8517 and 8449, also specified multiple stakeholders and subsequently consultation was carried out with the Heritage Council, the City of Sydney Council, as well as other stakeholders including but not limited to former workers, Aboriginal stakeholders, volunteers, local community, heritage professionals, and railway associations.

The consultation process served a dual purpose. Through presentations, meetings, and collaborative workshops, participants were presented updated details regarding the redevelopment of the South Eveleigh precinct, including heritage management strategies as well as key design elements identified during preliminary Heritage Interpretation planning. In addition, consultation provided an important opportunity for stakeholder participants to contribute feedback on all aspects of the developments as well as present their own ideas toward key tangible and intangible elements and stories significant to the entire South Eveleigh Precinct.

The results of the comprehensive consultation fed into on-site design, and subsequent Stage 2 Interpretation planning and implementation. All feedback (including that regarding all aspects of the broader precinct, those specific to the developments, sentiments, stories and significance of the precinct), was recorded in multiple Consultation Outcomes Reports and remained integral throughout the projects to ensure appropriate levels of celebration of the sites' history and heritage were addressed through design, refit and interpretation.

8.6. Heritage Interpretation

The redevelopment of the LES provides the perfect opportunity to expand and complete the layered interpretation and integrated cultural tourism already established throughout the South Eveleigh precinct. This site is the final building in the former ELW (southern portion of ERW) to be redeveloped and offers the chance to continue the interpretive experience at key locations, encouraging visitors to explore the entire precinct upon arrival from one of the many entry points, including the immersive approaches from Redfern Station, and as part of the already established Eora Journey.

Curio Projects have been engaged to prepare the Heritage Interpretation Plan (HIP) for the subject site with a focus on identifying key themes and stories that form a strong basis for the development of interpretive opportunities relevant to the history of the site. By incorporating innovative interpretation elements within the LES site, Curio will ensure an inclusive account of the site's rich history, its function within the Eveleigh Railway Workshop, as well as accounts from the local community, both past and present are told authentically and as engaging as possible.

Curio's involvement in the interpretation, planning, community engagement and design installation across the entire precinct, including at South Eveleigh, Redfern Station and North Eveleigh, ensures the continuity of heritage interpretative storytelling across the whole of the site, with a strong focus on delivering unique experiences across the precincts without being repetitive or reductive.

Key objectives of the HIP are:

- Outline the interpretive process which is proposed for the subject area.
- Identify constraints and opportunities that may impact the implementation of interpretation in the subject area.
- Identify and summarise the key interpretive themes and storylines for the subject area.
- Ensure continuity with existing heritage interpretation strategies and plans undertaken to date at South Eveleigh, North Eveleigh, and Redfern Station, into the overall context of proposed elements.
- Provide emblematic recommendations for appropriate interpretive elements that will provide a clear and engaging interpretation of the relevant parts of the LES history, natural environment, and community.

The storytelling and interpretative vision as part of the HIP are:

The first and final stops are the ultimate milestones of any journey. They mark a significant point in any event, story or people.

The re-purposed Large Erecting Shop will be both the beginning, from Erskineville Station, and the final stop, from Redfern Station, of the South Eveleigh precinct.

It will contribute to the already rich layers of storytelling throughout the precinct, as well as provide meaningful interpretation of the LES and its integral role within the historical and contemporary landscape.

As the LES sits within the South Eveleigh precinct and was an integral part of the working life at the Eveleigh Railway Workshop, a selection of themes from the South Eveleigh HIP have been reproduced, which will ensure the relevant history that links the precinct is aligned. However, the themes for this HIP have been expanded upon to ensure unique stories of LES and its specific history are told.

Following this thematic framework, the HIP has identified potential interpretive products and corresponding appropriate zones for such products across the LES site. Please refer to the *Stage 1 Heritage Interpretation Plan: Large Erecting Shop* (Curio, 2022) for further details on the proposed heritage interpretation strategies and opportunities for the subject site.

8.7. Assessment Against the Overarching CMP Heritage Management Principles

Table 8.1 presents an assessment of the proposed works against the Heritage Management Principles described in the Overarching CMP prepared by Otto Cserhalmi + Partners for the subject site updated by Curio in 2022.

Table 8.1: Assessment of the Rezoning Proposal against the Overarching CMP Heritage Management Principles.

Heritage Management Principle	Assessment/Comment
HMP 1. <i>All future decisions should be based on an understanding of the heritage values of the place and with a view to retain the identified significant values.</i>	Met. TAHE has commissioned multiple heritage studies as part of the Rezoning process to understand the heritage values of the place and how as part of any adaptive reuse process, the significant fabric and remnant moveable heritage collection, key external views & vistas can be protected and interpreted within any redevelopment of the site.

Heritage Management Principle	Assessment/Comment
	Internally, the concept design allows for adaptive reuse that provides an understanding of the full volume of the internal spaces, as well as insertions that will allow for a viable Net Lettable Area (sustainable adaptive reuse).
HMP 2. <i>A coordinated approach to management of the individual precincts and areas within the ERW site should be undertaken by future owners to ensure that future decisions are based on consideration for the heritage significance of the whole ERW and its overall presentation.</i>	Met. This Statement of Heritage Impact, as well as the Non-Aboriginal Heritage Study, the Aboriginal Cultural Heritage Study and the Heritage Interpretation Plan – have been prepared with close reference to, and acknowledgment of, the development works, heritage context, and interpretation, undertaken between 2015-2022 as part of Mirvac's South Eveleigh redevelopment to ensure that decisions made for LES site are consistent with the overall South Eveleigh Precinct, and with strong consideration of the Overarching Conservation Management Plan for Eveleigh Railway Workshops (prepared by OCP, updated 2022).
HMP 3. <i>The ERW site is assessed as being of state significance and therefore any future owners and managers of the site and its individual precincts should manage the place in accordance with best-practice heritage guidelines.</i>	Met. TAHE recognises the State significance of the ERW site and is bringing that knowledge into the design of the Rezoning Proposal, through its demonstrated experience, knowledge and understanding of the requirements for the successful redevelopment and management of the rest of the South Eveleigh Precinct (including the Locomotive Workshops).
HMP 4. <i>Proposals for change should not unduly affect the significant heritage values of the ERW site and its individual precincts and should facilitate understanding of the place and its heritage values.</i>	<p>Met. The building and its immediate surroundings are currently inaccessible, are not BCA compliant and, as a result, are underutilised and at the risk of becoming derelict in the future.</p> <p>The proposal intends to adaptively reuse the LES to facilitate contemporary commercial use and future users. The proposed adaptive reuse of the LES will ensure it remains relevant, activated and fully accessible.</p> <p>By proposing a sympathetic and primarily reversible insertion within the building, the heritage impacts on fabric and spaces can be mitigated/offset by the activation of the building and the implementation of meaningful, engaging, and carefully considered heritage interpretation initiatives, as well as retention and adaptive re-use of the heritage building and moveable items, all of which are currently unavailable to public access.</p> <p>As a result, the precinct renewal project also seeks to ensure that the redevelopment of the precinct will facilitate and improve understanding of the ERW complex and its heritage values to the future site users and the local community.</p>
HMP 5. <i>The history and significant values of the ERW site and its individual precincts should be interpreted, including within any future redevelopment and reuse of the site where existing elements are to be removed or modified. The</i>	Met. TAHE is committed to ensuring the full interpretation of the precinct for future users. They have commissioned Curio to prepare a Heritage Interpretation Plan ⁷⁶ to ensure that the site will continue to be interpreted for future users, proposing engaging and innovative interpretive solutions. The intended interpretation will take into account the interpretation objectives of the rest of the precinct.

