

Department of Planning, Housing and Infrastructure


[dpie.nsw.gov.au](http://dpie.nsw.gov.au)



# Central Precinct Design Guide

August 2025





# Acknowledgement of Country

The Department of Planning, Housing and Infrastructure acknowledges that it stands on Aboriginal land. We acknowledge the Traditional Custodians of the land and we show our respect for Elders past, present and emerging through thoughtful and collaborative approaches to our work, seeking to demonstrate our ongoing commitment to providing places in which Aboriginal people are included socially, culturally and economically.

---

Published by NSW Department of Planning, Housing and Infrastructure

[dpie.nsw.gov.au](https://dpie.nsw.gov.au)

Central Precinct Design Guide

First published: August 2025

## Copyright and disclaimer

© State of New South Wales through Department of Planning, Housing and Infrastructure 2025. Information contained in this publication is based on knowledge and understanding at the time of writing, August 2025, and is subject to change. For more information, please visit [dpie.nsw.gov.au/copyright](https://dpie.nsw.gov.au/copyright)

TMP-MC-R-SC-V1.2

# Contents

<b>Figures .....</b>	<b>7</b>
<b>Tables .....</b>	<b>8</b>
<b>1.0 Introduction.....</b>	<b>9</b>
1.1 Name of Guide.....	9
1.2 Citation .....	9
1.3 Commencement .....	9
1.4 Land to which this Design Guide applies.....	9
1.5 Purpose and status of this Design Guide .....	9
1.6 Relationship to planning instruments and documents .....	12
1.7 How to use this Design Guide .....	12
1.8 Amendments to this Design Guide .....	13
1.9 Interpretation.....	13
<b>2 Vision &amp; Principles.....</b>	<b>14</b>
2.1 Vision .....	14
2.2 Principles .....	14
<b>3 Sub Precinct Intent &amp; Development Principles .....</b>	<b>18</b>
3.1 Central Square.....	20
3.2 Regent Street Sidings sub-precinct.....	20
3.3 Prince Alfred Sidings sub-precinct.....	20
3.4 Goulburn Street sub-precinct.....	21
<b>4 Connecting with Country .....</b>	<b>22</b>
4.1 Connecting with Country strategy .....	22
4.2 Engagement with the Aboriginal community .....	23
4.3 Acknowledging and celebrating Aboriginal language .....	24
4.4 Aboriginal heritage interpretation.....	25
<b>5 Heritage.....</b>	<b>26</b>
5.1 Heritage conservation.....	26
5.2 Heritage conservation in sub-precincts.....	28
5.3 Heritage interpretation.....	29
5.4 Archaeology.....	30
<b>6 Land Use .....</b>	<b>32</b>
6.1 Land Use and Residential accommodation.....	32

6.2	Advertising and signage.....	32
<b>7</b>	<b>Building Form and Design .....</b>	<b>34</b>
7.1	Building and podium heights.....	34
7.2	Setbacks and separation distances.....	36
7.3	Building dimensions and articulation .....	40
7.4	Ground level frontages.....	41
7.5	Reflectivity.....	44
<b>8</b>	<b>Amenity.....</b>	<b>45</b>
8.1	Sunlight access .....	45
8.2	Wind safety and comfort.....	46
8.3	Views and vistas .....	49
8.4	External lighting.....	51
8.5	Contamination .....	52
8.6	Noise and vibration .....	52
<b>9</b>	<b>Design Excellence Strategy .....</b>	<b>54</b>
<b>10</b>	<b>Public Space.....</b>	<b>57</b>
10.1	Public Open Space - General .....	57
10.2	Publicly accessible space .....	61
10.3	Public art.....	67
<b>11</b>	<b>Transport.....</b>	<b>68</b>
11.1	Vehicle access .....	68
11.2	Footpaths.....	70
11.3	Vehicle access and footpath crossings.....	70
11.4	Bus access .....	71
11.5	Vehicle parking rates.....	71
11.6	Car share and point-to-point services.....	72
11.7	Location and design of private and visitor vehicle parking.....	73
11.8	Servicing strategy and emergency access.....	74
11.9	Service vehicle parking and loading.....	75
11.10	Waste collection and loading .....	75
11.11	Future transport .....	76
11.12	Cycle network.....	76
11.13	Amount of bicycle parking.....	78
11.14	Secure bike parking and end of trip facilities.....	78
11.15	Construction traffic management.....	79
<b>12</b>	<b>Environmental Sustainability, Climate Change &amp; Waste Management.....</b>	<b>81</b>
12.1	Energy and greenhouse gas emissions.....	81

12.2	High performance precinct and buildings.....	82
12.3	Electrical energy .....	82
12.4	Thermal energy .....	83
12.5	Solar gain.....	83
12.6	Natural ventilation.....	84
12.7	Climate risk and resilience.....	84
12.8	Circular economy and materials .....	84
12.9	Waste management.....	85
<b>13</b>	<b>Green Infrastructure, Ecology, Urban Forest &amp; Greening.....</b>	<b>88</b>
13.1	Landscape character .....	88
13.2	Locally indigenous flora and fauna species .....	90
13.3	Soil requirements.....	90
13.4	Significant trees .....	91
13.5	Biophilic design .....	91
13.6	Biodiversity.....	92
<b>14</b>	<b>Water Quality, Flooding &amp; Stormwater .....</b>	<b>93</b>
14.1	Integrated Water Management .....	93
14.2	Water positivity .....	94
14.3	Local drainage management.....	95
14.4	Flood planning.....	96
14.5	Stormwater quality.....	98
<b>15</b>	<b>Social Infrastructure.....</b>	<b>99</b>
15.1	Affordable housing .....	99
15.2	Universal and adaptable housing .....	100
15.3	Accessible design.....	100
<b>16</b>	<b>Safety and Security .....</b>	<b>101</b>
16.1	Event management.....	101
16.2	Crime prevention through environmental design .....	102
<b>17</b>	<b>Aeronautical Requirements .....</b>	<b>103</b>
<b>18</b>	<b>Schedules.....</b>	<b>104</b>
	Schedule 1: Definitions.....	104
	Schedule 2: Sun access plane.....	106
	Schedule 3: Planting species index and location.....	108
	Schedule 4: Public Art Curatorial Framework.....	111
	Schedule 5: List of amendments .....	113



# Figures

Figure 1: Central Precinct land application.....	10
Figure 2: Indicative development parcels.....	11
Figure 3: Central Precinct Structure Plan .....	17
Figure 4: Central Precinct sub-precincts.....	19
Figure 5: Central Precinct street wall and podium heights.....	35
Figure 6: Central Precinct ground level setbacks.....	38
Figure 7: Central Precinct tower setbacks and key separation distances.....	39
Figure 8: Maximum horizontal dimension of a non-residential tower.....	41
Figure 9: Central Precinct ground level frontages.....	43
Figure 10: Central Precinct significant views.....	50
Figure 11: Design excellence process requirements for sites across Central Precinct.....	56
Figure 12: Central Precinct Public Open Space.....	59
Figure 13: Central Precinct pedestrian network .....	60
Figure 14: Typical cross-section, Goods Line .....	61
Figure 15: Central Precinct publicly accessible open space, plazas and spaces.....	64
Figure 16: Central Precinct vehicle access.....	69
Figure 17: Central Precinct cycle network.....	77
Figure 18: Prince Alfred Park Sun Access Plane A.....	106
Figure 19: Belmore Park Sun Access Plane .....	107
Figure 20: Location of public domain areas and planting species.....	110



# Tables

Table 1: Central Precinct guidelines for ground level frontages .....	42
Table 2: Central Precinct sunlight access .....	45
Table 3: Central Precinct target wind comfort conditions.....	48
Table 4: Central Precinct guidance for pedestrian network.....	63
Table 5: Central Precinct guidance for open space areas.....	65
Table 6: Central Precinct maximum on site car parking rates.....	71
Table 7: Central Precinct loading and service vehicle parking rates.....	74
Table 8: Central Precinct guidance for on-site bike parking rates.....	78
Table 9: Central Precinct minimum targets for tree canopy and green cover .....	89
Table 10: Central Precinct guidelines for number of adaptable dwellings .....	100
Table 11: Key terms and definitions.....	104
Table 12: Key heritage terms and definitions.....	105
Table 13: Planting species index and location .....	108
Table 14: Public art curatorial frameworks, potential forms and locations .....	111
Table 15: List of amendments to the Central Precinct Design Guide .....	113



# 1.0 Introduction

---

## 1.1 Name of Guide

This document is the Central Precinct Design Guide.

---

## 1.2 Citation

This document may be referred to as the Design Guide.

---

## 1.3 Commencement

The Design Guide commences on the day on which it is endorsed by the Department of Planning, Housing and Infrastructure.

---

## 1.4 Land to which this Design Guide applies

The Design Guide applies to land shown in Figure 1: Central Precinct land application to the extent that is mapped as 'Central Precinct' in the Sydney Local Environmental Plans 2012. In the event of any inconsistencies the LEP maps will prevail.

Indicative development parcels are shown in Figure 2: Indicative development .

---

## 1.5 Purpose and status of this Design Guide

The purpose of this Design Guide is to support implementation of the Sydney Local Environmental Plan 2012 (Sydney LEP 2012) by providing more detailed provisions to guide development.

It is given effect by reference in Part 8 Local Provisions – Central Precinct of the Sydney LEP 2012.

This Design Guide replaces the provisions of the Sydney Development Control Plan 2012.

Part 8, Section 8.3 of the Sydney LEP 2012 requires the consideration of this Design Guide as part of the assessment of any future development applications (DAs) within Central Precinct.

---

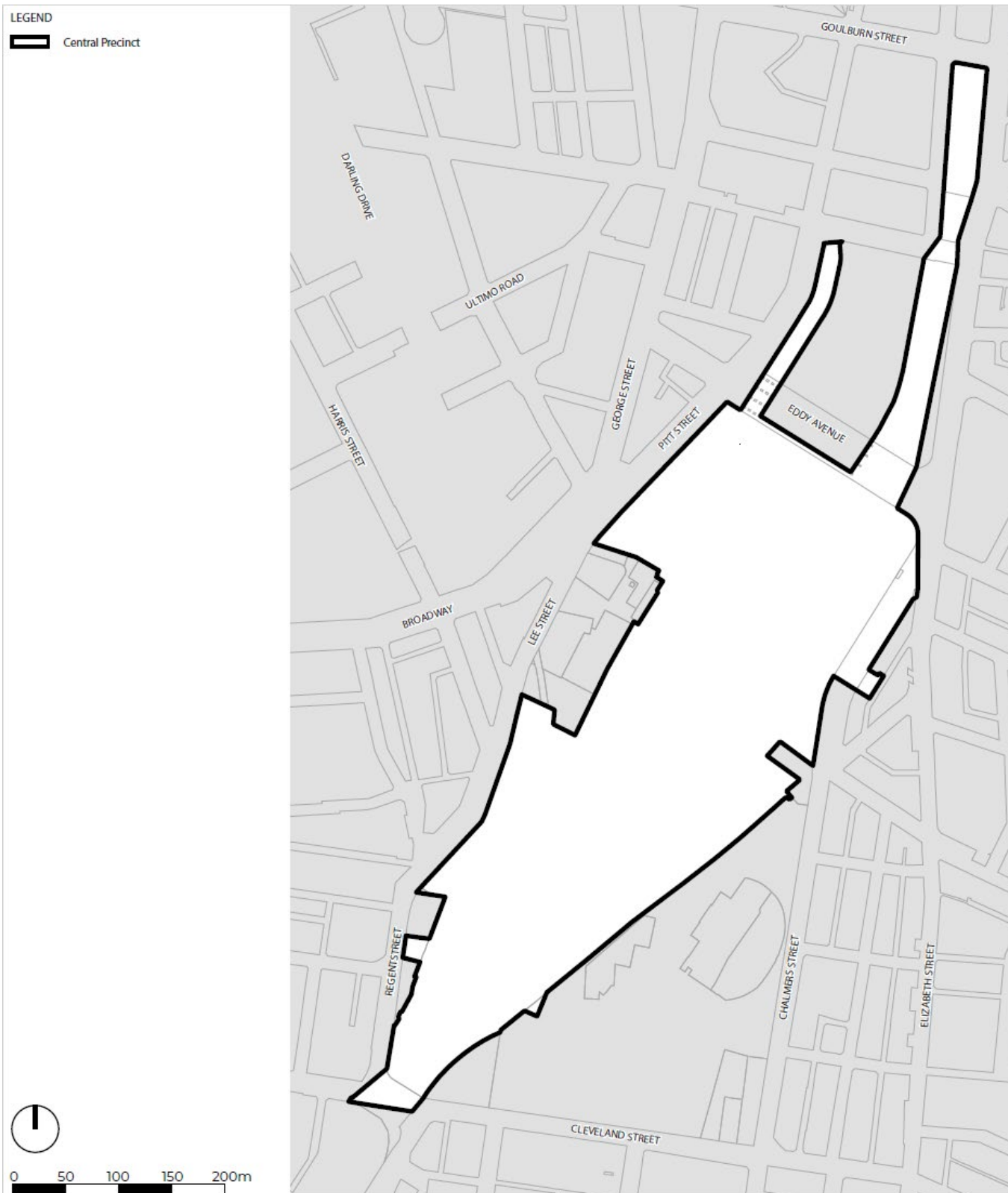


Figure 1: Central Precinct land application

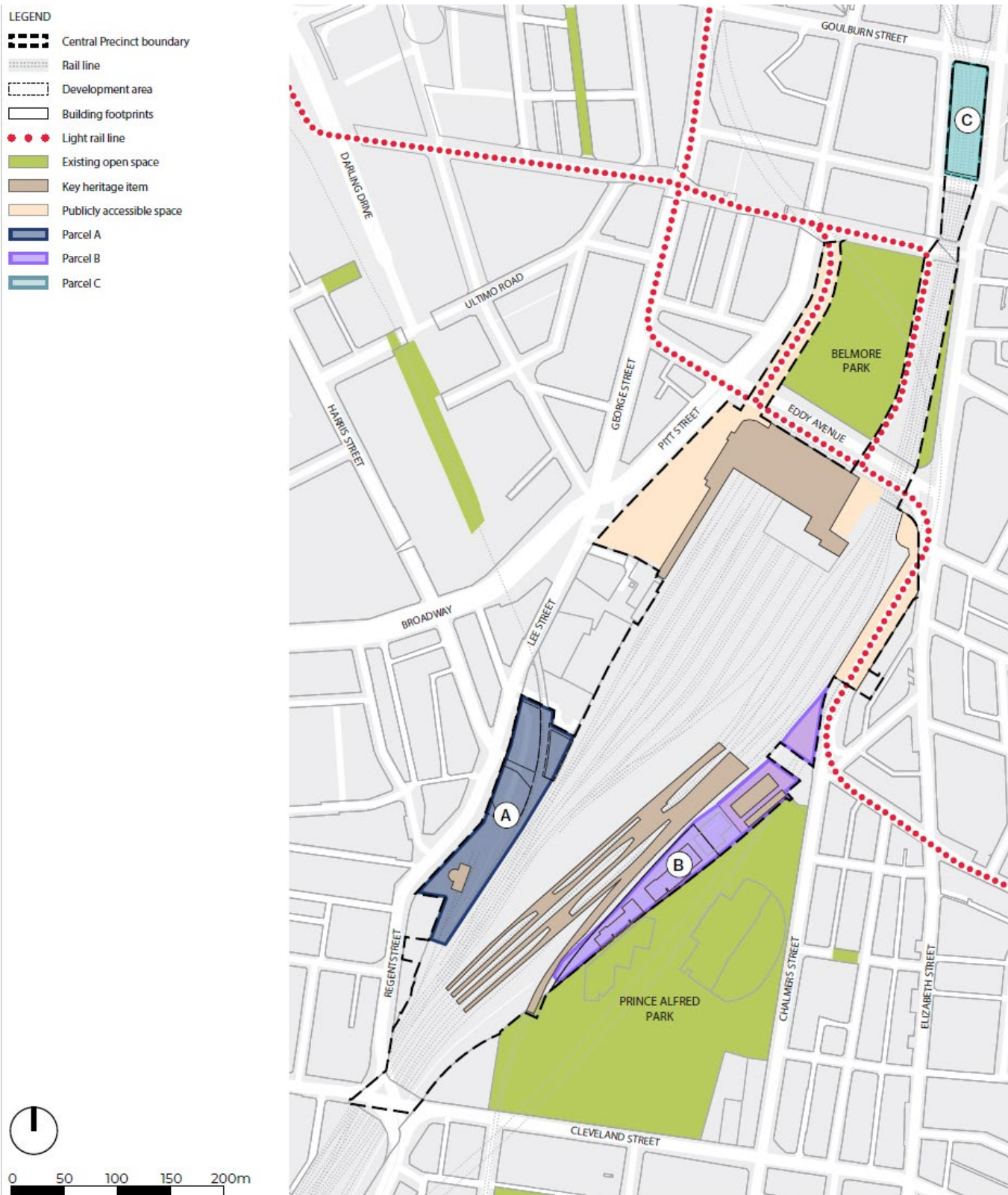


Figure 2: Indicative development parcels

---

## 1.6 Relationship to planning instruments and documents

The Design Guide forms part of a suite of planning provisions that apply to Central Precinct. This includes Acts, Regulations and State Environmental Planning Policies (SEPPs).

Relevant Acts and Regulations include:

- Environmental Planning and Assessment Act 1979
- Heritage Act 1977
- Environmental Planning and Assessment Regulation 2000.

Relevant State environmental planning policies include:

- State Environmental Planning Policy (Housing) 2021
- State Environmental Planning Policy (Planning Systems) 2021
- State Environmental Planning Policy (Biodiversity and Conservation) 2021
- State Environmental Planning Policy (Transport and Infrastructure) 2021
- State Environmental Planning Policy (Industry and Employment) 2021
- State Environmental Planning Policy (Exempt and Complying Development Codes) 2008.

The Design Guide is sub-ordinate to the Sydney LEP 2012 and SEPPs. Where a provision of this Design Guide is inconsistent with a provision in the Sydney LEP 2012 or a SEPP, the Sydney LEP 2012 or SEPP prevails to the extent of the inconsistency.

---

## 1.7 How to use this Design Guide

This Design Guide provides performance based controls that recognise the significance, complexity and long-term timeframe of development in Central Precinct.

DAs will be assessed on their individual merit having regard to the Sydney LEP 2012, this Design Guide, other matters listed in Section 4.15 of the Environmental Planning and Assessment Act 1979 and any other adopted and relevant policies that relate to development within Central Precinct.

The Design Guide comprises a hierarchy of provisions to guide development as follows:

1. **Vision and principles:** outline the overall outcomes sought.
2. **Sub-precinct intent and development principles:** describe the place-specific desired future character of each sub-precinct.
3. **Objectives:** describe the objectives for key matters outlined within this Design Guide.
4. **Guidance:** sets out how objectives on key matters can be achieved through design responses.

Development is to demonstrate how it meets relevant objectives. The guidance provides benchmarks for how the objectives could be achieved. The guidance does not represent the only way the objectives can be achieved. Where alternative solutions to the guidance are proposed they must demonstrate how they achieve the objectives.

---

## 1.8 Amendments to this Design Guide

Any amendment to this Design Guide will require the endorsement of the Department of Planning, Housing-and Infrastructure.

Refer to Schedule 5: List of amendments for amendment notes to this Design Guide.

---

## 1.9 Interpretation

Schedule 1: Definitions defines terms used in this Design Guide. Other terms have the same meaning as the Environmental Planning and Assessment Act 1979, the Environmental Planning and Assessment Regulation 2000 and other applicable planning instruments.

# 2 Vision & Principles

---

## 2.1 Vision

The vision for the Central Precinct is as follows:

Central Precinct is a vibrant and exciting place to live that unites a world-class transport interchange with public spaces.

The structure plan represents the vision for Central Precinct and is shown in Figure 3: Central Precinct Structure Plan

---

## 2.2 Principles

The principles for the Central Precinct are as follows.

### Connecting with Country

1. Embed Connecting with Country principles into the layout and design of the public domain, landscape and buildings, to recognise the important role of Central Station to Aboriginal people prior to and post European contact.
2. Ensure development is guided, co-designed and informed by the Aboriginal community and their cultural knowledge and practice of caring for Country, respecting the diversity of Aboriginal cultures, and prioritises place-specific cultural identity.
3. Respect Indigenous Cultural and Intellectual Property (ICIP) rights throughout the delivery process of the entire precinct. This is to be achieved by developing and maintaining genuine relationships with Aboriginal and Torres Strait Islander peoples especially when there is any reference to First Nations cultures.
4. Ensure the provision of high quality public art and heritage interpretation is coordinated with Connecting with Country objectives to create a meaningful cultural contribution to the precinct and broader city centre.

### Heritage

1. Respect and celebrate the heritage significance of the State Heritage Register (SHR) listed Sydney Terminal and Central Railway Station Group (Central Station group) including buildings, station yard, viaducts, building interiors and their individual components and significant view lines.
2. Protect and manage Aboriginal and archaeological resources.



3. Ensure heritage interpretation opportunities are integrated into the built forms and public spaces.

## **Public space**

1. Deliver high quality publicly accessible open spaces that provide high amenity environment for a variety of both passive and active uses appropriate to their location, that respond to community needs, are safe and accessible to all.
2. Ensure publicly accessible open spaces have appropriate microclimatic conditions (i.e., sunlight and daylight access and wind) for their intended purpose.
3. Ensure heritage interpretation opportunities and Connecting with Country objectives are integrated into the public spaces of the precinct.
4. Facilitate the delivery of a major public square adjacent to Railway Square and Lee Street.
5. Integrate local indigenous landscaping, stormwater management and water sensitive urban design (WSUD) in public spaces.

## **Transport**

1. Maintain Central Station's transport role as Sydney's main transport interchange, meeting the transport needs of local, regional and international travellers now and into the future.
2. Support efficient and safe movement of vehicles for parking, servicing and loading.
3. Revitalise the Goods Line as a movement corridor to the Haymarket area for pedestrians and cyclists.
4. Establish a safe and efficient process for the delivery of development and public spaces, with minimal impact on rail operations and broader functioning of the city including the Sydney (Kingsford-Smith) Airport.

## **Land use and built form**

1. Deliver a highly urban type and scale of built form with variation and diversity to provide for an interesting and engaging cityscape, reduce the appearance of bulk and mass and respond to the desired future character of sub-precincts.
2. Ensure built form provides adequate daylight access, solar access and safe and comfortable wind conditions to public spaces.
3. Establish a co-ordinated process that ensures development demonstrates design excellence in architectural, urban and landscape design.

## **Urban Greening**

1. Establish new greening and canopy cover that is aligned with the City's Greening Sydney strategy now and into the future to provide shade, visual amenity, reduce urban heat, local wind mitigation, and support biodiversity within Central Precinct.
2. Establish a diversity of endemic indigenous flora and fauna that is tolerant and resilient within the urban environment.



