

TOGA DEVELOPMENTS
[210607 RTS] — REV C — June 2021

Supplementary Design Report
Response to Submissions
Central State Significant Precinct
Western Gateway - Block C

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Acknowledgement of Country

fjmtstudio respectfully acknowledges the Gadigal of the Eora Nation.

We pay our respect to their Elders, both past and present.

We acknowledge and uphold their continuing relationship to this land.



Introduction

This report has been prepared by fjmtstudio in response to the NSW Department of Planning and Infrastructure Submissions Summary dated March 2021 and Project Review Panel Report dated May 2021.

It also addresses each of the key urban design issues raised in submissions made in the DPIE Submissions Summary including by City of Sydney and Heritage NSW. It provides additional justification and / or clarification to particular urban design issues as raised.

Key issues including appropriate views and vistas, setbacks, structural viability and vertical separation to the former Parcels Post building (also referred to as fPPB within this report) have been carefully considered in determining the proposed envelope.

This envelope and associated setbacks preserves significant views to key items of heritage and gives visual primacy to the former Parcels Post building within its setting.

Issues raised in Project Review Panel Report May 2021 have been covered in the following sections of this RTS report:

- | | |
|---|---|
| • Built form and heritage context | P15 Balance of setbacks and other controls |
| • Physical and structural impacts on the former Parcels Post building | P16-27 and RBG report |
| • Vertical separation above the former Parcels Post building | P16-27 and RBG report |
| • Reconstruction of the original former Parcels Post building roof | P19 Principles for sensitive intervention |
| • View and visual impacts | P6-7 Excerpts from Visual Impact Assessment |

1.0 Views and Vistas

Concern

The view and visual impact analysis does not consider appropriate views and the proposal will result in negative impacts on existing views and vistas of Central Station and its surrounds.

1.1 Visual Impact and Identification of significant/appropriate views

City of Sydney DCP 2012

The significant views identified in the Sydney DCP 2012 (Special Character Areas 2.1.11- Railway Square/Central Station Special Character Area) have been included in the analysis of view impacts and determination of urban form envelope and setbacks.

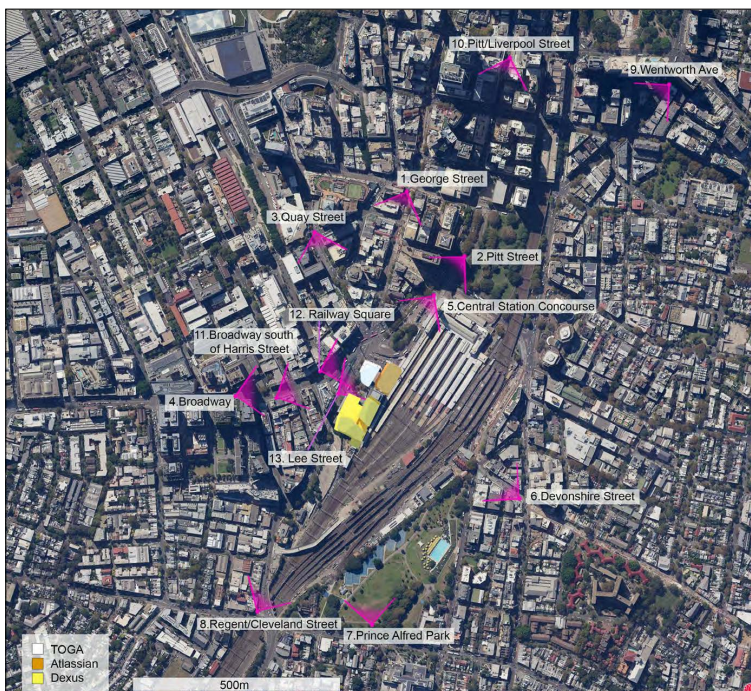
Additional Views PRP

Additional views were determined in consultation with the Project Review Panel (Meeting November 2020) and included consideration of the view impacts to the Central Station Clock Tower from Railway Square and Lee Street.

The Visual Impact Assessment demonstrates how the planning envelope principles as reached through an extensive State Design Review Panel (SDRP) process create a slender building form which sits predominately behind and above the former Parcels Post building, and is situated appropriately within its new context.

Key map indicating location of photography positions

VIRTUAL IDEAS



1. George Street/Hay Street
2. Pitt Street/Belmore Park
3. Quay Street/Ultimo Road
4. Broadway UTS
5. Central Station Concourse
6. Devonshire Street/Elizabeth Street
7. Prince Alfred Park
8. Regent/Cleveland Street
9. Wentworth Avenue/Wemyss Lane
10. Pitt Street/Liverpool Street
11. Broadway south of Harris Street
12. Railway Square
13. Lee Street

1.2 Western Gateway Sub-Precinct

Future Context

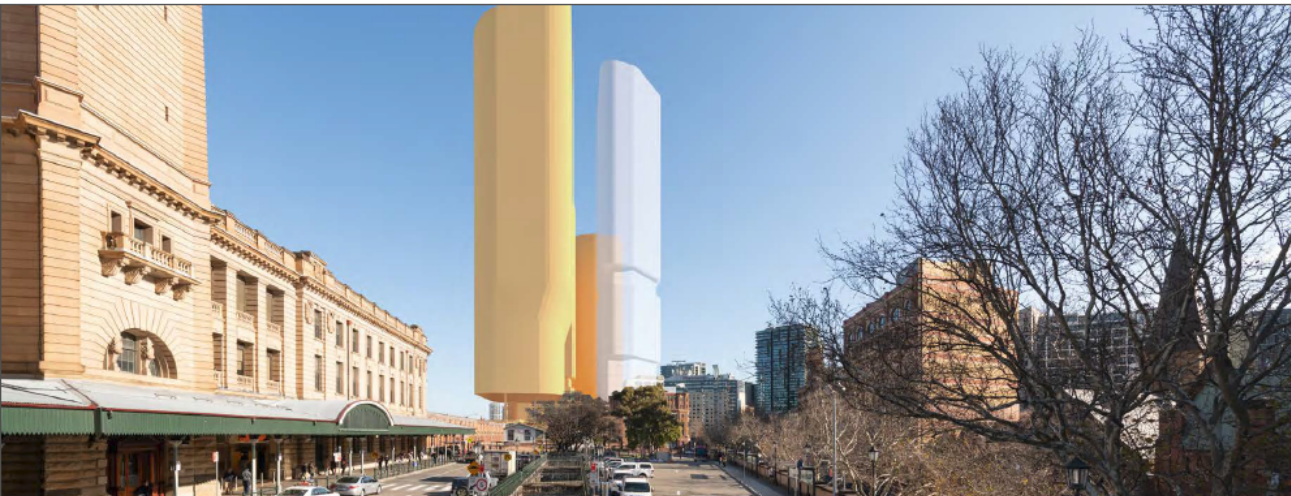
View impacts were considered within the context of the approved Block A and B rezoning facilitating tall buildings within the Western Gateway Sub-Precinct, the future development vision for the Central Station Precinct, and the future towers anticipated within the City of Sydney 'Tower Cluster' (as described within the CSPS). The southern most of these relates to the Central Station renewal area created by the NSW Government and referenced in the Sydney Innovation and Technology Precinct Panel Report.

Visual modelling incorporating the Western Gateway Sub-Precinct has been included in the Visual Impact Assessment study which accompanied the proposal and illustrates the impact of the approved Block A and B towers. Within this context the impact of the proposed Block C envelope is incremental and mediated.

Visual Impact Assessment - Reference Scheme



View from Broadway to Railway Square and the Central Station Clock Tower. Views of Clock Tower obstructed by the existing



View from Railway Square (Western Forecourt - future Central Square) to the Central Station Clock Tower



View from George Street south to the former Parcels Post building

1.3 Western Gateway Sub-Precinct

Future Context

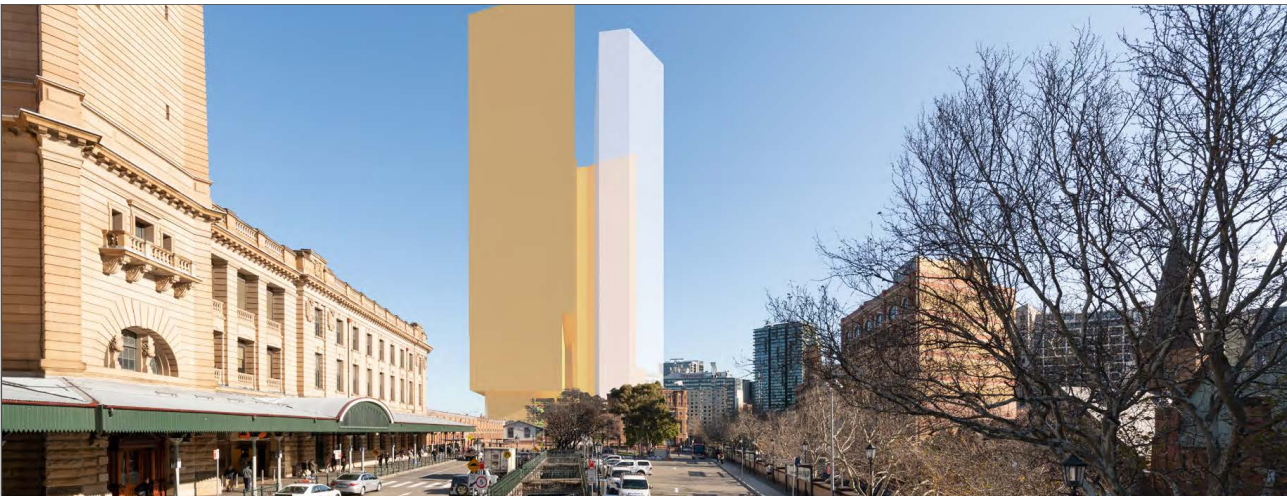
The images below illustrate the visual impact of the Planning Envelope. It is noted that the reference design fits within this envelope. Likewise, all future design competition schemes will be required to be located within the Planning Envelope. The visual impact of any future building will therefore be less than the Planning Envelope which cannot be fully occupied due to additional massing controls such as the 1300 sqm Gross Building Area floorplate control and an overall maximum Gross Floor Area control.

The Vertical Separation Zone which is not delineated in these Planning Envelope images will ensure clear separation between the reinstated pitched roof and the base of any new vertical addition above.