⁷⁶ Curio Projects, 2022. *Stage 1 Heritage Interpretation Plan: Large Erecting Shop.*

Heritage Management Principle	Assessment/Comment
<i>place should be interpreted as a major railway workshop facility.</i>	
HMP 6. <i>Skilled conservation professionals should be engaged to advise on, document and/or implement conservation and upgrading work and future development proposals for heritage assets of the ERW site and its individual precincts.</i>	Met. Curio Projects has been engaged to advise on and document the heritage impacts relating to the Rezoning Proposal, with respect to non-Aboriginal and Aboriginal heritage values. For future works proposed for individual buildings and moveable heritage items, it is recommended that TAHE continues to engage appropriately skilled conservation professionals, thus ensuring the heritage values of this site will be maintained.
HMP 7. <i>Planning for new development, adaptive reuse of heritage assets and heritage interpretation should include opportunities for community consultation.</i>	Ongoing . Relevant to the development of the Large Erecting Shed is the previous and extensive consultation programs and reporting undertaken in association with the South Eveleigh developments of the Public Domain, Buildings 1-3 and the Locomotive Workshop at South Eveleigh. The detailed consultation programs, undertaken by Ethos Urban in collaboration with the broader consulting teams, on behalf of TAHE, have been carried out during different stages in the design and development of the sites, both pre- and post-DA submissions, and will continue during the LES redevelopment process.

8.8. Assessment Against the Overarching CMP Policies

Since the LES CMP was prepared in 2003, the ERW precinct has undergone several substantial alterations, in particular the South Eveleigh redevelopment (2015-present). CMPs are generally required to be updated every 5-10 years, yet the 2003 CMP has not been revisited in nearly 20 years, which resulted in the 2003 version of the document being no longer fit for purpose, especially given that the document was prepared with the intention of turning the building into an extension of the Powerhouse Museum, along with other museum-related tasks which have never eventuated. Therefore, the primary 2003 CMP policies are not reflective of the current opportunities, constraints, and reuse options available to TfNSW/TAHE.

As a result, in order to support the intended Rezoning Proposal, the suitability of the proposed rezoning from a heritage perspective has been assessed against the various Heritage NSW guidelines, the principles of the Burra Charter, and the Overarching Precinct CMP policies and guidelines.

Throughout the process, Heritage NSW has been consulted about this matter and has acknowledged that the 2003 CMP, which only allows museum use at the site, is no longer relevant or adequate to be used as a guide for the Rezoning Proposal, hence the reference to and assessment of the LES Rezoning Proposal against the 2022 Overarching CMP. The Overarching CMP examines and considers future adaptive reuse opportunities for buildings within the ERW and guides the holistic management of the ERW precinct.

The LES has been reassessed within this report (Chapter 6) to ensure it includes the most up-to-date information pertaining to the Assessment of Significance against the NSW Heritage Manual criteria (Section 6.1), the Statement of Significance (Section 6.2) and fabric Gradings of Significance (Section 6.3). Likewise, the physical description of the LES provided by this report (Section 4.1) has also been updated from the 2003 CMP to ensure it reflects the building's current condition as well as any alterations after 2003.

Table 8.2 presents an assessment of the proposed works against the policies described in the Overarching CMP prepared by Otto Cserhalmi + Partners for the subject site (with minor updates by Curio in 2022).

Following the rezoning process, the draft South Eveleigh Precinct CMP will be updated to include relevant policies and guidelines for the LES that are consistent with its significance and any rezoning approval issued. In addition, the South Eveleigh CMP, along with the 2022 Overarching CMP, will be used to inform any proposed future redevelopment of the site, following the finalisation of the rezoning process.

Table 8.2: Assessment of the Rezoning Proposal against the Overarching CMP Policies.

Overarching Policy	Policy Description	Assessment/Comment
Overarching Policy 1. Retention of Significance	<i>The Statement of Significance for the Eveleigh Railway Workshops site contained in this Overarching CMP (...) should be adopted as the basis for its heritage management. All decisions should consider and seek to retain the values identified in this Statement of Significance.</i>	<p>Met. The Statement of Significance for the ERW provided by the 2022 Overarching CMP has been adopted as the basis for its heritage management in this SoHI.</p> <p>As the building and its immediate surroundings are currently inaccessible to the general public, the proposal to adaptively reuse the LES to facilitate contemporary commercial use will ensure it remains relevant, activated and fully accessible.</p> <p>By proposing a sympathetic and primarily reversible insertion within the building, the heritage impacts can be mitigated/offset by the activation of the immediately adjacent space and the implementation of meaningful and engaging heritage interpretation initiatives.</p> <p>Therefore, the Rezoning Proposal is considered to be appropriate and will not detract from the heritage values identified in the Statement of Significance.</p>
Overarching Policy 2. State Heritage Register Boundary	<p><i>An appropriate State Heritage Register boundary for the Eveleigh Railway Workshops site must be retained to ensure that the heritage significance of the whole complex is considered when planning for future changes.</i></p> <p><i>Any future proposal to amend the State Heritage Register boundary should ensure that elements, spaces and values of Moderate or greater significance are incorporated, as identified in:</i></p> <ul style="list-style-type: none"> - <i>the Statement of Significance for Eveleigh Railway Workshops (...);</i> - <i>Gradings of Significance (...); and</i> - <i>detailed assessments undertaken in reports for individual precincts of the site.</i> 	<p>Met. The proposal does not include any alterations to the Eveleigh Railway Workshops boundary defined by the SHR.</p>
Overarching Policy 3. SHR Listing	<i>The various SHR listings for the Eveleigh Railway Workshops should be updated to reflect revised site context and recent findings, including as documented in reports for individual precincts of the site, to facilitate a holistic approach to management.</i>	<p>Met. The Statement of Significance, Assessment of Significance against the NSW Criteria, and the Grading of Significance for the LES presented in this report (Chapter 6) are based on the original CMP for the LES (2003) and have been revised and updated to take into account the changes that have occurred since 2003, including primarily, its change of use.</p>