## Social sustainability and infrastructure

1. Deliver a minimum 30% of residential floorspace as affordable housing to support low to middle income households.

## Sustainability

1. Deliver a net-zero Central Precinct.
2. Contribute to the NSW Government's target of 70% emissions reduction by 2035 and net zero emissions by 2050.
3. Embed environmental, economic and sustainability into the fabric of the precinct, including delivering best practice initiatives and ensuring a green, biodiverse and resilient precinct.
4. Ensure a water positive precinct through water efficiency, preservation of non-renewable water resources and reduction in consumption of mains potable water.
5. Adopt circular economy measures to eliminate waste and reduce the continual use of new resources in line with NSW Government's policies and guidelines.
6. Embed and demonstrate the principles and objectives of the NSW Government's waste and sustainable materials strategy 2020 – 2041 and NSW plastics action plan.
7. Minimise the impact of stormwater and flooding on surrounding development and public spaces.



Figure 3: Central Precinct Structure Plan

# 3 Sub Precinct Intent & Development Principles

The Central Precinct comprises the sub-precincts as shown in Figure 4: Central Precinct sub-precincts

This section provides statements of intent and place-specific development principles for each of the sub-precincts.

1. Central Square and Western Forecourt
2. Western Gateway sub-precinct
3. Regent Street Sidings sub-precinct
4. Prince Alfred Sidings sub-precinct
5. Goulburn Street sub-precinct

**Note:** The Western Gateway sub-precinct is covered by the Western Gateway Sub-precinct Design Guide.

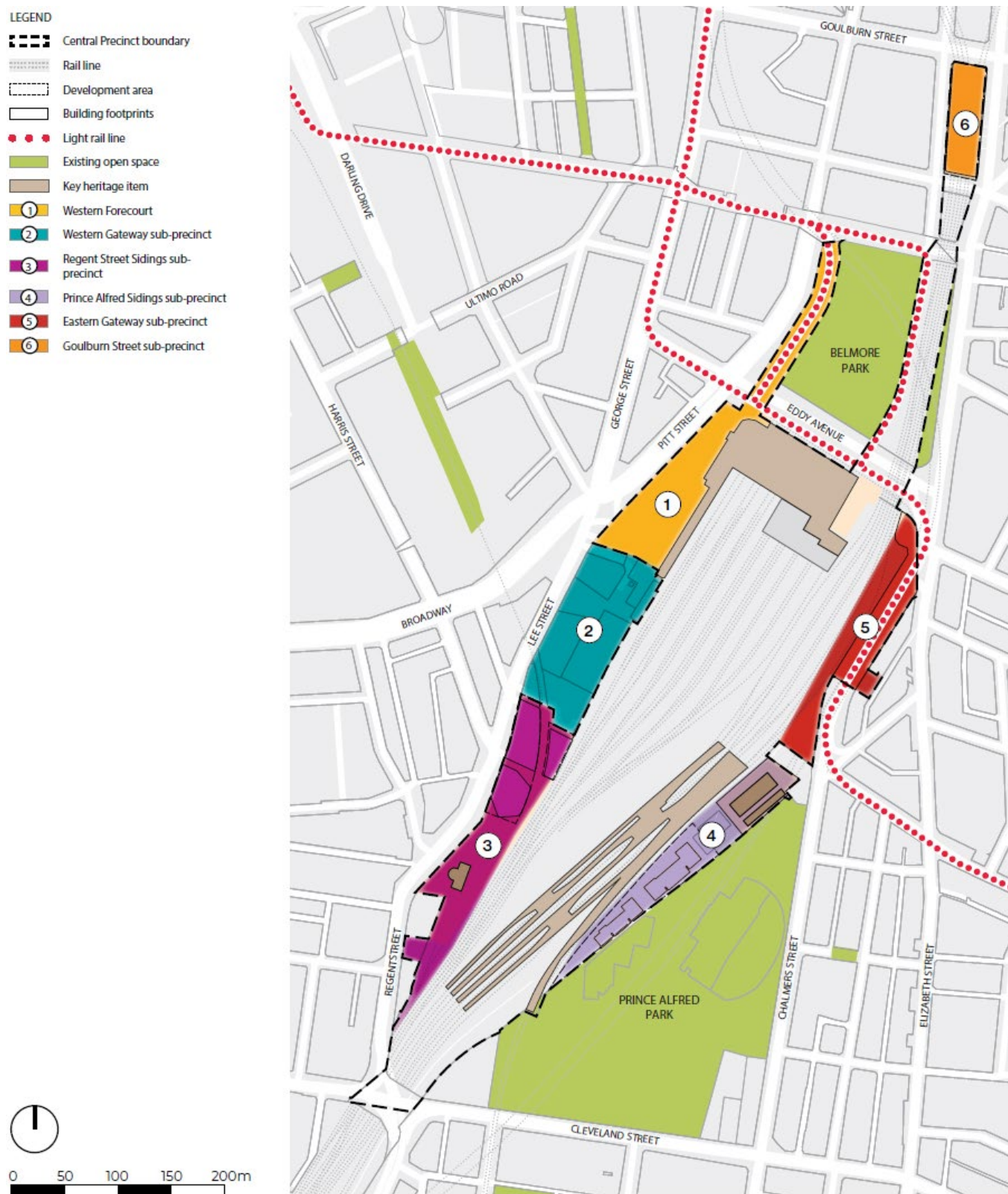


Figure 4: Central Precinct sub-precincts

---

## 3.1 Central Square

### Central Square

The sub precinct principles are:

1. Contribute to the vision for a Central Square, with the partial closure of Lee Street and connection to Railway Square.
2. Establish a public space that provides a major pedestrian connection to the new platform entrances, retains a grand entrance to the main concourse and creates a significant urban public space.

---

## 3.2 Regent Street Sidings sub-precinct

The Regent Street Sidings sub-precinct contains the exceptionally heritage significant Mortuary Station and Goods Line. These two elements are the oldest structures in Central Precinct, with the Goods Line being one of the oldest pieces of infrastructure in the NSW rail system and Mortuary Station forming a significant landmark. The development intent for this sub-precinct is a mixed-use precinct anchored by Mortuary Station that interacts with and positively responds to the changing urban context of Chippendale.

The sub-precinct development principles are:

1. Celebrate the heritage values of Mortuary Station and the Goods Line as the earliest remaining elements of Redfern and Central stations.
2. Facilitate development that responds to the existing and future scale of the neighbouring area and to the unique landmark qualities, scale and setting of Mortuary Station.
3. Provide new publicly accessible space that works with the civic form and rail heritage of Mortuary Station Building with significant tree canopy, greening and good amenity.
4. Create new pedestrian and cycle connections that maximise access, including the revitalisation of the unused Goods Line.
5. Allow residential accommodation as part of mixed use development that includes active ground floor uses.

---

## 3.3 Prince Alfred Sidings sub-precinct

The development intent for the Prince Alfred Sidings sub-precinct is to provide a lower scale western edge to Prince Alfred Park, ensure appropriate solar access, respond sensitively to heritage items (Railway Institute Building, Prince Alfred Substation and Switch House, Whitton era railway buildings).



The sub-precinct development principles are:

1. Improve the interface of Central Precinct with Prince Alfred Park as an important green space for the city that supports active and passive recreational functions while protecting and maintaining existing uses.
2. Facilitate low-scale well-considered development that presents an attractive and activated edge and sensitively responds to the park, in relation to solar access, amenity, wind and key views.
3. Extend the green landscaped nature of Prince Alfred Park by encouraging design that contributes to the sub-precinct's park setting, enhances pedestrian amenity and protects local biodiversity.

---

## 3.4 Goulburn Street sub-precinct

The development intent for the Goulburn Street sub-precinct is to establish a new urban block of development which responds to the operational rail below, integrates and activates the adjoining public domain and provides a new high-quality edge to Goulburn Street.

The sub-precinct development principles are:

1. Re-imagine the city block in a manner that better interfaces with the surrounding streets, including improved street activation and pedestrian connections at the ground plane.
2. Deliver a design outcome that appropriately responds to the intersection of Elizabeth Street, Wentworth Avenue and Campbell Street and respects and enhances the heritage context of the precinct, especially at the interface with the Elizabeth Street viaduct.
3. Deliver larger scale mixed use development that reflects the sub-precinct's Central Sydney location, and which can overcome the design and development challenges presented by the underlying operational rail land.

# 4 Connecting with Country

Country is not a Western concept; it is an Aboriginal worldview. It is nature at a deeper level, where all things are interconnected, and the spiritual underlies the physical. The Aboriginal sense of Country is that past, present, and future are not confined by time, they merge into a continuum. Aboriginal thinking therefore embraces what was on Country before, what is there now and what might come back or evolve in the future. Country commands care and respect. Aboriginal principles for sustaining Country are embedded in language, stories and Songlines which all reflect physical and spiritual understandings of the land. All things belong to Country, Country does not belong to anyone.

The Central Precinct Connecting with Country Framework (Balarinji, 2023) will assist planners, architects, and project teams to understand and implement Connecting with Country principles for the Precinct. It aims to provide a basis for co-designing with Country, through embedding continuing local Aboriginal engagement in processes and outcomes.

The objectives and guidance in this section are to be read with the relevant principles in **Sections 2.0 and 3.0**.

---

## 4.1 Connecting with Country strategy

### Objectives

a) To ensure development:

- i. is guided, co-designed and informed by the Aboriginal community (including representation by traditional custodians of the Gadigal community) and their cultural knowledge and practice of caring for Country
- ii. supports the health and wellbeing of Country
- iii. creates a welcoming and safe place, that is physically and culturally accessible for the Aboriginal community that strengthens Aboriginal identity
- iv. values and respects Aboriginal knowledge and stories
- v. connects and respects areas of cultural significance to the Aboriginal community
- vi. provides ongoing benefits for Aboriginal people through renewal of Central Precinct (including but not limited to employment, educational, economic and cultural)
- vii. acknowledges the Aboriginal Community's right to financially benefit from ICIP, knowledge and cultural narrative is valued and respected.



- b) To reconnect with natural landscape including topography and native vegetation and caring for Country.

## Guidance

1. Development applications are accompanied by a Connecting with Country Strategy that demonstrates how the themes and opportunities outlined in the 'Central Precinct Connecting with Country Framework' (Balarinji, 2023) has informed physical design.
2. Landscaping incorporates local/ endemic planting species in accordance with Schedule 2: Sun access plane and Schedule 3: Planting species index.
3. Water management infrastructure and processes respond to Country and prioritise natural solutions that enhance overall waterway systems condition, function and connections.
4. Development that provides for Connecting with Country opportunities adheres to the Indigenous Cultural and Intellectual Property protocols.

---

## 4.2 Engagement with the Aboriginal community

### Objectives

- a) To ensure development is guided and informed by the Aboriginal community and their cultural knowledge and practice of caring for Country.

### Guidance

1. Engagement with the Aboriginal community to seek community views on:
  - a. whether the proposed development impacts on existing or recent spaces or activities on the site, or on surrounding properties, that are important for the Aboriginal community
  - a. whether the proposed development impacts on the wider context of the Precinct being a place of belonging and pride for Aboriginal people
  - b. how the development may best maximise the presence, visibility, benefits and celebration of Aboriginal people, organisations, businesses and living cultures
  - c. how the development is accessible to the Aboriginal community both physically and culturally.
2. All engagement is carefully designed to:
  - a. be undertaken in a sensitive and culturally appropriate manner.
  - b. provide meaningful opportunities for co-design and collaboration, which includes allowing appropriate time for genuine co-design and collaboration.
  - c. build on previous conversations to reduce consultation fatigue and be coordinated with other engagement in the Precinct and wider area.
  - d. be connected to outcomes.

3. Engagement is undertaken with all relevant groups that may be affected by the proposal, including Traditional Custodians, the local Aboriginal community and key Aboriginal stakeholders who have a connection with the precinct.
4. Engagement activities are designed and led by Aboriginal-owned consultation advisories or by an organisation with Aboriginal consultation experience.
5. Development applications are supported by an 'Aboriginal Co-design Report' that outlines:
  - a. pre-lodgement engagement activities.
  - b. the outcomes of engagement, including with the Aboriginal Advisory Group.
  - c. measures to address issues and concerns raised in engagement.
  - d. how the engagement has informed the planning and design of the proposed development.
  - e. what opportunities there are on the site to enhance Aboriginal cultural values and meet the needs/ aspirations of Aboriginal people.
  - f. what mitigation measures are proposed to reduce any impacts to areas of Aboriginal cultural value.
6. Development implements the recommendations of the 'Aboriginal Co-design Report' where they:
  - a. have evidence of broad support from those engaged.
  - b. are within the capacity of the development to deliver.
  - c. are commensurate with the scale of the development.

---

## 4.3 Acknowledging and celebrating Aboriginal language

### Objective

- a) To enhance and strengthen Aboriginal connection to place through acknowledging Aboriginal language.

### Guidance

1. Development provides opportunities to name publicly accessible spaces and provide wayfinding signage in Sydney language or implement dual naming.

**Note:** for Aboriginal naming and dual naming, the proponent is to consult with the NSW Geographical Names Board, local language subject matter experts and with Aboriginal stakeholder groups.

2. Where Aboriginal naming is adopted, consideration is given to providing physical material that outlines the pronunciation and history behind the Aboriginal name, where appropriate and agreed to by relevant Aboriginal stakeholders.

**Note:** depending on the result of Aboriginal engagement, signage may need to display multiple spelling and pronunciation of an Aboriginal name or word.

3. Aboriginal naming aligns with the City's Naming Policy and the Aboriginal Naming Trusts guidance.

---

## 4.4 Aboriginal heritage interpretation

### Objective

- a) To create engaging and appropriate Aboriginal heritage interpretation that is responsive to culture and Country.

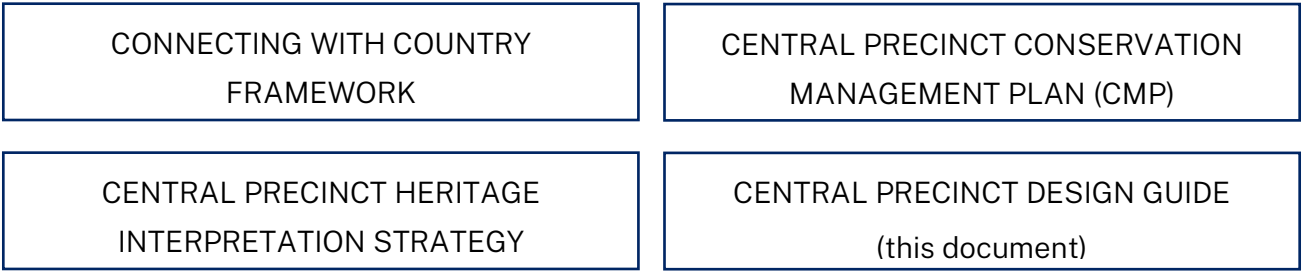
### Guidance

1. Development applications are accompanied by a Heritage Interpretation Strategy that addresses Aboriginal cultural heritage and provides opportunities for heritage interpretation media which are:
  - a. responsive to culture and Country.
  - b. co-designed with Aboriginal knowledge holders.
  - c. integrated within the landscaping and built form.
  - d. consider use of technology to help tell the story of Country.
2. Aboriginal heritage interpretation adheres to the Indigenous Cultural and Intellectual Property protocols.

# 5 Heritage

All decisions about the future of Central Precinct must be undertaken with regard to the management of the key heritage values and significance of the place, and the potential for adverse heritage impacts. Where new developments or changes are proposed, the potential adverse impacts to the Precinct and its individual components must be managed, minimised or mitigated. These decisions should be guided by the below heritage documents which form the core heritage framework for Central Precinct and will guide the preparation of future plans and assessments for proposed works.

## HERITAGE FRAMEWORK



The intent of this section is to ensure that decisions about change are made having regard to heritage significance, and that opportunities to improve the understanding and appreciation of this significance are taken.

The objectives and guidance in this section are to be read in conjunction with the relevant principles set out in **Section 2.0** and **3.0**.

Further relevant objectives and guidance are also provided in **Section 8.3** Views and vistas **Views and vistas**.

**Note:** Definitions for terms used in this section are provided at Schedule 1: Definitions. Other terms used within this section are in accordance with the definitions of the Burra Charter, Article 1.

## 5.1 Heritage conservation

### Objectives

- a) To achieve development that:
  - i. celebrates the heritage significance of the precinct as a major historic transportation hub.

- ii. embeds heritage values into the design and development of a unique, place-based destination.
- iii. promotes conservation of heritage values.
- iv. enhances aspects of the place that are intrinsic to its value including design, fabric, built form, spaces, uses, functions, settings, landscapes, views, and intangible values.
- v. is informed by heritage specialist advice and specialist expertise.
- vi. appropriately responds to the historic visual, physical and spatial character of Central Precinct, having regard for the scale, design and materiality of significant buildings and structures.
- vii. remains contemporary in architectural expression but sympathetic in design detail, clearly demarcating historic and new fabric.
- viii. avoids unacceptable visual impacts to identified significant views and vistas.
- ix. prioritises the retention and sensitive adaptive reuse of heritage items, ensuring the protection of their significant features, spaces and fabric and sustaining their long-term viability.
- x. ensures the legibility of the different phases of historic development at Central Railway Station and its place as the centre of the railways in NSW.

## Guidance

1. Ensure development appropriately responds, respects, conserves, celebrates and enhances the heritage significance and legibility of the SHR listed Sydney Terminal and Central Railway Stations group.
2. Development proposals are informed by specialist heritage advice, with heritage input into the design development process and heritage participation through the design excellence process.
3. A Statement of Heritage Impact (SoHI) is to accompany a development application for new buildings or works or alteration and additions and is to:
  - a. demonstrate how the development has been informed by the policies and guidelines of the Central Precinct CMP, any other relevant conservation management document or expert recommendations that may apply
  - b. is prepared in accordance with the relevant Heritage NSW guidelines for assessing heritage impacts.
4. Where a development application proposes the full or partial demolition of a heritage item or element, the Statement of Heritage Impact is to:
  - a. demonstrate why the element, building or part of the building is not capable of retention or re-use
  - b. identify and describe the options that were considered when arriving at a preferred development outcome and the reasons for choosing the preferred option
  - c. include a statement from a quantity surveyor comparing the cost of demolition to the cost of retention if the demolition is recommended primarily on economic grounds
  - d. include a report by a suitably qualified structural engineer if the demolition is proposed based on poor structural condition.

5. Prior to the full or partial demolition of a heritage item or element archival photographic recording is required prior to any work being carried out.
6. Proposed development is designed with regard to the heritage values of the Central Precinct as a whole, the buildings, elements and spatial arrangements of the immediate and broader site encapsulated in its heritage curtilage. Building forms and design treatments of any new development must consider and positively respond to the heritage values of the place.
7. Commensurate with the exceptional heritage values of the place, new development aspires to achieve excellence in design and innovation. New elements are sympathetic to the significance of the site, while offering a creative and high-quality layer of design that is in keeping with the significance of Central Precinct.
8. Architectural detailing of new developments considers their interface to heritage buildings within the immediate vicinity, and has regard to form, colour, materials, and the prevailing character of the heritage setting.
9. Buildings and public domain elements are constructed of durable and robust materials, commensurate in quality with their historic counterparts and in keeping with the relevant policies of CMPs. New materials seek to unify historic elements and remain complementary to the historic character of Central Precinct.

---

## 5.2 Heritage conservation in sub-precincts

The following precinct specific heritage guidance is provided in addition to the broader guidance in Section 5.1 of this Design Guide.

### Guidance for Regent Street Sidings sub-precinct

1. Conserve Mortuary Station, including its platform and remnant tracks in the Sydney Yard. Changes to the overall layout and fabric of the Station are to be minimised in keeping with the policies and guidelines of any relevant conservation management document.
2. The historic landscape and railway setting of Mortuary Station closely informs future design and adaptive reuse proposals.
3. Mortuary Station and Gardens accommodates continued public engagement with rail heritage including public access to heritage steam trains and Transport heritage programs.
4. New buildings within the SHR curtilage of Mortuary Station are not permitted.
5. No vertical additions<sup>1</sup> to Mortuary Station Building.
6. Interpretation is included in any proposal associated with Mortuary Station.
7. Adaptively reuse Mortuary Station for viable uses within the existing building and spaces.

---

<sup>1</sup> Vertical additions includes additional floors or structures. Minor vertical additions for the purpose of providing ventilation etc may be permitted subject to a heritage impact assessment of the works.

8. New development is designed to respond to the heritage context in terms of character, scale, form, siting, materials and colour, and detailing.
9. Development setbacks provide adequate separation of potential built/ tower forms to mitigate visual impacts on the heritage context of Mortuary Station, adjacent heritage items and local Heritage Conservation Area (C9 Chippendale).
10. Development is set back from Mortuary Station to respect the heritage setting, character, form and scale of the item and its visual connection to other heritage items in the vicinity, in accordance with Section 7.3.
11. Retain and interpret the remnant railway tracks adjacent to Mortuary Station leading to the Goods Line and minimise impacts to the extant structures of the Railway Square Road Overbridge and Darling Harbour Cut, where possible.
12. Retain significant view corridors and vistas to Mortuary Station.
13. Reintroduce discovered artefacts such as the Lee Street turntable as elements of interpretation in the sub-precinct.

### **Guidance for Prince Alfred Sidings sub-precinct**

1. Conserve and restore the c.1870s workshop buildings and adaptively reuse them for a viable function and reinstate links to the surrounding precinct.
2. Prioritise conservation of the two brick Prince Alfred Substation buildings, restore significant elements, retain and interpret significant moveable heritage onsite and reinstate original fenestration.
3. Vertical additions to the Prince Alfred Substation, Switch House c.1870s workshop buildings are guided by the policies and guidelines in any relevant conservation management document.
4. Visual impacts to the Prince Alfred Substation building, Switch House and c.1870s workshop buildings are guided by the policies and guidelines contained in any relevant conservation management document.
5. New development is setback to maintain a heritage curtilage around the significant buildings.
6. Retain significant view corridors and vistas. Where these cannot be retained, open up new sightlines or deliver new elevated viewing locations within nearby new developments.
7. Reintroduce discovered artefacts such as the Chalmers Street turntable as elements of interpretation in the sub-precinct.

---

## **5.3 Heritage interpretation**

### **Objectives**

- a) To provide a cohesive suite of heritage interpretation and interpretive elements to celebrate the diverse cultural heritage significance, including tangible and intangible heritage values.



- b) Incorporate high quality, holistic interpretation integrated within the design and delivery of new public spaces and experiences.
- c) Ensure interpretation speaks to the diversity, depth, and longevity of the site's history to enrich the visitor experience and understanding of the place.

## Guidance

1. A Heritage Interpretation Plan is to accompany a development application and identify opportunities for the presentation of the tangible and intangible heritage of the precinct and surrounds.
2. The Heritage Interpretation Plan is to:
  - a. include an approach aligned with the key themes of 'Journeys' and 'Gatherings' (as identified in the Central Precinct Heritage Interpretation Strategy, 24 August 2023), together with specific site stories related to the tangible and intangible significance of the heritage item/ area
  - b. integrates heritage interpretation within the built form and public domain of Central Precinct
  - c. includes both Aboriginal and non-Aboriginal heritage interpretation opportunities
  - d. be aligned with the Connecting with Country Framework
  - e. be aligned with public art initiatives and any Public Art Strategy
  - f. be aligned with the policies and frameworks outlined in any relevant CMP
  - g. accounts for the archaeological significance of the site and the immediate surrounds
  - h. include a range of interpretive media that are accessible, engaging and informative
  - i. is prepared in accordance with the Heritage NSW Interpreting Heritage Places and Items Guidelines (Heritage NSW, 2005).