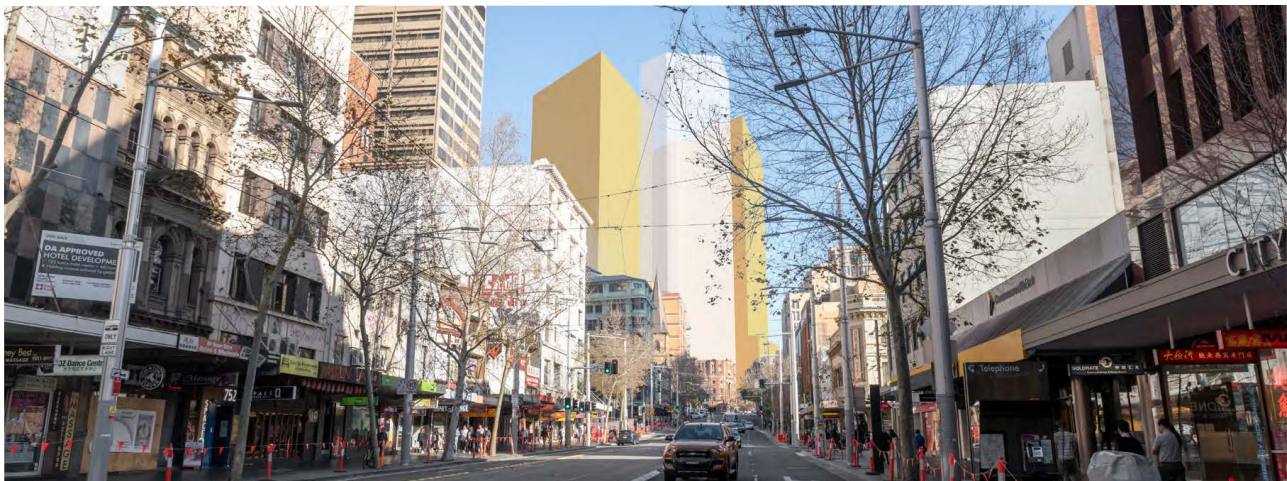
Visual Impact Assessment - Planning Envelope



View from Broadway to Railway Square and the Central Station Clock Tower. Views of Clock Tower obstructed by the existing



View from Railway Square (Western Forecourt - new Central Square) to the Central Station Clock Tower



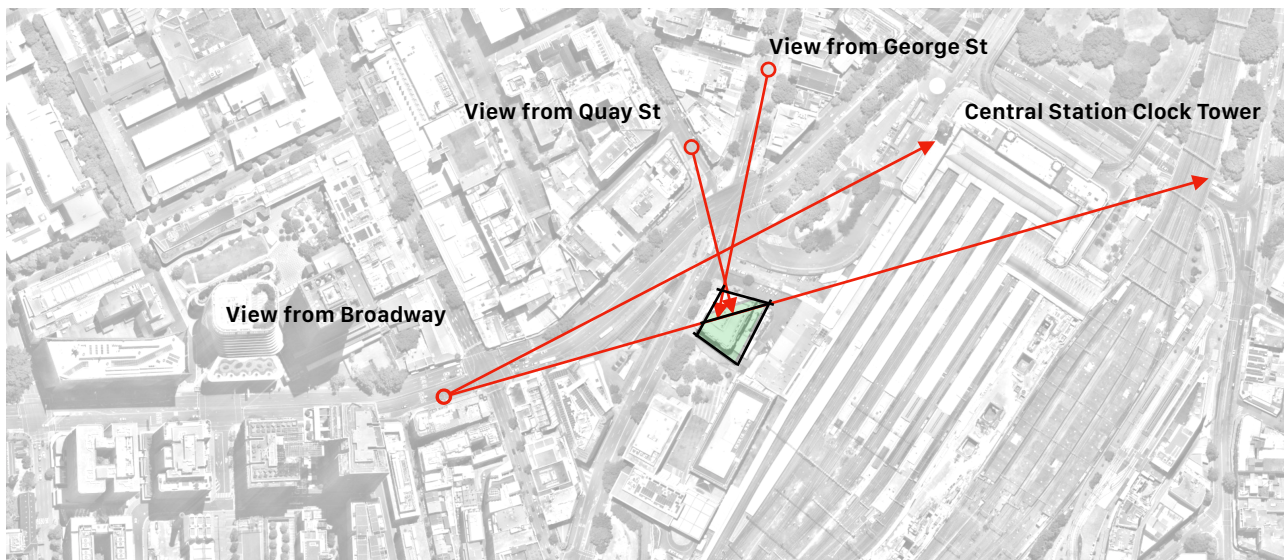
View from George Street south to the former Parcels Post building

1.4 Envelope Principles and Setbacks

The proposed envelope has been developed to preserve the primacy of the former Parcels Post building and the relationship of the other heritage items at the intersection of Lee Street, Railway Square and Broadway.

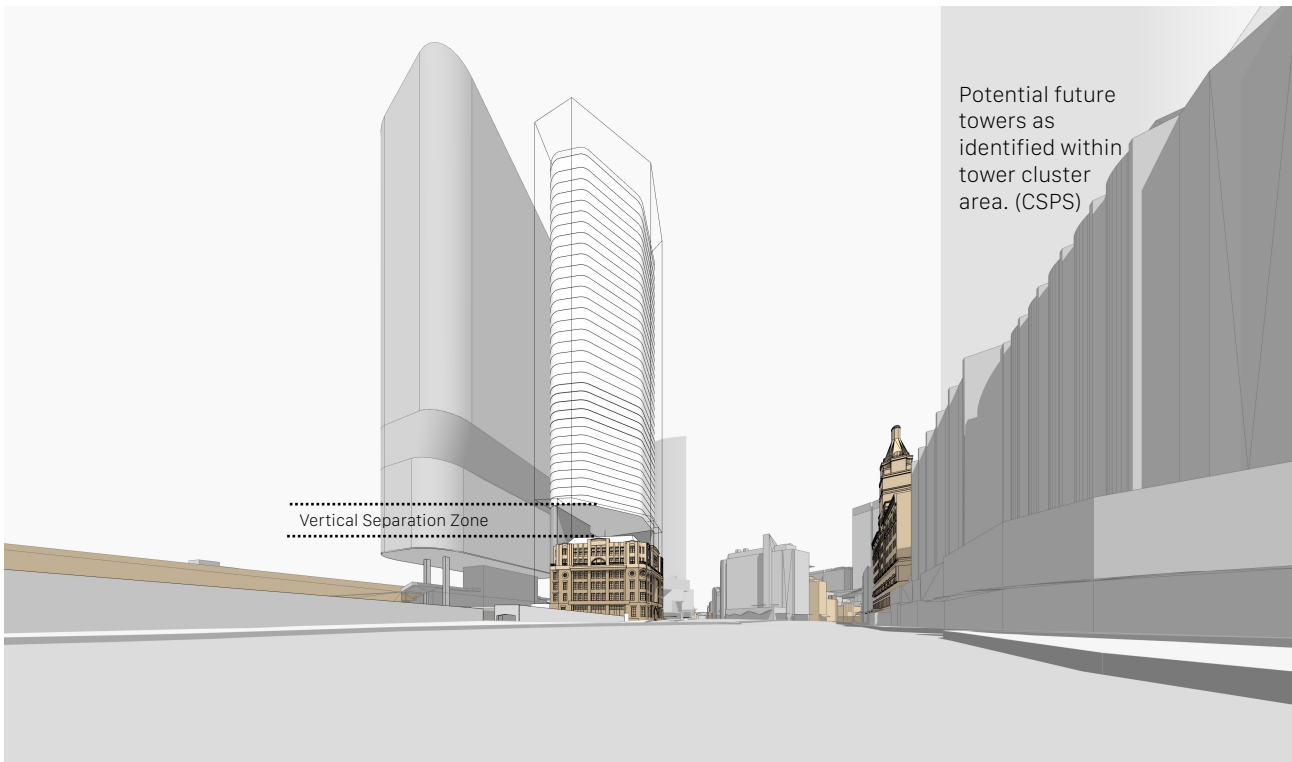
The proposal considers in detail the key views in the determination of the envelope principles, configuration and setback.

The preservation of views to the Central Station Clock Tower and the primacy of the former Parcels Post building is achieved through a diagonal tower setback from the northern corner. To maintain the primacy of the former Parcels Post Building from George and Quay Street this setback from the northern corner has been maximised. This setback creates an effective separation of the proposed tower and the former Parcels Post building from these important street views.

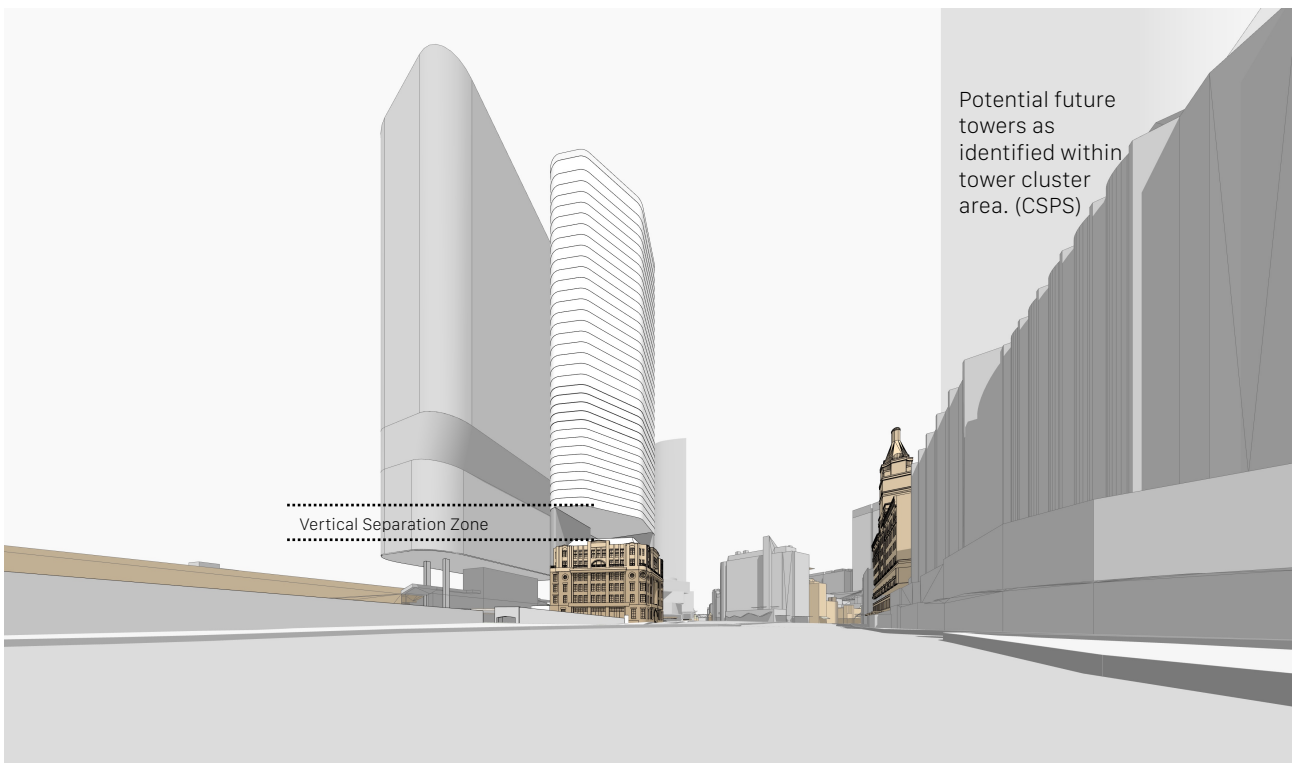


Northern, Western and Diagonal setbacks retain fPPb prominence from these three key views, and reduces visual bulk

Vertical Separation Zone not to comprise building enclosures other than necessary structural elements to support vertical addition. Specific layout subject to Design Excellence Competition, Heritage Consultation and approval application



Reference scheme and Planning Envelope overlay. View from Quay Street. Showing Block A and B competition scheme



Reference scheme. View from Quay Street. Showing Block A and B competition scheme

1.5 Summary

Appropriate views have been carefully considered in determining the proposed envelope. This planning envelope and associated setbacks preserve significant views to key items of heritage and gives visual primacy to the former Parcels Post building within its setting. Additional controls including the 1300sqm GBA maximum floor plate further constrain the massing to ensure a slender form.

2.0 Setbacks

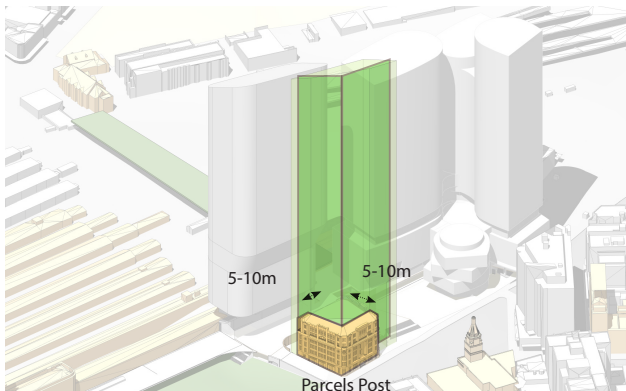
Concern

In order to preserve the heritage significance of the former Parcels Post building any new development needs to be adequately setback from the heritage facades, specifically the northern and western facades. A number of submissions raised concerns that the setbacks established through the four SDRP sessions in 2020 are not sufficient.