Overarching Policy	Policy Description	Assessment/Comment
Overarching Policy 4. Further Research	<i>Further research should be undertaken in the future to provide greater understanding of the values that contribute to the significance of the Eveleigh Railway Workshops, with consideration for the identification of potential national values.</i>	Note: Extensive additional research undertaken with respect to the ERW since 2015 to date for South Eveleigh, Redfern Station and North Eveleigh has not revealed any additional information or research that would alter or impact the existing established levels of significance for the site.
Overarching Policy 5. Heritage Management of the Operational Rail Precinct	<i>A CMP should be developed for the Operational Rail Precinct, including the former Macdonaldtown Gas Works and Stabling Yards, prior to any major proposals which could impact on heritage significance. This should be developed in accordance with this Overarching CMP.</i>	Not applicable.
Overarching Policy 6. Early Advice	<i>Ensure that appropriate heritage professionals are involved at an early stage for major works proposed to any part of the Eveleigh Railway Workshops site, including to address relevant heritage opportunities and constraints relating to proposals, prior to design work commencing.</i>	Met. TAHE has commissioned Curio Projects to provide specialist heritage input and advice to the conceptual design and prepare all relevant heritage documentation to assist with the Development Application for the Rezoning Proposal.
Overarching Policy 7. Collaboration	<i>New design and conservation teams should work together from initial stages through design concepts, design development and construction.</i>	Met. The design team and Curio (heritage specialists, including architectural, archaeological, buildability and interpretation specialists) have been working collaboratively together to ensure the original, significant fabric and heritage values of the building are retained, highlighted, and reinterpreted in a sensitive and careful manner.
Overarching Policy 8. Authority Consultation	<i>For major development proposals and for overall site masterplans for any part of the Eveleigh Railway Workshops site, incorporate appropriate consultation with authorities during the concept design and design development stages so that any relevant stakeholders' concerns can be addressed.</i>	Met. The Rezoning Proposal has been discussed and reviewed by several key stakeholders to date, including the City of Sydney Council, Heritage NSW, Transport for NSW, Aboriginal stakeholders, and the local community. Comments, suggestions and concerns have been discussed and addressed to ensure the proposal is respectful and appropriate. Ongoing consultation with continue throughout the next phases of the project.
Overarching Policy 9. Ownership	<i>If any part of the Eveleigh Railway Workshops site, including the identified precincts and any smaller areas within these, passes from public ownership or its use changes, appropriate heritage covenants and/or a heritage conservation agreement should be placed on land titles where there are identified heritage assets in perpetuity to ensure the adequate maintenance of the heritage assets and the provision of public access where appropriate.</i>	Noted.

Overarching Policy	Policy Description	Assessment/Comment
Overarching Policy 10. Obligations and Opportunities of Ownership	<i>The obligations of and opportunities for future owners in relation to heritage conservation should be clearly defined. Relevant heritage management documents, including Conservation Management Plans, Heritage Assessments, and this Overarching CMP, should be included affecting any part of the Eveleigh Railway Workshops site.</i>	Met. The Rezoning Proposal and its conceptual design have been developed to be fully aligned with the obligations, opportunities, and constraints identified in the 2022 Overarching CMP and its addendum prepared by Curio ⁷⁷ , as well as other relevant heritage documentation, such as the Burra Charter and the NSW Heritage Manual.
Overarching Policy 11. Management Framework	<i>The Overarching CMP and individual precinct CMPs must be adopted by land owners and managers as the basis for the effective management of the significant heritage values of the Eveleigh Railway Workshops site. Any change of ownership must be planned and well managed to ensure the item's significance is conserved. An effective management structure must be prepared to include identification of management roles and responsibilities, the role of specialist consultants, and identify how future site users will be made fully aware of their responsibilities to ensure that the principles, policies and guidelines are integrated fully into the ongoing conservation and management of the place.</i>	Met. All relevant heritage documentation, including the 2022 Overarching CMP, has been utilised as a basis for the development of the proposal. The potential change of ownership is being carefully studied by TAHE and a team of specialist consultants, including Curio, to ensure the site is appropriately managed and conserved in the future. The Rezoning Proposal is a result of this careful approach and it has been developed in consultation with key stakeholders and statutory bodies to ensure the intended design and future use of the site are compatible with the principles, policies and guidelines identified in the 2022 Overarching CMP and other relevant heritage documentation, such as the Burra Charter and the NSW Heritage Manual.
Overarching Policy 12. Management of Precincts	<i>The management structure implemented for areas under separate ownership within the Eveleigh Railway Workshops site should integrate conservation work, new development and ongoing maintenance and management of their respective sites with demonstrated consideration for their context as part of the Eveleigh Railway Workshops complex where applicable.</i>	Met. To ensure the management structure for the LES site is implemented cohesively and consistently with the other buildings within the ERW, especially with the adjacent Locomotive Workshops, the South Eveleigh Precinct CMP (currently being developed by Curio) has been updated to include the LES and its management policies and guidelines.
Overarching Policy 13. Coordination	<i>Establish measures to coordinate management objectives between different owners and managers of any part of the Eveleigh Railway Workshops site in the context of the collective heritage conservation and management of the whole site. Attendance at regular meetings between the various representatives should be implemented at minimum.</i>	Noted.

⁷⁷ Curio Projects, 2022. *Overarching Opportunities & Constraints - ERW*. Prepared for Transport for NSW.