---

## 5.4 Archaeology

### Objective

- a) Ensure the conservation of archaeological relics.

### Guidance

1. Where development is likely to affect an archaeological site or a place of Aboriginal heritage significance or potential archaeological site that is likely to have non-Aboriginal heritage significance, development applications are to be accompanied by an Archaeological Assessment prepared by a suitably qualified archaeologist in accordance with the guidelines prepared by the NSW Office and Environment and Heritage.
2. An Archaeological Assessment of the archaeological site or place of Aboriginal heritage significance is to include:
  - a. an assessment of the archaeological potential
  - b. its heritage significance

- c. the probable impact of the proposed development on the heritage significance
  - d. the compatibility of the development with conservation policies contained within an applicable CMP or conservation management strategy
  - e. a management strategy to conserve the heritage significance.
3. Where intact Botany Sands/Grey sands soil profiles are identified in areas of Aboriginal archaeological potential, further assessment and investigation is undertaken in consultation with the relevant Registered Aboriginal Parties to manage and mitigate impacts to potential Aboriginal objects.
  4. If there is any likelihood that the development will have an impact on significant archaeological relics, development ensures that the impact is managed according to the assessed level of significance of those relics.

# 6 Land Use

This section provides objectives and guidance on the future land use outcomes for Central Precinct. The objectives and guidance in this section are to be read in conjunction with the relevant principles set out in Sections 2.0 and 3.0.

---

## 6.1 Land Use and Residential accommodation

### Objectives

- a) To support opportunities for a variety of mixed uses including residential accommodation.
- b) To ensure land uses do not compromise on Central Precinct's primary function as a major transport interchange.
- c) To ensure development for a residential flat building, shop top housing or mixed use development with a residential accommodation component has an acceptable impact on the environment.

### Guidance

- 1. Development for a residential flat building, shop top housing or mixed use development with a residential accommodation component complies with chapter 4 of '*State Environmental Planning Policy (SEPP) Housing*' and the relevant provisions of the '*Apartment Design Guide*' (Department of Planning and Environment, July 2015).
- 2. Development for a residential flat building, shop top housing or mixed use development with a residential accommodation component is to include a Sun Access Study.

---

## 6.2 Advertising and signage

### Objective

- a) To ensure development for advertising and signage has an acceptable impact on the environment.

### Guidance

- 1. Development for advertising and signage complies with '*State Environmental Planning Policy (Industry and Employment) 2021*'.

**Note:** some advertising and signage is exempt development. Refer to '*State Environmental Planning Policy (Exempt and Complying Development Codes) 2008*'.

2. Development for advertising and signage demonstrates consideration of the relevant provisions of the 'Transport Corridor Outdoor Advertising and Signage Guidelines' (Department of Planning and Environment 2017).

# 7 Building Form and Design

This section provides objectives and guidance on the future building form and design outcomes for Central Precinct and provides guidance on the following:

1. Building and podium heights
2. Setbacks and separation distances
3. Building dimensions and articulation
4. Ground level frontages
5. Reflectivity

The objectives and guidance in this section are to be read in conjunction with the relevant principles set out in Sections 2.0 and 3.0.

---

## 7.1 Building and podium heights

### Objectives

- a) To establish a clear and legible street wall that will define and activate public spaces.
- b) To ensure built form enables good solar access, daylight and comfortable microclimate conditions in adjacent and affected public spaces.
- c) To protect sunlight to surrounding public parks and places throughout the year, and during periods in the day when they are most used.
- d) To provide street wall heights that define the edge of publicly accessible spaces and activates and responds to Prince Alfred Park and Mortuary Station Gardens.

### Guidance

1. Building heights are in accordance with the maximum building height and Sun Access Planes (SAP) of the Sydney LEP 2012 (Schedule 2: Sun access plane indicatively shows the maximum height achievable for land affected by Sun Access Planes).

**Note:** To determine the actual height of a Sun Access Plane at any point, the description of the Sun Access Planes in Sydney LEP 2012 prevails over the diagrams in this Design Guide, in the case of an inconsistency. When preparing Sun Access Planes care must be taken to adjust for the difference between grid north and solar north.

- LEGEND**
- Central Precinct boundary
  - Rail line
  - Development area
  - Building footprints
  - Light rail line
  - Existing open space
  - Key heritage item
  - Maximum 6 storeys (25m) street wall podium height
  - Maximum 6 storeys (25m) street wall podium height from existing ground plane
  - Street wall height to Mortuary Station Garden
  - 35m street wall podium height from ground plane (no. of storeys varies due to rail corridor)
  - Prince Alfred Park street wall - up to Prince Alfred Park solar access plane

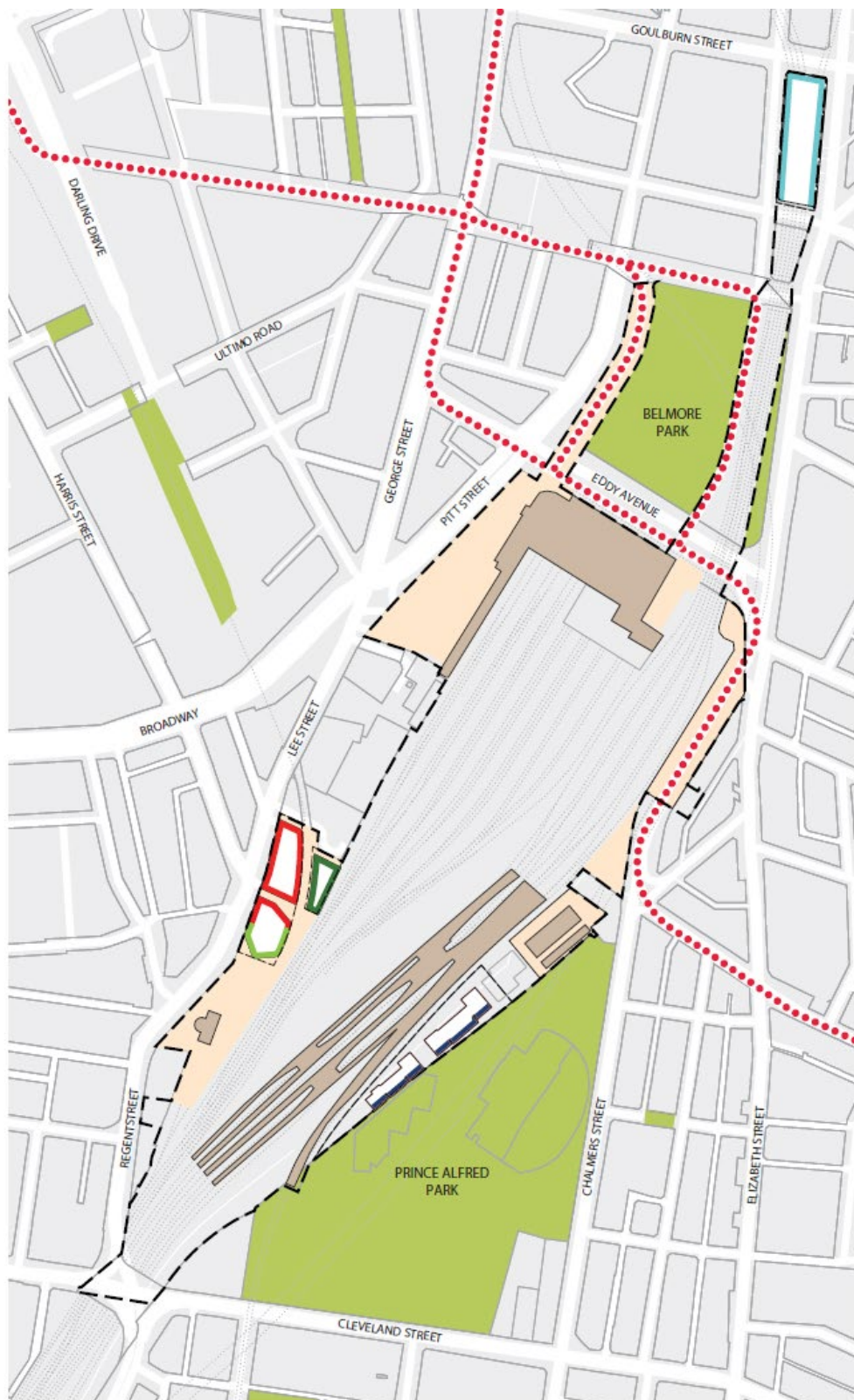


Figure 5: Central Precinct street wall and podium heights

---

## 7.2 Setbacks and separation distances

### Objectives

- a) Ensure buildings are appropriately sited in relation to heritage buildings and publicly accessible spaces.
- b) Ensure adequate separation between buildings to provide appropriate curtilage to heritage buildings of exceptional and high significance.
- c) Provide adequate tower setbacks from public domain and open space areas to preserve human scale from.
- d) Ensure buildings frame and activate public spaces.
- e) Ensure high amenity for publicly accessible spaces, having regard to daylight, wind conditions and protection from weather.
- f) Provide articulation in the built form that reduces the visual scale of tower forms.
- g) Provide appropriate amenity for building occupants having regard to daylight and outlook.
- h) Mitigate the appearance of building scale and bulk when seen from locations in the public domain.
- i) Retain views to the Central Station Clocktower against the sky.

### Guidance

1. Building setbacks are in accordance with Figure 6: Central Precinct ground level setbacks and Figure 7: Central Precinct tower setbacks and key separation distances.

**Note:** For the purposes of this control:

- Ground level setbacks refers to the setback between the public space/ site boundary and the facing alignment of the building at ground level.
  - Tower setbacks refers to the setback of any building elements above the height of any podium or other element that forms a ground level setback.
2. The distance between Mortuary Station Building and any tower on Regent Street Sidings is 70m and the distance between the podium of the proposed Regent Street built form and Mortuary Building is 66m.
  3. For residential flat buildings, shop top housing and the residential component of mixed use developments, the minimum separation distance between buildings is in accordance with the relevant provisions of the 'Apartment Design Guide' published by the NSW Department of Planning and Environment (July 2015).
  4. Building elements designed for decoration or as sun shading devices may project beyond the public domain alignment or setback, if the projection:
    - a. extends not more than 450mm over the road/footpath alignment
    - b. is at least 3m above the footpath
    - c. is at least 800mm from the face of the kerb














- d. and is constructed primarily of masonry, reinforced concrete, steel or other approved non combustible material.
5. Balconies and bay windows may project beyond the public domain alignment if the projection:
- a. extends not more than 450mm over the road/footpath alignment,
  - b. comprises not more than 50% of any road frontage of the building at any level,
  - c. is at least 3.2m above the footpath and at least 800mm from the face of the kerb, and
  - d. does not result in adverse impacts on the amenity of an adjoining property.



Figure 6: Central Precinct ground level setbacks

# LEGEND

-  Central Precinct boundary
-  Rail line
-  Development area
-  Building footprints
-  Light rail line
-  Existing open space
-  Key heritage item
-  Development Block A1
-  5m tower setback
-  6m tower setback
-  Minimum 70m separation distance between Mortuary Station Building and tower element on Regent Street Sidings Sub-Precinct

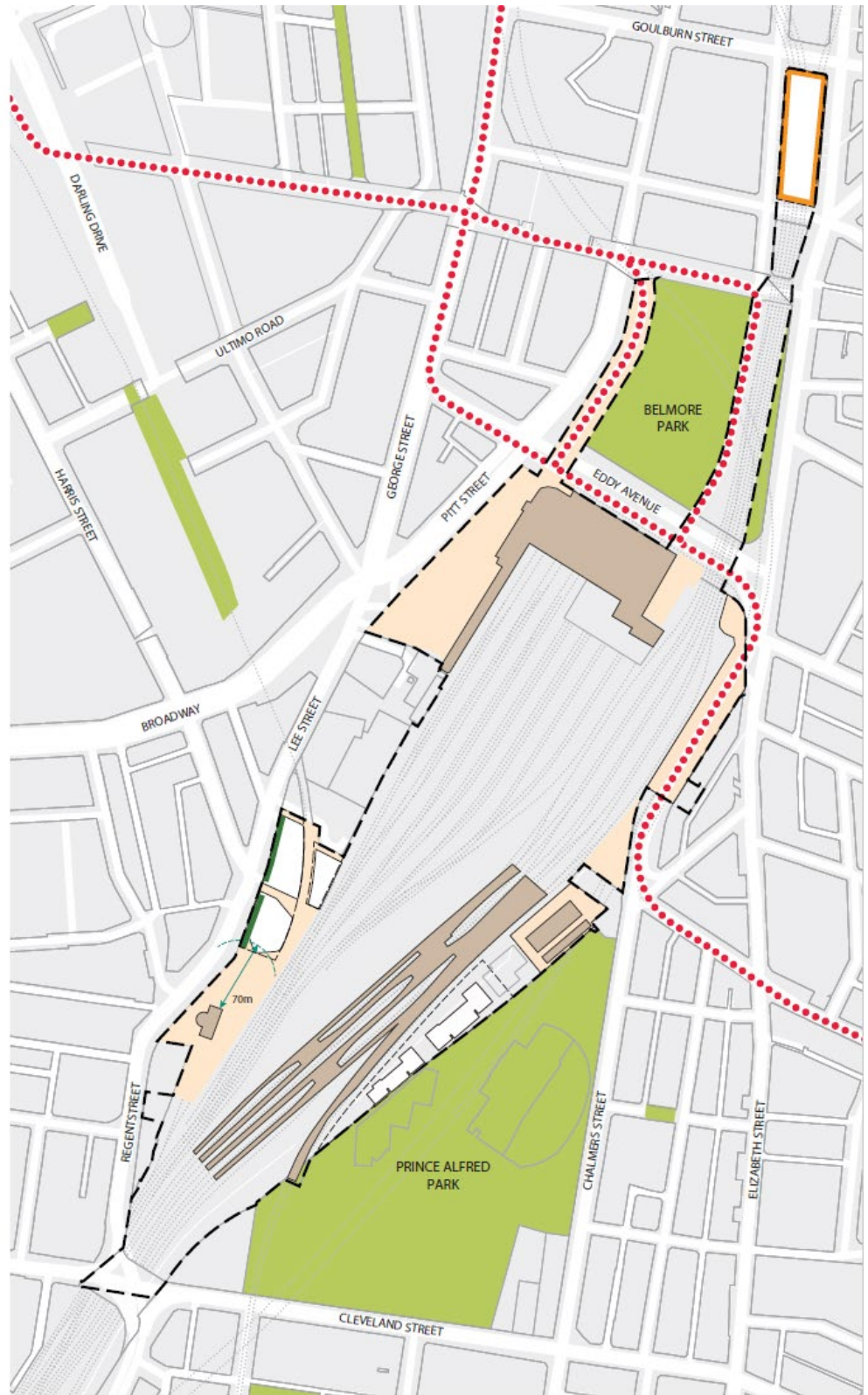


Figure 7: Central Precinct tower setbacks and key separation distances

---

## 7.3 Building dimensions and articulation

### Objectives

- a) To ensure appropriate building scale above podium street walls.
- b) To ensure building dimensions:
  - i. support a street wall and tower typology
  - ii. reduce the visual impact of building scale
  - iii. contribute to high amenity publicly accessible spaces, having regard to daylight access and wind conditions.
- c) To ensure building elevations provide visual interest and reduce the scale and bulk of buildings

### Guidance

1. Buildings may cover 100% of the developable part of a block, up to and including street wall building height

**Note:** The developable part of a block is that which remains following creation of publicly accessible spaces that are open to the sky.

**Note:** Setbacks, separation distances and articulation zones reduce the actual amount of the developable part of a block that a building may cover.
2. Above the podium height:
  - a. the maximum gross building area (GBA) is 85% of building site cover
  - b. the maximum horizontal dimension of a building (including all external elements such as horizontal or vertical fins) measured in any direction (including diagonally across the building) is not to exceed:
    - i. 50m for residential accommodation and serviced apartment developments,
    - ii. 100m for all other development (see Figure 8: Maximum horizontal dimension of a non-residential tower )
3. Podiums do not exceed a maximum horizontal length of 150m, without a minimum of one laneway or through-site link. The maximum distance between a laneway or through-site link is not to exceed 75m.
4. Building elevations are to incorporate measures such as:
  - a. modulation of massing in the vertical and / or horizontal planes, including recesses and projections
  - b. elements of a finer grain attached to the main structural framing such as sun shading devices.



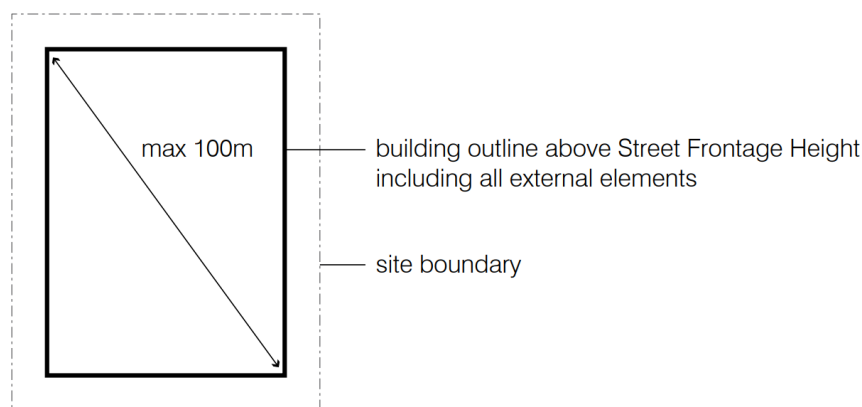


Figure 8: Maximum horizontal dimension of a non-residential tower

Source: City of Sydney

## 7.4 Ground level frontages

### Objectives

- a) To ensure ground level building frontages:
  - i. contribute to a vibrant, lively, high amenity and safe public domain
  - ii. provide people oriented and well-designed frontages appropriate to the role and characteristics of adjoining publicly accessible spaces
  - iii. allow for active frontages and outdoor dining opportunities

### Guidance

1. Ground level frontages are in accordance with Figure 9: Central Precinct ground level frontages and Table 1: Central Precinct guidelines for ground level frontages.
2. Ground level frontages include measures such as:
  - a. positioning areas for respite and pause in locations that promote overlooking of publicly accessible spaces
  - b. incorporating large doors or windows into building lobbies and spaces
  - c. not locating activities that are sensitive to public view where they are directly overlooked by publicly accessible spaces
  - d. minimising and consolidating the extent of vents, mechanical plant and other operational requirements in areas that front onto publicly accessible spaces
  - e. fitting security grilles internally behind a shopfront that are fully retractable and at least 50% transparent when closed.
3. Ground level frontages are at the same level as the adjacent publicly accessible space, subject to existing flooding and heritage constraints.
4. Ground level frontages incorporate adequate protection for pedestrians from wind and rain through the use of elements such as cantilevered awnings.

5. Outdoor dining areas:
  - a. are in appropriate locations where they do not conflict with high volume pedestrian movement areas and have comfortable wind conditions.
  - b. have a layout and design that supports the role and characteristics of publicly accessible spaces and adjoining buildings.
6. Ground, levels 1 and 2 of building frontages are encouraged to include uses that provides activation during the day and night where appropriate

Table 1: Central Precinct guidelines for ground level frontages

Frontage	Guidelines
<b>Active frontage</b>	<ul style="list-style-type: none"> <li>• Primarily small units, many doors</li> <li>• Encourage multi-level uses and activation, with a large variation in function</li> <li>• Limit blank walls and few passive units</li> <li>• High quality materials and detailing, provide character in façade relief and provide primarily vertical facade articulation</li> <li>• Avoid blank walls or exhaust penetrations associated with station services or exhaust stacks</li> </ul>
<b>Prince Alfred Park frontage</b>	<ul style="list-style-type: none"> <li>• Carefully interface with park and active recreation uses.</li> <li>• Limit blank walls</li> </ul>
<b>Goods Line frontage</b>	<ul style="list-style-type: none"> <li>• To be activated, noting the level change and heritage constraints</li> <li>• Passive surveillance measures implemented</li> </ul>
<b>Goulburn Street site frontage</b>	<ul style="list-style-type: none"> <li>• Active frontages along Goulburn Street</li> <li>• Lobby access from Goulburn Street and Castlereagh Street</li> <li>• Retain rail infrastructure frontage along Castlereagh Street and Elizabeth Street where elevation change exposes existing rail corridor, provide active uses if possible</li> </ul>

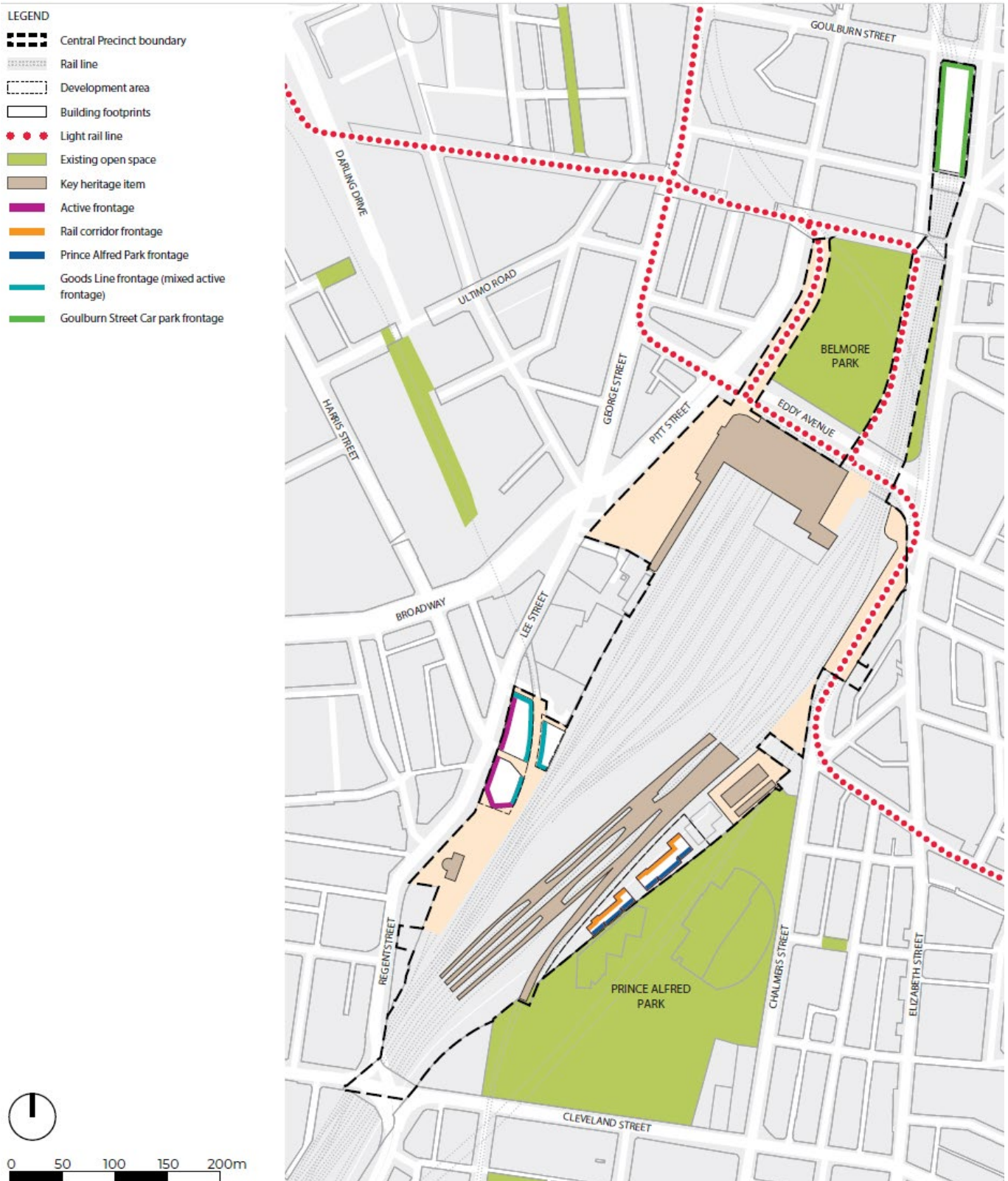


Figure 9: Central Precinct ground level frontages



---

## 7.5 Reflectivity

### Objectives

- a) To minimise the reflection of sunlight from buildings to surrounding areas and buildings.
- b) Ensure that building materials do not lead to hazardous, undesirable or uncomfortable glare to pedestrians, motorists or occupants of surrounding buildings.