2.1 Prominence of the former Parcels Post building

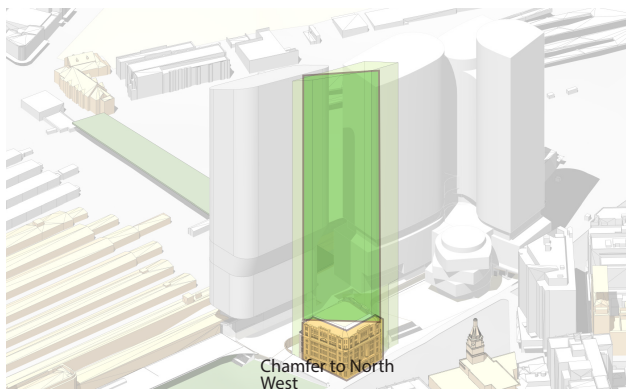
The key consideration of the north and western setback condition was to maintain the visual prominence of the former Parcels Post building within a future setting.

The new tower has been setback substantially from the north west corner which is the most effective setback when viewed from significant positions on Quay St, Railway Square, George Street and Broadway. The planning envelope includes a 5-10m minimum setback from the northern and western façades.



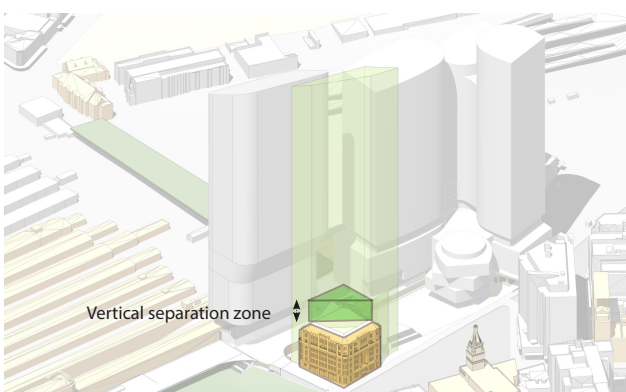
Primacy of Parcels Post

5-10m minimum setbacks on North and Western sides



North West Corner Setback

Following heritage and wind analysis, a chamfered setback was introduced on the north western corner to ameliorate visual impacts and wind conditions. This chamfer is defined by a line from the NE and SW corners of the former Parcels Post building



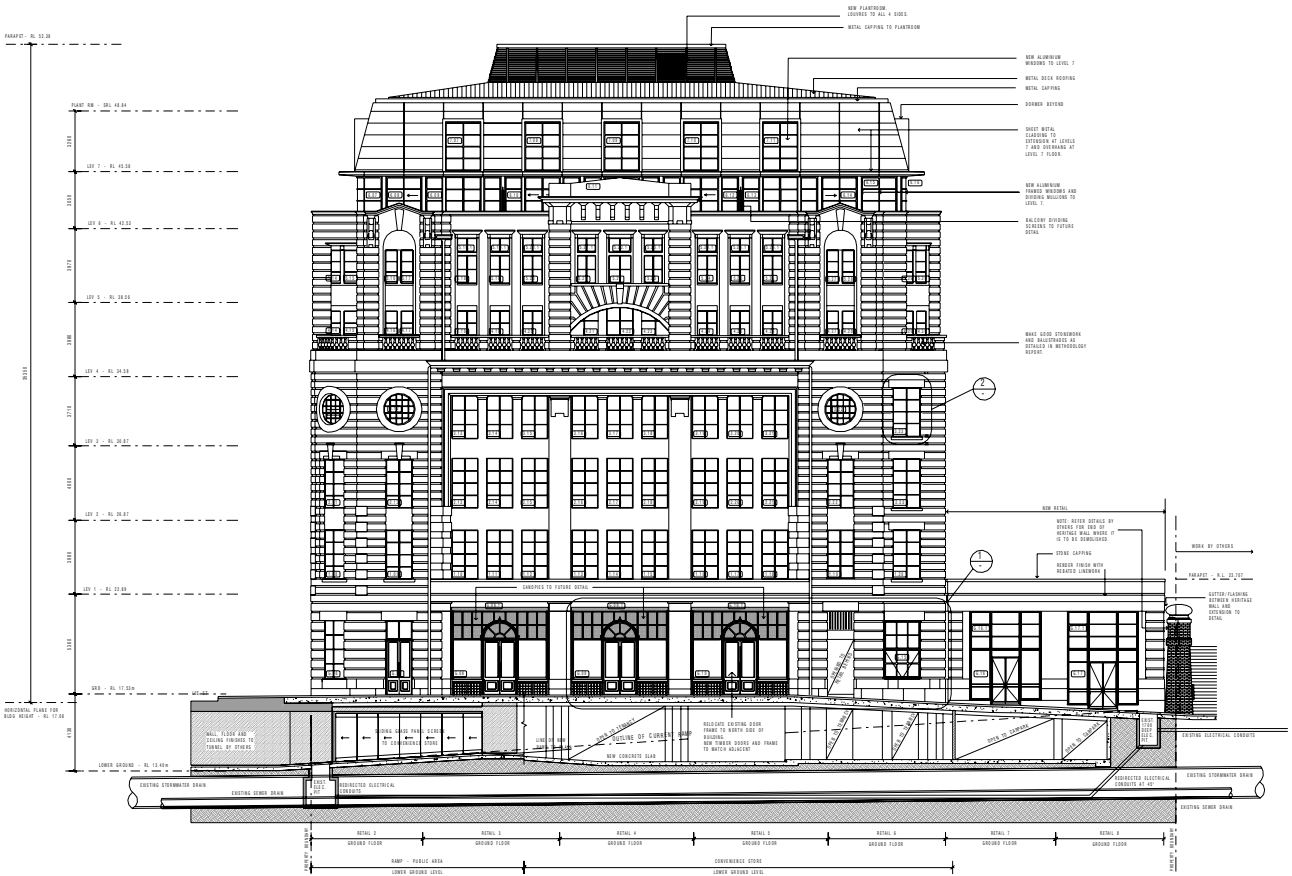
Vertical Separation Zone

A Vertical Separation Zone (equivalent to 3 commercial floors, minimum 12.6m) has been established between the new tower element and the ridge line of the reconstruction of the double pitched roof to the former Parcels Post building.

This zone is to be clearly articulated such that it provides a clear visual break between the tower and heritage building below. Structural encroachment within this zone is to be minimised, with no building enclosures other than necessary structural elements to support the vertical addition

Views to Southern heritage facade

Articulation zone on the southern facade at lower levels. Include the use of materials or other measures such as transparent materials and void spaces to ensure a significant portion of the original southern facade can be interpreted from the public domain and that any enclosure is limited as much as possible. Wording to this effect have been amended in the design guide.



fPPb existing Southern elevation

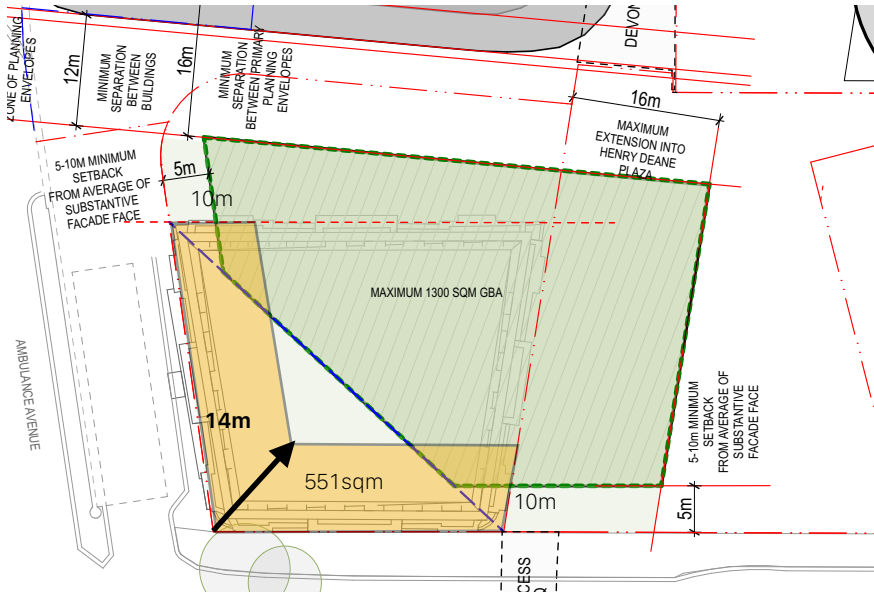


2.2 Comparison of Setbacks

The City of Sydney includes provisions within their Development Control Plan (DCP) on appropriate setbacks over heritage buildings. Section 5.1.2.1 Front Setbacks under point (3) stipulates that "new buildings or additions above a heritage item must have a setback of at least 10m from the street frontage".

This comparison illustrates that the net or total weighted average setback is similar. However, the proposed diagonal setback alignment provides a substantially increased setback from the visually sensitive northern corner.

For comparative purposes, setbacks of 10m for the Lee Street/Railway Square and Carriage Lane/Central alignments have been overlaid on the proposed variable setback, refer Figure 1.



Block C comparison to the 10m setback (CoS DCP)



Block C proposed setback control

Proposed setback controls with 5-10m Northern and Western Setback, and a diagonal setback from the North Eastern to South Western point of fPPb.

This provides an increased Northern corner setback relative to the DCP comparison control. There is an indistinguishable difference in the total area of the setback zone.

2.3 Comparison of Setbacks

Concern

Heritage NSW Submission seeking greater setbacks on the north-east and south-west corners of the fPPb (without reducing the 21m between the north-west corner and the chamfer).

Maximising the setbacks to the north and west have been balanced against:

- Achieving PCA A Grade minimum floor plate and sufficiently attractive floor plate size to achieve the Tech central objectives
- Separation to Block A building to the east - minimum 12m
- Limiting extension to the South - maximum 16m from the southern facade of the fPPB.

The following diagram shows the effects of additional setbacks to the North and West.



Block C additional controls to the 10m setback + diagonal setback

Additional setbacks to 10m on the North and West will not permit a minimum 1300sqm floor plate to be achieved

Impact on floor plate of increased setbacks

Allowing for minimal building articulation as shown above, additional setbacks to 10m further reduces the achievable floor plate to 1210sqm GBA. Based on 0.81% GBA to NLA efficiency, the maximum permitted NLA is 980sqm including any interconnecting void spaces.

This floor plate is not viable as it results in an area which is below PCA A grade standards and does not allow for sufficient building articulation within the planning envelope

2.4 Development of proposed envelope, massing studies and analysis

The proponent undertook extensive consultation with the State Design Review Panel that consisted of the Government Architect, the City of Sydney and leading heritage and architectural experts. Significant analysis was undertaken to assist in establishing the setback with focus on maintaining the prominence of the fPPb.

As part of the engagement process with the SDRP, a number of massing studies were produced and analysed with consideration of heritage prominence of fPPb, bulk and scale, minimum viable floor plate, building separation, views and vistas, open space and microclimatic effects.

Scheme F1 evolved as the preferred option as it retained the prominence of fPPb via street and diagonal setbacks whilst limiting the extent of building occupying the current Henry Deane Plaza. Additionally, it provided manageable microclimatic effects.