Overarching Policy	Policy Description	Assessment/Comment
Overarching Policy 14. Eveleigh Railway Workshops	<i>Within the individual precincts of the Eveleigh Railway Workshops, promote the idea that these are part of the wider Eveleigh Railway Workshops complex via coordinated management initiatives relating to heritage conservation, adaptation, signage and wayfinding, interpretation, urban design elements, landscape, views and vistas and physical site links. Initiatives should be developed in collaboration with managers and owners of the individual precincts to ensure that a consistent approach is taken.</i>	Met. One of the main objectives of the Rezoning Proposal is to integrate the LES into the broader South Eveleigh Precinct with the upgrade of the LES being the final building in the precinct to be adaptively reused to support the vision for a new tech ecosystem and to provide an activated precinct for locals, day to day workers and cultural heritage tourism visitors alike. The proposed upgrade of Locomotive Street has been designed to enable the street to function as a connective element between the LES and the South Eveleigh Precinct, including the addition of interpretation and public art products to ensure visitors are drawn to the LES as part of the whole ERW experience.
Overarching Policy 15. Best Conservation Practice	<i>Ensure that conservation, maintenance and new work within the Eveleigh Railway Workshops site is undertaken in accordance with current conservation and planning methodologies.</i>	Met. Although the scope of the Rezoning Proposal does not yet include a detailed design of the intended works, the proposal has been developed in accordance with best practice heritage management principles and guidelines of the relevant heritage documentation, including the Burra Charter and the NSW Heritage Manual.
Overarching Policy 16. Adoption of Overarching CMP and Precinct Specific Heritage Management Documents	<i>The conservation policies set out in this Overarching CMP, and related precinct documents, must be adopted by owners, managers and site users as a guide to future conservation and development of the place.</i>	Met. The conservation policies set out in the 2022 Overarching CMP have been adopted as a guide to the heritage management of the LES and the design development of the Rezoning Proposal. In addition, this assessment of the proposal against the CMP policies has been written to ensure an appropriate redevelopment of the site and the future conservation of its heritage values.
Overarching Policy 17. Review of Conservation Management Plans	<i>Conservation Management Plans, including the individual precinct Conservation Management Plans, must be reviewed and updated within five years to remain relevant to ongoing change and use of the place, and statutory compliance. A review of these documents should also be undertaken after significant changes to the property.</i>	Met. The CMP prepared for the LES building was prepared in 2003 and, since then, the precinct formerly identified as ATP has been redeveloped (2015-present) and is now known as the South Eveleigh Precinct. CMPs are generally required to be updated every 5-10 years, yet the 2003 CMP has not been revisited in nearly 20 years. The majority of the aspirations, intended partnerships (with the Powerhouse Museum, for example), objectives and future visioning which underpinned the 2003 CMP have never been realised for the LES. Therefore, a re-examination of the building and its adaptability for change and sustainable future use is being closely considered as part of the ongoing future management of all heritage assets within the South Eveleigh Precinct, as part of the redrafting of the South Eveleigh Precinct CMP (currently being prepared by Curio).

Overarching Policy	Policy Description	Assessment/Comment
Overarching Policy 18. Recording of Maintenance and Change	<i>Undertake detailed recording of the site components, spaces, fabric and features before, during and after any works including archival photographic records and measured drawings in accordance with NSW Heritage Division guidelines.</i>	<p>Not applicable at this stage. However, prior to any construction works associated with the next phase of the project commencing, a detailed archival recording of the LES building and its site components, spaces, fabric and features should be undertaken.</p> <p>The works during the construction stage and after completion should also be included in the archival photographic recording document as well as the measured drawings in accordance with the NSW Heritage Division guidelines.</p>
Overarching Policy 19.1. Moveable Heritage	<p><i>The management of items of moveable heritage within the Eveleigh Railway Workshops should be coordinated across the various site precincts and items must be managed in accordance with:</i></p> <ul style="list-style-type: none"> <i>- Moveable Heritage Principles, NSW Heritage Office (now Heritage NSW, Department of Premier & Cabinet) and the Ministry of Arts, 2000;</i> <i>- Object in their Place, NSW Heritage Office (now Heritage NSW, Department of Premier & Cabinet) 1999; and</i> <i>- Policies and recommendations for items of moveable heritage contained within the individual precinct-specific CMPs and heritage assessment.</i> 	<p>Not applicable at this stage. Transport Heritage has been relocating heritage trains and carriages across NSW, including Eveleigh, to the new facility at Chullora since 2018. The new space offers secure undercover storage for the heritage fleet, ensuring the moveable items listed on the RailCorp S170 Register are better protected and preserved.</p> <p>In the next phase of work, a revised and updated survey of all the moveable heritage items and associated collections within the LES will be undertaken to ensure all heritage items are managed in accordance with the relevant legislation.</p>
Overarching Policy 19.2. Moveable Heritage Survey & Registers	<i>Current surveys and production and/or update of moveable heritage collection registers should be undertaken for all precincts with a relevant moveable heritage collection, to ensure the documentation of moveable heritage items across the entire Eveleigh Railway Workshops site are current and able to be managed accordingly. The relevant SHR listings should be updated with current details regarding the nature and location of moveable heritage collections across all precincts.</i>	Not applicable at this stage. However, sub-section 4.1.1 and 9.4 of this SoHI report recommend that an overall revised and updated survey of all the moveable heritage items and associated collections within the LES be undertaken as part of the next phase of works for the project.
Overarching Policy 20. Archaeology	<p><i>The archaeological (including Aboriginal and/or European/historical) potential of parts of the Eveleigh Railway Workshops site should be managed and conserved in accordance with:</i></p> <ul style="list-style-type: none"> <i>- the archaeological provisions of the NSW Heritage Act 1977;</i> <i>- National Parks & Wildlife Act 1974; and</i> <i>- policies and recommendations for archaeology contained within the individual precinct-specific CMPs and heritage assessment.</i> 	Met. Curio undertook relevant heritage assessments in accordance with the NSW Heritage Act 1977 and National Parks & Wildlife Act 1974 to assess historical and Aboriginal archaeological potential. Policies and recommendations for archaeology, such as the 2022 Overarching CMP and other relevant heritage documentation were also drawn upon to guide Curio's heritage assessment process.

Overarching Policy	Policy Description	Assessment/Comment
Overarching Policy 21. Future Use	<p><i>Any future use of the Eveleigh Railway Workshops site or part thereof must respect the cultural significance of the place and its association as part of a larger railway precinct. Appropriate future uses should be determined by site owners and managers with consideration for the following criteria:</i></p> <ul style="list-style-type: none"> - sympathetic to the significance of the overall site and the configuration of existing buildings; - sympathetic to the industrial character of the place; - sympathetic to established uses within the locality; - utilise traditional entry points and circulation routes as a priority over new circulation routes and entry points; - do not result in unacceptable levels of wear and tear on extant fabric to be retained. 	<p>Met. The LES site is currently inaccessible to the general public, thus underutilised and at the risk of becoming derelict in the future. Therefore, the Rezoning Proposal intends to adaptively reuse the building for commercial and retail purposes, in line with the current uses of the buildings within the South Eveleigh Precinct, including the adjacent Locomotive Workshop. This change in use will allow people to utilise the building on a daily basis, ensuring it remains relevant, activated and fully accessible.</p> <p>As the LES original, primary circulation routes were designed to prioritise trains and carriages, they are closely connected with and located near the railway tracks, which could be unsafe and problematic to pedestrians. As a result, the Rezoning Proposal intends to relocate the main entry points to the southern facade of the LES, facing Locomotive Street, which is consistent with the Locomotive Workshop's main access points. The proposed location reinforces the intended seamless connectivity between the LES and the wider South Eveleigh Precinct as it establishes Locomotive Street as the primary street providing access to the buildings.</p> <p>In addition, the western and eastern arched doorways will be retained to ensure the original, primary circulation routes are still represented and can be fully understood by the general public as the main gateways to the building in the past.</p> <p>Further, the proposal has been developed to be a respectful and primarily reversible insertion within the building and to retain and highlight the industrial character of the ERW precinct through the use of sympathetic materials, colours, and finishes.</p> <p>Overall, the proposed Rezoning Proposal has been designed to ensure the future use of the site is appropriate to and respectful of the cultural significance of the LES and the wider ERW precinct.</p>
Overarching Policy 22. Future Work	<p><i>The site should be considered holistically when planning future works, including open space areas, buildings, extant structures and site elements. Future work should be planned with demonstrated</i></p>	<p>Met. The Rezoning Proposal has been developed as part of the extended vision for the South Eveleigh precinct as the cultural significance of the LES is deeply connected to the wider ERW.</p>