### Guidance

- 1. A Reflectivity Report that analyses potential solar glare from the proposed building design may be required for tall buildings.
- 2. Light reflectivity from building materials used on facades is not to exceed 20%.

# 8 Amenity

This section outlines objectives and guidance to manage the impact of the proposed built form on the amenity in the public domain and publicly accessible spaces of the Precinct.

The objectives and guidance in this section are to be read in conjunction with the relevant principles set out in **Sections 2.0 and 3.0**.

## 8.1 Sunlight access

### Objectives

- a) To provide acceptable levels of sun access for publicly accessible open space in Central Precinct and its immediate surrounds relative to their intended use.
- b) To enable significant tree growth and during the period of the day when the public spaces are most used by the workforce, visitors, residents and the wider community.

### Guidance

1. Development provides sunlight access in accordance with Table 2.

Table 2: Central Precinct sunlight access

Public space	Sunlight access
Mortuary Station Garden	Direct sunlight to at least 50% of the area for a minimum of 2 hours from 9am to 3pm on 21 June
Eddy Avenue Plaza	Direct sunlight to at least 50% of the area for a minimum of 2 hours from 9am to 3pm on 21 June
Central Square	Direct sunlight to at least 50% of the area for a minimum of 4 hours from 9am to 3pm on 21 June
Belmore Park	As per the Belmore Park sun access planes identified in the Sydney LEP 2012
Prince Alfred Park	As per the Prince Alfred Park sun access planes identified in the Sydney LEP 2012

---

## 8.2 Wind safety and comfort

### Objectives

- a) To ensure development delivers safe and comfortable wind conditions in the publicly accessible space within the Precinct and in the surrounding public spaces.
- b) To ensure the cumulative impact of development creates a safe and comfortable wind environment.
- c) Wind comfort conditions are to deliver the intended primary use of the space.
- d) To ensure the cumulative impact of development on the wind environment does not result in unsafe or uncomfortable wind conditions within publicly accessible spaces and surrounds, taking into consideration the intended primary purpose of that space.

### Guidance

- 1. Development applications are accompanied by a Quantitative Wind Effects Report prepared by a suitably qualified and experienced person that addresses this section of the Design Guide.
- 2. A quantitative wind effects report must contain wind speed data:
  - a. for the existing built form, proposed built form, and any other options tested.
  - b. as tested at the least favourable locations.
  - c. as tested in areas such as bus stops, light rail stops, outdoor dining areas, major pedestrian crossings, building entries and other areas used by large numbers of people.
  - d. The wind comfort and wind safety conditions for each wind testing location.

**Note:** Wind testing locations are to be informed by previous wind reports.

- 3. Wind impacts from development on publicly accessible space or the public domain do not exceed the Wind Safety Standard.

**Note:** refer to Schedule 1 of this Design Guide for the definition of the Wind Safety Standard.

- 4. Wind impacts from development on publicly accessible space within Central Precinct achieves the Wind Comfort Standard shown in Table 3: Central Precinct target wind conditions taking into consideration the intended use and function of areas within the public space.

**Note:** refer to the Schedule 1 of this Design Guide for the definitions of the Wind Comfort Standard for walking, sitting and standing.

- 5. Despite Guidance 8.3 (0), the following Wind Comfort Standards apply to certain outdoor uses:
  - a. building entrances, bus stops, coach terminal stops, taxi ranks, and light rail stops – standing
- 6. Despite Guidance 8.3 (2), (3) and (0), wind impacts from development on the public space, or publicly accessible space may exceed the Wind Safety Standard or Wind Comfort Standard but only where:
  - a. it can be demonstrated that the existing wind speeds in affected locations exceed the standard(s).
  - b. development does not result in a meaningful increase in existing wind speeds as measured by the relevant standard(s).

7. Wind impacts from development on surrounding public open spaces outside of the Precinct including Prince Alfred Park, Belmore Park and Railway Square, are not substantially worse than existing wind conditions.
8. Any wind mitigation must be addressed at the detailed design stage and should consider the following:
  - a. be localised, well-designed and demonstrates that high quality outcomes can be achieved.
9. The following built form elements are to be considered to optimise wind conditions:
  - a. rounded corners on towers.
  - b. limiting the length of facades at exposed locations.
  - c. limiting the joining of tower floor plates to the first five floors above podium, to maintain permeability for wind flow.

Table 3: Central Precinct target wind comfort conditions

Location	Target
Central Square	<ul style="list-style-type: none"> <li>• Sitting in primary dwell areas</li> <li>• Minimum standing criteria to southern side of Central Square (adjacent to Central Walk circulation route)</li> <li>• Minimum standing criteria to all areas excluding circulation areas</li> <li>• Minimum walking criteria in all circulation/ movement areas</li> </ul>
Mortuary Station Gardens	<ul style="list-style-type: none"> <li>• Predominantly standing criteria</li> <li>• Localised sitting criteria in northern half of the gardens</li> </ul>
Eddy Avenue Plaza	<ul style="list-style-type: none"> <li>• Predominantly sitting criteria in primary dwell areas</li> <li>• Minimum standing criteria to all areas excluding circulation areas</li> <li>• Minimum walking criteria in all circulation/ movement areas</li> </ul>
Ibero-American Plaza	<ul style="list-style-type: none"> <li>• Minimum standing criteria</li> </ul>
Suburban platforms	<ul style="list-style-type: none"> <li>• Minimum standing criteria</li> </ul>
Belmore Park and Prince Alfred Park	<ul style="list-style-type: none"> <li>• No substantial change to existing wind conditions.</li> </ul>
Circulation/ movement area	<ul style="list-style-type: none"> <li>• Minimum walking criteria in all circulation/ movement areas not already defined in this table.</li> </ul>

---

## 8.3 Views and vistas

### Objectives

- a) To ensure development:
  - i. protects significant views to important heritage items from the public domain.
  - ii. mitigates the impact of built form from surrounding public areas.

### Guidance

1. Development protects the view to the Central Station Clocktower against the sky when viewed by pedestrians in the ground level public domain as shown in Figure 10: Central Precinct significant views.
2. Development is to mitigate the impact of new built form on the views identified in Figure 10: Central Precinct significant views.
3. A Visual Impact Assessment is to be provided with a development application for new buildings or significant alterations and additions.

- LEGEND**
- Central Precinct boundary
  - Rail line
  - Development area
  - Building footprints
  - Light rail line
  - Existing open space
  - Key heritage item
  - Publicly accessible space
  - ① Sydney Terminal and clock tower
  - ② Former Prince Alfred Sidings electric sub-station
  - ③ Mortuary Station
  - Ground level sight-line
  - Upper level sight-line
  - Public view protection
  - Clock tower

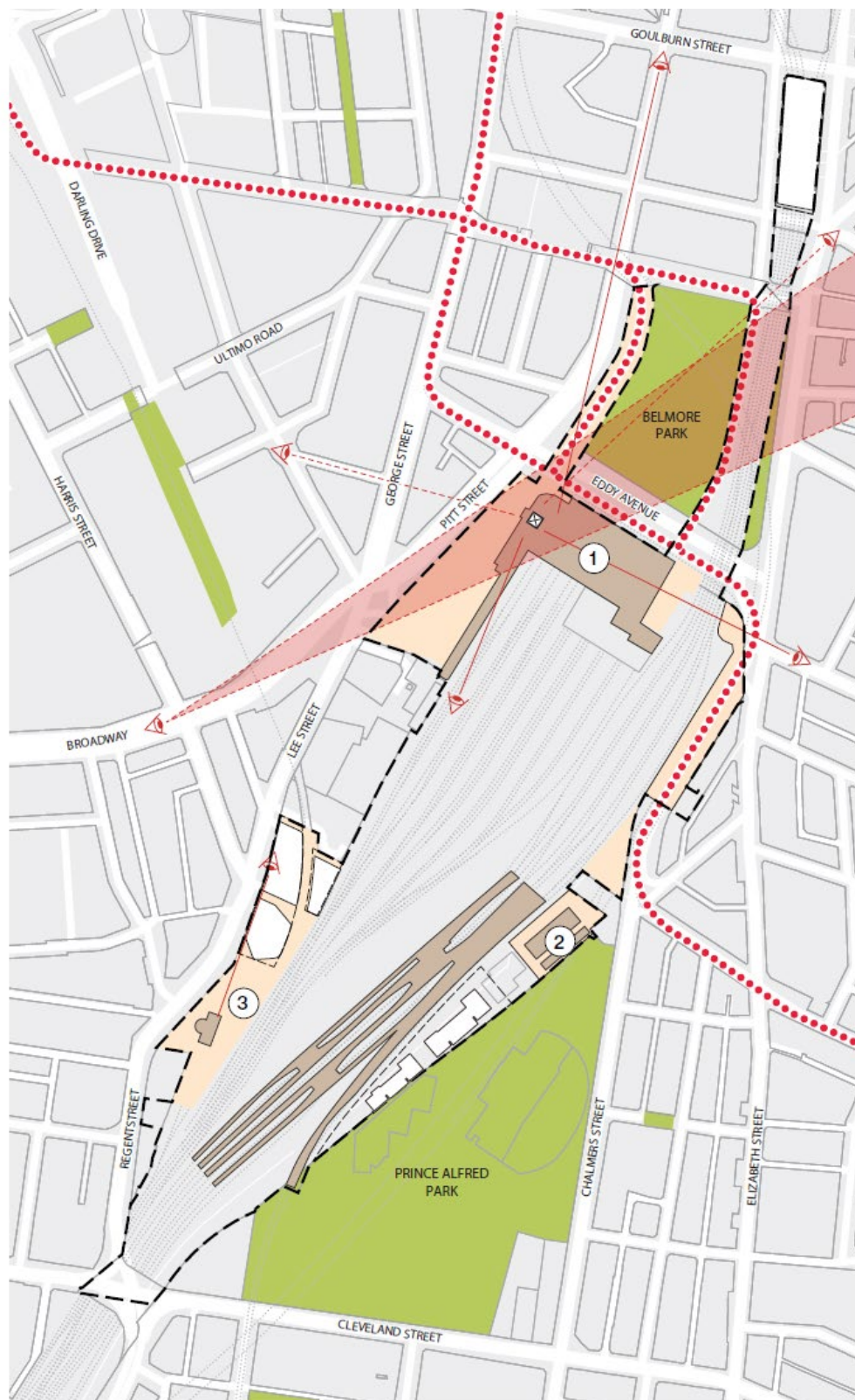


Figure 10: Central Precinct significant views



---

## 8.4 External lighting

### Objectives

- a) To ensure external lighting minimises light spill into the night sky.
- b) To support the character of the sub-precinct in which it is located.
- c) To positively contribute to high quality architecture.
- d) To ensure no reduction in existing residential amenity.
- e) To reduce greenhouse gas emissions and energy consumption.
- f) To ensure external lighting does not adversely affect local native fauna.

### Guidance

1. External lighting is in accordance with:
  - a. AS/NZS 4282:2019 Australian/New Zealand Standard, Control of the obtrusive effects of outdoor lighting.
  - b. City of Sydney's Public Domain Design Codes – Sydney Lights.
2. External lighting, including fixtures:
  - a. does not visibly intrude outside the building silhouette.
  - b. is integrated with the architecture of the building.
3. Any decorative lighting (permanent or temporary) does not include bud-lights and similar festoon lighting on buildings which detract from the architectural qualities of the building.
4. External lighting is located an appropriate distance from and is directed away from residential uses, in particular openings that permit light intrusion.
5. External lighting comprises LED down-lighting.
6. Any decorative lighting (permanent or temporary) does not include:
  - a. broad floodlighting of facades from large light sources located separate to the building
  - b. up lighting of flag poles and banner poles.
7. A night-time lighting masterplan is prepared as part of any future development application for new buildings and public domain areas, which considers:
  - a. safety and accessibility of publicly accessible areas during night-time.
  - b. the implementation of energy efficient lighting.
  - c. design measures for external lighting manage potential impacts to native fauna including nocturnal birds, bats, microbats and frogs, habitat and movement corridors.

---

## 8.5 Contamination

### Objective

- a) To ensure that the site is made suitable from an environmental and human health perspective for the proposed land uses, by incorporating remediation and management of contamination both on-site and the surrounding environment.

### Guidance

1. Development applications for change of use of existing building/s or construction of new building/s are supported by:
  - a. a Contamination Study and Remediation strategy demonstrating that contaminants can be reduced to a level appropriate for the proposed land use(s).
2. Planning for below ground construction activities requires a comprehensive Groundwater Risk Assessment prepared by a suitably qualified specialist and must outline the following:
  - a. pre-construction conditions.
  - b. predicted impacts.
  - c. proposed mitigation and reporting commitments during construction and post-construction
  - d. operational phase conditions.
3. Where works will require temporary dewatering construction, an evaluation of the discharge water quality and degree of potential impact the water may have on the environment, including all pollutants and water quality characteristics that may impact human health or environmental quality.

---

## 8.6 Noise and vibration

### Objective

- a) To ensure an appropriate level of amenity in relation to noise and vibration is provided for workers, visitors and residents.

### Guidance

1. A Noise and Vibration Impact Assessment is to be prepared by a suitably qualified acoustic consultant when submitting a development application for a new building in the Precinct.
2. The Noise and Vibration Impact Assessment is to consider and respond to noise and vibration impacts from the Sydney rail yard and adjacent suburban network, the surrounding road network, mechanical equipment and other systems, construction and other potential noise and vibration sources.
3. The repeatable maximum LAeq(1hour) for residential buildings and serviced apartments must not exceed the following levels:
  - a. 35dB for bedrooms (10pm-7am)

- b. 40dB anywhere else in the residential accommodation (other than a garage, kitchen, bathroom or hallway) at anytime.

**Note:** Residential apartments are to be in accordance with Section 4B – Natural ventilation and Section 4J – Noise and pollution of the ‘Apartment Design Guide’ (Department of Planning and Environment, July 2015).

- 4. Internal noise levels and vibration for non-residential uses are to comply with AS2107 2016: recommended design sound levels and reverberation times for non-residential uses.
- 5. To limit the transmission of noise to and between dwellings, all floors are to have a weighted standardised impact sound level ( $L'_{nT,w}$ ) less than or equal to 55 where the floor separates a habitable room and another habitable room, bathroom, toilet, laundry, kitchen, plant room, stairway, public corridor, hallway and the like.
- 6. The overall design and layout of dwellings, where appropriate, is to include the following noise attenuation measures:
  - a. a limit on window size and number where oriented towards an intrusive noise source
  - b. seals at entry doors to reduce noise transmission from common corridors or outside the building.
  - c. minimisation of the number of shared walls with other dwelling units.
  - d. storage, circulation areas, and non habitable rooms to buffer noise from external sources
  - e. double or acoustic glazing.
  - f. operable acoustic screens to balconies.
- 7. A Construction Noise and Vibration Management Plan is to be prepared by a suitably qualified acoustic consultant when submitting a development application for a new building in the Precinct. The management plan is to detail:
  - a. the relevant noise and vibration criteria.
  - b. the planned hours of work.
  - c. any significant works that will take place at these times, and any that are likely to exceed noise and vibration criteria.
  - d. what steps will be taken to mitigate this, including reducing noise levels.

# 9 Design Excellence Strategy

This section contains the Design Excellence Strategy for Central Precinct and provides objectives and guidance to guide design excellence requirements at Central Precinct.

## Objectives

- a) To establish a methodology and framework for a collaborative design process that ensures new development exhibits design excellence in accordance with the precinct-wide Design Excellence Strategy for Central Precinct.
- b) Deliver the highest standard of architectural, urban and landscape design having regard to the design excellence considerations of the Sydney LEP 2012 (Clause 6.21C).
- c) Ensure high quality and varied design by using competitive design processes for sites within Central Precinct.
- d) Ensure development delivers variety in architectural design across the precinct.
- e) Encourage involvement of Indigenous architects and designers throughout the design excellence process, including in competitors and juries.
- f) Ensure Connecting with Country is embedded in the design excellence process.
- g) Establish a design excellence governance framework to ensure the delivery of the vision for Central Precinct and to ensure coordination between project stages, buildings and the public domain.

## Guidance

- 1. Buildings and publicly accessible spaces within Central Precinct are to exhibit design excellence in accordance with this Strategy and Sydney LEP 2012.
- 2. All competitive design processes within Central Precinct are to be undertaken in accordance with the City of Sydney Competitive Design Policy.
- 3. Additional floor space may be awarded to a building through a design excellence process (refer Part 8 of Sydney LEP 2012). No additional building height is to be awarded as a result of a design excellence process.
- 4. A competitive design process is to be undertaken for each competitive design process site shown in Figure 11: Design excellence process requirements for sites across Central Precinct.
- 5. For each competition, a minimum of five (5) competitors/ design teams must participate, with each design team to include an established and emerging architectural practice
- 6. An EOI design led procurement is to be undertaken for the Key Places sites shown in 'green' in Figure 11: Design excellence process requirements for sites across Central Precinct. Further diversity and vibrancy within the public domain will be delivered through collaboration with artists, responding to the Public Art Strategy. The terms of the EOI will be developed in

consultation with the Consent Authority, TfNSW, City of Sydney and agreement of the NSW Government Architect.

7. The DPHI Design Competition Guidelines, 2023 (DCG) provides additional guidance in specific areas relating to Jury composition and Design Integrity Processes.
  - a. Jury – The jury will be endorsed by the Government Architect NSW, with either three (3) or five (5) member jury composition in accordance with DCG.
  - b. Design Integrity Process – All post competition design integrity processes will be run in accordance with the DCG.
8. Design Governance - In recognition of the complexity, scale and longevity of the renewal program for Central Precinct, TfNSW must establish a Design Excellence Governance Framework to ensure the delivery of the renewal vision. This framework will provide the decision support, expert guidance to ensure the delivery of design excellence for the development. The framework will be developed by TfNSW in consultation with the selected delivery partner, Consent Authority, GANSW and City of Sydney and agreement of the NSW Government Architect. The framework must be finalised and in place prior to the first design excellence competition. The Framework is to consist of the following:
  - a. Design Governance Panel – A panel that provides oversight over the entire design process for the Central Precinct.
  - b. Selection Panel / Design Competition Jury – A panel of design and industry experts that oversee and judge a Design Competition Process for buildings subject to a competitive design process.
9. An observer from the City of Sydney, GANSW, DPHI, TfNSW and proponent shall be invited to observe all competitive process mid-point reviews, jury presentation day proceedings including jury deliberations, and the design integrity process.
10. This Design Excellence Strategy may only be varied, subject to consultation with the Consent Authority, TfNSW, City of Sydney and agreement of the NSW Government Architect.
11. Building and public space design is to be guided by Central Precinct Connecting with Country Framework (Balarinji, 2023).
12. Aboriginal involvement throughout the design excellence process is encouraged and may include (but not limited to):
  - a. Traditional Custodian/Knowledge-holder engagement
  - b. First Nations community engagement, particularly those with connections to the area
  - c. First Nations artists and designers (interpretive in built form, landscape and artworks)
  - d. Other key First Nations stakeholder business owners and community leaders.

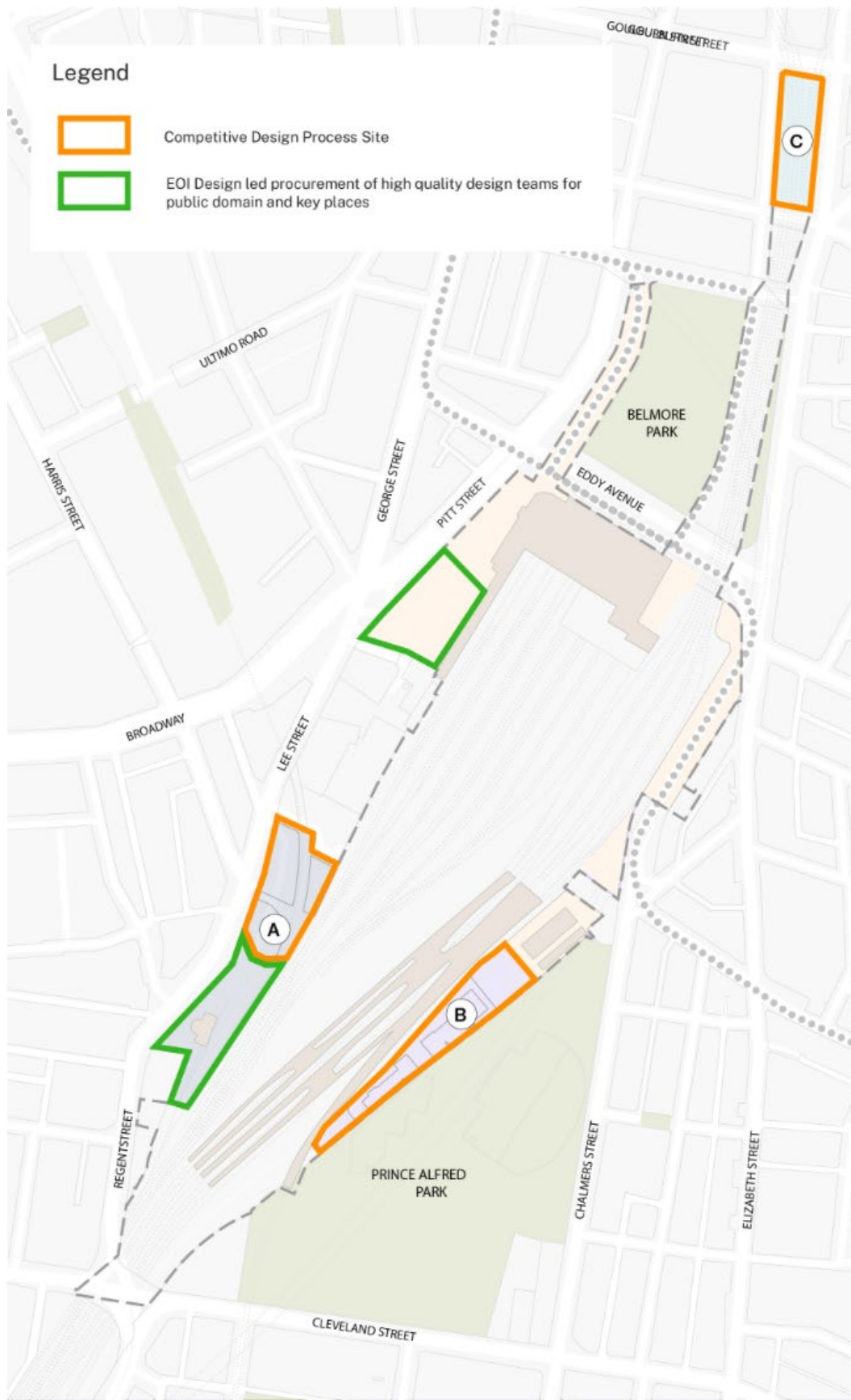


Figure 11: Design excellence process requirements for sites across Central Precinct

# 10 Public Space

The objectives and guidance in this section describe design outcomes and requirements for the treatment of public spaces within Central Precinct. This covers the type and configuration of existing and proposed public spaces in the precinct.