Key benefits as identified with Scheme F1 include:

Heritage

Retains fPPb's civic presence and position as strong element within the urban context

Open Space

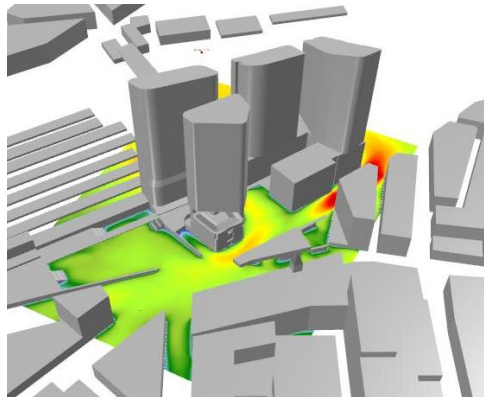
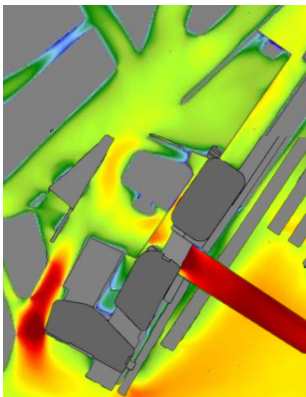
Positive relationship with Henry Deane Plaza. The final location of the connection to Lee Street tunnel underpass was identified as being critical to resolving the position of the new structural support and potential core zone within the plaza and coordinating with predicted and proposed pedestrian flows

Building height and envelope

The offset floor plate frees up the building's corners and preserves the civic prominence of the north western corner

Microclimatic effects

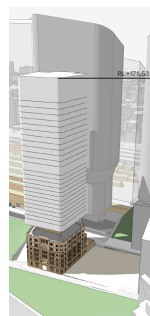
Wind mitigation measures as managed through the form of the envelope and the surrounding public realm were identified as a key design criteria. Scheme F1 performed well in these regards.



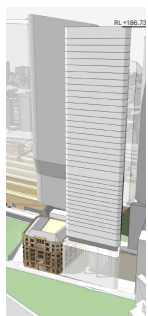
Sitting Standing Strolling Walking Uncomfortable

/ North West Aerial View

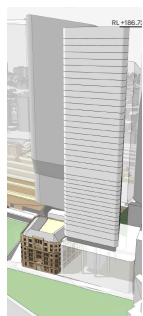
/ Wind Tunnel Test



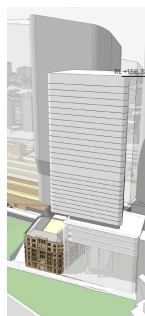
/ Option A1



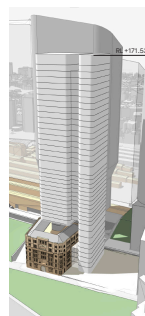
/ Option D1A



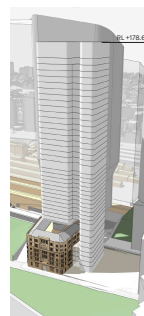
/ Option D1A



/ Option D1C



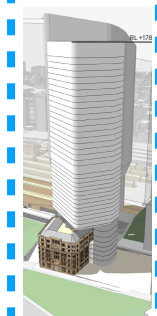
/ Option D2A



/ Option D2B



/ Option E1



/ Option F1

Broad range of massing options were evaluated for bulk and mass, view impacts, prominence to fPPb and wind effects

2.5 Balance of setbacks

The heritage setbacks as described above are balanced against maintaining a sufficient eastern setback from the neighbouring Atlassian Tower (Block A). Additionally, there is a desire to limit the southern extension into Henry Deane Plaza to no more than 16m, all whilst enabling a minimum viable floor-plate size within the tower. It is central to the viability of the commercial component that it delivers in accordance with the minimum PCA standards and market requirements for an A Grade Office Buildings (PCA Office Quality Grade Matrix).

The eventual planning envelope as supported by the SDRP provides very limited flexibility to shift the massing of the final tower form. The planning envelope closely follows the potential tower form. This limited flexibility was intentional and reflects the extensive and considered SDRP advice. The envelope as proposed balances the key criteria of preserving the significance of the heritage items against the other crucial requirements to enable the success of the Western Gateway.

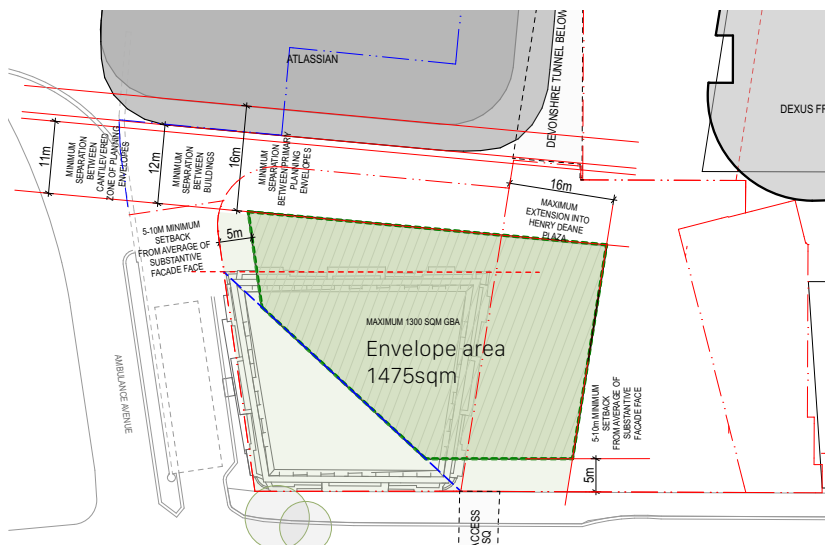
Extensive wind analysis demonstrated that moving the massing any further south into Henry Deane Plaza caused significant impacts to the micro climatic conditions at plaza level and potentially resulted in unsafe conditions for both standing and walking.

2.6 Other Controls

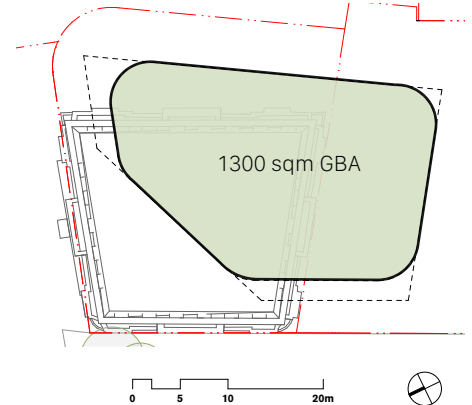
In addition to the envelope setbacks, other controls are also imposed to preserve the prominence of the heritage item. These include a maximum floorplate GBA of 1,300m² and a minimum Vertical Separation Zone of 12.6m (approximately equivalent to 3 commercial floors) above a reinstated original pitched roof form.

This floor plate constraint will produce a slender tower form which when viewed from Railway Square will appear to sit above and behind the former Parcels Post building acting as a foil to the ensemble of much larger floor plates for Block A and Block B.

The Vertical Separation Zone requires architects to provide a distinct separation between the old and the new, further preserving the prominence of the former Parcels Post building. The requirement to remove the existing mansard roof addition and reinstate a reinterpretation of the original roof form will enhance the heritage item by further restoring and returning it to a more accurate version of its original form.



Proposed Envelope Plan from Urban Design Report



Overlay of reference scheme floor plate within proposed envelope. Maximum planning envelope dashed.

The image above illustrates how the reference floor plate of a maximum 1300 sqm GBA sits within the planning envelope whilst maintaining a limited, but sufficient, degree of flexibility for the Design Excellence Competition.

Summary

The proposed envelope setbacks from the heritage building are appropriate and have been carefully considered and specifically determined in relation to visual and heritage impacts.

3.0 Physical & Structural Impacts on the fPPb

Concern

The proposal should not permit structural impacts on the former Parcels Post building.

3.1 Heritage Viability

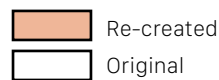
Prominence and significance

In the context of the Western Gateway precinct, and as a backdrop to Sydney's new Central Square, the fPPb prominence is significantly derived from its facade, in particular the facade in original condition on the North, Western and Southern elevations.

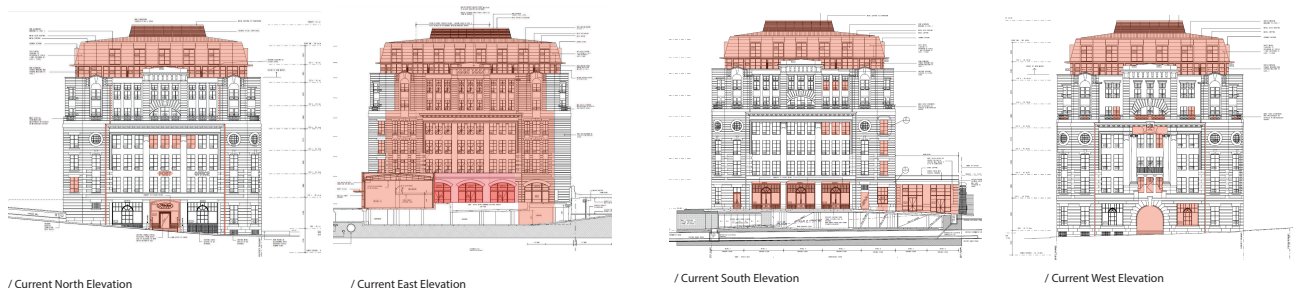
The interiors of the fPPb although carrying significance for their associative values are less significant than the exterior and have been substantially adapted over time to suit various changes in use. (Refer HIS Urbis)

The former Parcels Post building is currently operating as a hotel with private access to rooms. All internal heritage structure is covered by set plasterboard ceilings.

The redevelopment as proposed offers an important opportunity to expose and restore the majority of the heritage structure. Additionally, the redevelopment will allow the building to be brought up to current building codes for seismic, fire protection and façade performance.



Original and modified sections of fPPb facades



/ Current North Elevation

/ Current East Elevation

/ Current South Elevation

/ Current West Elevation

Summary of interventions and adaptive uses

The fPPb has a diverse history of uses including mail handling, office, materials testing laboratories, retail and hotel functions.

1910's - 1950

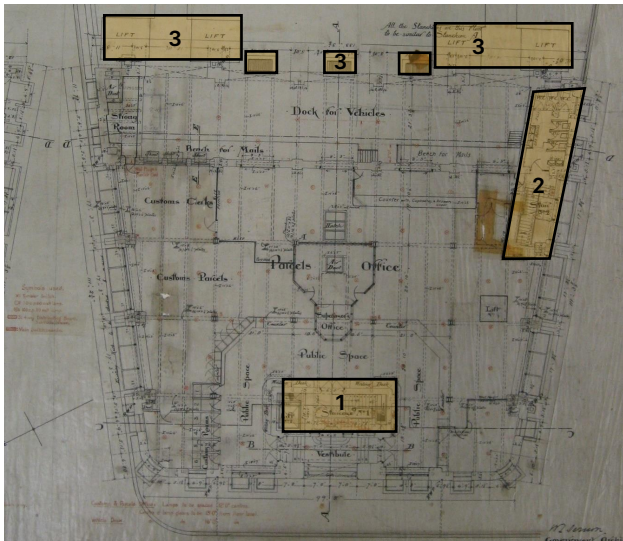
Originally operating as a mail sorting facility in connection to the railways, the ground level contained vehicle loading docks to the east and customer service areas. The upper levels included offices for the Commonwealth Department of Home affairs, and were later changed to mail sorting and dispatching requiring the addition of the external lifts on the east. Parcels were delivered to the basement and distributed by conveyors to various sorting floors.