Overarching Policy	Policy Description	Assessment/Comment
	<i>consideration for the significant heritage qualities of the whole Eveleigh Railway Workshops site, in addition to that of its individual components and the surrounding heritage conservation areas.</i>	As a result, the design choices, future use, public domain improvements, interpretation and public art initiatives, among others, are all consistent with the work developed throughout the ERW in addition to being expanded upon to ensure the unique and specific history of the LES are also represented.
Overarching Policy 23. Symbiotic Relationships	<i>Maintain a symbiotic relationship between all parts of the Eveleigh Railway Workshops site when introducing new elements. For example, the design of boundary fencing or other elements situated on the site boundaries, planning layouts, signage, materials and plantings, should reflect that the individual precincts are part of a larger site.</i>	Met. Although the scope of the Rezoning Proposal does not yet include a detailed design, the proposal has been developed to be consistent with the South Eveleigh Precinct redevelopment. As a result, the intended look and feel include similar design principles to the Locomotive Workshop and wider precinct, clearly reflected in the scale, bulk, form, materiality and landscape solutions for the subject site.
Overarching Policy 24. Maintain and Develop Public Access	<i>Management objectives for future use and development should: - encourage uses and/or opportunities to facilitate public visitation and interpretation of the whole Eveleigh Railway Workshops site and its elements, where viable, within the limits of security required for operation of the site and physical security; and - maintain and develop visual and pedestrian connections between the individual precincts of the Eveleigh Railway Workshops site based on the major historical vistas, access points and cultural significance of the place.</i>	Met. The Rezoning Proposal intends to adaptively reuse the site for commercial and retail purposes to service the community and to allow the general public to access the building, which has been unoccupied since 2017. The extension and upgrade of Locomotive Street will contribute to the activation of the site as it will improve the visual and pedestrian connections between the LES and the South Eveleigh Precinct. The addition of interpretation and public art initiatives will reinforce these connections, attracting and encouraging visitors to interact with and appreciate the heritage fabric and cultural significance of the LES and the wider ERW precinct.
Overarching Policy 25. Public Domain	<i>Public domain areas should be consistent across the whole site i.e. they should be designed and managed with demonstrated consideration for their interrelationship with other public domain areas throughout the Eveleigh Railway Workshops site to facilitate coherent presentation and linkages throughout the wider site. Future design of the public domain areas should be sympathetic and respond to the industrial character of the site.</i>	Met. The proposed incorporation of the LES as part of the broader South Eveleigh Precinct aims to establish a seamless connection between them, creating a cohesive experience for the general public. As a result, the western end of Locomotive Street is proposed to be upgraded, including landscape solutions and interpretation and public art initiatives consistent with the wider precinct, to improve the overall user experience and activate the public domain.
Overarching Policy 26. Funding Public Domain Works	<i>Obtain and allocate necessary funding for public domain works, for example via developer contributions or other grant funding to be identified.</i>	Met. The redevelopment and upgrade of the public domain works are part of the Rezoning Proposal and will be funded by the future developer.
Overarching Policy 27. Coordinated Approach to Interpretation	<i>The individual precincts within the site should be interpreted as part of a major railway workshop facility and the approach should be consistent in terms of form and scale across the entire Eveleigh</i>	Met. The redevelopment of the LES has provided the perfect opportunity to expand and complete the layered interpretation and integrated cultural tourism already established throughout the South Eveleigh precinct. This site

Overarching Policy	Policy Description	Assessment/Comment
	<i>Railway Workshops site. Interpretation of the Eveleigh Railway Workshops site should interpret the historic use and layout of the site and also its links to the surrounding context.</i>	is the final building in the former ELW (southern portion of ERW) to be redeveloped and offers the chance to continue the interpretive experience at key locations, encouraging visitors to explore the entire precinct upon arrival from one of the many entry points, including the immersive approaches from Redfern Station, and as part of the already established Eora Journey.
Overarching Policy 28. Associations	<i>The coordinated approach to interpretation for the Eveleigh Railway Workshops site should convey a coherent story about how the place operated, in particular addressing the functional layout of the workshops, yards, sheds and general stores etc. that enabled the various divisions to communicate, in the manner of a production line, yet operate without interference. The general planning and layout of rails contributes to this interpretation.</i>	Met. As the LES sits within the South Eveleigh precinct and was an integral part of the working life at the Eveleigh Railway Workshop, a selection of themes from the South Eveleigh HIP have been reproduced, which will ensure the relevant history that links the precinct is aligned. However, the themes for this HIP have been expanded upon to ensure unique and specific stories of the LES are told.
Overarching Policy 29. – Review of Interpretation	<i>The interpretative media and strategy that is implemented in the future should be reviewed at maximum five year intervals as part of the management and maintenance of the site and its individual precincts, including to check for condition/vandalism, upgrading of content and location etc. The need to establish a funding mechanism over time to provide for coordinated ongoing maintenance and upgrading of interpretive media should be considered by all site managers/owners in a collaborative manner.</i>	Noted.
Overarching Policy 30. Funding Interpretation	<i>Obtain and allocate necessary funding for interpretation, for example via developer contributions or other grant funding to be identified.</i>	Met. The design development, fabrication, and installation of interpretation products within the LES site will be part of the redevelopment proposal and will be funded by the future developer. The products will be further developed and detailed as part of the Stage 2 HIP which will be aligned with the Stage 1 HIP prepared by Curio and submitted as part of the Rezoning Proposal.
Overarching Policy 31. Appropriate Expertise and Skills	<i>Skilled conservation professionals, including but not limited to conservation architects, archaeologists, builders and engineers, should be engaged to advise on, document and/or implement conservation and upgrading work and future development proposals for heritage assets on any part of the Eveleigh Railway Workshops site.</i>	Met. TAHE has commissioned highly experienced, specialist consultants to develop the Rezoning Proposal. Curio has been involved in the South Eveleigh redevelopment as the principal heritage advisor since 2015 and is extremely familiar with the heritage significance and values of the ERW precinct. Curio has been providing key specialist heritage and archaeological advice throughout the pre and post-SSD stages and has developed physical heritage