All public spaces within the precinct will remain in the ownership of Transport for NSW and will be managed by either Transport for NSW or the relevant leaseholder. Public domain elements outside the precinct are typically managed by the City of Sydney.

The objectives and guidance in this section are to be read in conjunction with the relevant principles set out in **Sections 2.0 and 3.0**.

---

## 10.1 Public Open Space - General

### Objectives

- a) To provide an integrated, functional, comfortable and attractive network of open space with movement and place functions that:
  - i. provide a cohesive yet multi-functional public domain that includes opportunities for passive recreation, active recreation and informal interaction during day and night
  - ii. conserve and enhance the heritage context of Central Precinct
  - iii. seamlessly connect with the adjoining public domain outside Central Precinct
  - iv. maximise safety for all at all times of the day and night
  - v. encourage Aboriginal inclusion, visibility comfort and access
  - vi. encourage universal access to facilitate creation of democratic public space where people can openly participate in public life
  - vii. reflect the highest standards of design consistent with the role of Central Precinct as Sydney's main transport hub.









### Guidance

- 1. Open space are provided in accordance with Figure 12: Central Precinct . Open spaces are designed to:
  - a. be publicly accessible 24 hours a day and 365 days of the year
  - b. be safe and accessible to all
  - c. provide clear lines of sight



- d. be easily identified by users and include wayfinding signage advising of the publicly accessible status of links and the places to which they connect
  - e. support a diversity of programming across the precinct, including the provision of spaces that encourage collaboration and information sharing.
2. Development applications that include new publicly accessible spaces are accompanied by a Public Domain Plan that demonstrates consideration of the Central Precinct Public Domain Strategy, and is to provide detail on:
- a. trees and other vegetation
  - b. paving and other hard surfaces
  - c. lighting
  - d. seating
  - e. bicycle parking spaces for share bikes and visitors
  - f. bins
  - g. signage, including wayfinding signs
  - h. public art
  - i. heritage interpretation
  - j. smart places technology.
3. Development applications are to be supported by a 'Wayfinding Strategy' that:
- a. enables pedestrians and cyclists to find their way with ease and confidence
  - b. applies a consistent design for signage elements
  - c. reinforces perceptions of safety and legibility
  - d. clearly defines the uses of a building
  - e. is accessible to people with a vision impairment or persons who speak a language other than English
  - f. incorporates the use of local traditional language or implements dual naming
  - g. considers the Legible Sydney Design Manual (CoS, 2019).

**Note:** for Aboriginal naming and dual naming, the proponent is to consult with the NSW Geographical Names Board, local language subject matter experts and with Aboriginal stakeholder groups (refer Section 4.3).

- LEGEND
-  Central Precinct boundary
  -  Rail line
  -  Development area
  -  Building footprints
  -  Light rail line
  -  Existing open space
  -  Key heritage item
  -  Publicly accessible space

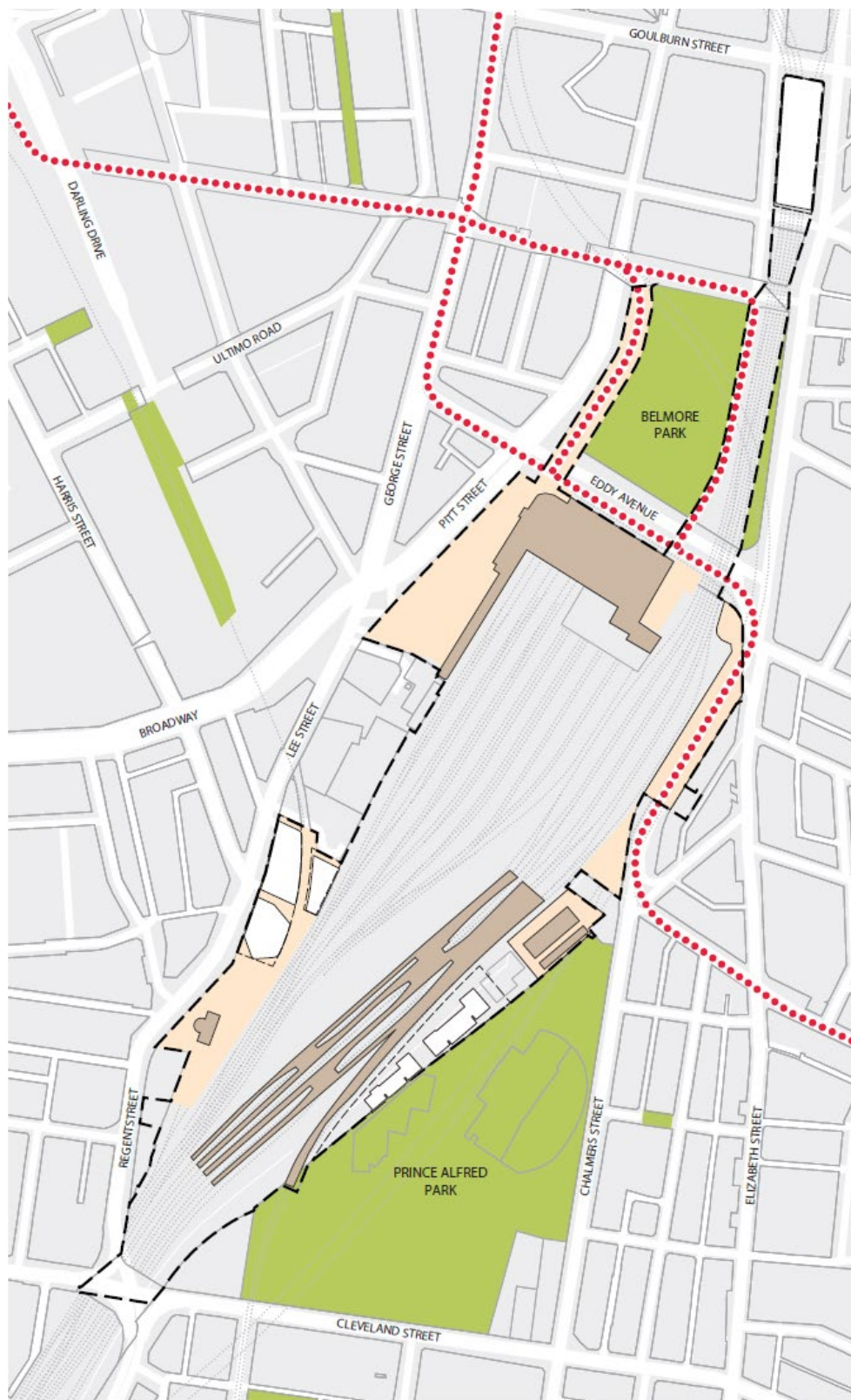


Figure 12: Central Precinct Public Open Space

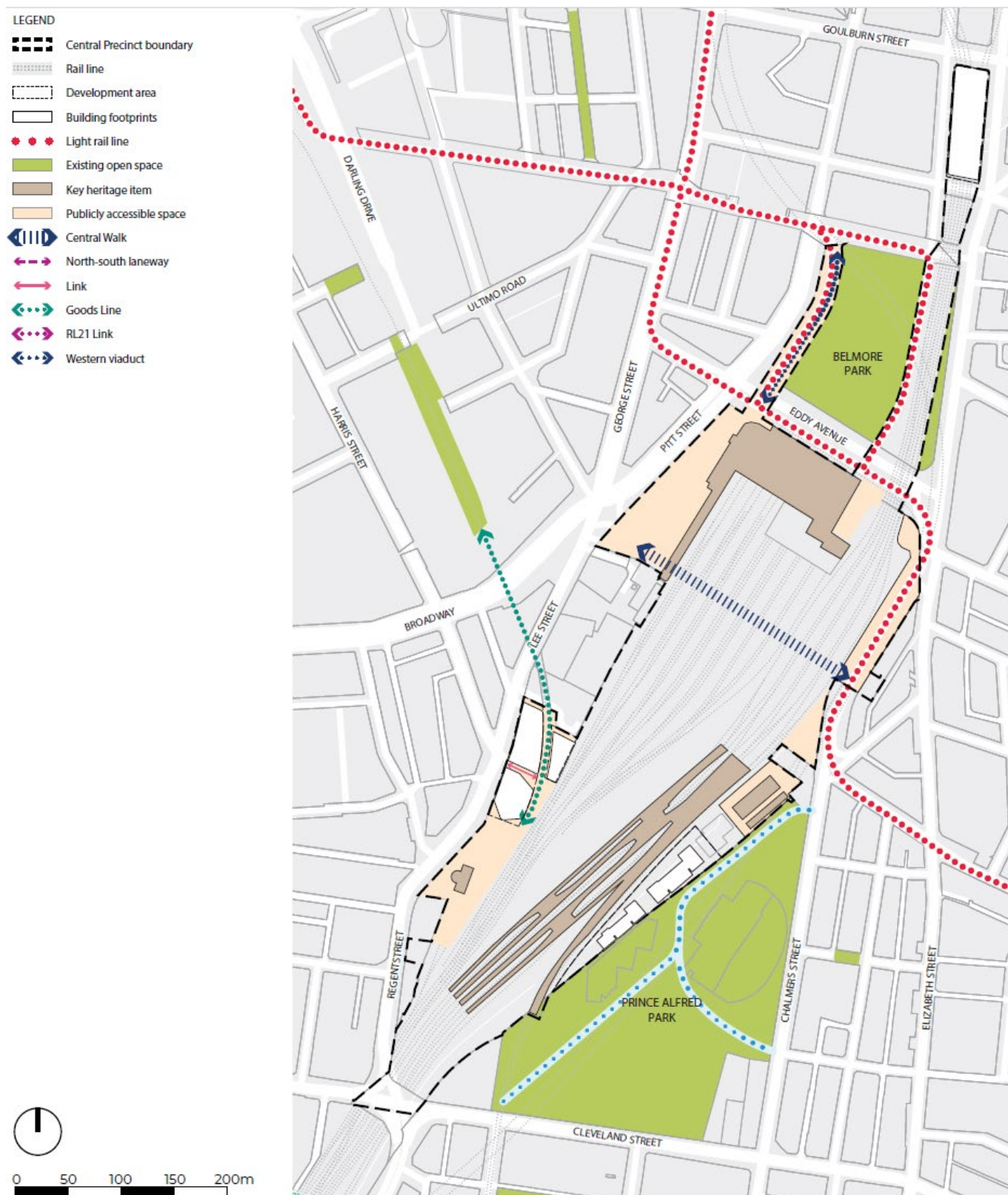


Figure 13: Central Precinct pedestrian network

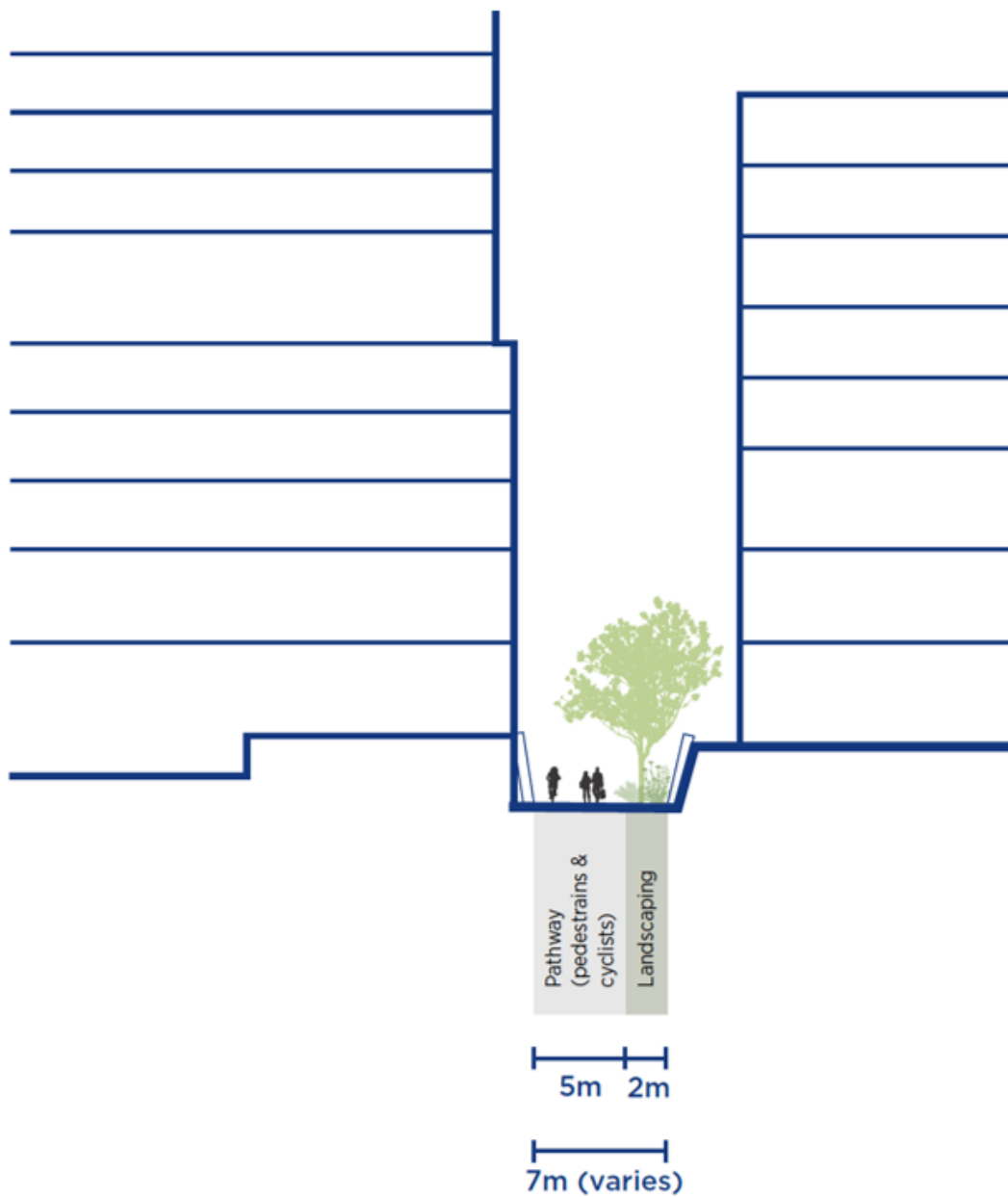


Figure 14: Typical cross-section, Goods Line

## 10.2 Publicly accessible space

### Objectives

- a) To provide cohesive and multi-functional open spaces each with a clear and distinct function.



- b) To ensure that open space is strategically located to assist with WSUD and stormwater management.
- c) To provide an integrated open space network that:
  - i. caters for the forecast recreation needs of Central Precinct
  - ii. provides for a range of appropriate recreation activities
  - iii. has suitable solar access and comfortable wind conditions for the intended uses
  - iv. supports an increase of urban canopy and greening
  - v. conserves and enhances the heritage context heritage interpretation and public art
  - vi. integrates with Prince Alfred Park
  - vii. facilitates the delivery of Central Square
- d) To provide a pleasant and safe environment for the enjoyment of workers, residents, students and visitors, which encourages interaction and improves the amenity of the area for through provision of solar access and mitigation of noise in the public domain.

## Guidance

1. The open space network:
  - a. is in accordance with Figure 15: Central Precinct publicly accessible open space, plazas
  - b. provides seating in accordance with the City of Sydney's public domain policies and codes.
2. The layout and design of the open space network is in accordance with Table 5: Central Precinct guidance for open space areas.
3. The open space network is to include public art, including interactive elements where appropriate.
4. Where shown as an 'Edge Improvement' in Figure 15: Central Precinct publicly accessible open space, plazas, development in the Prince Alfred Sidings Sub-precinct:
  - a. provides a new ground plane at the sub-precinct's edge at the same level as that part of Prince Alfred Park
  - b. provides co-ordinated paving, vegetation and street furniture
  - c. includes measures to ensure high quality amenity and safety outcomes for users of the recreation facilities and people who may be impacted by the recreation facilities, including appropriate noise mitigation, lighting, signage, hours of operation, security and consistency with Crime Prevention Through Environmental Design principles.

**Note:** The design of this edge treatment is subject to collaboration with and the approval of the City of Sydney Council and the managers of Prince Alfred Park.

Table 4: Central Precinct guidance for pedestrian network

Name and type	Requirements	Guidance
Link (Regent Street Sidings)	<p><b>Width (min)</b></p> <ul style="list-style-type: none"> <li>6m for pedestrians</li> </ul> <p><b>Height</b></p> <ul style="list-style-type: none"> <li>Open to the sky</li> </ul> <p><b>Location</b></p> <ul style="list-style-type: none"> <li>As per Figure 13: Central Precinct pedestrian network</li> </ul>	<ul style="list-style-type: none"> <li>Designed to have a clear line of sight between publicly accessible spaces</li> <li>Provide a visible connection to Regent Street.</li> </ul>
Goods Line	<p><b>Width (min)</b></p> <ul style="list-style-type: none"> <li>7m, including: <ul style="list-style-type: none"> <li>Shared path for pedestrians and cyclists – 5m</li> <li>Landscaping zone – 2m</li> <li>As per Figure 14</li> </ul> </li> </ul> <p><b>Height (min)</b></p> <ul style="list-style-type: none"> <li>Partially open to the sky outside of the Goods Line tunnel section</li> </ul> <p><b>Location</b></p> <ul style="list-style-type: none"> <li>As per Figure 13: Central Precinct pedestrian network</li> </ul>	<ul style="list-style-type: none"> <li>Primarily a movement space for pedestrians and cyclists to connect to Mortuary Station Gardens, and to the existing Goods Line and Powerhouse Museum</li> <li>Activated edge with small-scaled retail uses and building entries</li> <li>Conserve and enhance the heritage fabric and context of the oldest rail infrastructure of Central Precinct</li> <li>Provides opportunities for activation and interpretation using lighting and public art</li> <li>Prioritises safe pedestrian/ cycle movement and designed to mitigate potential user conflict between cyclists and pedestrians.</li> </ul>



Figure 15: Central Precinct publicly accessible open space, plazas and spaces



Table 5: Central Precinct guidance for open space areas

Name and type	Guidance
<p><b>Central Square</b></p> <p><b>Area (min):</b> 7,000 sqm</p>	<p><b>Character</b></p> <ul style="list-style-type: none"> <li>• A major transport plaza connected to the new station entrance and exit at Central Walk.</li> <li>• Caters for pedestrian desire lines, commuter demands, large volume pedestrian flows and events.</li> <li>• Integrates a terraced landscaped landform that supports significant tree canopy and extends the City street tree planting into the southern half to offer shade.</li> <li>• Provides spaces for groups of different sizes to dwell comfortably.</li> <li>• Includes spaces scaled to enable programming and activation for a diverse range of public uses including events, and considers the usage of the space in relation to other open space including Belmore Park, Railway Square, the Grand Concourse and Central Green.</li> <li>• Includes a major public artwork to engage the multitudes of people who move through the space each day.</li> </ul> <p><b>Heritage</b></p> <ul style="list-style-type: none"> <li>• Retains and enhances views to the clocktower from Broadway looking north and keeping the centre of the square free of trees to maintain these views.</li> <li>• Interprets the former heritage gardens in this area in the design of new landscape.</li> <li>• Consider the original flora of the site including species found on sandstone ridges and in gullies.</li> </ul> <p><b>Amenity (also refer to Section 8 - Amenity)</b></p> <ul style="list-style-type: none"> <li>• Is a comfortable place to move through as part of a large crowd.</li> <li>• Provides places to linger, meeting points and a defensible space to sit.</li> <li>• Provides a suitable level of shade provided through tree canopy.</li> <li>• Is of a suitable scale for event use.</li> </ul>
<p><b>Mortuary Station Gardens</b></p> <p><b>Area (min):</b> 6,500 sqm excluding station building</p>	<p><b>Character</b></p> <ul style="list-style-type: none"> <li>• Provides public access from Regent Street.</li> <li>• Provides opportunities to view Mortuary Station and gardens from Regent Street.</li> <li>• Extends public access via the Goods Line, providing a new connection to Darling Harbour.</li> <li>• Provide opportunities for recreation i.e. children's wild play area.</li> </ul> <p><b>Heritage</b></p> <ul style="list-style-type: none"> <li>• The original fabric of Mortuary Station will inform the future use, adaptation and approach to this open space.</li> <li>• Maintain sufficient space for the buildings, elements and rail use to be understood and seen without development encroaching on the setting.</li> <li>• Enables continued public engagement and public access to heritage steam trains and Transport heritage programs.</li> </ul>

Name and type	Guidance
	<ul style="list-style-type: none"> <li>Integrates heritage interpretation that celebrates the rail heritage of the location and Mortuary Station's role in the early Redfern Station development and overlays other interpretive themes, e.g. the physical connection to the Rookwood Cemetery landscape.</li> <li>Explores opportunities for the sensitive and compatible adaptive re-use of the station building, activation and public use, through links and a direct interface between the public and rail history.</li> </ul> <p><b>Amenity (also refer to Section 8 - Amenity)</b></p> <ul style="list-style-type: none"> <li>Provides comfortable space to dwell all year round, good sight lines and pedestrian safety.</li> <li>Provides a space that creates a cool, green setting including the introduction of canopy trees on the rail side of Mortuary Station.</li> <li>Retain the existing trees and garden to the Regent Street frontage and the mature figs to the north of the station.</li> <li>The podium interface with the northern edge of the Gardens includes opportunities for small scale cafe to assist with activation.</li> </ul>
<p><b>Eddy Avenue Plaza</b></p> <p><b>Area:</b> As per current dimensions</p>	<p><b>Character</b></p> <ul style="list-style-type: none"> <li>A key pedestrian connection from Central Station north to the CBD.</li> <li>Maintains a clear and open path of movement between Eddy Avenue and the station entrance.</li> </ul> <p><b>Heritage</b></p> <ul style="list-style-type: none"> <li>Conserves and enhances the heritage setting of the historic pedestrian link between Central Station, Eddy Avenue and Belmore Park.</li> <li>Explores opportunities for meaningful heritage interpretation related to the plaza and its role in Central Station group, e.g. the historic market gardens and the electrification of the train network.</li> </ul>
<p><b>Ibero-American Plaza</b></p> <p><b>Area:</b> As per current dimensions</p>	<p><b>Character</b></p> <ul style="list-style-type: none"> <li>Provides an arrival space to Central Station from the east.</li> <li>Important station entry to the southern concourse, Devonshire Street tunnel and provides vertical transport to the Devonshire Bridge.</li> <li>Maintain recent upgrades including enlarged pedestrian areas, new bike lanes, planting areas and trees.</li> <li>Maintains the visual prominence of the heritage listed Railway Institute Building from the plaza.</li> <li>Incorporates clear wayfinding signage to access multiple transport modes (light rail, heavy rail, metro, buses), legible paths of movement and clear entry and exit points.</li> <li>Retains existing public art installations.</li> </ul> <p><b>Amenity (also refer to Section 8 - Amenity)</b></p> <ul style="list-style-type: none"> <li>Provide seating areas to support role as a key meeting point location.</li> <li>Provides opportunity for increased tree canopy.</li> </ul>

---

## 10.3 Public art

### Objectives

- a) Ensure that public art is an integrated and cohesive part of new development in the public domain and within private developments.
- b) Deliver public art that is responsive to Aboriginal culture and Country.
- c) Ensure an overarching conceptual approach/ curatorial rationale for the selection, commissioning and delivery of public art.
- d) Deliver essential infrastructure in creative and innovative ways through the use of public art.
- e) Recognise former uses through interpretive public art.