In this period, there were minor modifications made to the fitout as the Post Office functions changed over time. The fPPb was designed as an overflow to the GPO, and replaced its functions by 1920. In 1920 the fPPb also became a sorting facility for overseas mail.

1965-1990's

By the 1960's the fPPb post services were progressively vacated to the Mail Exchange at Redfern. The building's use changed from parcels handling towards training, telegraphic services, two district works divisions and equipment services.

Part of the fPPb upper levels were changed to material testing laboratories, requiring minor modifications for air exchange. An Annex structure was added to the eastern facade to facilitate uses in this time.



1910 Plan

1. lift and stair on Western side
2. amenities block and stair on South East
3. attached lifts on East



2000 Plan

1. central lift and stair core
2. modified eastern facade following removal of attached lifts
3. partitioned hotel plan



1910



1947

Interior spaces changed from open plan offices, to sorting and handling spaces and then hotel functions



1960's annex to Eastern elevation



2000's ground level foyer

3.2 Vertical Separation to former Parcels Post building

Concern

The reference design shows the zone between the fPPb and the base of the tower as enclosed. This does not result in appropriate curtilage to the heritage building.

Envelope and Reference Design

The envelope, associated controls and principles outline the intent and requirements for the Vertical Separation Zone between the heritage building and the new tower form. The reference design indicated a recessive void within this zone as an uncluttered backdrop to the roof and parapet profile of the heritage building. However this is not intended as the only design response. The competition process will allow opportunity to explore design options and alternatives for review.

Envelope Principles

The proposed envelope identified the area between the roof of the fPPb and the underside of the proposed tower as a Vertical Separation Zone.

No enclosure is proposed in this Vertical Separation Zone except for structural support (refer planning report). This modified approach addresses the PRP request for further metrics, evidence and analysis in the Vertical Separation Zone.

The following principles serve to appropriately address the curtilage of the heritage building:

Reconstruction of original roof form

Interpretive reconstruction of roof to the original pitched profile and material quality.

Setback ridge line

Setting back built form and structure to behind the reconstructed pitched ridge line.

As the resolution of the recessed floor plate is dependant on a number of factors including the interpreted reconstructed roof profile and a viable structure, the setback is described as a principle and an objective.

Differentiation

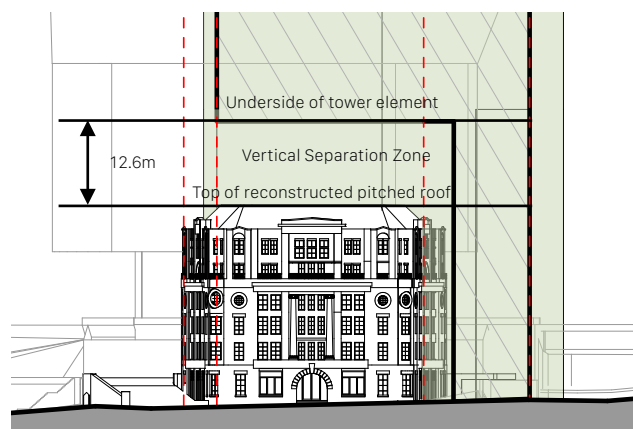
Differentiating the setback level from the tower and the fPPb by expression and material quality.

Heritage

Composition and layout of elements in the setback zone not detracting from the heritage prominence and significance of fPPb.

Innovative structural solutions

The structural solution for the scheme is to adopt innovative solutions to minimise visible structural elements within the Vertical Separation Zone. Indicative structural solutions are included in the following pages and within RBG report.



West Elevation - Vertical Separation Zone

Principles for sensitive intervention

The envelope proposed and the relationship to the fPPb have been informed by expert heritage and structural input and guidance. Given the constrained nature of the site and sensitive adjacencies it is not functionally or structurally possible to avoid penetrations of the fPPb.

"tower columns will be required to pass through the existing Parcels Post building, footings for these columns will need to be founded on suitable material within the basement of the existing Parcels Post building" (RBG Structural Viability Report)

The Urban Design Report and the Heritage Impact Statement described the following considerations for sensitive intervention:

Acknowledge and preserve primary significance

Acknowledge that the primary significance is from the fPPb's facade, more so than the original interiors.

Future proposals should have regard for the Conservation Management strategy (Urbis 2020), which identifies the heritage significance of the site and component elements. Modifications to the building are subject to heritage advice, and the preparation of a formal Conservation Management Plan.

Reinstate original form and features

Proposals should seek to allow reinterpretation of the original character and spatial qualities of the building, including the reinstatement of the original pitched roof, and uncovering of structure concealed by contemporary partitions. Proposed new works to the interiors should seek to reinstate the open spatial characteristics of the building and enhance visibility to the grid pattern of the internal columns.

Minimise and limit structural intrusion

Where possible, structural zones and penetrations through the fPPb should be minimised, and located in areas of less prominence.

Sensitive to the heritage fabric

Location of structural zones should be sensitive to the heritage fabric. The reference scheme illustrated one possible solution with a line of new columns, within the heritage building aligning to the location of the existing diagonal column grid. It also indicated a potential structural zone on the eastern facade which is identified as non-original building fabric and suitable for re-interpretation to facilitate development.

Potential eastern structural zone

The eastern facade is not original fabric and has been subject to modifications over time which included a number of alterations to the historic lift structures and the eventual removal of the lifts.

Based on the evolving nature of the eastern façade and in detailed consultation with heritage experts the eastern façade zone is appropriate for a contemporary intervention and the possibility of reinterpreting through a modern lift structure.

It is noted that the envelope setback to the northern façade will ensure that the eastern façade will remain partially visible and assists in maintaining the appearance of heritage facades to all sides. The intervention to the eastern side will be carefully managed and assessed in coordination with TfNSW, and heritage and structural experts.



Eastern Facade in dilapidated condition - 1993



/ Lift attachments to Eastern elevation



/ Lift attachments to Eastern elevation



/ Current rebuilt Eastern elevation

3.3 Structural Viability

Analysis relies on the RBG structural report and the investigation of options as described below.

Full Cantilever

This option positions the cores on the Eastern side of fPPb and Southern side, minimising their impact on the internal fabric of fPPb. The fully cantilevered option has an excessive overturning mass vs restoring mass and deemed as structurally unviable from RBG.

Core Version 1

Commercial core to the East, and wholly external to the fPPb. Hotel core to the South and also wholly external to the fPPb. The cores are positioned to minimise their impact on the internal fabric of fPPb.

Tower columns are aligned in their position to existing fPPb positions and would require bracing as shown in the RBG report.

Core Version 2A and 2B

Core revision 2A and 2B consolidates the core elements to the eastern elevation, and partially overlap the existing footprint of fPPb. This option has no southern core and has improved views towards the southern existing elevation of fPPb

This option requires bracing on the southern footprint of the new tower from footing to roof.

Summary

RBG's report indicates both Core version 1 and 2A and 2B are structurally viable.

3.4 Visual Impacts & Principles

The intent of the following diagrams is to demonstrate that the planning proposal envelope and associated design principles allow for design excellence flexibility, structural innovation and a balancing of visual impact on heritage fabric. The final decision on the core should be subject to the design excellence process where this can be considered with respect to urban design, impact upon heritage fabric and public space.

When viewed from the south (Broadway) or from the north (Quay Street, George Street south and new Central Square), this Vertical Separation Zone combines with the setback tower to preserve the heritage prominence of the fPPb.

The massing indicated in the reference scheme represented a potential setback zone and did not define a specific architectural resolution.

Core Version 1

Commercial core to the East, and wholly external to the fPPb. Hotel core to the South and also wholly external to the fPPb.

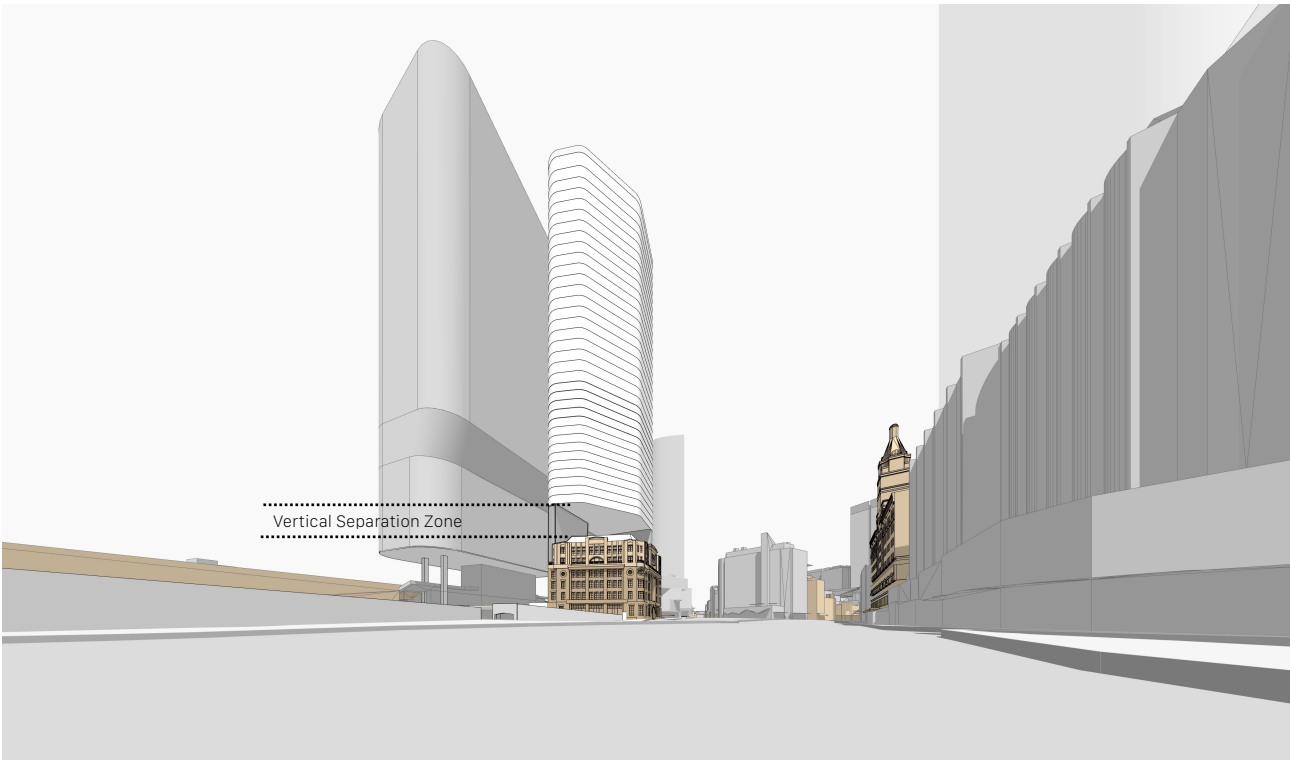
This option seeks to minimise an impact of cores on the internal fabric of fPPb

Benefits

- Minimises encroachment of core elements within the fPPb

Disadvantages

- Places excessive bulk & solid elements adjacent the southern facade impacting views & physical connection to Henry Deane Plaza
- The narrow eastern core creates structural imbalance challenges. High overturning mass vs restoring mass. See RBG report. Likely to require additional columns bracing within and above the fPPb
- Significantly more bulk & solid mass to the east adjacent to the new pedestrian Link Zone



Vertical Separation Zone for vertical structure only, not to be enclosed.
East and south core shown, columns intentionally excluded pending Design Excellence Competition and future detailed design development

Reference scheme - Core Version 1. View from Quay Street. Showing Block A and B competition scheme