Overarching Policy	Policy Description	Assessment/Comment
		<p>interpretative installations, exhibitions, cultural heritage and digital products displayed throughout the South Eveleigh precinct.</p> <p>Along with Curio, TAHE has commissioned other highly-experienced consultants that have been involved with the South Eveleigh redevelopment, including FJMT and Ethos Urban, to ensure the future development of the LES site remains consistent with the wider precinct.</p>
Overarching Policy 32. - Community Participation	<i>Ensure that adaptive reuse, interpretation and new development on any part of the Eveleigh Railway Workshops site includes meaningful community consultation. Provide opportunities to involve railway interest groups and other interested community groups and individuals in the development of proposals for the site as appropriate, and as identified in the individual conservation planning documents for each site.</i>	Met. Consultation meetings to present and discuss the LES Rezoning Proposal have been carried out with Heritage NSW, the City of Sydney Council, as well as other key stakeholders including but not limited to former workers, Aboriginal stakeholders, volunteers, local community, heritage professionals, and railway associations.

8.9. Summary of Heritage Impact

Table 8.3 presents a preliminary assessment of the potential heritage impacts associated with the Rezoning Proposal on the heritage fabric of the LES building. Detailed design will take place during a future stage of the planning and redevelopment process and will require an accompanying additional heritage impact assessment (subsequent and additional to this report), to specifically respond to and assess its impacts, once developed.

Table 8.3: Summary of the Heritage Impact associated with the Rezoning Proposal.

Proposed Alterations	Summary of Heritage Impact
Change of use	<p>Moderate impact.</p> <p>The LES has been underutilised and inaccessible to the public for the past 5 years, which could lead to future neglect and decay. The proposed adaptive reuse creates an opportunity for the building to remain relevant and meet contemporary and future users' needs. The proposed change to commercial use, although a shift from its former industrial self, will ensure the building is relevant, highly activated and occupied once again. The proposal offers the opportunity to return the LES to being a busy, industrious workplace that will be utilised and appreciated on a daily basis by workers and visitors, alike.</p> <p>In addition, the proposal is consistent with the South Eveleigh vision for a new tech ecosystem that will increase jobs and employment and strengthen NSW's position and global recognition in the innovation and technology sector. The proposal is also aligned with the SEPP strategy and guidelines for a Business Park as it will enable a mix of uses/functions in one place to meet the day-to-day demands of workers in the immediate area and provide significant employment opportunities to the community⁷⁸.</p> <p>With the LES redevelopment also proposing meaningful, engaging, and carefully considered heritage interpretation initiatives, as well as retention and adaptive re-use of the heritage building and moveable items, the project seeks to renew life in this area of the site while increasing the longevity of the building and ensuring the facilitation of greater understanding of the ERW complex, its heritage values and important stories for future site users and the local community.</p>
Internal reconfiguration of the LES, including the addition of two new levels within the building to accommodate commercial and retail tenancies.	<p>Moderate impact.</p> <p>The repurposing of the LES from a decommissioned, inaccessible and unsafe industrial space into a lively and busy technical workplace, is sympathetic to the original use of the space and commensurate with the historical use of the building albeit a shift in the "technical" work undertaken.</p> <p>The proposed internal reconfiguration has been intentionally concept designed to include lightweight, primarily reversible mezzanine-style floors suitably distanced from the magnificent external walls of the building. This lightweight approach and reversible nature anticipate that in the future,</p>

⁷⁸ NSW Department of Planning and Environment, 2021. *Employment Lands: 2021 report on business parks*

Proposed Alterations	Summary of Heritage Impact
	<p>new site users may again reinvent the use of the space and require the complete removal of the proposed commercial layout and services.</p> <p>The development proposes improved access to the space and has been designed to allow views to the remarkable industrial architecture of the building. The new design will create an active work environment that will recover a degree of the high-density use of the space. The reintroduction of workers into the building is commensurate with the intangible heritage significance of the space as a busy and industrious workplace, whilst retaining views and access to the tangible heritage significance of the building's industrial built fabric and architecture.</p> <p>The proposal includes the insertion of two full-height atria to interrupt the solidity of the floorplate and provide samples of the original scale, form, and clear views of retained elements (e.g., cast-iron columns, beams, roof trusses, etc.). In addition, the two eastern bays, where the communal space will be located, will be kept intact and free from any insertions, allowing users to experience the original openness, scale, height and bulkiness of the LES. The area also creates opportunities to display items of the moveable collection as well as other interpretation initiatives. The future design will encourage open-plan fit-outs to minimise the construction of unnecessary partition walls and avoid the interruption of the open nature of the space.</p> <p>Further, the look and feel of the proposed design (subject to future detail) will be contemporary and industrial to ensure a respectful and sympathetic interaction between modern and heritage fabrics. Also, the significant physical heritage fabric of the LES building will be required to undergo conservation works throughout the proposed redevelopment. To minimise the heritage impact, the proposal includes the insertion of two full-height atria to interrupt the solidity of the floorplate and provide samples of the original scale, form, and clear views of retained elements (e.g. cast-iron columns, beams, roof trusses, etc.).</p>
<p>Alteration to heritage fabric (subject to future detail):</p> <ul style="list-style-type: none"> ▪ In Ground Pits ▪ Arch Windows ▪ Arch Doors ▪ Overhead Gantry Cranes ▪ Roof / Skylights 	<p>Minor to nil impact</p> <p>The impact of the intended alterations has been preliminarily assessed only, as the Rezoning proposal does not include any detailed design. The impact of such alteration varies from minor to nil as per the summarised assessment below:</p> <ul style="list-style-type: none"> • The original in-ground pits are highly significant elements due to their importance during the locomotive overhaul process. The pits present the opportunity of being reinterpreted and could be converted into unique displays to showcase salvaged items or be kept as exemplars of the work conditions at the time, in particular the ones located in the communal space. • The arch windows will be retained, and a condition assessment report of the existing original panes will need to be prepared prior to any future SSDAs and DAs for the site, in order to ensure that the character of the original windows is retained as much as possible and to meet current standards (e.g., natural light). It is recommended that only damaged glazing be replaced with clear and transparent glass. The intact frosted panels should be retained. The contrast between the two glasses (modern glazing and original) will create a clear distinction