### Guidance

1. Development applications that involve the creation of new publicly accessible space and/ or buildings are accompanied by a Public Art Plan that:
  - a. responds to the five curatorial frameworks outlined in Schedule 4: Public Art Curatorial Framework
  - b. includes an overarching conceptual approach / curatorial rationale for the selection, commissioning and delivery of public art as part of future development applications
  - c. ensures the strategic intent, vision, artistic integrity and quality of all public artworks is maintained throughout the process
  - d. integrates with Connecting with Country and heritage interpretation measures (refer Section 4 - Connecting with Country and Section 5.3 - Heritage Interpretation
  - e. provides a variety of media types, ranging from small or ephemeral to large or permanent and media specifically created to be experienced in public spaces.
2. Consult with the City of Sydney Public Art Advisory Panel as part of the preparation of the Public Art Plan.
3. Public Art is to be in accordance with the City of Sydney's Public Art Strategy, Public Art Policy, Guidelines for Public Art in Private developments and Guidelines for Acquisitions and Deaccessions.
4. Where a development proposal has identified the opportunity to deliver public art that is responsive to culture and Country, an Indigenous curator or Indigenous cultural consultant is engaged to:
  - a. develop and facilitate a co-design process to be undertaken with Aboriginal communities
  - b. provide input into the preparation of the public art brief
  - c. contribute to the design of the public art.
5. The procurement and engagement of Aboriginal artists recognises the Indigenous Cultural and Intellectual Property (ICIP) protocols.

# 11 Transport

This section of the Design Guide contains provisions for managing vehicle access, servicing, waste, cycle network and parking needs for Central Precinct.

The objectives and guidance in this section are to be read in conjunction with the relevant principles set out in **Sections 2.0 and 3.0**.

---

## 11.1 Vehicle access

### Objectives

To provide co-ordinated, limited and managed vehicle access that ensures pedestrian priority within the public domain

### Guidance

1. Vehicle access is provided in accordance with Figure 16: Central Precinct vehicle access



Figure 16: Central Precinct vehicle access

---

## 11.2 Footpaths

### Objectives

- a) To ensure the capacity of existing footpaths and intersections surrounding Central Precinct.
- b) To identify and contribute to footpaths and intersections requiring improvements to increase pedestrian movement capacity.

### Guidance

- 1. Development applications for new buildings:
  - a. assess the impact of walking trips generated by the development on the capacity of pedestrian footpaths and intersections within and surrounding Central Precinct
  - b. identify and contribute to improvements and upgrades required to support additional pedestrian capacity to accommodate development.

---

## 11.3 Vehicle access and footpath crossings

### Objective

- a) To ensure vehicle access across footpaths and pedestrian throughfares provides for the safety and comfort of pedestrians and cyclists.

### Guidance

- 1. Development applications are to include a Transport Impact Assessment to address the potential impact of the development on the surrounding movement systems and to ensure appropriate safety measures that reinforce pedestrian priority are provided, where:
  - a. vehicle access to a building and/or an integrated basement is proposed across a major pedestrian thoroughfare or footpath
  - b. the proposed development is in the opinion of the consent authority, likely to generate significant traffic impacts.
- 2. Vehicle access and circulation is designed to:
  - a. cross footpaths at footpath level
  - b. prevent vehicles from queuing or reversing across pedestrian crossings or footpaths
  - c. minimise impact on existing street trees and to maximise opportunities for new street tree plantings
  - d. continue the type of footpath material and grade as publicly accessible spaces.
- 3. Access is designed to avoid reversing movements into or out of a public street, footpath or pedestrian area outside of the Central Precinct.
- 4. Vehicular access and circulation design is to integrate with the primary façade design and materiality.

---

## 11.4 Bus access

### Objective

- a) To retain Central Precinct's role as a major transport interchange, while providing capability to accommodate appropriate alternative uses, including at Central Square and the Regent Street Sidings Sub-precinct (including the Lee Street bus layover) over the longer term.

### Guidance

- 1. Development provides for a coach facility catering for intercity and interstate travel on the upper level of the western forecourt adjacent to the main western entrance to the Grand Concourse. The coach facility is to provide coach parking bays and bus shelters.
- 2. The Lee Street bus layover is to be either relocated into a basement (on-site) or to another location (off-site)

---

## 11.5 Vehicle parking rates

### Objective

- a) To ensure vehicle and servicing parking is provided at a rate consistent with Central Precinct's excellent levels of public transport.
- b) Deliver limited car parking across the precinct to support sustainable transport behaviour.
- c) To ensure provision is made for accessible car parking.

### Guidance

- 1. On-site car parking is provided in accordance with the maximum rates as outlined in Table 6.

Table 6: Central Precinct maximum on site car parking rates

Land use		Rate
Residential	1 bed dwelling	0.3 spaces / dwelling
	2 bed dwelling	0.7 spaces / dwelling
	3 bed dwelling	1 spaces / dwelling
Student accommodation		0.1 spaces per room
Hotel		0.1 spaces per room
Commercial		1 space per 2,000 sqm GFA
Retail		1 space per 2,000 sqm GFA
Education		1 space per 2,000 sqm GFA
Community		1 space per 2,000 sqm GFA



2. Where development comprises a land use not specified in Table 6: Central Precinct maximum on site car parking rates, on-site carparking is provided at a rate justified by a Parking and Access Report.
3. Development proposing less than the maximum number of on-site carparking is to adjust the number of visitor parking spaces in accordance with the reduction of total car parking spaces.
4. For buildings that provide onsite parking:
  - a. 1 motorcycle parking space for every 12 car parking spaces is provided as separate parking for motorcycles
  - b. Each motorcycle parking space is designated and located so that parked motorcycles are not vulnerable to being struck by a manoeuvring vehicle.
5. Provision is made within the basement design for charging stations for all visitor, occupant, servicing and car share spaces.
6. Accessible car parking spaces are to be provided and included in the number of car parking spaces permitted at a rate of:
  - a. 1 accessible car parking space for every adaptable residential unit
  - b. 2 spaces for every 20 residential car parking spaces or part thereof to be allocated as accessible visitor parking
  - c. 1 accessible car parking space for every 100 non-residential car parking spaces.

---

## 11.6 Car share and point-to-point services

### Objective

- a) To ensure car share schemes are provided to encourage the reduction of car ownership within Central Precinct.

### Guidance

1. A minimum of 1 car share scheme space per 25 on-site car parking spaces is made available.
2. Car share parking spaces are provided in addition to the maximum number of car parking spaces permitted in the development.
3. All car share parking spaces are:
  - a. publicly accessible 24 hours a day seven days a week
  - b. located together in the most convenient locations relative to car parking entrances and pedestrian lifts or access points
  - c. have access to and from a public road
  - d. located in well-lit spaces that allow for casual surveillance
  - e. clearly designated by signs as being for car share scheme use and which indicate the availability of the scheme to building occupants.

4. The location of drop-off and pick areas for point-to-point services (i.e. taxis and ride share) is in accordance with Figure 16: Central Precinct vehicle access.

---

## 11.7 Location and design of private and visitor vehicle parking

### Objectives

- a) To ensure the location and design of car parking achieves a high quality ground level relationship between buildings and all public domain interfaces.

### Guidance

1. On-site car parking is provided underground in basement levels and in accordance with the relevant Australian Standards for off street parking.
2. On-site car parking for visitors is:
  - a. grouped together in convenient locations relative to car parking area entrances, pedestrian lifts and access points
  - b. separately marked and clearly sign-posted
  - c. appropriately accessible, including arrangements for access into a secure area if proposed.
3. Basement parking areas and structures do not protrude above the level of the adjacent street or public domain.
4. Vehicle ramps are not visible from the public domain and are located inside the building.
5. Car parking areas are:
  - a. well lit, visible, and avoid hidden and enclosed areas to allow for casual surveillance
  - b. include mirrors or similar devices where hidden and enclosed areas such as staircases and lift lobbies cannot be avoided
  - c. well ventilated and provide natural rather than mechanical ventilation where practicable
  - d. subordinate in appearance to the main building.
6. Car parking areas are:
7. Car parking spaces are not located in areas used for the manoeuvring of service vehicles.
8. Minimal parking is to be provided at the Prince Alfred Sidings sub-precinct to reduce impact on the surrounding road network.

## 11.8 Servicing strategy and emergency access

### Objectives

- a) To provide a servicing strategy that delivers a high level of functionality and enables responsiveness to new and emerging practices, including those that contribute to carbon reduction outcomes.
- b) To ensure appropriate provision for emergency service vehicles.

### Guidance

1. Development applications are to be accompanied by an Integrated Delivery and Servicing Management Plan demonstrating how the development will be serviced and include details on:
  - a. ongoing servicing of Central Station and associated rail infrastructure
  - b. operation of freight and logistics
  - c. parking and servicing requirements
  - d. the method and frequency of transfer to waste between the development and collection area
2. The provision of service vehicle parking is in accordance with the rates in Table 7.

Table 7: Central Precinct loading and service vehicle parking rates

Land Use	Rate
Residential	<ul style="list-style-type: none"><li>1 space for the first 50 dwellings or serviced apartments; plus</li><li>0.5 spaces for every 50 dwellings/ serviced apartments or part thereafter.</li></ul>
Commercial	<ul style="list-style-type: none"><li>1 space per 3,300 sqm GFA, or part thereof, for the first 50,000 sqm; plus</li><li>1 space per 6,600 sqm, or part thereof, for additional floor area over 50,000 sqm and under 100,000 sqm; plus</li><li>1 space per 13,200 sqm, or part thereof, for additional floor area over 100,000 sqm.</li></ul>
Hotel	<ul style="list-style-type: none"><li>1 space per 50 hotel bedrooms, or part thereof, up to 100 bedrooms; then</li><li>1 space per 100 hotel bedrooms; plus</li><li>1 space per 400 sqm of reception, lounge, bar and restaurant area GFA, or part thereof, for the first 2,000 sqm; then</li><li>1 space per 8000 sqm of reception, lounge, bar and restaurant area GFA thereafter.</li></ul>
Retail	<ul style="list-style-type: none"><li>1 space per 350 sqm GFA, or part thereof, up to 2,000 sqm; then</li><li>1 space per 800 sqm GFA thereafter.</li></ul>

3. Freight infrastructure is consolidated and supports emerging transitions in the freight network, including the electrification of logistics systems.
4. Private vehicle parking is to be provided only once the minimum servicing provisions are met.
5. All development is to implement adequate access arrangements for emergency vehicles and consultation with relevant emergency services is to be undertaken.

---

## 11.9 Service vehicle parking and loading

### Objective

- a) To ensure the location and design of service vehicle parking supports functional and efficient movement of service vehicles and sufficient space for loading and unloading.

### Guidance

1. Service vehicle parking spaces, including spaces for bike couriers are:
  - a. located completely within the boundary of the precinct, clear of parked vehicles and clear of through traffic
  - b. located near vehicle entry points and/or near lifts
  - c. clearly designated and signposted for service vehicles only.
2. Parking spaces for service vehicles may be shared with parking for other purposes, subject to a management plan being prepared that identifies operational procedures to manage the shared use of parking spaces.
3. Each service core has a medium rigid vehicle loading bay immediately adjacent with adequate vertical clearance for the medium rigid vehicle along the path of travel leading to the bay.
4. Minimal servicing bays are provided at the Prince Alfred Sidings sub-precinct to reduce impacts on the surrounding road network.

---

## 11.10 Waste collection and loading

### Objective

- a) To enable the safe and efficient collection and transport of waste and recycling from the precinct.

### Guidance

1. Waste collection and loading is wholly accommodated in the building's basement.
2. Any waste collection and loading point is designed to:
  - a. allow waste collection and loading operations to occur on a level surface away from vehicle ramps
  - b. provide sufficient side and vertical clearance to allow the lifting arc for automated bin lifters to remain clear of any walls or ceilings and all ducts, pipes and other services
3. Vehicle access for waste collection and loading provides for:
  - a. sufficient clearance and circulation area for waste and loading vehicles
  - b. minimum vertical clearance of 4.0 metres clear of all ducts, pipes and other services, depending on the gradient of the access and the type of collection or loading vehicle
  - c. collection vehicles to enter and exit the premises in a forward direction. Where a vehicle turntable is necessary to meet this requirement, it has a capacity of 30 tonnes

- d. maximum grades of 1:20 for the first 6m from the street, then a maximum of 1:8 with a transition of 1:12 for 4m at the lower end
4. Where vehicle access is via a ramp, design requirements for the gradient, surface treatment and curved sections are critical and analysed at an early stage in the design process.

---

## 11.11 Future transport

### Objective

- a) To support future mobility transitions, including vehicle sharing and ride-sharing.

### Guidance

1. Development has the capability of allowing for 100% electric recharging of delivery and service vehicles as well as any vehicles parked on site.
2. Each parking bay includes power supply capable of supporting a single phase Level 2 electric vehicle charger.
3. Development includes initiatives to promote walking, cycling and the use of public transport, through the submission of a Green Travel Plan.

---

## 11.12 Cycle network















### Objectives

- a) To provide an integrated, convenient, legible and safe cycle network that makes commuter and recreation cycling attractive to a broad range of people and integrates with and supports the surrounding cycle network.

### Guidance

1. Development contributes to a direct, step-free route suitable to be part of the regional cycling network from George Street south cycleway to Regent Street (refer Figure 17: Central Precinct cycle network).
2. The cycle network is to integrate with existing bicycle lanes/ low traffic streets in the surrounding area.
3. Wayfinding measures for cyclists are provided to support legible cycle movements and access to bike hubs.
4. Where bike vertical transportation is proposed, it is to be capable of accommodating a cargo bike.

# LEGEND

-  Central Precinct boundary
-  Rail line
-  Development area
-  Building footprints
-  Light rail line
-  Existing open space
-  Key heritage item
-  Publicly accessible space (link, streets and shared access)
-  Existing shared path
-  Existing low traffic street or bike lane
-  Existing separated cycleway
-  Potential new cycling connection
-  Bike hub access and recreational cycling only
-  Bike hub

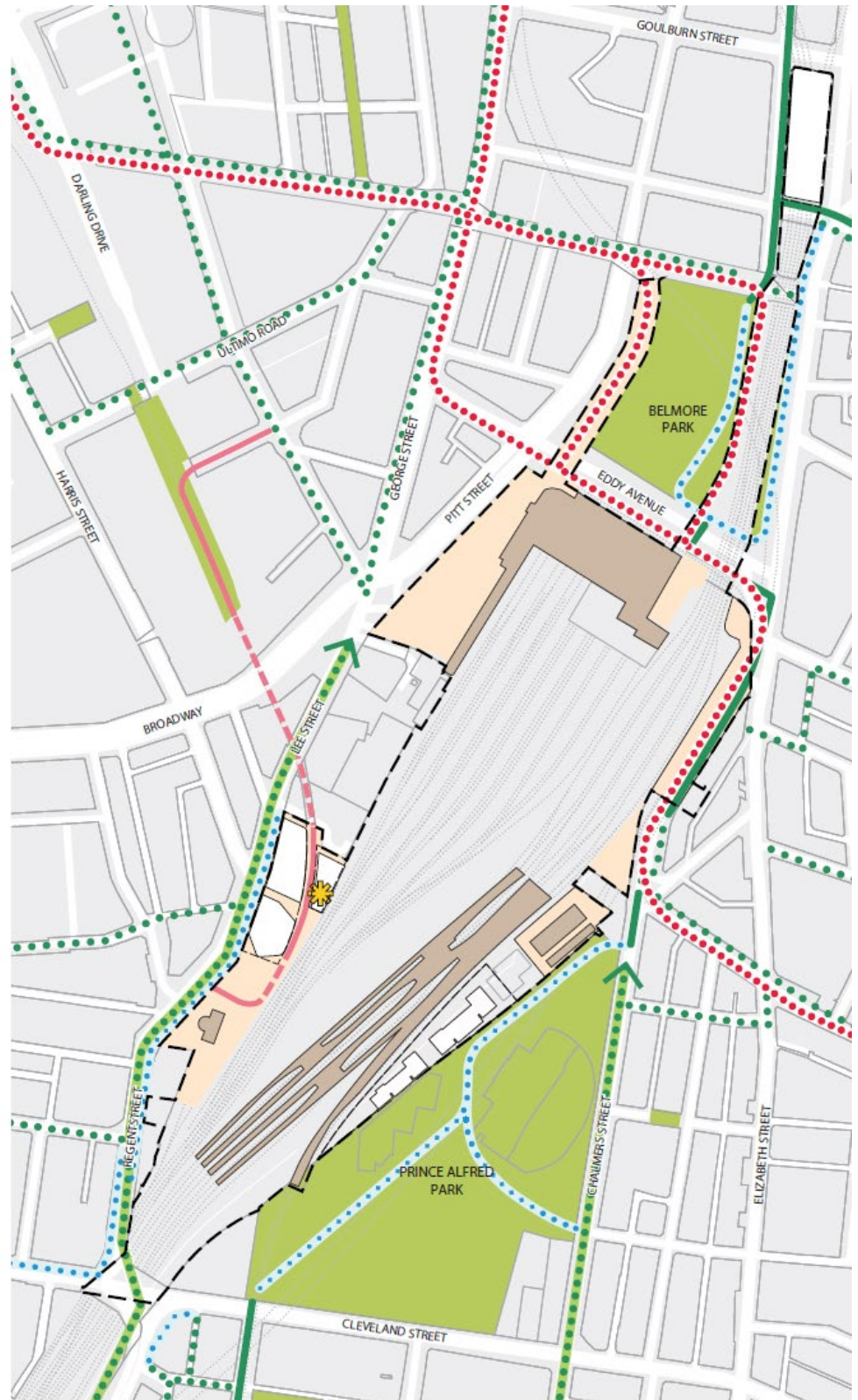


Figure 17: Central Precinct cycle network



---

## 11.13 Amount of bicycle parking

### Objective

- a) To provide bicycle parking that encourages cycling and reduces reliance on vehicles.

### Guidance

1. The number of bicycle parking spaces provided:
  - a. is consistent with forecast demand
  - b. is consistent with Table 8: Central Precinct guidance for on-site bike parking rates

**Note:** where application of this guidance does not result in a whole number, it is to be rounded up to the nearest whole number

Table 8: Central Precinct guidance for on-site bike parking rates

Land Use	Residents/ employees (long-term)	Customers/ visitors (short-term)
Residential	1 per dwelling	1 per 10 dwellings
Student accommodation	1 per dwelling	1 per 10 dwellings
Hotel	1 per 4 staff	1 per 20 rooms
Commercial	1 per 150 sqm	1 per 400 sqm
Retail	1 per 150 sqm	1 per 400 sqm
Education	1 per 10 staff plus 1 per 10 students	N/A
Community	1 per 150 sqm	1 per 200 sqm
Any other use	To contribute to delivery of Central Precinct's mode share target for trips by bike	

---

## 11.14 Secure bike parking and end of trip facilities

### Objectives

- a) To ensure bike parking and associated facilities provide an appropriate level of security from theft or damage
- b) To ensure bicycle parking and associated end of trip facilities are provided in convenient, accessible locations and are designed in a manner that provides for safety, security and comfortable access.

### Guidance

1. Secure bike parking facilities are provided in accordance with the following:



- a. Class 1 bike lockers or Class 2 bike facilities for occupants of residential buildings
  - b. Class 2 bike facilities for staff/employees of any land use
  - c. Class 3 bike rails for visitors of any land use.
2. For non-residential uses, the following end of trip facilities are provided:
- a. 1 personal locker for each bike parking space
  - b. 1 shower and change cubicle for up to 10 bike parking spaces
  - c. 2 shower and change cubicles for 11 to 20 or more bike parking spaces are provided
  - d. 2 additional showers and cubicles for each additional 20 bike parking spaces or part thereof
  - e. showers and change facilities may be provided in the form of shower and change cubicles in a unisex area in both female and male change rooms
  - f. locker, change room and shower facilities are to be located close to the bike parking area, entry and exit points and within an area of security camera surveillance where there are such building security systems.
  - g. a repair station with storage space for bike tools, pumps, e-bike battery chargers etc.
3. On-site bike parking and end of trip bike hubs are designed in accordance with the relevant Australian Standards for the design criteria of bike parking facilities.
4. Where bike parking for tenants is provided in a basement, it is located:
- a. on the uppermost level of the basement
  - b. close to entry / exit points
  - c. subject to security camera surveillance where such security systems exist.
  - d. A safe path of travel from bike parking areas to entry / exit points is marked.
5. Access to bike parking areas are:
- a. a minimum of 1.8m wide to allow a pedestrian and a person on a bike to pass each other and may be shared with vehicles within buildings and at entries to buildings
  - b. accessible via a ramp
  - c. clearly identified by signage
  - d. accessible via appropriate security or intercom systems.

---

## 11.15 Construction traffic management

### Objective

- a) To manage associated impacts of development construction.

### Guidance

- 1. A Construction Traffic Management Plan is to be prepared for each stage of development that describes:
  - a. the proposed construction works

- b. predicted construction vehicle routes
- c. the proposed management and access arrangements of construction vehicles
- d. the traffic impacts on the local area, including traffic, pedestrian and bicycle networks, and how these impacts will be managed and mitigated
- e. how pedestrian access will be maintained throughout construction activities.

# 12 Environmental Sustainability, Climate Change & Waste Management

This section identifies objectives and guidance to contribute to the NSW Government's goal to reduce emissions by 70% below 2005 levels by 2035 and reach net zero emissions by 2050, by setting best practice sustainability and environmental performance measures for the design, construction and operation of public spaces and buildings.

The objectives and guidance in this section are to be read in conjunction with the relevant principles set out in **Sections 2.0 and 3.0**.

---

## 12.1 Energy and greenhouse gas emissions

### Objectives

- a) To commit to delivering a net-zero Central Precinct.
- b) To contribute to the NSW Government's target of 70% emissions reduction by 2035 and net zero emissions by 2050.