Core Version 2A and 2B

Commercial core to the East combined with Hotel core.
Combined core partially intersects eastern portion of fPPb existing floorplate

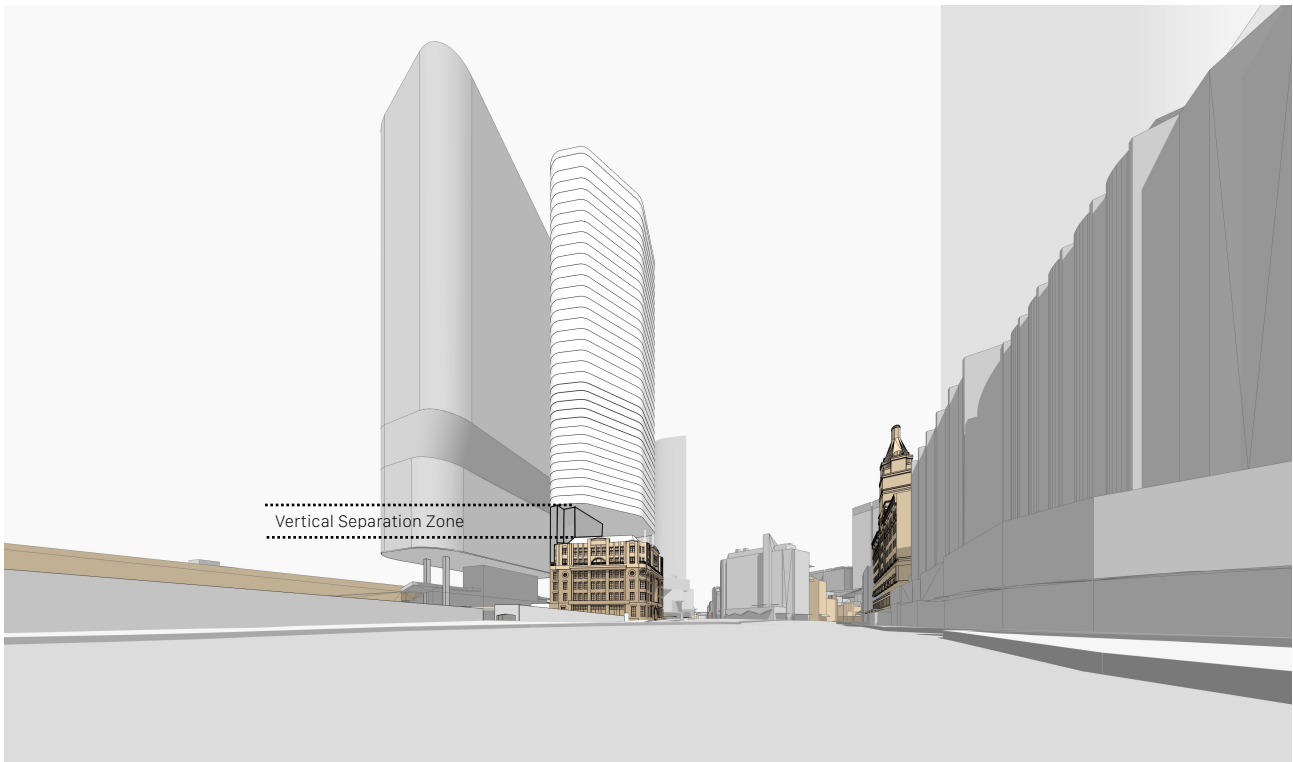
This option seeks to minimise the impact of a southern core by consolidated core to the eastern side of fPPb. It is acknowledged that this option does impact on the interior fabric of the heritage building

Benefits

- Frees the southern facade from solid elements & improves relationship to Henry Deane Plaza
- Preserves greater visibility of North, Western and Southern facades of fPPB and particularly the visibility of fPPb from the South Western Broadway view
- Provides structural stiffening to the fPPb to achieve BCA compliance

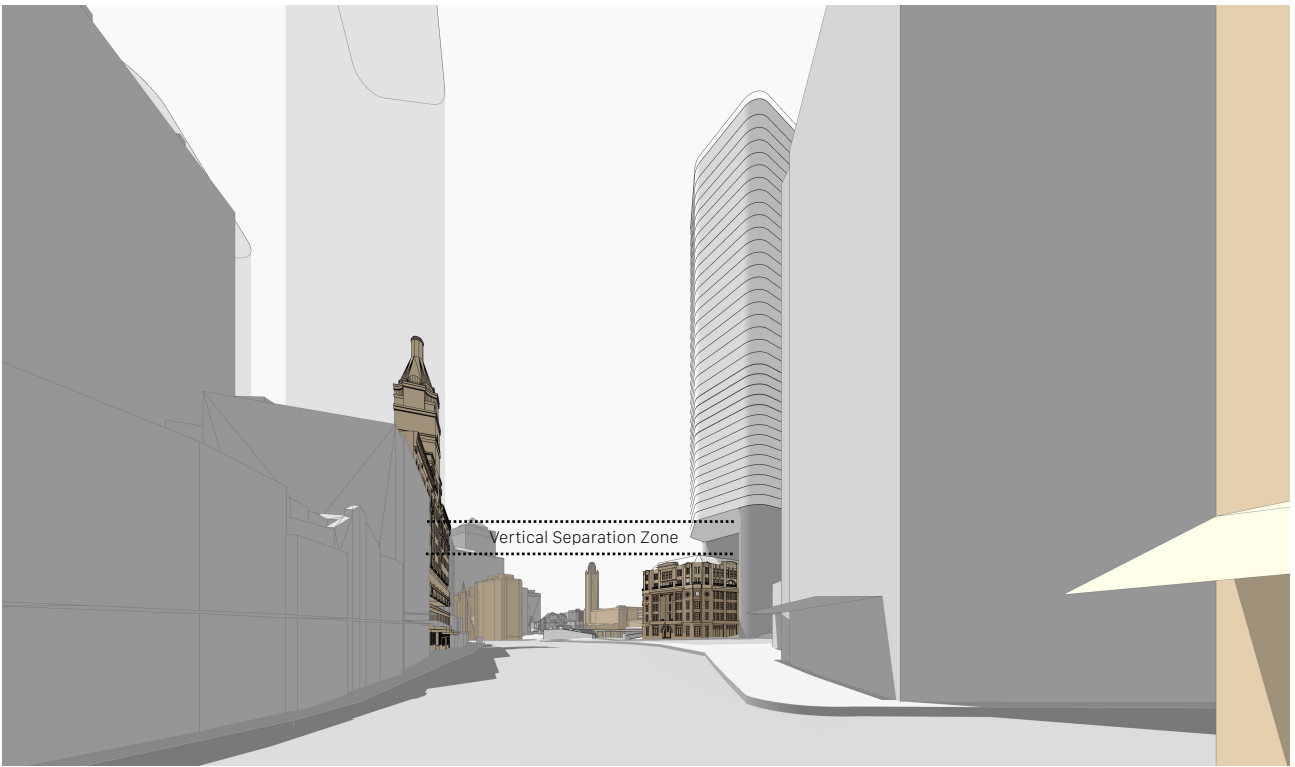
Disadvantages

- Partial modification required for fPPB internal layout (although equivalent to modifications made in 1993 with internal stair, lift and risers)



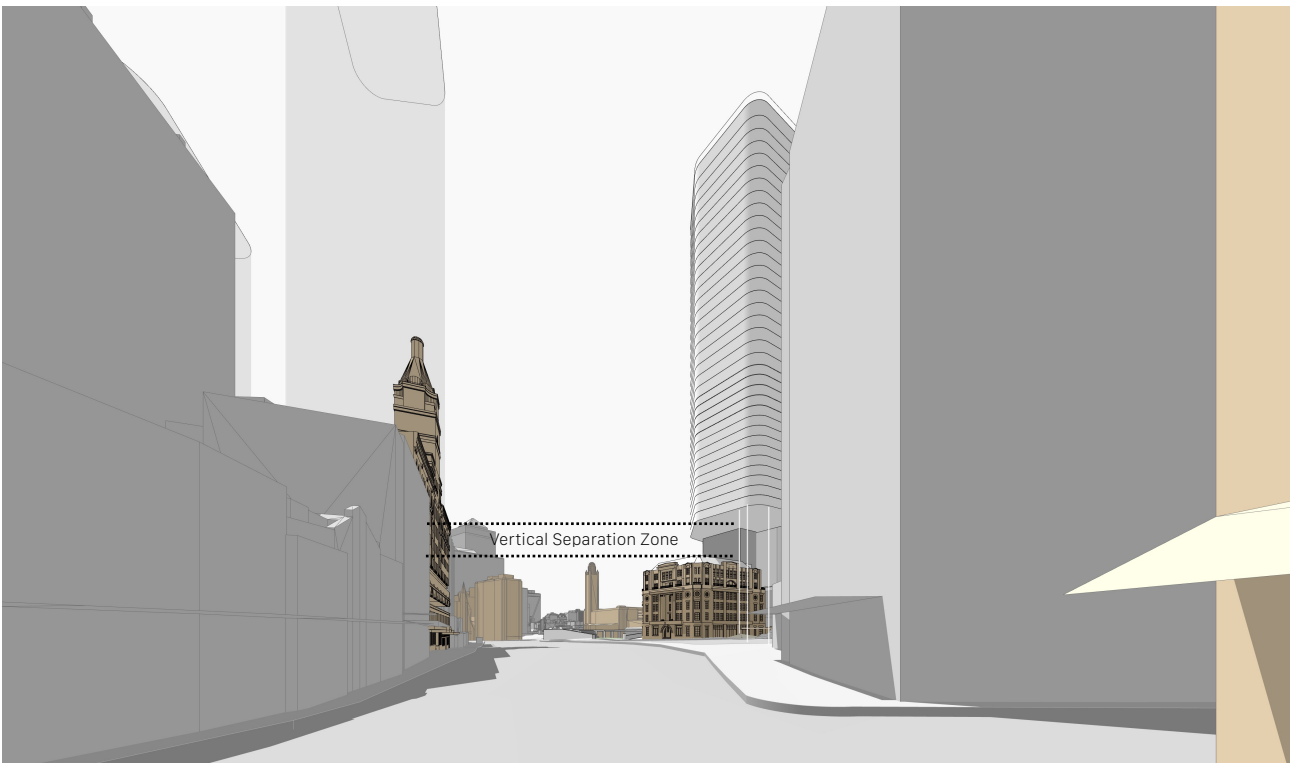
Vertical Separation Zone for vertical structure only, not to be enclosed.
Consolidated eastern core shown, columns intentionally excluded pending Design Excellence Competition and future detailed design development

Reference scheme - Core Version 2A and 2B. View from Quay Street. Showing Block A and B competition scheme