Proposed Alterations	Summary of Heritage Impact
	<p>between historical and contemporary fabrics, similar to the arched windows on the Locomotive Workshop building, having an overall positive visual and physical impact on the heritage values of the LES.</p> <ul style="list-style-type: none"> • All arched doors on the eastern and western facades have been replaced, deteriorated or had temporary repair work to keep them operational. Therefore, the proposal's intention will not have an adverse visual or physical heritage impact on the modern doors as they are not part of the original fabric of the building. It is recommended that the only original door left (Road 6 – east end) is either kept in place in an open position or reused as part of the Heritage Interpretation Strategy for the site. • Due to the proposed subdivision of the internal space into three levels, the 7 existing overhead gantry cranes will be relocated from their current positions to meet head clearance requirements and in order to maximise the continuity of floorspace. Although subject to future detail, it can be anticipated that the cranes will be relocated to the full height atria and to the communal area at the eastern end of the building to not only meet the head clearance requirements but also to place them within areas that retain original characteristics of the heritage building (e.g. scale, openness, height, etc.) as well as house communal activities (e.g. lobbies, events, circulation). Therefore, the cranes will remain visually accessible to the public on a daily basis, having a neutral impact on the heritage values of the building. • The existing skylights have been introduced in 1997 and do not form part of the original fabric of the LES, not having any physical impact on the heritage values of the site. In addition, the proposal will have a positive visual impact on the overall significance of the LES as it will restore the original longitudinal configuration of the skylights.
Extension of Locomotive Street	<p>Moderate impact.</p> <p>The extension of Locomotive Street will have a positive moderate impact on the heritage significance of the LES as it will improve the public realm conditions and attract users to explore the full extent of the building, enhancing the visual connectivity between the subject site and the remaining South Eveleigh Precinct, in particular the Locomotive Workshop. Likewise, the creation of a new gateway at the western end of the precinct will help to activate and define it as a significant portal to South Eveleigh.</p>
Construction of a car park zone along Locomotive Street	<p>Minor impact.</p> <p>The proposed western extension of Locomotive Street will enable the creation of a small car parking area on the northern side of the street. The car parking will attend to the future demand for parking arising from the redevelopment of the subject site and its chosen location is consistent with the existing parking zone on the southern side of Locomotive Street.</p> <p>The location of the car parking will be to the west of the eastern public domain seating/public art zone to not interrupt the pedestrian experience along the street. The car park will be to the east of the new annexe (loading dock) and will help to soften the visual impact of the new volume,</p>

Proposed Alterations	Summary of Heritage Impact
	being assessed as an overall appropriate solution with a minor impact on the heritage significance of the site.
Landscaping and public domain improvements	<p>Moderate impact.</p> <p>Although the Rezoning Proposal does not include specific details about landscaping design, it is anticipated that alterations to the public domain will be done in accordance with best practice heritage and design principles that will ensure the alterations are sympathetic and have an overall positive visual impact on the significance of the heritage site.</p> <p>The improvements will be located along Locomotive Street and between the LES and the Locomotive Workshops in order to create a seamless connection with the public domain improvements throughout the South Eveleigh Precinct as well as to attract users and visitors to the site to explore the extended heritage setting.</p>
Construction of two annexes at the western end of the site to accommodate the Loading Dock and the Plant & Equipment Room	<p>Moderate impact.</p> <p>The new volume of the proposed Loading Dock will have a moderate visual impact on the heritage fabric and significance of the site as it will be located adjacent to the southern facade of the building (western end).</p> <p>The location of this new annexe has carefully considered the significant view lines between the LES and the wider precinct, in particular the Locomotive Workshop, and therefore has been positioned at the western end of the site, to the west of the proposed car park and public domain area destined for seating, interpretation products and public art, ensuring no views to those areas are blocked from Locomotive Street.</p> <p>Further, the proposed annexe that will accommodate the Plant & Equipment Room will be fully detached from the LES building in order to minimise the visual impact on the legibility of the southern façade. In addition, part of the plant & equipment will be installed on the roof to ensure the annexe's size is as minimal as possible.</p> <p>The modern external walls of the two annexes create the opportunity for future interpretation solutions and sympathetic materiality will be developed to help mitigate their impact while proposing an innovative and engaging treatment to their façades.</p>
Installation of part of the plant and equipment on the roof	<p>Minor impact.</p> <p>The installation of plant and equipment on the roof will interact with modern fabric only (i.e. roof sheeting) and therefore will not have any impact on the physical original fabric of the LES.</p> <p>Visually, the plant and equipment will be predominantly concealed from any significant view lines, as the proposal includes a set of conditions to allow for the items to be installed on the roof, including conditioning to not be higher than the roof apex, not be visible from the ground floor or have difficult access for maintenance purposes. Therefore, the proposed works are considered an appropriate solution and will help to reduce the size of the annexe on the ground floor.</p>

9. Conclusions and Recommendations

9. Conclusions and Recommendations

This Statement of Heritage Impact has assessed the nature of the Rezoning Proposal in the context of the heritage significance and context of the Large Erecting Shop and the wider Eveleigh Railway Workshops in order to provide overarching recommendations to guide future redevelopment within the subject site, with specific consideration given to bulk, height, and scale of the development against the existing significant elements of the site.

The conclusions and recommendations of this report, as summarised below, have been informed by a comprehensive understanding of the setting, context, streetscape, visual and physical characteristics of the subject site and surrounding heritage items.

9.1. Heritage Building and Fabric

Recommendations and preliminary principles regarding the concept design elements of the Rezoning Proposal that will require further development and careful consideration through the detailed design phase in order to reduce or avoid the impact to heritage fabric and significance include the following:

- The proposed redevelopment should be recessive, deferential, and respectful to the original fabric, form, and industrial character of the LES.
- External additions/annexes to the subject site should be avoided where possible. If necessary (e.g. loading dock, plant and equipment annexe), their design, final location and facade treatment should be developed in consultation with an experienced and qualified heritage specialist to minimise impacts where possible. The final design should ensure the significant view lines to the LES remain unobstructed and sympathetic materiality should be selected to ensure a respectful interaction between modern and heritage fabrics. Interpretive solutions to re-purpose and include the new additions/annexes as part of the interpretation strategy for the precinct should be explored as opportunities to mitigate their visual impact on the overall significance of the site.
- Treatment of the remnant rail tracks, in ground pits, overhead gantry cranes, and other original elements within the site to ensure that they are interpreted and appropriately managed within the adaptive reuse of the site.
- Details and design of future adaptive re-use of the LES, including the proposal for management, conservation, and treatment of associated moveable heritage collection contained within—including additional surveys and structural assessment to inform future design constraints and opportunities with respect to form and condition.

9.2. Future and Detailed Design

Recommendations and preliminary principles regarding future detailed design for the LES include the following:

- Detailed design should be developed in close consultation with an appropriately experienced and qualified heritage consultant to provide heritage advice and input throughout the detailed design process, especially with respect to the key additions, changes, and modifications proposed to elements that have potential to have an adverse physical and/or visual impact to heritage items and significance, that will require minimisation and mitigation through careful, sensitive, and sympathetic design solutions.