### Guidance

- 1. Development is to be delivered and operated with net zero emissions.
- 2. Development is to be capable of enabling a wide range of energy systems approaches, and does not preclude:
  - a. a central thermal plant
  - b. embedded energy systems.
- 3. New buildings deliver a reduction in embodied carbon of at least 30% compared to a reference building as assessed in accordance with the Green Star Life Cycle Assessment (LCA) criteria.
- 4. Development is to encourage the reduction of emissions from other sources (e.g. refrigerants and waste), prior to offsetting.

---

## 12.2 High performance precinct and buildings

### Objective

- a) To ensure development achieves best practice sustainability and environmental performance measures having regard to energy and greenhouse gas emissions.

### Guidance

1. Development applications for new buildings are supported by an 'Ecologically Sustainable Development Strategy' that demonstrates how the following requirements will be achieved or exceeded for the relevant area and land use:
  - c. all uses: 6 star Green Star for Buildings rating
  - d. all uses: 6 star Green Star – Performance rating
  - e. commercial premises: 6-star NABERS Energy rating with a Commitment Agreement
  - f. commercial premises: 5-star NABERS Water rating
  - g. commercial premises: silver core and shell WELL rating (or equivalent industry standard)
  - h. commercial premises: 5.5-star NABERS Waste Whole Building
  - i. residential accommodation (common areas only, not for individual dwelling units): 4.5-star NABERS Energy rating with a Commitment Agreement
  - j. hotel: 4.5-star NABERS Energy rating with a Commitment Agreement
  - k. hotel: 4-star NABERS Water rating.

---

## 12.3 Electrical energy

### Objectives

- a) To reduce the reliance of fossil fuels.
- b) To implement an embedded network that is supplied by 100% renewable electricity.

### Guidance

1. All normally operating building services (including for food and beverage tenancies) is to operate using electricity as 100% of its energy source (i.e. no reliance on fossil fuels).
2. Design of new development is supported by an embedded network that is supplied by 100% renewable electricity.
3. Development applications are supported by renewable energy delivery plan, which details the proposed approach to 100% renewable energy supply.
4. Subject to feasibility and other relevant consideration, a private electricity network is encouraged. In the event a private electricity network is not established, 100% renewable energy must be procured for the precinct.
5. The electricity network is capable of:

- a. embedded network retail capability for 100% renewable energy
- b. embedded generation and battery storage (sized for equivalent performance to emergency generator requirements)
- c. smart grid controls (demand control, building information modelling (BIM))
- d. sufficient capacity to support electric vehicle charging, stand-alone and alongside multi-function pole design.

---

## 12.4 Thermal energy

### Objective

- a) To be able to treat and re-use 100% of precinct generated waste water for heat rejection and irrigation.

### Guidance

1. Encourage development to be capable of including a Central Thermal Utility that includes centralised heat rejection and an all-electric chilled water and hot-water plant with thermal storage and a 4-pipe thermal distribution network that connects the central plant to all buildings.

---

## 12.5 Solar gain

### Objectives

- a) To minimise undesirable solar gain within buildings.
- b) To incorporate passive design measures.

### Guidance

1. New buildings seek to achieve no greater than 80W/sqm peak solar cooling load within building perimeter zones (4m adjacent to facade).
2. No more than 55% of new building facades have transparent glazing.
3. New buildings include suitable self-shading elements.
4. On-site solar photovoltaic (PV) systems are encouraged on roofs that do not function as a green roof.

---

## 12.6 Natural ventilation

### Objective

- a) To provide capability for natural ventilation within parts of buildings suitable for their intended function and use.

### Guidance

1. Natural ventilation of residential apartments is to be in accordance with Section 4B – Natural ventilation of the Apartment Design Guide.

**Note:** Central Precinct exists in a noise constrained environment. Any passive design measures are required to consider and demonstrate how noise has been addressed and achieve a comfortable indoor environment suitable for the building's intended purpose.

---

## 12.7 Climate risk and resilience

### Objectives

- a) To improve resilience to potential shocks and stresses, including flooding, heat, storms, bushfire smoke events.
- b) To provide for flexible and adaptive systems with the capacity to be changed subject to uncertain future pressures.

### Guidance

1. Development applications for new buildings are supported by a 'Climate Risk and Adaptation Plan' that:
  - a. demonstrates how the precinct is capable of functioning effectively under predicted climate change impacts associated with the Representative Concentration Pathways (RCP) 8.5 2090 scenario (flooding, heat, extreme storm, humidity)
  - b. adopts the applicable Green Star protocol
2. Sufficient space for heating, ventilation, and air conditioning and other building services is provided that is capable of accommodating equipment sized for future climate scenarios and designed to manage extreme events like bushfire smoke, hailstorms, etc.

---

## 12.8 Circular economy and materials

### Objectives

- a) To contribute to circularity in the use of resources and materials, including through reducing waste generation and diversion of waste to landfill.
- b) To support building longevity beyond their original intended use.



- c) To implement the NSW Government's 'Circular design guidelines for the built environment'.

## Guidance

1. Development seeks to maximise re-used material or material from a renewable source in construction.
2. Development is capable of delivering a low-emissions approach to the construction of the precinct and its embodied carbon impacts.
3. Development applications:
  - a. demonstrate consideration of NSW Government and City of Sydney circular economy policy and guidelines, such as the following (or updated versions):
    - i. NSW Circular Economy Policy Statement: Too Good To Waste (EPA, 2019)
    - ii. Better practice guide for resource recovery in residential developments (EPA, 2019)
    - iii. Circular design guidelines for the built environment (OECC, 2023)
    - iv. Leave nothing to waste: Waste strategy and action plan (City of Sydney, 2017).
  - b. identify the circular economy strategies to be adopted in the delivery and operational phases of the development.

---

## 12.9 Waste management

### Objectives

- a) To apply the Australian, NSW, and City of Sydney waste management and circular economy policies and strategies.
- b) To reduce the amount of construction and demolition waste going to landfill.
- c) To reduce amount of waste generated in the operation of a development and maximise resource recovery.
- d) To incorporate well-designed, adequately sized and innovative waste and recycling facilities in buildings at the design stage.
- e) To minimise amenity impacts associated with waste storage, transfer and collection.

### Guidance

1. A Waste and Recycling Management Plan consistent with City of Sydney's Guidelines for Waste Management in New Developments and NABERS Waste is submitted with any development application and will be used to assess and monitor the management of waste and recycling during construction and operational phases of the proposed development.
2. The Waste and Recycling Management Plan includes the following details on the management of demolition and construction waste:
  - a. how waste is to be minimised during the demolition and construction phase

- b. estimations of quantities and types of materials to be re-used or left over for removal from the site
  - c. the types of waste and likely quantities of waste to be produced
  - d. a site plan showing storage areas away from public access for reusable materials and recyclables during demolition and construction and the vehicle access to these areas
  - e. targets for recycling and reuse
  - f. nomination of the role/person responsible for ensuring targets are met and the person responsible for retaining waste dockets from facilities appropriately licensed to receive the development's construction and demolition waste
  - g. confirmation that all waste going to landfill is not recyclable or hazardous
  - h. measures to reuse or recycle at least 90% of construction and demolition waste.
3. The Waste and Recycling Management Plan includes the following details with regard to the management of operational waste:
- a. estimates of waste and recycling that will be generated, the number of bins and collection frequency to manage the waste generated within tenancy, building and precinct storage/loading area
  - b. plans and drawings of the proposed development that show:
    - i. the location and space allocated within buildings to the waste and recycling management systems
    - ii. the nominated waste collection point/s for the site
    - iii. the safe and efficient path of access between tenancies (if applicable), buildings and integrated basement storage and collection facilities
  - c. the on-going management of the storage, separation and collection of waste and recycling, including responsibility for cleaning, transfer of bins between storage areas and collection points, maintenance of signage, and security of storage areas
  - d. where appropriate to the nature of the development, a summary document for tenants and residents to inform them of waste and recycling management arrangements
  - e. measures to reuse or recycle waste from industrial, commercial and residential operations, in line with relevant third party certification requirements
  - f. capacity to achieve a minimum of 80% resource recovery rate from all operational material streams, including through consideration of:
    - i. establishment of a Centralised Waste Management Network for storage and collection
    - ii. separation and recycling of recoverable waste by type.
4. Development provides access for the safe and efficient collection of waste by waste servicing vehicles.
5. The waste and recycling storage area is adequately ventilated by either:
- a. natural ventilation openings to external air with openings not less than 5 per cent of the bin bay or bin room floor area
  - b. a mechanical exhaust ventilation system in accordance with relevant Australian standards.

6. Waste and recycling management systems provide adequate space with tenancies, buildings and shared waste storage areas for waste infrastructure, source separated streams including general waste, recycling, organics, bulky and problem waste.
7. Waste management systems provide adequate space for on-site organics processing infrastructure or storage for off-site processing, sized for the modelled organic waste generation from the precinct.
8. Buildings in the precinct have separate dedicated space for the separate interim storage and management of strip-out waste for re-use or recycling.

# 13 Green Infrastructure, Ecology, Urban Forest & Greening

This section of the Design Guide identifies objectives and guidance to ensure the delivery of a high quality and resilient green infrastructure network.

The objectives and guidance in this section are to be read in conjunction with the relevant principles set out in Sections 2.0 and 3.0.

---

## 13.1 Landscape character

### Objectives

- a) To provide an integrated, connected and multifunctional landscape character including a network of greening and canopy cover that:
  - i. connects with natural features and processes
  - ii. enhances people's physical and mental health and wellbeing
  - iii. enhances peoples understanding of Country
  - iv. supports biodiversity and connection and access to nature by using local native provenance species and provides habitat for suitable native fauna
  - v. considers heritage-significant landscape features
  - vi. provides high quality shade and reduces urban heat
  - vii. creates microclimates
  - viii. filters stormwater on-site through natural measures, reduces pests and weeds and contributes to cleaner air
  - ix. contributes to delivery of the Sydney Green Grid.
- b) To ensure green and canopy cover contributes to the City's greening and urban forest targets outlined in the Greening Sydney Strategy.

**Note:** the Sydney Green Grid is outlined in the Greater Sydney Region Plan and the Eastern City District Plan.

### Guidance

1. Greening and canopy cover is:

- a. in accordance with the relevant planting species identified in Schedule 3: Planting species index and location
- b. considers microclimatic conditions (i.e. solar and wind), including for green roofs and walls.
2. Development achieves the tree canopy and greening targets in Table 9: Central Precinct minimum targets for tree canopy and .
3. Development includes greening and canopy cover on rooftops.
4. Vegetation is planted to encourage cool microclimates including through:
  - a. clustering of planting and grouping of canopy trees
  - b. layered plantings, comprising a canopy layer of trees with dense canopies and a high leaf area index, a mid-storey layer that is relatively clear to increase visibility and permeability, and an understorey/ groundcover layer that includes forbs that prefer a high moisture environment and enhance humidity such as ferns and rushes
  - c. providing planting beds with soils with a high level of organic matter to retain higher moisture content.
5. A Landscape Plan is to be submitted with a development application for new buildings and/or publicly accessible spaces that details the design of soil depths, profiles and type, taking into account tree species, irrigation, size of root-ball, placement and environmental conditions.
6. A landscaping maintenance plan is submitted with any development application that proposes new plantings to ensure successful establishment, structural integrity and ongoing health of canopy trees and planting, green roofs and walls, as well as managing weeds, pests and diseases. Any plant losses and maintenance replanting should replace plants by the same local native provenance species, or where that species is not available, with the same growth form (i.e. a tree with a tree etc) and must not decrease species diversity.
7. Landscaping design in private open spaces (e.g. roof gardens) is to consider the principles and considerations of the City of Sydney Landscape Code with proposed planting design and species being appropriate for the environmental conditions.
8. Articulation of the public realm design during design development should ensure that landscape elements (such as trees) do not overly obstruct valued sight lines (such as significant views).

Table 9: Central Precinct minimum targets for tree canopy and green cover

Location	Land use type*	Tree canopy cover target (minimum)	Green cover target (minimum)
Central Square	Civic Open Space	50%	60%
Eddy Avenue Plaza	Civic Open Space	30%	30%
Mortuary Station Gardens	Civic Open Space	28%	55%
Goods Line	Public domain within historic rail cutting	10%	15%

Location	Land use type*	Tree canopy cover target (minimum)	Green cover target (minimum)
Ibero-American Plaza and Chalmers St light rail station	Civic Open Space, Light Rail tracks and platforms, bicycle path	35%	40%

\* Land use types based on City of Sydney's Greening Sydney Strategy

\*\* Minimum targets subject to engineering advice

## 13.2 Locally indigenous flora and fauna species

### Objectives

- To improve the diversity and abundance of locally indigenous flora and fauna species across the Precinct, consistent with ecological communities that would have naturally occurred in the area.

### Guidance

- Greening and canopy cover is to include species from endemic vegetation communities as set out in Schedule 3: Planting species index and location.
- Aboriginal stakeholders are to be included in endemic species selection and landscape management to contribute to health and wellbeing of Country.

## 13.3 Soil requirements

### Objectives

- To provide sufficient soil volumes to support healthy growth of trees and turfed ground cover.

### Guidance

- Publicly accessible open spaces incorporate areas of turfed and planting on structure with sufficient soil volume, depth and area to support tree planting and growth.
- The design of publicly accessible spaces considers the use of permeable paving or other methods to allow gaseous exchange with soil where trees are planted in paved areas.



---

## 13.4 Significant trees

### Objective

- a) To protect significant trees that make a valuable contribution to the environment, culture and heritage of the City of Sydney.

### Guidance

1. Development retains and protects all significant trees within the Precinct.
2. Significant figs in Prince Alfred Park and adjacent to Regent Street (in the Regent Street Sidings sub-precinct) are to be clearly identified in future development applications and protected.
3. An Arboricultural Impact Assessment (AIA) is to be submitted, in accordance with the Australian Standard for the Protection of trees on Development Sites (AS4970), with any development applications that may impact on trees. This report is to assess trees located within adjoining properties (including street trees) where they may be impacted by the proposed works.
4. An AIA submitted with any development application is to be prepared by an Australian Qualification Framework Level 5 arborist who has extensive demonstrated experience in managing significant trees and complex development sites.

---

## 13.5 Biophilic design

### Objective

- a) To establish a biophilic environment that provides a material connection for tenants and visitors to natural systems.

### Guidance

1. Building design enables the provision of green roofs and green walls (vertical greening) with preference to green roofs.
2. Buildings are designed to provide a minimum 50% of their roof area as green roofs (excluding areas for solar photovoltaic (PV) systems).
3. Building roofs are to be clear of mechanical plant to provide capability for green infrastructure.
4. Planting design as part of new green roofs includes a diverse range of plants including locally endemic species, including a range of species which are adaptive to Representative Concentration Pathways (RCP) 8.5 conditions for 2090
5. Watering and maintenance of planting, green roof and walls is to be provided from recycled and harvested water sources.

---

## 13.6 Biodiversity

### Objectives

- a) To ensure development has a net positive impact on biodiversity and protects threatened species inhabiting Central Precinct.
- b) To create new and improve existing urban habitat for the city's ecosystem.
- c) To provide habitat connectivity for mobile species between:
  - i. Prince Alfred Park and Belmore Park
  - ii. the Domain, Hyde Park and Moore Park.

### Guidance

1. Development applications are accompanied by a 'Biodiversity Management Plan' that details measures to be implemented prior to, during and post construction for:
  - a. Survey and habitat assessment for fauna species
  - b. protection of threatened and non-threatened native fauna species
  - c. establishment and maintenance of appropriate foraging, roosting and/or breeding habitat for highly mobile native species such as microbats, birds and insects
  - d. control of pests and weeds
  - e. requirements for monitoring and adaptive management to demonstrate an increase in biodiversity
  - f. where impacts are proposed to threatened and non-threatened species habitat, establishment of alternative nearby suitable habitat prior to construction commencing.
2. New buildings provide appropriate habitat (including foraging and roosting spaces) for mobile species, such as microbats, birds and insects.
3. The design of new buildings considers local and regional flyways in landscape, building and infrastructure design and avoids building design elements that could harm flying fauna, such as highly reflective glass building facades.
4. For development that will impact on existing habitat/ threatened species, an Ecological Assessment report is to be submitted, prepared by a qualified and appropriately experienced urban ecologist:
  - a. to determine the likely impacts on flora and fauna species and communities onsite and in the vicinity during demolition, construction and post-construction stages of the proposed development;
  - b. to outline the mitigation measures that will be undertaken to keep any adverse impacts to a minimum;
  - c. to demonstrate consistency with the provisions of this Design Guide.

# 14 Water Quality, Flooding & Stormwater

This section of the Design Guide identifies objectives and guidance related to the management of water within Central Precinct, including how water is utilised sustainably within the precinct while minimising impacts to the capacity of surrounding stormwater infrastructure.

The objectives and guidance in this section are to be read in conjunction with the relevant principles set out in **Sections 2.0 and 3.0**.

---

## 14.1 Integrated Water Management

### Objectives

- a) To manage and balance urban water elements (potable water, wastewater, and stormwater) through integrated water cycle management.
- b) To design and deliver infrastructure, servicing and development driven by an integrated water cycle management approach to build resilience in Sydney's water supply by using recycled water, optimising stormwater management and maximising efficiency in the use of potable water.
- c) To ensure integrated water cycle management is safe, practicable and provides the best environmental outcomes.
- d) To facilitate coordinated delivery of an integrated water management approach across development stages.

### Guidance

- 1. An Integrated Water Management Strategy is prepared that demonstrates how development will maximise water efficiency and reuse, improve stormwater quality, support green infrastructure and urban cooling. The strategy is to:
  - a. include provision of dual plumbed water systems to enable utilisation of the recycled water network for permitted non-potable uses which may include flushing, irrigation, and certain industrial purposes
  - b. identify how rainwater and/ or stormwater will be collected conveyed, stored, harvested and reused across the precinct to maximise sustainable water reuse
  - c. consider how the stormwater infrastructure will be delivered given the individual development stages
  - d. identify opportunities for water sensitive urban design (WSUD), including green walls and roofs, to be optimally integrated across the public and private domains

- e. define stormwater quality treatment targets for each sub-precinct based on their unique opportunities and constraints
- f. consider how the WSUD will be delivered given the individual development stages
- g. consider how the development be designed to enable future connection to the George Street recycled water scheme network
- h. consider both sewer mining and provision for export of treated water to the George Street recycled water scheme network (either in conjunction or separately)
- i. consider how recycled water supply will be delivered given the individual development stages.
- j. identify opportunities to exceed best-practise stormwater quality targets and how these opportunities will be delivered through the development stages.

---

## 14.2 Water positivity

### Objective

- a) To be water positive through water efficiency, preservation of non-renewable water resources and reduction in consumption of mains potable water.

### Guidance

1. Development targets 50% of water demand being met with non-potable (or recycled) water supply sources, including through:
  - a. use of local and renewable water sources
  - b. best practice water conserving systems, equipment, fixtures, fittings and appliances
  - c. effective water harvesting and re-use
  - d. waste-water treatment and re-use.
2. Development provides for secure, recycled water supply for use in irrigating trees and vegetation.
3. Recycled water of suitable quality (based on its intended purpose) is used for:
  - a. irrigation of vegetation
  - b. any proposed aquatic habitat.
4. A reliable recycled water supply is provided through, either:
  - a. private water recycling scheme
  - b. public authority water recycling scheme
  - c. connection to a shared water recycling scheme via the George Street recycled water main, with recycled water being supplied by an independent party (either public or private).
5. Each building has a rainwater tank that is capable of being 'topped up' by potable water.

---

## 14.3 Local drainage management

### Objectives

- a) To manage stormwater, minimise flooding and impacts to surrounding upstream and downstream areas.
- b) To reduce the effects of stormwater pollution on receiving waterways.

### Guidance

1. As part of an Integrated Water Cycle Management Plan (as required by **Section 14.1**, a Local Drainage Management Plan prepared by a suitably qualified engineer with experience in drainage design that addresses:
  - a. the hydrology of the locality and its relationship to the drainage system
  - b. the distribution of soil types and the scope for on-site infiltration
  - c. any expected rise in ground water level due to development
  - d. the role of the principal landscape components on the site for water conservation and on-site detention
  - e. the scope for on-site stormwater detention and retention, including collection of water for re-use
  - f. how any detrimental impacts on the existing hydrology and water quality are proposed to be minimised
  - g. how pedestrian safety is to be ensured
  - h. integration of drainage management responses and open space areas.
2. Drainage systems are designed for the following conditions:
  - a. stormwater flows up to the 5% annual exceedance probability event are conveyed by a minor drainage system
  - b. stormwater flows above the 5% annual exceedance probability event are conveyed by a major drainage system.
3. The civil drainage design across the precinct is to comply with relevant Australian standards including 3500.3 (2021) Plumbing and Drainage – Part 3 Stormwater Drainage.
4. Civil drainage is to be designed for the RCP 8.5 in 2090 climate change scenario.
5. Development applications demonstrate how the major drainage system addresses any site-specific conditions and connects to the downstream drainage system.
6. Major drainage systems are designed so that public safety is not compromised.
7. Minor flows from a development site are not discharged to the kerb if direct connection to an existing stormwater pipe is available, unless it can be demonstrated there is sufficient capacity within the existing gutter and the flow velocity and depth within the gutter will remain below 25l/s.

8. The post development run-off from impermeable surfaces (such as roofs, driveways and paved areas) is to be managed by stormwater source measures that:
    - a. contain frequent low-magnitude flows
    - b. provide a natural balance between run-off and infiltration
    - c. remove some pollutants
    - d. prevent nuisance flows from affecting adjacent properties
    - e. enable appropriate use of rainwater and stormwater.
  9. Stormwater detention devices are designed to ensure that the overflow and flow path have sufficient capacity during all design rainfall events, discharge to the public stormwater system without affecting adjoining properties, and are free of obstructions, such as fences.
  10. Where filtration and bio-retention devices are proposed, they are designed to capture and provide temporary storage for stormwater.
  11. Car parking areas and access aisles are designed, surfaced and graded to reduce run-off, allow stormwater to be controlled within the site, and provide for natural infiltration of stormwater runoff through landscaping.
- 

## 14.4 Flood planning

### Objectives

- a) To ensure flood planning:
  - i. manages and mitigates flood risk
  - ii. does not exacerbate the potential for flood damage or hazard to existing development and to the public domain
  - iii. ensures that flood risk management addresses public safety and protection from flooding.
- b) To ensure development which may be affected by flooding adequately factors in climate change impacts.