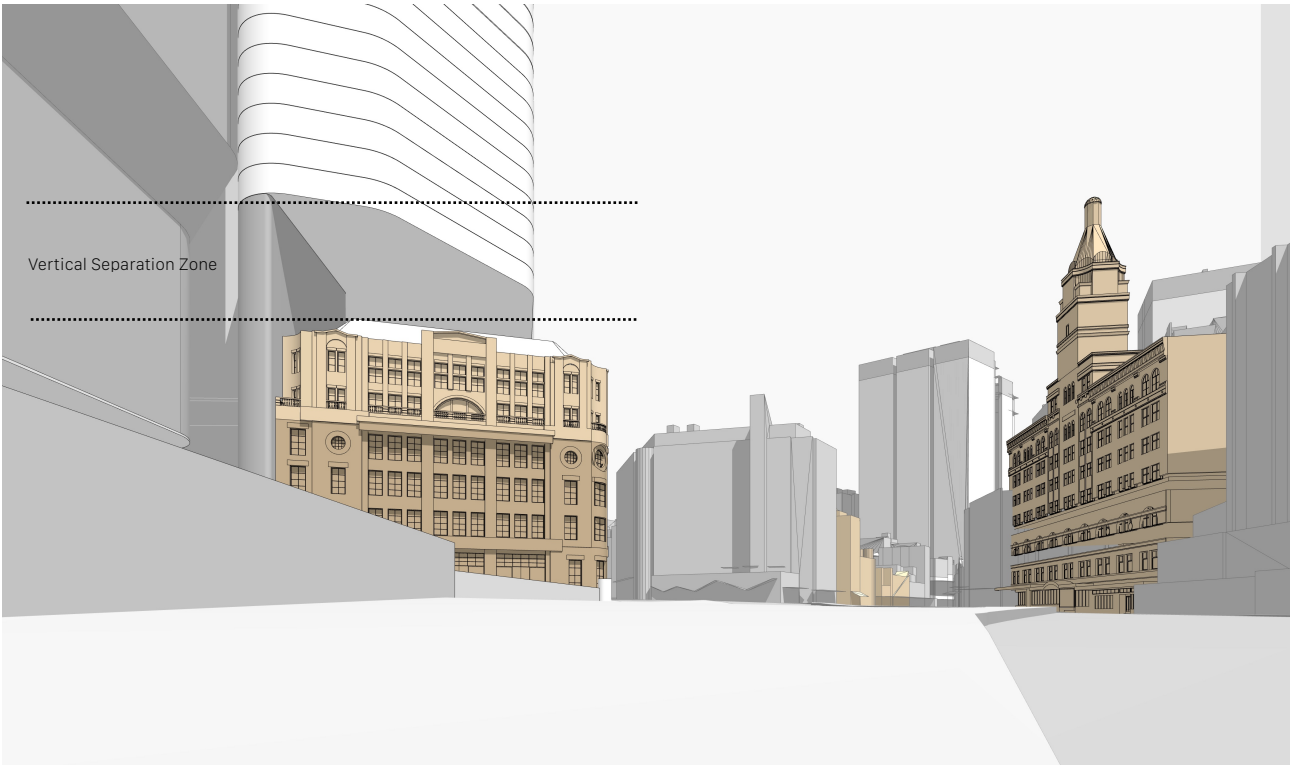
Vertical Separation Zone for vertical structure only, not to be enclosed.
 East and south core shown, columns intentionally excluded pending Design Excellence Competition and future detailed design development

Reference scheme - Core Version 1. View from Broadway. Showing Block A and B competition scheme



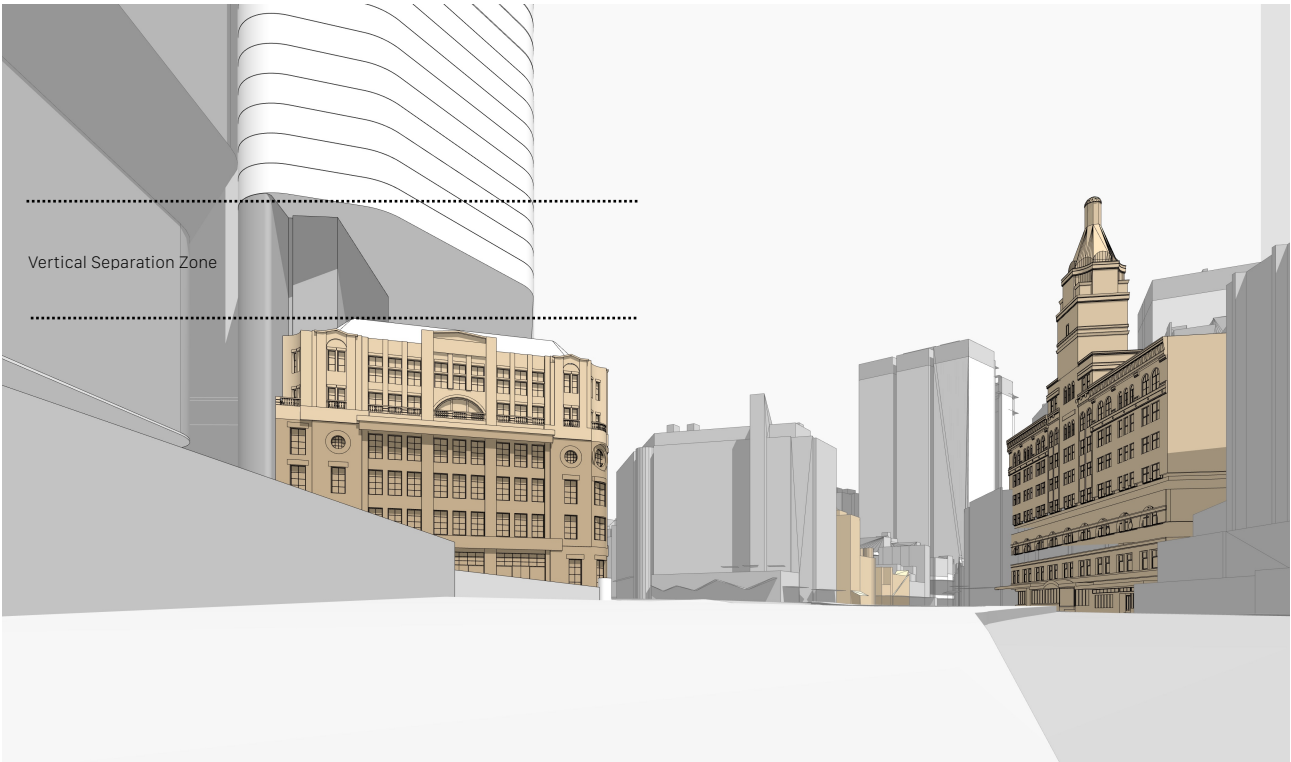
Vertical Separation Zone for vertical structure only, not to be enclosed.
 Consolidated eastern core shown, columns intentionally excluded pending Design Excellence Competition and future detailed design development

Reference scheme - Core Version 2A and 2B. View from Broadway. Showing Block A and B competition scheme



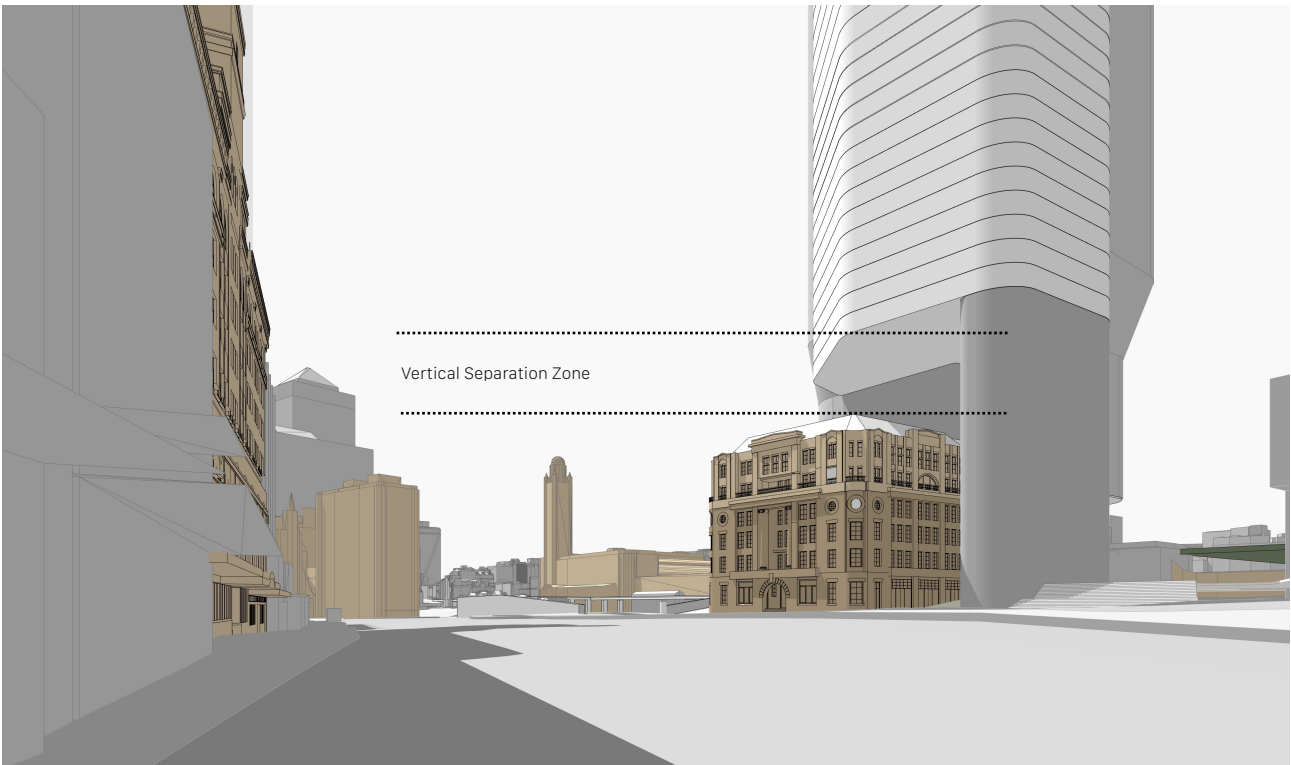
Vertical Separation Zone for vertical structure only, not to be enclosed.
 East and south core shown, columns intentionally excluded pending Design Excellence Competition and future detailed design development

Reference scheme - Core Version 1. View from George Street. Showing Block A and B competition scheme



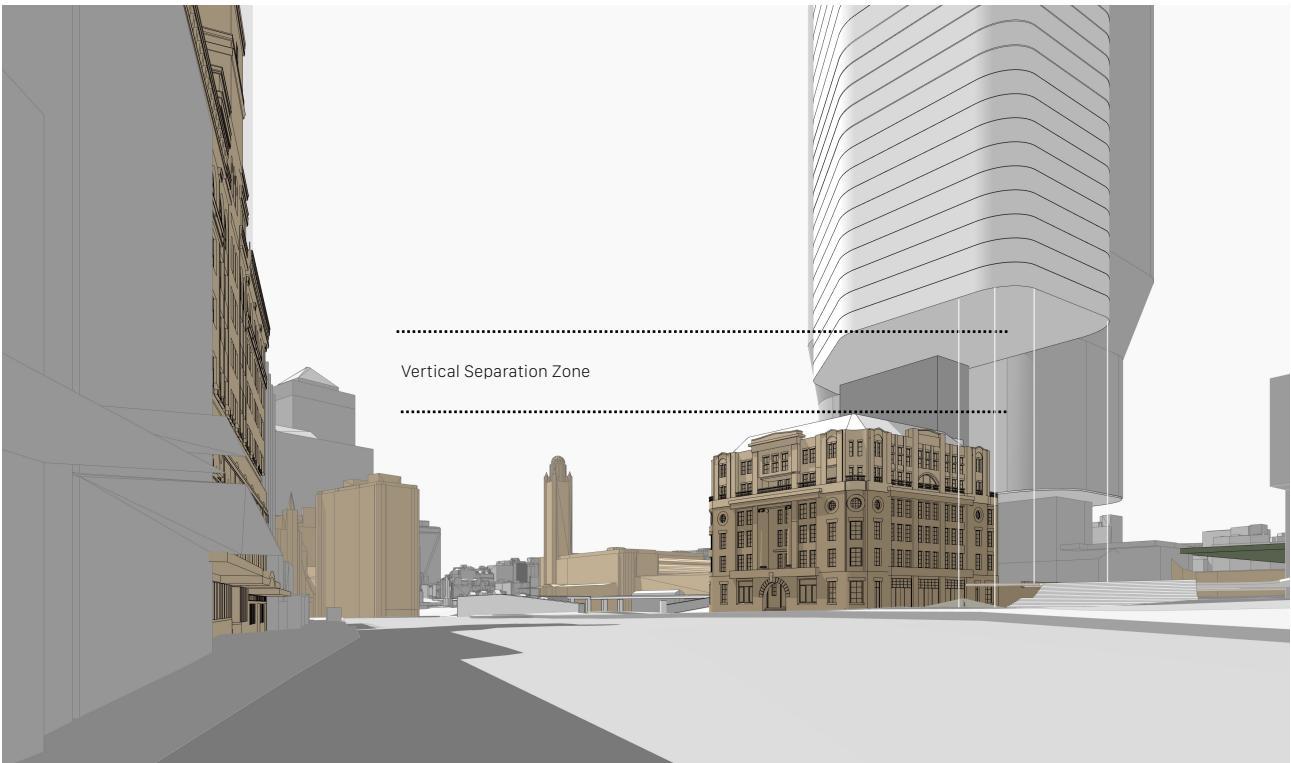
Vertical Separation Zone for vertical structure only, not to be enclosed.
 Consolidated eastern core shown, columns intentionally excluded pending Design Excellence Competition and future detailed design development