- Adaptive reuse of the heritage building (specifics to be detailed through future detailed design phases) should be undertaken in a sympathetic, reversible, and sensitive manner that ensures the original use of the building continues to be understood and communicated to audiences through retention of as much of a building's original fabric and internal layout as possible, supported by complementary heritage interpretation initiatives where appropriate. Where modern additions must be incorporated into the significant heritage item, the design of these elements should adhere to the Burra charter principles of 'as much as necessary, as little as possible', and should complement the original structure and form while also using contrasting but sympathetic materials to ensure that modern additions can be readily distinguished from the original form and fabric.
- Detailed design of the Rezoning Proposal elements – presumably to be undertaken through future stages of the planning and redevelopment process – will require the preparation of future heritage impact assessments (subsequent and additional to this SoHI) to specifically respond to and assess the impacts of the detailed design, once developed.
- New additional volume to the southern facade should consider the visual connectivity between the LES and Locomotive Workshop, as well as the significant view lines of the LES along Locomotive Street, Rowley Lane and when approaching the building from the South East Village Square, to ensure that the key physical and visual attributes of the heritage items are respected, prominent, and not obstructed.
- New elements should adopt appropriate materiality and colour palettes commensurate with the surrounding heritage fabric and context of the South Eveleigh Precinct.
- Investigate opportunities for reuse and recycling of materiality and fabric of little significance (e.g., rolling stock parts, memorabilia, etc.).

9.3. Landscaping & Public Domain

The detailed design of the landscaping and public domain should:

- Ensure a seamless connection between the LES and the South Eveleigh Precinct, proposing cohesive elements throughout Locomotive Street and in-between the LES and Locomotive Workshop to extend the overall look and feel (e.g. materiality, furniture, interpretation, etc.) to the subject site.
- Avoid over-landscaping, especially along the southern and eastern facades, as it would risk a reduction of the legibility of the significant fabric of the LES.
- Develop appropriate lighting solutions to improve the visibility of the façade during daytime and nighttime, highlighting the original and interpretive elements, and activating the public domain.
- Include inclusive, durable, safe, and inviting elements to help active the precinct by attracting visitors to the site. This includes the development of innovative and engaging interpretation and public art products that will help to retell significant stories related to the subject site and are consistent with the interpretation strategy developed for the South Eveleigh precinct.

- Any proposed car parking areas should not overtake the public domain area along Locomotive Street or interrupt the pedestrian route towards the LES building or any interpretive/public art element.

9.4. Moveable Heritage

The scope of the Rezoning Proposal does not include specific details about the proposed treatment, management, and incorporation of the significant moveable heritage collection associated with the LES. It will be required to be addressed in future detailed design phases and is also addressed as part of the Heritage Interpretation Plan (Curio, 2022).

Recommendations regarding the future management of the moveable heritage collection include:

- Transference and placement of rolling stock and heritage moveable items require careful consideration prior to being undertaken by TAHE, in collaboration with an experienced and qualified heritage consultant. The heritage consultant should manage the process, taking into consideration the strategies and guidelines provided by the Heritage Interpretation Plan (Curio, 2022) prepared for the site.
- Undertake a comprehensive moveable heritage survey and establish a current Moveable Collections Catalogue that records and catalogues all moveable heritage items across the LES, and updates relevant heritage listings with the details of this updated inventory. The inventory should form a sub-section of the broader South Eveleigh Moveable Heritage Catalogue being prepared by Curio and ICS and should be included as a sub-section of the Moveable Collections Management Plan for South Eveleigh (currently in progress).
- Include moveable heritage conservation and long-term storage into any detailed development planning for the site so that the immediate, medium and long-term costs associated with keeping and conserving this significant resource can be built into the project costs and ongoing asset maintenance costs.

9.5. Historical Archaeology

As the LES site is adjacent to South Eveleigh and both are elements of the same overarching site of the former ERW, a consistent approach will be applied to the management of archaeological resources. Therefore, it is proposed that a similar differentiation between 'relics' and 'works' (where relevant) be applied to the subject site (the context in which archaeological relics and works have been successfully managed at South Eveleigh previously), as while both relics and works may have the potential to be present, the way in which they are required to be managed may differ.

Historical archaeology within the LES study area should be managed in accordance with the archaeological potential and management framework outlined in Section 5 of this report, which should be further developed through a future Archaeological Research Design (ARD), specific to the impacts and final design of the precinct renewal development. It should also be managed in accordance with the archaeological provisions of the Draft South Eveleigh Precinct CMP.

Archaeological works carried out within the study area should be undertaken under a Section 60 Excavation Application under the NSW Heritage Act 1977, depending on the assessed significance of the potential archaeological deposit and location in question. The appropriate approvals pathway (i.e., a s60 Application, or a s60 Fast Track Application) would depend on the nature and extent of works proposed, and the corresponding impact to potential historical archaeological resources. This would be determined and confirmed during the preparation of the ARD.

Archaeological mitigation program developed through the ARD should be specific to the location and nature of ground penetrating works at the site, but may include:

- Targeted archaeological test excavation undertaken in the key areas identified as having moderate-high archaeological potential before construction works commence
- Unexpected finds protocol.
- Archaeological monitoring in areas with nil - low archaeological potential.
- Archival recording, surveying, photogrammetry, and 3D scanning should be performed to selected features for future heritage interpretation products if any archaeological features are uncovered during monitoring and testing.
- Significant archaeology uncovered will need to be incorporated into the heritage interpretation on site.

Landscaping may also be redesigned to avoid physical impacts on the archaeological resource by changing the nature of the intended planting, i.e., low impact shallow-rooted bushes rather than mature trees with the potential to create future damage through expansion of the root ball. Where it is not possible to avoid archaeology through redesign or use of previously disturbed areas, further archaeological assessment and investigation will be necessary.

Where possible, detailed design options and solutions should be investigated to reposition structural and built elements that have potential to impact archaeology, for example, minor relocation of support columns or options to span across areas identified as having high archaeological potential with two or more columns. A similar avoidance approach may also be employed with the installation of services, either avoiding areas of the archaeological resource and redesigning the route of the services or identifying existing service corridors, trenches etc and laying new services within areas that have previously been subject to excavation disturbance.

9.6. Heritage Interpretation

Heritage interpretation strategies as identified within the Stage 1 HIP will support the development of innovative interpretation elements within the LES site that will convey an inclusive account of the site's rich history, its function within the Eveleigh Railway Workshop as well as accounts from the local community, both past and present.

Heritage interpretation initiatives throughout the LES site should be engaging, authentic, relevant and appropriate to ensure that they not only continue to celebrate the significance of the subject site, but also the highly significant ongoing Aboriginal cultural heritage connection to the place.

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10. References

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