### Guidance

1. Development manages and mitigates flood risk and does not exacerbate the potential for flood damage or hazard to:
  - a. development within Central Precinct
  - b. to the public domain (including publicly accessible spaces)
  - c. surrounding development upstream and downstream.
2. For all development, a detailed site-specific flood study is prepared by a suitably qualified engineer with experience in drainage design that addresses:
  - a. whether on site detention is required to avoid:



- i. peak flood level increases in the downstream network for the present day climate conditions 20% Annual Exceedance Probability (AEP), 5% AEP or 1% AEP design rainfall events. The full range of standard duration design rainfall events from 10 mins to 3 hours
    - ii. where connected to the City of Sydney Council drainage network, increases in the downstream peak flow rate of more than 10%
  - b. flood impacts, including determining under present day climate conditions, any change as a result of the development in:
    - i. peak flood levels
    - ii. flood extents
    - iii. flood risk areas
    - iv. flood hazard categories
  - c. cumulative flood impacts under present day climate conditions. In addition to assessing the impact of the individual development, the flood study is to assess the combined impact of all other approved developments across the wider precinct to ensure the cumulative impacts of the development are considered
  - d. present day climate conditions 20% AEP, 5% AEP, 1% AEP and probable maximum flood (PMF) design rainfall events for the full range of standard duration design rainfall events from 10 mins to 6 hours
  - e. the impact of the proposed development with a RCP 8.5 in 2090 climate change scenario to inform flood planning levels.
  - f. consideration of the PMF flood level impacts on existing basement car parks. For existing basement car park entries located above the PMF flood level, should PMF flood impacts remove the flood immunity potential mitigation measures are to be proposed.
  - g. refinement of the existing condition flood model based on recent detailed ground survey that defines flow paths, storage areas and hydraulic controls.
3. The City of Sydney Council's Interim Floodplain Management Policy 2014 is used to determine flood planning levels across the precinct. Adopted flood planning levels are to be outlined in the prepared flood study.
  4. The Precinct Flood Model is to be maintained and updated with each development application, as relevant, with the latest design information and used to inform the design of the precinct as it progresses.
  5. A flood emergency management response plan is developed in consultation with the City of Sydney Council and State Emergency Services. For residential and commercial development where a shelter-in-place strategy is proposed provision of easily accessible habitable areas above the PMF level is to be provided.
  6. For areas below the PMF flood level, all buildings are constructed with flood compatible materials and designed to withstand the impact of floodwater, debris and buoyancy.
  7. Explore opportunities to reduce existing flood risk where flood mitigation is required. Consider potential impacts of the City of Sydney Flood Risk Management Plans and proposed stormwater infrastructure capital works which may impact flood conditions.

---

## 14.5 Stormwater quality

### Objectives

- a) To provide a net improvement to environmental water quality as a result of development, in particular contributing to improving the health of Sydney Harbour.
- b) To ensure an integrated approach to water management through the use of WSUD principles.

### Guidance

1. The Integrated Water Management Plan is accompanied by a Stormwater Quality Assessment that demonstrates that the development will achieve the following post-development pollutant load standards as outlined in the Precinct wide Integrated Water Cycle Management Strategy:
  - a. reduce the baseline annual pollutant load for litter and vegetation larger than 5mm by 90%
  - b. reduce the baseline annual pollutant load for total suspended solids by 85%
  - c. reduce the baseline annual pollutant load for total phosphorous by 65%
  - d. reduce the baseline annual pollutant load for total nitrogen by 45%.
2. Any Stormwater Quality Assessment is prepared by a suitably qualified engineer with experience in water WSUD and include:
  - a. modelling of pollutant load standards with an industry standard water quality model (i.e. MUSIC software)
  - b. the design of WSUD measures used to achieve the post-development pollutant load standards, with consideration of:
    - i. City of Sydney Council Sydney Streets Technical Specifications 2019 A4 Stormwater Drainage Design
    - ii. Transport for NSW Water Sensitive Urban Design Guideline 2017.
  - c. maintenance schedules of any proposed WSUD measure that requires maintenance or full replacement including the likely recycling or disposal location of any wastes that may be generated
  - d. integrated water quality management measures, including green infrastructure in accordance with best practice stormwater management guidelines.
3. Development considers inclusion of WSUD measures to slow stormwater runoff and improve stormwater quality flowing into waterways such as:
  - a. gross pollutant traps
  - b. passive irrigation
  - c. bio-retention areas
  - d. rainwater harvesting.
4. Opportunities to reuse water in the landscape through WSUD is maximised.

# 15 Social Infrastructure

This section of the Design Guide identifies objectives and guidance to ensure social infrastructure, affordable and adaptable housing is delivered to accommodate a diverse range of needs and household structures. Social infrastructure includes open space, community facilities and spaces to support cultural expression and creative activities, and social and health services for the wider community.

The objectives and guidance in this section are to be read in conjunction with the relevant principles set out in **Sections 2.0 and 3.0**.

---

## 15.1 Affordable housing

### Objectives

- a) Affordable housing is provided to ensure a diverse and inclusive mix of residents in the precinct.
- b) To ensure that affordable housing is durable, sustainable, achieves similar levels of amenity as private housing and meets the needs of the local community.
- c) To contribute to the provision of affordable housing in accordance with the relevant Region and City Plan.

### Guidance

- 1. A minimum of 30% of any new residential floor space is to be provided as affordable housing, owned and managed by State Government.
- 2. The provision of affordable housing is to be delivered on-site within the Central Precinct.
- 3. Development applications for residential uses are to include an Affordable Housing Needs Assessment and Implementation Strategy, which addresses the following matters:
  - e. affordable housing needs within the Precinct at the point of delivery and projected at least 10-15 years into the future
  - f. identification of specific targets and goals, including the split of targets for diverse forms of affordable housing (e.g. social housing, Aboriginal and Torres Strait Islander housing, key worker housing, affordable student housing etc.).
  - g. consideration should be given to allocating a minimum of 10 per cent of the total number of affordable housing dwellings for Aboriginal and Torres Strait Islander housing.
  - h. mechanisms to ensure long-term affordability and availability of affordable housing units
  - i. potential innovative mechanisms for delivery

- j. compliance with the NSW Affordable Housing Ministerial Guidelines 2022/23, including in relation to tenure-blind delivery
- k. process to monitor and report on progress and outcomes of the affordable housing initiative, and to enable intervention if misalignment occurs.

---

## 15.2 Universal and adaptable housing

### Objective

- a) To provide adaptable housing that is designed to be capable of easy modification in the future for occupation and visitation by people with a disability or people who may acquire a disability gradually as they age.

### Guidance

- 1. Where residential accommodation is provided:
  - a. a minimum 20% of the total apartments are to incorporate the Liveable Housing Guideline's silver level universal design features
  - b. adaptable dwellings are provided in accordance with the target rates shown in Table 10: Central Precinct guidelines for number of adaptable dwellings.
- 2. Adaptable dwellings should be designed and constructed in accordance with the National Construction Code's Liveable Housing Design Guidelines (LHDG).

Table 10: Central Precinct guidelines for number of adaptable dwellings

Total number of dwellings	Number of adaptable dwellings to be provided
Nil – 7	Nil
8 – 14	1 dwelling
15 – 21	2 dwellings
21 – 29	3 dwellings
30 or more	15% of the total number of dwellings (target)

---

## 15.3 Accessible design

### Objective

- a) To ensure development, including publicly accessible spaces and buildings, provides equitable access to persons with a disability.

## Guidance

1. Development complies with relevant legislative requirements for accessible design.

**Note:** Relevant legislative requirements are contained in various Acts and standards. Of particular relevance is the 'Disability (Access to Premises — Buildings) Standards 2010', 'AS1428.1 Design for access and mobility: General requirements for access – New building work' and the City of Sydney's Inclusive and Accessible public domain policy and public domain design codes.

# 16 Safety and Security

This section of the Design Guide identifies objectives and guidance to address crime and safety concerns for Central Precinct, to ensure its ability to become an inclusive and safe environment for all people.

The objectives and guidance in this section are to be read in conjunction with the relevant principles set out in **Sections 2.0 and 3.0**.

## Objectives

- a) To create a safe environment for people and property and minimise opportunities for criminal and anti-social behaviour.
- b) To establish ongoing operational management of safety and security.
- c) To ensure the appropriate selection of security treatments for events.
- d) To ensure an emergency management plan is in place during the operation.

---

## 16.1 Event management

### Guidance

2. Security treatments for events held at Central Precinct, are to be informed by an event specific Security Risk Assessment (ESRA). The ESRA should include detail on gender-sensitivity, anti-social behaviour and sexual harassment issues with the most vulnerable event users in mind.
3. Development applications for areas within the precinct that will hold multiple events are to be accompanied by an Event Management Plan. This must include, but is not limited to, the following:
  - a. Vehicle and parking access arrangements.
  - b. Patron and movement generated by the event.

- c. Servicing setup and close out functions.
- d. Any road closures and associated traffic management plans / measures.

---

## 16.2 Crime prevention through environmental design

### Guidance

1. Locate design features and activities to improve casual and active surveillance of publicly accessible spaces, to create a perception of increased risk detection for perpetrators of criminal activity and increased safety and security for general users
2. Locate active spaces and windows of habitable rooms within buildings to maximise casual surveillance of publicly accessible spaces, including links, laneways and shared access paths.
3. In publicly accessible spaces, locate and design facilities such as toilets and parents rooms to maximise casual surveillance to facility entries.
4. Minimise blind-corners, recesses and other external areas that have the potential for concealment or entrapment.
5. Building entries are clearly visible, unobstructed and easily identifiable from adjoining publicly accessible spaces.
6. Where practicable and appropriate for streetscape activation, lift lobbies, stairwells, hallways and corridors are visible from publicly accessible spaces.
7. Ground floors of non-residential buildings, the non-residential component of mixed use developments, and the foyers of residential buildings, are designed to enable surveillance from publicly accessible spaces to the inside of the building at night.
8. Hostile vehicle mitigation measures are considered in the future design of buildings and integrated with the publicly accessible spaces.
9. Building target hardening measures are integrated into building design and located within building envelopes and not located with the publicly accessible spaces.
10. An appropriate bollard/barrier system should be considered to prevent vehicles from entering locations not intended for vehicles (i.e. publicly accessible open spaces).
11. Use of bollards are minimised to reduce the impact on pedestrian permeability on surrounding publicly accessible spaces.



# 17 Aeronautical Requirements

The objective and guidance in this section are to be read in conjunction with the relevant principles set out in **Sections 2.0 and 3.0**.

## Objective

- a) To ensure the operational safety of the Sydney (Kingsford-Smith) Airport.

## Guidance

12. Where a proposal will penetrate the Obstacle Limitation Surface (OLS), the Procedures for Air Navigation Services Operations Surface or Radar Terrain Clearance Charts (RTCC) for Sydney (Kingsford-Smith) Airport, the consent authority is to consult with the body responsible for development decisions relating to the Sydney (Kingsford-Smith) Airport under Commonwealth legislation.

# 18 Schedules

## Schedule 1: Definitions

The following table defines selected key terms used in this Design Guide.

Table 11: Key terms and definitions

Term	Meaning
<b>Advertising and signage</b>	has the same meaning as advertisement and signage in the ‘Standard Instrument – Principal Local Environmental Plan’
<b>Building envelope area</b>	is the area including all internal and external built elements and enclosed voids between that floor level and the next floor level measured in plan
<b>Country</b>	includes land, waters, and sky. It can be tangible or intangible aspects, knowledge and cultural practices, belonging and identity, wellbeing and relationships. People are part of Country, and our identity is derived in a large way in relation to Country. (Government Architect NSW & Dr Danièle Hromek, 2020)
<b>Design excellence</b>	<p>in NSW the definition of Design Excellence is broadly consistent across planning legislation where it is often summarised as ‘the highest standard of architectural, urban and landscape design (Design Competition Guidelines, GANSW, 2023)</p> <p>It is a term that refers to the design quality of a building or project and to a variety of requirements intended to lift design quality.</p>
<b>Gross building area</b>	means the total enclosed and unenclosed area of the building at all building floor levels measured between the normal outside face of any enclosing walls, balustrades and supports that could be achieved within the defined planning envelope inclusive of any cantilever zone to meet the required qualitative and performative standards. The unit of measurement for building areas is the square metre
<b>Open to the sky</b>	means a space that directly opens to the sky without any structures above.
<b>Sky view factor (SVF)</b>	means the extent of sky observed above a point as a proportion of the total possible sky hemisphere above the point. SVF is calculated as the proportion of sky visible when viewed from the ground (as an abstract horizontal surface) up. SVF is a dimensionless value that ranges from 0 to 1. A SVF of 1 denotes that the sky is completely visible to the horizon in all directions; for example, in a flat terrain. When a location has topography or buildings

Term	Meaning
	blocking view to any part of the sky, it will cause the SVF to decrease proportionally
Wind comfort standard for walking	is an hourly mean wind speed, or gust equivalent mean wind speed, whichever is greater for each wind direction, for no more than 292 hours per annum measured between 6 am and 10 pm Eastern Standard Time (i.e. 5% of those hours) of 8 metres per second
Wind comfort standards for sitting and standing	is hourly mean wind speed, or gust equivalent mean wind speed, whichever is greater for each wind direction, for no more than 292 hours per annum measured between 6 am and 10 pm Eastern Standard Time of; 4 metres per second for sitting; and 6 metres per second for standing
Wind safety standard	is an annual maximum peak 0.5 second gust wind speed in one hour measured between 6am and 10pm Eastern Standard Time of 24 metres per second

## Heritage definitions

Table 12: Key heritage terms and definitions

Term	Definition
Conservation	Conservation includes preservation, protection, maintenance, restoration and adaptation. (as defined in the NSW Heritage Act 1977)
Heritage item	Heritage item means a building, work, place, relic, tree, object or archaeological site the location and nature of which is described in Schedule 5 (as defined in <i>Sydney Local Environmental Plan 2012</i> )
Heritage significance	<p><b>State heritage significance</b>, in relation to a place, building, work, relic, moveable object or precinct, means significance to the State in relation to the historical, scientific, cultural, social, archaeological, architectural, natural or aesthetic value of the item.</p> <p><b>Local heritage significance</b>, in relation to a place, building, work, relic, moveable object or precinct, means significance to an area in relation to the historical, scientific, cultural, social, archaeological, architectural, natural or aesthetic value of the item.</p> <p>(as defined in the NSW Heritage Act 1977)</p>
Heritage Interpretation	<p>Interpretation means all the ways of presenting the cultural significance of a place. (as defined in Burra Charter, Article 1.17).</p> <p>The cultural significance of many places is not readily apparent, and should be explained by interpretation. Interpretation should enhance understanding and engagement, and be culturally appropriate. (as defined in Burra Charter, Article 25)</p>

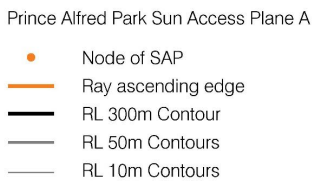


Figure 18: Prince Alfred Park Sun Access Plane A

Source: City of Sydney



Belmore Park Sun Access Plane

- Node of SAP
- Ray ascending edge
- RL 300m Contour
- RL 50m Contours
- RL 10m Contours

Figure 19: Belmore Park Sun Access Plane

Source: City of Sydney



## Schedule 3: Planting species index and location

Table 13: Planting species index and location

Public Domain Location	Species	Common Name	Vegetation type or local ecological community
1. Central Square	Planting character: species palette to draw on the character of Sydney Turpentine-Ironbark Forest and Sydney Coastal Sandstone Foreshores Forest, which would once have occurred in the area, as well as connect with tree planting in adjacent city streets.		
	<i>Angophora costata</i>	Sydney Red Gum	Sydney Turpentine-Ironbark Forest, Sydney Coastal Sandstone Foreshores Forest
	<i>Zelkova serrata</i> to connect with street tree planting	Japanese Zelkova	exotic deciduous
	Understorey planting should consider species from the Sydney Turpentine-Ironbark Forest and the Sydney Coastal Sandstone Foreshores Forest vegetation communities. Areas with shaded or lower light conditions should consider species from 4. Southern Plaza, Goods Line, south Central Avenue, Devonshire Link and east-west connections.		
2. Goods Line	Planting character: species palette of palms, tree ferns, rainforest and shade tolerant species to create rich layered planting in areas with less solar access.		
	<i>Livistona australis</i>	Cabbage Tree Palm	Sydney Turpentine-Ironbark Forest, Sydney Coastal Sandstone Foreshores Forest
	<i>Ficus macrophylla</i> , <i>Ficus microcarpa</i> x <i>Hillii</i>	Moreton Bay Fig, Hill's Fig	native to Sydney Region
	<i>Ficus rubiginosa</i>	Port Jackson Fig	Sydney Coastal Sandstone Foreshores Forest
	<i>Cyathea cooperi</i>	Lacy Tree Fern	Sydney Coastal Sandstone Foreshores Forest
	<i>Cyathea cunninghamii</i>	Slender Tree Fern	native to Australia, including NSW
	<i>Cyathea australis</i>	Rough Tree Fern	Sydney Turpentine-Ironbark Forest, Sydney Coastal Sandstone Foreshores Forest
	<i>Dicksonia antarctica</i>	Soft Tree Fern	native to Sydney region
	<i>Elaeocarpus reticulatus</i>	Blueberry Ash	Sydney Turpentine-Ironbark Forest, Sydney Coastal Sandstone Foreshores Forest
	<i>Elaeocarpus eumundi</i>	Eumundi Quandong	native to Australia, including NSW
	<i>Syzigium luehmannii</i>	Riberry Lilly Pilly	native to Australia, including NSW



Public Domain Location	Species	Common Name	Vegetation type or local ecological community
	<i>Acmena smithii</i>	Common Lilly Pilly	Sydney Turpentine-Ironbark Forest, Sydney Coastal Sandstone Foreshores Forest
	<i>Austromyrtus dulcis</i>	Midyim Berry	native to Australia, including NSW
	<i>Backhousia citriodora</i>	Lemon Myrtle	native to Australia
	<i>Breynia oblongifolia</i>	Coffee Bush	Sydney Turpentine-Ironbark Forest, Sydney Coastal Sandstone Foreshores Forest
	<i>Macrozamia communis</i>	Burrawang	Sydney Coastal Sandstone Foreshores Forest
	<i>Licuala ramsayi</i>	Australian Fan Palm	native to Australia
	<i>Crinum pedunculatum</i>	Swamp Lily	Sydney Coastal Sandstone Foreshores Forest,
	<i>Prostanthera rotundifolia</i>	Round-leaved Mintbush	native to Sydney region
	<i>Ajuga australis</i>	Australian Bugle	native to Sydney region
	<i>Adiantum aethiopicum</i>	Maidenhair Fern	Sydney Turpentine-Ironbark Forest, Sydney Coastal Sandstone Foreshores Forest
	<i>Asplenium australasicum</i>	Bird's Nest Fern	Sydney Coastal Sandstone Foreshores Forest
	<i>Blechnum cartilagineum</i> syn. <i>Oceaniopteris cartilaginea</i>	Gristle Fern	Sydney Turpentine-Ironbark Forest, Sydney Coastal Sandstone Foreshores Forest
	<i>Christella dentata</i>	Soft Fern	native to Sydney region
	<i>Doodia aspera</i>	Rasp Fern	Sydney Turpentine-Ironbark Forest
	<i>Lastreopsis acuminata</i>	Shiny Shield Fern	native to Sydney region
	<i>Pteris tremula</i>	Australian Brake	Sydney Turpentine-Ironbark Forest
<b>3. Mortuary Station Gardens</b>	Planting character: retains the existing trees and garden facing Regent St and draws on the cultural history of Mortuary Station and its connection with Rookwood Cemetery.		
	<i>Araucaria cunninghamii</i>	Hoop Pine	native to Australia, including NSW
	<i>Cupressus sempervirens</i>	Pencil Pine	exotic
	Understorey planting should be selected to respond to the context of the railyard and existing rail tracks, and to suit environmental conditions, such as below the mature Fig trees.		



Figure 20: Location of public domain areas and planting species

## Schedule 4: Public Art Curatorial Framework

Table 14: Public art curatorial frameworks, potential forms and locations

Curatorial Framework	Purpose	Potential form/s	Potential Location/s
<b>Landmark Works</b>	<ul style="list-style-type: none"> <li>• Create place identity</li> <li>• Signify major site gateways and connections</li> <li>• The inspiration of big art</li> </ul>	<ul style="list-style-type: none"> <li>• Large scale works by established artists</li> <li>• May be stand-alone works or integrated with built form and site infrastructure</li> </ul>	<ul style="list-style-type: none"> <li>• Central Square</li> <li>•</li> </ul>
<b>Transformation</b>	<ul style="list-style-type: none"> <li>• Bring the precinct alive with night time activation</li> <li>• Illuminate difficult spaces</li> <li>• Enhance safety and reduce vandalism</li> <li>• Aid intuitive wayfinding</li> <li>• Respond to programmed events</li> <li>• Extend concept of innovation to creative expression throughout the precinct</li> </ul>	<ul style="list-style-type: none"> <li>• A new precinct alive with moving imagery, light and colour utilising the latest technologies to express cultural innovation.</li> <li>• Programmable wall, tunnel, undercroft, building facade and ground plane projections, LED screens, lighting design, animation, digital moving image artworks, augmented artwork.</li> </ul>	<ul style="list-style-type: none"> <li>• Mortuary Station Garden</li> <li>• Central Square</li> <li>• Goods Line</li> <li>• Publicly accessible spaces in new buildings</li> </ul>
<b>Active Art</b>	<ul style="list-style-type: none"> <li>• Activate Prince Alfred Park and Mortuary Station Garden with a diversity of experiences</li> <li>• Interpret Aboriginal and heritage stories in engaging and entertaining ways linked to sensory experience</li> <li>• Play for all ages</li> </ul>	<ul style="list-style-type: none"> <li>• Art integrated into sporting and active leisure infrastructure</li> <li>• Artist designed playscape artworks, water works and works that engage the senses.</li> </ul>	<ul style="list-style-type: none"> <li>• The primary location is the western edge of Prince Alfred Park</li> <li>• Mortuary Station Garden</li> </ul>
<b>Temporary and Live Works</b>	<ul style="list-style-type: none"> <li>• Develop Sydney's cultural ecosystem by providing spaces for cultural production.</li> <li>• Appeal to new audiences.</li> </ul>	<ul style="list-style-type: none"> <li>• Creative production — creative studios, maker spaces and residencies that</li> </ul>	<ul style="list-style-type: none"> <li>• Central Square</li> <li>•</li> </ul>

Curatorial Framework	Purpose	Potential form/s	Potential Location/s
	<ul style="list-style-type: none"> <li>• Initiate early engagement with temporary works, residency programs, activations and exhibitions during pre-construction phases.</li> <li>• Activate laneways, streetscapes and urban spaces with immersive programming.</li> <li>• Extend the reach of the precinct with creative and cultural innovation.</li> <li>• Create social connection with interaction and engagement.</li> <li>• Contribute to a cultural ecology that breathes life into this new neighbourhood in the heart of Sydney.</li> <li>• Activate the precinct with a diversity of cultural expression.</li> <li>• Express stories that support and extend the narrative of the permanent works.</li> <li>• Offer a more experimental framework for emerging artists and practitioners.</li> <li>• Enable cross-pollination of disciplines and practices.</li> </ul>	<p>showcase cultural innovation.</p> <ul style="list-style-type: none"> <li>• Cultural performance.</li> <li>• Temporary exhibitions.</li> <li>• Outreach programs through partnerships with Sydney's cultural institutions and events including hosting segments of Sydney's large format and scale events</li> <li>• Pop ups</li> <li>• Tours, talks, lectures, workshops</li> </ul>	

---

## Schedule 5: List of amendments

Table 15: List of amendments to the Central Precinct Design Guide

Central Precinct Design Guide – List of Amendments			
Date	Page	Section	Amendment
n/a	n/a	n/a	n/a