Reference scheme - Core Version 2A and 2B. View from George Street. Showing Block A and B competition scheme



Vertical Separation Zone for vertical structure only, not to be enclosed.
 East and south core shown, columns intentionally excluded pending Design Excellence Competition and future detailed design development

Reference scheme - Core Version 1. View from Broadway close. Showing Block A and B competition scheme



Vertical Separation Zone for vertical structure only, not to be enclosed.
 Consolidated eastern core shown, columns intentionally excluded pending Design Excellence Competition and future detailed design development

Reference scheme - Core Version 2A and 2B. View from Broadway close. Showing Block A and B competition scheme

Summary

The intent of these diagrams is that the planning proposal envelope and associated design principles allow for design.

3.5 Case Study - Substation 164

There are many examples of sensitively considered projects where heritage and towers intersect via a recessive setback elements.

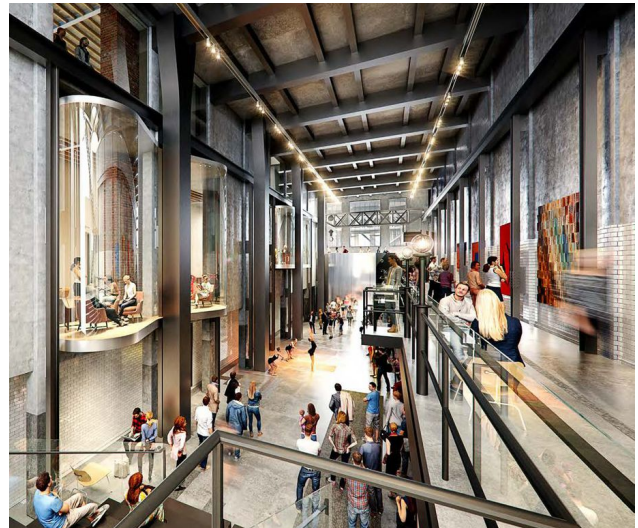
An example of vertical separation is Substation 164, 183-185 Clarence Street, Sydney which has a setback and differentiation zone between the heritage facade and the new addition.

Substation no.164 seeks to deliver an alternative paradigm to the conventional extension approach to heritage buildings. The proposal delivers a singular, sculptural response that appears to 'float' over the two heritage buildings below. The form has been sculpted with consideration of sight lines at street level. The form of the extension is then rounded and softened to ensure reflections and light roll off the edges thus avoiding a harsh silhouette against the sky.

The materiality and detailing of the extension are in strong contrast to the existing heritage facades.

Structural interventions are resolved and articulated within the building to maintain heritage prominence and significance.

A key driver behind the design was the retention of the Machine Hall within 183 Clarence Street (Former Electrical substation No. 164) as a major publicly accessible space. The structural core and associated fire stairs serving the new office levels were located in the adjacent 185 Clarence Street (Former Shelley warehouse), whilst the new tower columns were located along the southern side of the Machine Hall to maintain the integrity of the major space and associated mezzanine (refer below image).



Substation No. 164, 183-185 Clarence Street, Sydney

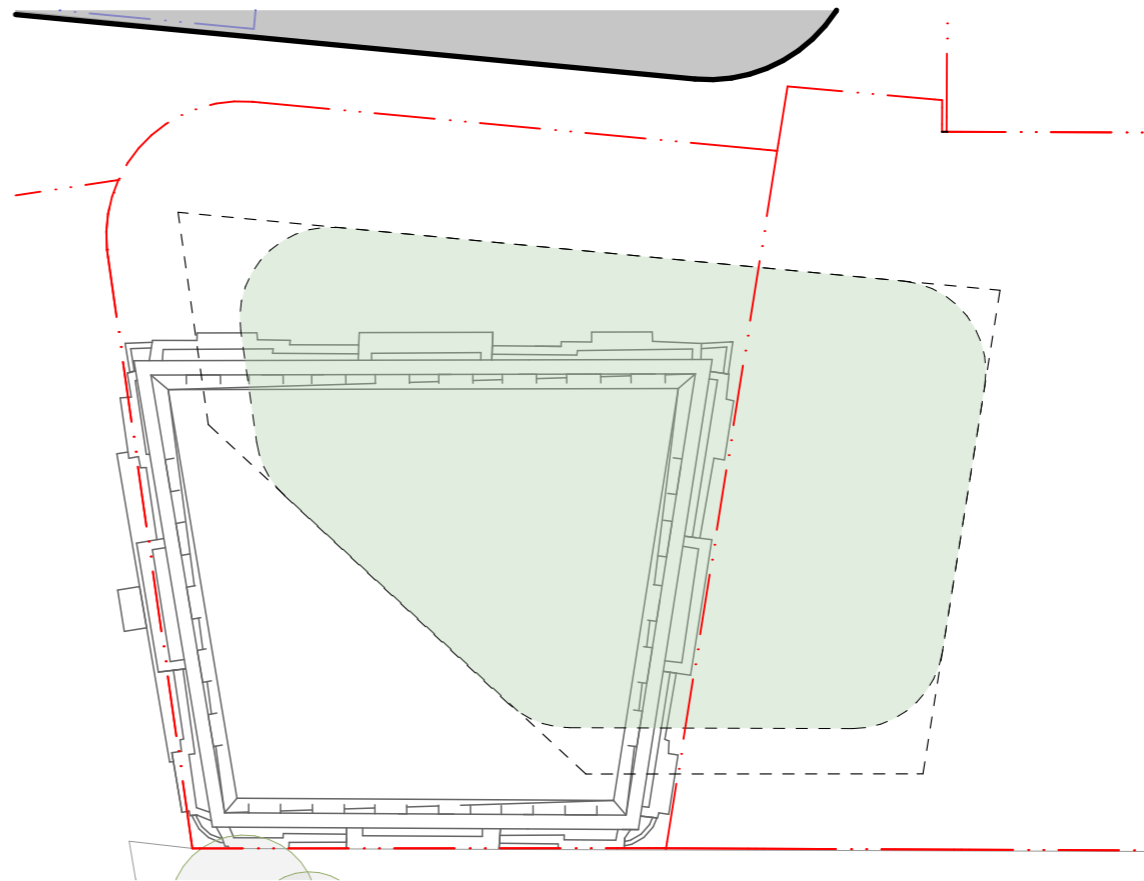
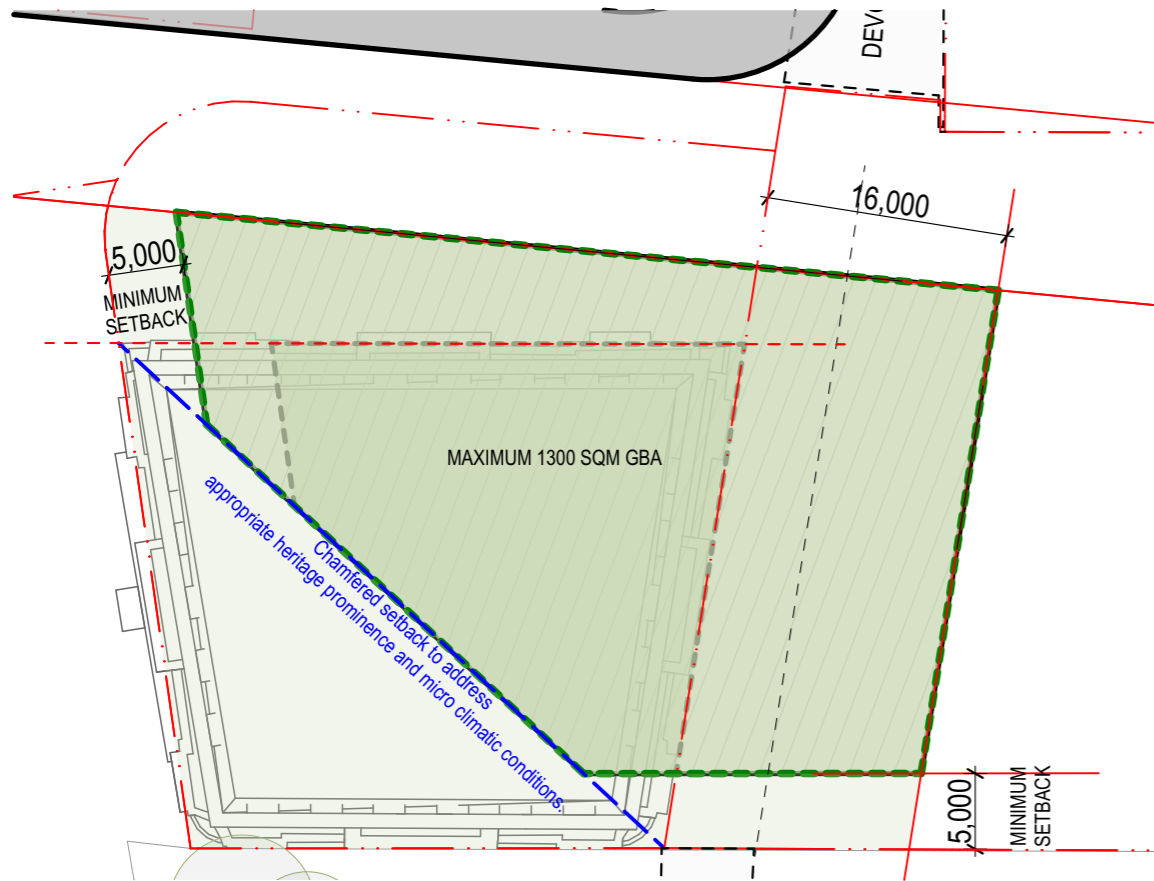
3.6 Summary / Conclusion

Structural and functional intervention within the former Parcel Post building is essential to the future repurposing and revitalisation. These interventions can be designed and integrated to create appropriate juxtaposition and interrelation with the existing heritage fabric.

The 'Vertical Separation Zone' serves to create separation and mediation between the heritage building and the new tower with visual primacy to the former Parcel Post building.

The proposed amendments to the Design Guide wording is intended to provide greater comfort that the amount of enclosure will be limited and that appropriate curtilage will be provided to the heritage building. Whilst the strategy for clear visual separation is expressly noted within the documents, the final expression and resolution should respond to the outcome of the Design Excellence process. As such, the proposed wording seeks to establish a clear strategy rather than stipulate specific setbacks.

The principles in this urban design RTS has been reflected in the amended Design Guides in the updated planning report from Urbis.



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